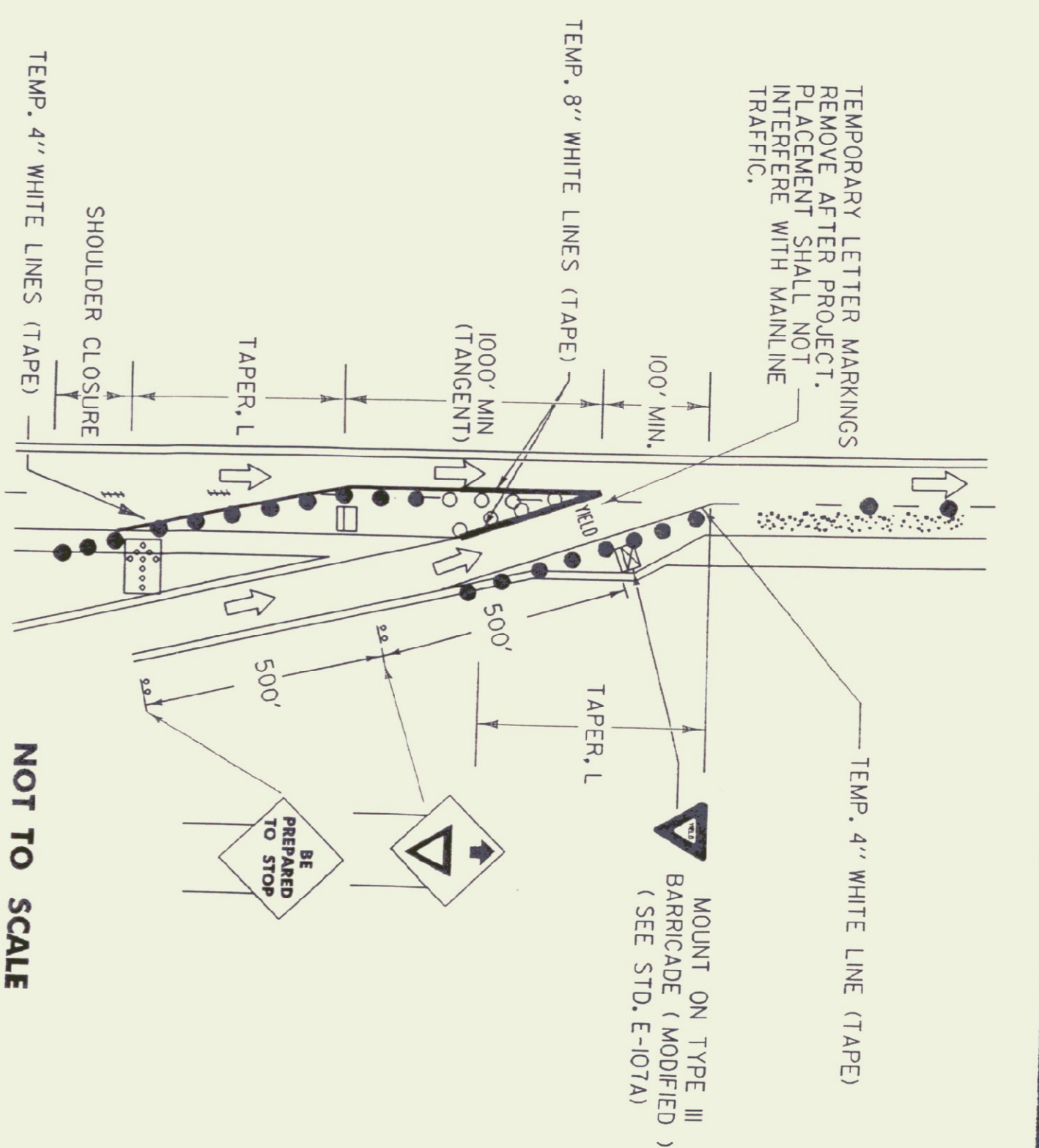


- NOTES:**
- 1) ALL SIGNS SHALL BE MOUNTED ON FIXED POSTS (YIELDING TYPE) UNLESS OTHERWISE NOTED.
 - 2) CHANNELIZING DEVICES SHALL BE PLACED AS FOLLOWS: TAPERS - DEVICES SHALL BE SPACED A MAXIMUM OF "5" (THE SPEED LIMIT IN FEET) APART. TANGENT - DEVICES SHALL BE SPACED 2 X "S" (THE SPEED LIMIT IN FEET) APART.
 - 3) ALL DISTANCES ARE DESIRABLE MINIMUMS FIELD CONDITIONS SHALL CONTROL THE ACTUAL PLACEMENT.
 - 4) TAPER RATES ARE BASED ON THE POSTED MAINLINE AND EXIT SPEEDS.
 - 5) TEMPORARY PAVEMENT MARKINGS ARE REQUIRED WHEN THE LAYOUT IS TO BE IN EFFECT FOR THREE DAYS OR MORE.
 - 6) LANE CLOSURES AND TAPER LENGTHS, L, AS DETAILED ON STANDARD E-103.

- LEGEND**
- REFL. 28" CONES
 - REFL. PLASTIC DRUMS
 - PAVEMENT MARKING REMOVAL
 - ⇨ INDICATES TRAFFIC FLOW
 - WORK AREA
 - ⊠ FLASHING ARROW PANEL
 - TYPE III BARRICADES
 - ⊞ TYPE III BARRICADES (MOD.)



- NOTES:**
- 1) AT THE CONTRACTOR'S OPTION:
 - A. THE POWER SUPPLY MAY BE AERIAL OR UNDERGROUND (SEE DETAIL).
 - B. POWER FOR A FLASHING BEACON MAY BE COMBINED WITH POWER FOR A TRAFFIC SIGNAL OR THEY MAY HAVE SEPARATE POWER SOURCES.
 - C. THE FLASHER MAY BE INSTALLED ON A STANCHION NEAR THE SIGNAL OR A UTILITY POLE (WITH UTILITY COMPANY APPROVAL) OR AT THE SAME LOCATION AS A TRAFFIC SIGNAL CONTROLLER.
 - 2) THE FLASHER UNIT SHALL BE ONE CIRCUIT AND INCLUDE A RADIO INTERFERENCE FILTER.
 - 3) BATTERY OPERATED FLASHERS WILL NOT BE ALLOWED.
 - 4) BOTTOM OF THE BEACON SHALL BE A MIN. OF 8'-0" AND A MAX. OF 12'-0" ABOVE THE EDGE OF THE PAVEMENT.
 - 5) FOR URBAN AREA PLACEMENT SEE STD. E-121.

- NOTES:**
- 1) ADVISORY SPEED AS DETERMINED BY THE NEAREST SIGN (40 M.P.H. MINIMUM) SHALL BE INDICATED ON FIXED POSTS.
 - 2) SIGNS MOUNTED ON FIXED POSTS.
 - 3) ALL DISTANCES ARE DESIRABLE MINIMUMS. FIELD CONDITION SHALL CONTROL THE ACTUAL PLACEMENT.
 - 4) THE BUMP SIGN MAY BE ELIMINATED WHEN THERE IS NO BUMP WHEN THE CONTRACTOR IS WORKING IN THE CONSTRUCTION AREA. THE ADVANCED WARNING SIGN PACKAGE SHALL BE USED. SEE STD. E-103.

MAINLINE LANE CLOSURE AT AN EXIT RAMP

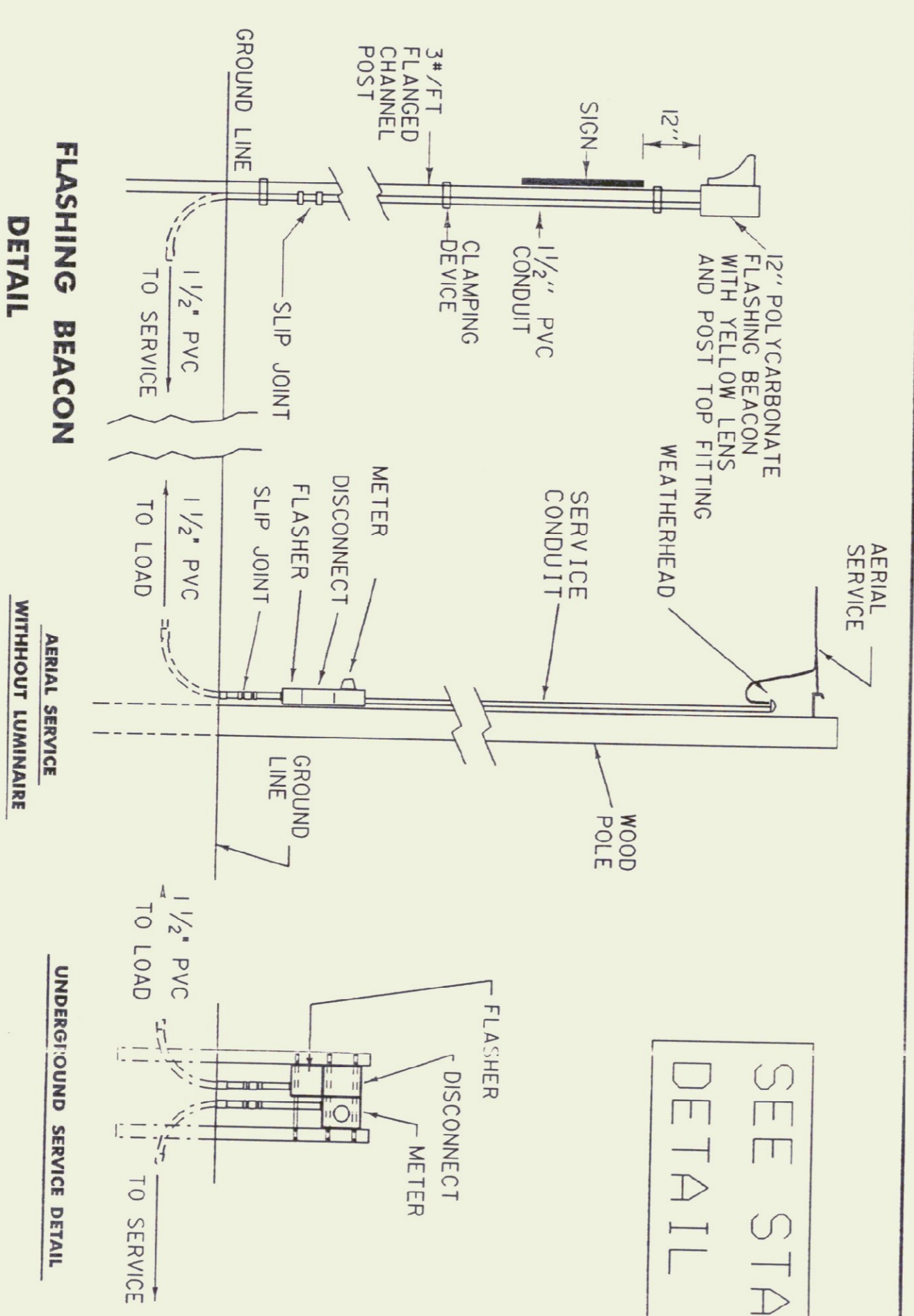
NOT TO SCALE

MAINLINE LANE CLOSURE AT AN ENTRANCE RAMP

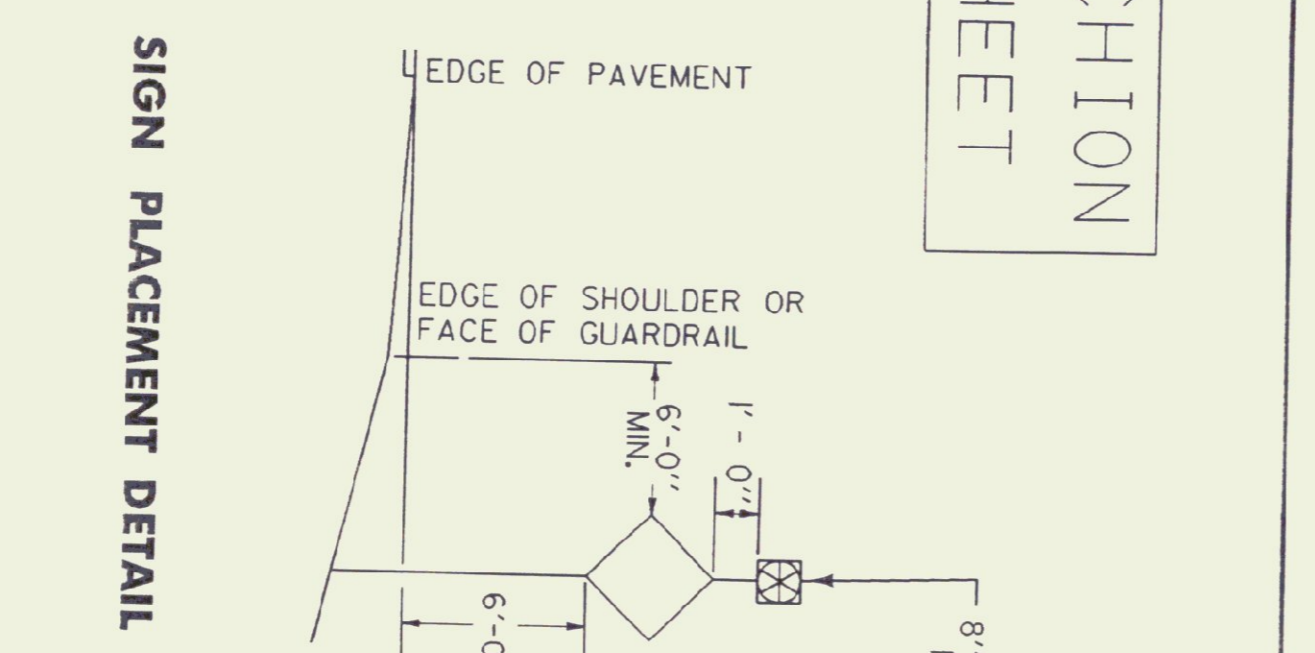
NOT TO SCALE

THIS DETAIL SHALL BE USED WHEN THE WORK ZONE BEGINS AT THE CORE OR THE MAINLINE LANE CLOSURE DRUM PLACEMENT INTERFERES WITH THE EXIT RAMP.

THIS DETAIL SHALL BE USED WHEN THE WORK ZONE BEGINS AT THE END OF THE ACCELERATION LANE OR THE MAINLINE LANE CLOSURE DRUM PLACEMENT INTERFERES WITH THE ON-RAMP TRAFFIC.



SEE STANCHION
DETAIL SHEET



SIGN PLACEMENT DETAIL

FLASHING BEACON DETAIL

UNDERGROUND SERVICE DETAIL

AERIAL SERVICE WITHOUT LUMINAIRE

REVISIONS AND CORRECTIONS
 JAN. 23, 1989 - REVISED EXIT SIGN - CLARIFIED EXIT TAPER
 SEPT. 20, 1993 - REVISED RAMP CLOSURES, FLASHING BEACON DETAILS, AND MOVED TYPE III BARRICADE (MOD.) TO STD. E-107A

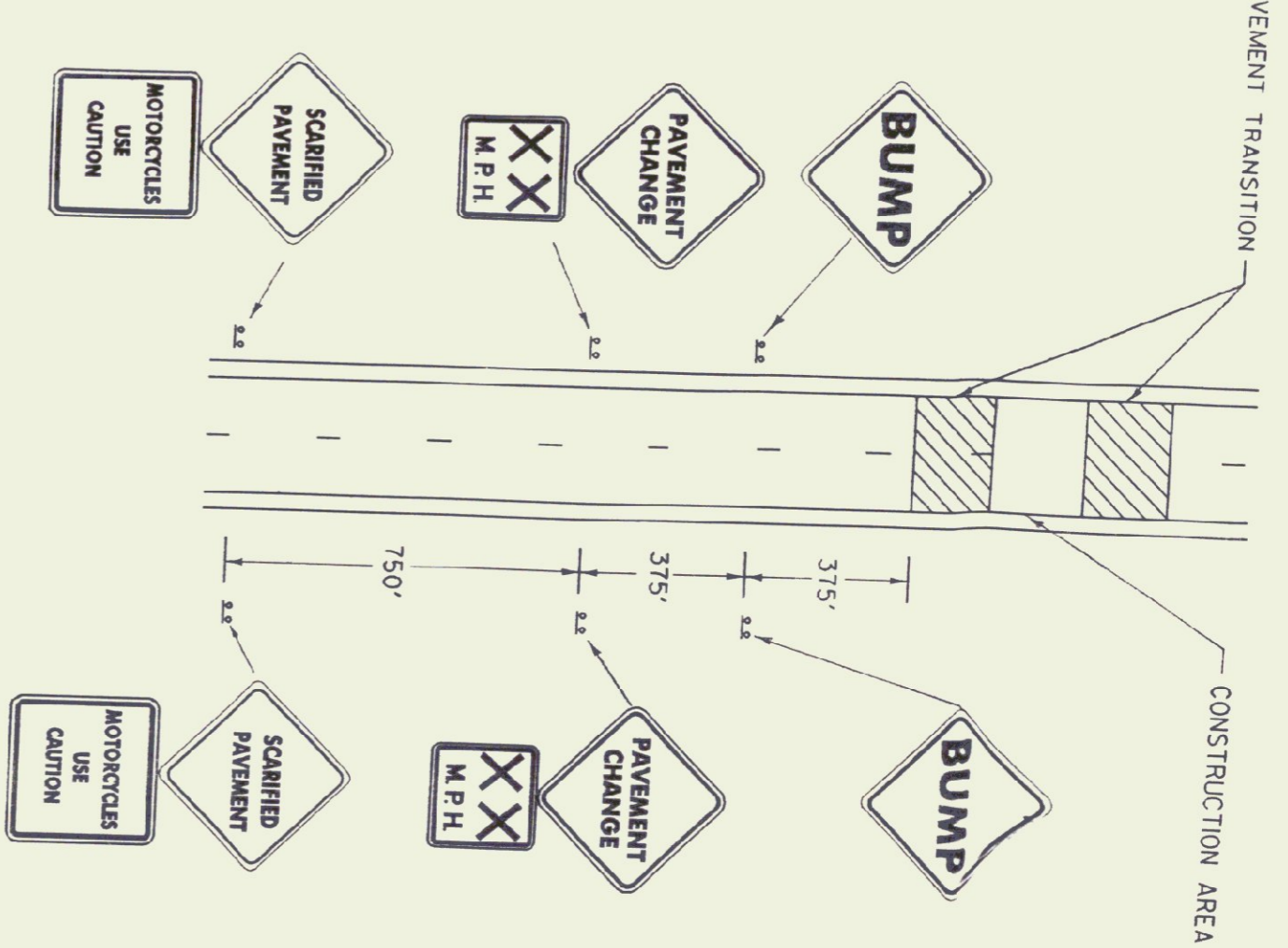
APPROVED _____ DATE APRIL 22, 1988

 DIRECTOR OF ENGINEERING

APPROVED FOR THIS PROJECT
 DESIGNATION: _____
 FINAL APPROVAL: _____

Acting TRAFFIC AND SAFETY ENGINEER
 Dale J. Feven

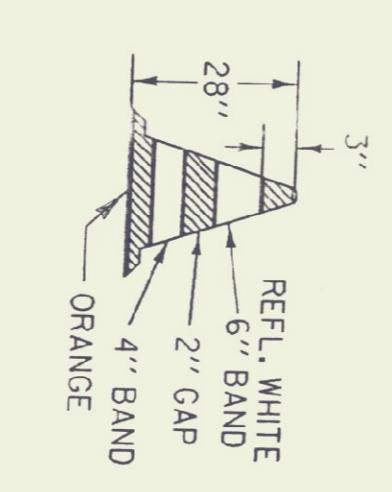
TRAFFIC CONTROL MISCELLANEOUS DETAILS



- NOTES:**
- 1) ADVISORY SPEED AS DETERMINED BY THE NEAREST SIGN (40 M.P.H. MINIMUM) SHALL BE INDICATED ON FIXED POSTS.
 - 2) SIGNS MOUNTED ON FIXED POSTS.
 - 3) ALL DISTANCES ARE DESIRABLE MINIMUMS. FIELD CONDITION SHALL CONTROL THE ACTUAL PLACEMENT.
 - 4) THE BUMP SIGN MAY BE ELIMINATED WHEN THERE IS NO BUMP WHEN THE CONTRACTOR IS WORKING IN THE CONSTRUCTION AREA. THE ADVANCED WARNING SIGN PACKAGE SHALL BE USED. SEE STD. E-103.

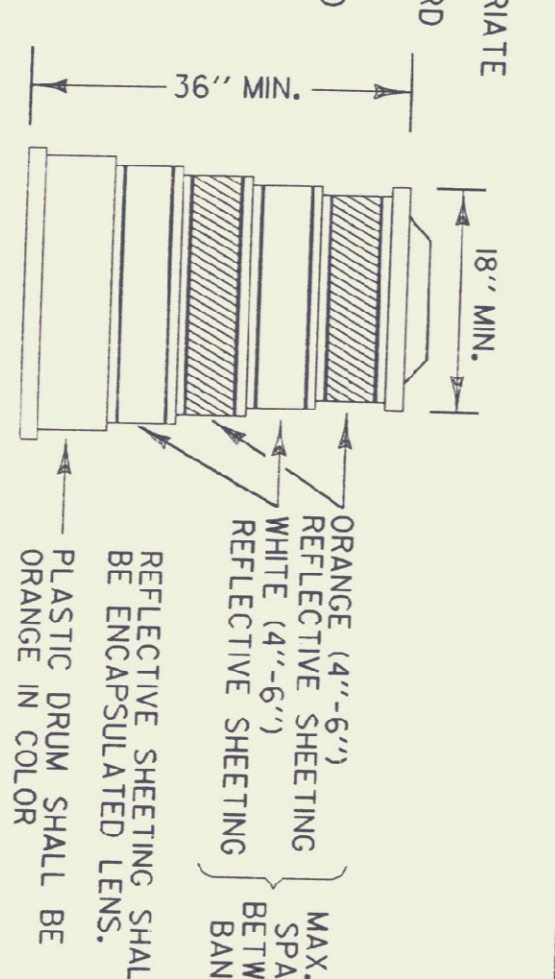
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ADVANCED WARNING SIGN PACKAGE FOR COLD PLANED (SCARIFIED) SURFACES.



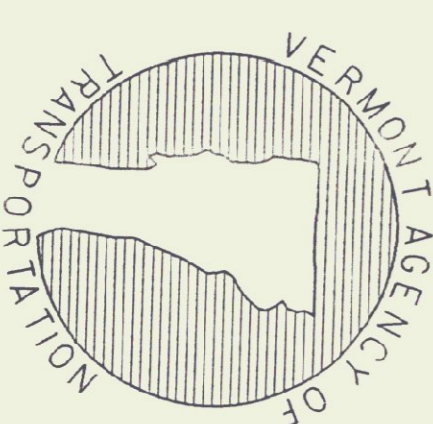
- NOTES:**
- 1) 28" CONES SHALL BE USED ON ROADWAYS WITH SPEED LIMITS OF 35 MPH OR MORE AND ON ALL ROADWAYS DURING HOURS OF DARKNESS.
 - 2) CONES MAY BE WEIGHTED TO PREVENT OVERTURNING, HOWEVER THE WEIGHTS SHALL NOT PRESENT A HAZARD IF THE CONE IS STRUCK.
 - 3) REFLECTIVE SHEETING SHALL BE ENCAPSULATED LENS.

28" REFLECTORIZED CONE



REFLECTORIZED PLASTIC DRUM

OTHER STDS. REQUIRED: E-101, E-102, E-103, E-107A, E-136, E-150
 STANCHION DETAIL SHEET



STANDARD E-106