

**BITUMINOUS CONCRETE FOR ACCEL AND DECEL RAMP WIDENING (ITEM 406.25)**

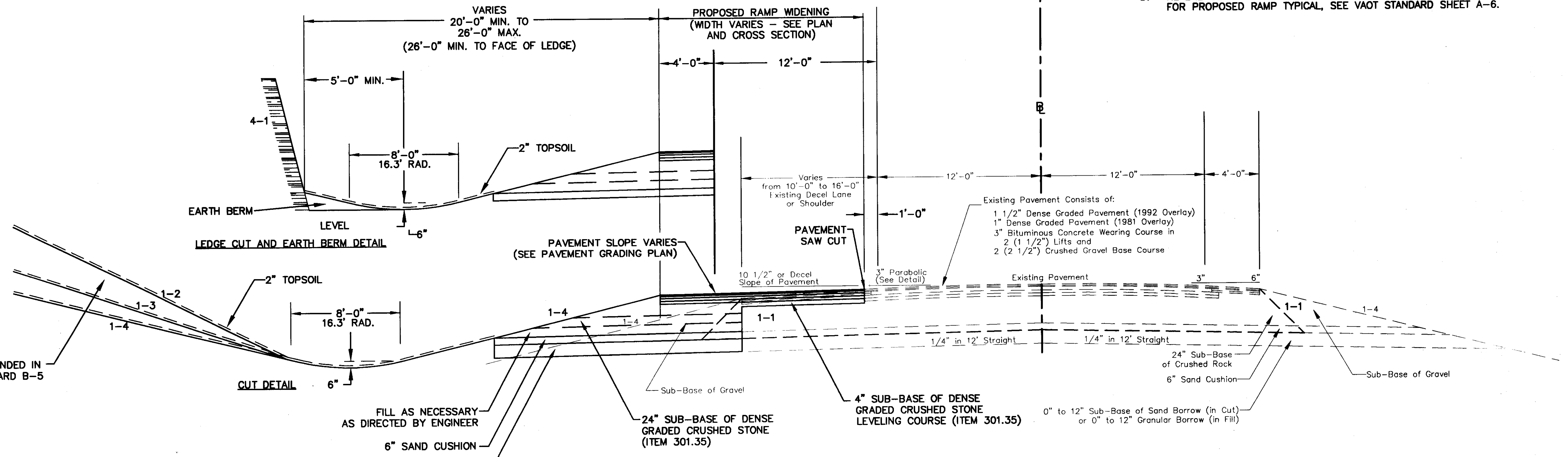
- 1-1/2" BITUMINOUS CONCRETE WEARING COURSE (TYPE III)
- 1-3/4" BITUMINOUS CONCRETE BINDER COURSE (TYPE II)
- 5" BITUMINOUS CONCRETE BASE COURSE (TYPE I) IN TWO 2 1/2" LIFTS

**TYPICAL SECTION NOTES:**

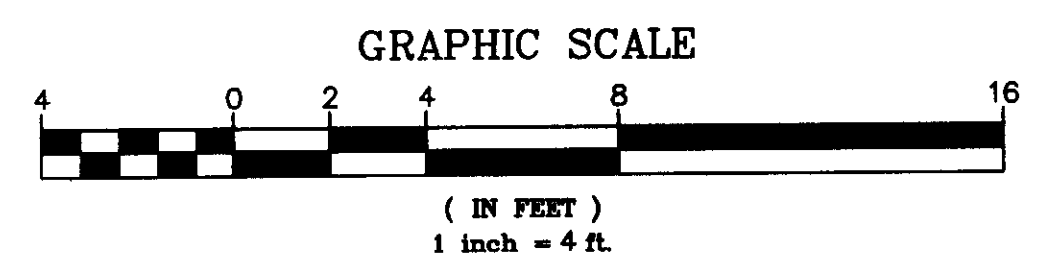
- THE DESIGN INTENT IS TO MATCH THE ORIGINAL DESIGN DEPTH OF PAVEMENT AND STRUCTURAL SECTION. WHERE THE EXISTING DECELERATION OR ACCELERATION LANE ENDS, THE PAVEMENT SHALL BE SAW CUT PERPENDICULAR TO THE SOUTHBOUND CENTERLINE. THE PAVEMENT SHALL ALSO BE SAW CUT LATERALLY 13'-0" PARALLEL TO THE CENTERLINE (ALONG THE EDGE OF EXISTING BASE COURSE) FOR THE PROPOSED DECELERATION OR ACCELERATION LANE AND TAPER. THIS AREA SHALL THEN BE EXCAVATED A MINIMUM OF 12" UNDER EXISTING PAVEMENT (PER TYPICAL) IN WIDENED AREAS FOR THE PLACEMENT OF NEW SURFACE MATERIAL AND BITUMINOUS CONCRETE IN ACCORDANCE TO DESIGN SPECIFICATIONS.
- THE EXISTING PAVEMENT AND THE STRUCTURAL SECTION CONSISTS OF:
  - A 1992, 1 1/2" DENSE GRADED PAVEMENT OVERLAY
  - And A 1981, 1" DENSE GRADED PAVEMENT OVERLAY
 FROM ORIGINAL CONSTRUCTION PROJECT I-91-1 (31):
  - 3" BITUMINOUS CONCRETE PAVEMENT,
  - 2 (2 1/2") CRUSHED GRAVEL PLANT MIXED BASE COURSE,
  - 24" SUB-BASE OF CRUSHED ROCK ON A 6" SAND CUSHION AND
  - AN OPTIONAL 0" TO 12" OF GRANULAR BORROW (IF THE ORIGINAL SECTION WAS IN FILL) OR 0" TO 12" OF SAND BORROW (IF THE ORIGINAL SECTION WAS IN CUT).
- FOR NORTH AND SOUTH BOUND RAMP TYPICALS AT EACH REST AREA SEE VAOT STANDARD DETAIL SHEET A-6

**PLAN REFERENCE:**

- RECORD DESIGN PLANS FOR HARTLAND-HARTFORD PROJECT I-91-1 (22) APPROVED FOR CONSTRUCTION JAN. 1963 (SHEET 4 AND 5 OF 185 SHEETS).
- STANDARD DETAIL B-15 FROM HARTLAND-HARTFORD PROJECT I-91-1 (31) APPROVED FOR CONSTRUCTION JULY 1963. STANDARD DETAIL B-15 APPROVED MAR. 1965.
- VAOT ROUTE LOG AND PROGRESS CHART FOR INTERSTATE ROUTE I-91-1 MM 68.2 TO MM 68.6 (SHEET 18 OF 52 SHEETS) REVISED DEC. 1989 AND BASED ON INPUT FROM PAVEMENT MANAGEMENT SECTION OVERLAYS SINCE 1989.
- REFERENCE TO VAOT STANDARD SHEETS A-2, A-6, AND A-60. FOR PROPOSED RAMP TYPICAL, SEE VAOT STANDARD SHEET A-6.



SLOPES ARE TO BE ROUNDED IN ACCORDANCE TO VAOT STANDARD B-5



OPTIONAL (MATCH EXISTING CONDITIONS)  
0" TO 12" SAND BORROW (ITEM 203.31)  
OR 0" TO 12" GRANULAR BORROW (ITEM 203.32)

<p><b>N·B·F</b> Nimtz·Berryhill·Fiegel ARCHITECTS P.C. 24 1/2 Center Street, Rutland, Vermont 05701</p>	<p><b>Steven J. Banik, P.E.</b> STRUCTURAL ENGINEER Cuttingsville, Vermont 05738</p>	<p><b>DuBois &amp; King inc.</b> Highway Design Site Engineering Mechanical and Electrical Engineers</p>	<p>HARTFORD, VT. I-91 INFORMATION CENTERS</p>	<p>TYPICAL SECTION FOR SOUTHBOUND DECEL &amp; ACCEL. LANES</p>	<p>FINAL PLANS 2/15/96</p>	<p>R2</p>
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