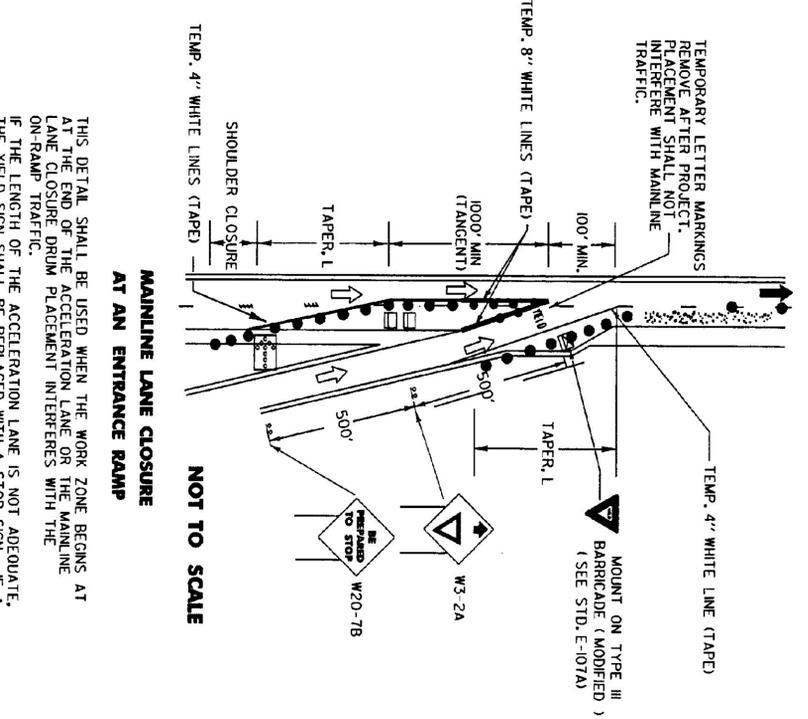


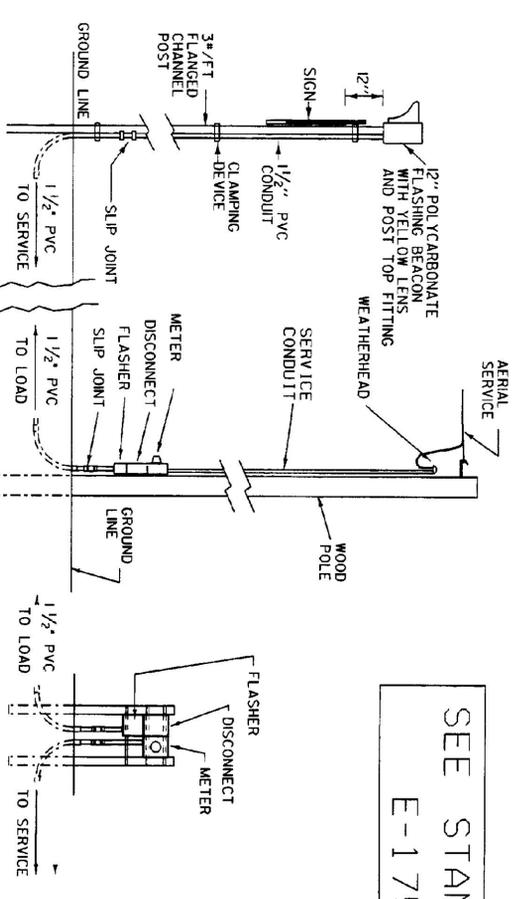
- NOTES:**
- 1) ALL SIGNS SHALL BE MOUNTED ON FIXED POSTS (YIELDING TYPE) UNLESS OTHERWISE NOTED.
 - 2) CHANNELIZING DEVICES SHALL BE PLACED AS FOLLOWS:
TAPERS - DEVICES SHALL BE SPACED A MAXIMUM OF "S" (THE SPEED LIMIT IN FEET) APART. TANGENT - DEVICES SHALL BE SPACED 2 X "S" (THE SPEED LIMIT IN FEET) APART.
 - 3) ALL DISTANCES ARE DESIRABLE MINIMUMS. FIELD CONDITIONS SHALL CONTROL THE ACTUAL PLACEMENT.
 - 4) TAPER RATES ARE BASED ON THE POSTED MAINLINE AND EXIT SPEEDS.
 - 5) TEMPORARY PAVEMENT MARKINGS ARE REQUIRED WHEN THE LAYOUT IS TO BE IN EFFECT FOR THREE DAYS OR MORE.
 - 6) LANE CLOSURES AND TAPER LENGTHS, L, AS DETAILED ON STANDARD E-103.
 - 7) EXIT SIGN SHALL BE MOUNTED A MINIMUM OF 3' ABOVE THE GROUND AND HIGH ENOUGH TO BE SEEN ABOVE CHANNELIZING DEVICES.

- LEGEND**
- REFL. 28" CONES
 - REFL. PLASTIC DRUMS
 - PAVEMENT MARKING REMOVAL
 - ⇨ INDICATES TRAFFIC FLOW
 - ▭ WORK AREA
 - ▭ FLASHING ARROW PANEL
 - ▭ TYPE III BARRICADES
 - ▭ TYPE III BARRICADES (MOD.)

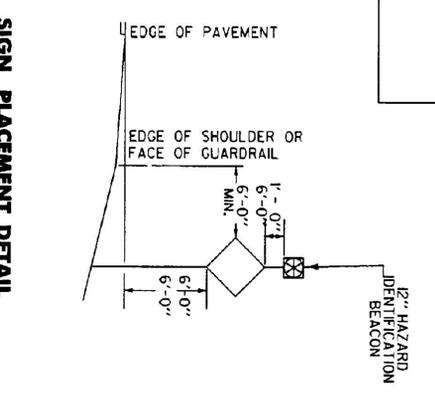


THIS DETAIL SHALL BE USED WHEN THE WORK ZONE BEGINS AT THE END OF THE ACCELERATION LANE OR THE MAINLINE LANE CLOSURE DRUM PLACEMENT INTERFERES WITH THE ON-RAMP TRAFFIC.

IF THE LENGTH OF THE ACCELERATION LANE IS NOT ADEQUATE, THE YIELD SIGN SHALL BE REPLACED WITH A STOP SIGN. IF STOP SIGN IS USED, IT SHOULD BE ACCOMPANIED BY A STOP BAR.



SEE STANDARD E-175



- NOTES:**
- 1) AT THE CONTRACTOR'S OPTION:
A. THE POWER SUPPLY MAY BE AERIAL OR UNDERGROUND (SEE DETAIL).
B. POWER FOR A FLASHING BEACON MAY BE COMBINED WITH POWER FOR A TRAFFIC SIGNAL OR THEY MAY HAVE SEPARATE POWER SOURCES.
 - 2) THE FLASHER UNIT SHALL BE ONE CIRCUIT AND INCLUDE A RADIO INTERFERENCE FILTER.
 - 3) BATTERY OPERATED FLASHERS WILL NOT BE ALLOWED.
 - 4) BOTTOM OF THE BEACON SHALL BE A MIN. OF 8'-0" AND A MAX. OF 12'-0" ABOVE THE EDGE OF THE PAVEMENT.
 - 5) FOR URBAN AREA PLACEMENT SEE STD. E-121.

REVISIONS AND CORRECTIONS

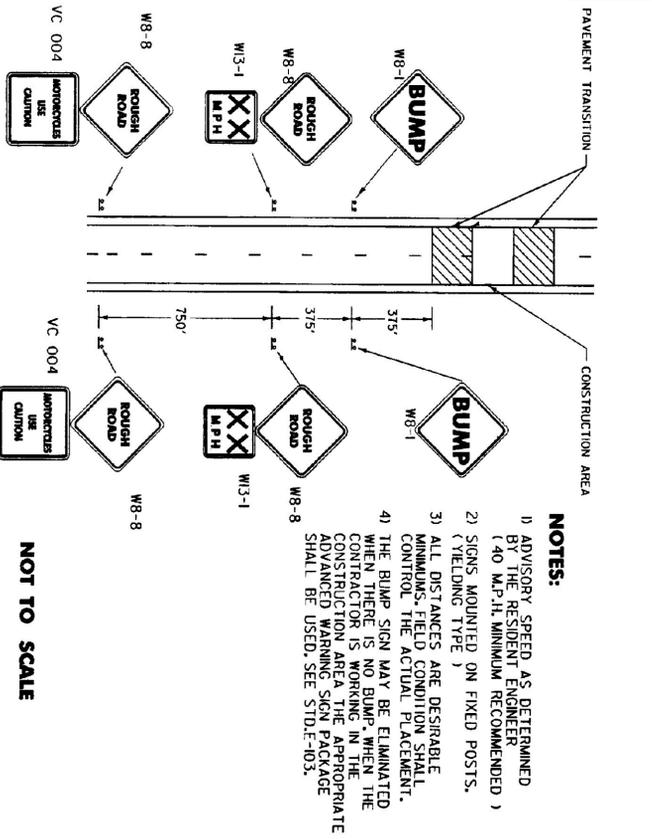
APR 12, 1988 - DATE OF ORIGINAL ISSUE
 JAN 23, 1989 - REVISED EXIT SIGN - CLARIFIED EXIT TAPER
 SEPT 20, 1993 - REVISED RAMP CLOSURES, FLASHING BEACON DETAILS AND MOVED TYPE III BARRICADE (MOD.) TO STD-E-107A
 AUG 08, 1995 - REVISED BEACON SIZE

APPROVED

Sharon D. McCall
 DIRECTOR OF ENGINEERING

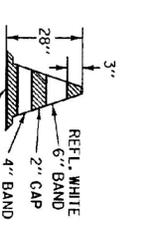
David A. Roy
 TRAFFIC AND SAFETY ENGINEER

TRAFFIC CONTROL MISCELLANEOUS DETAILS

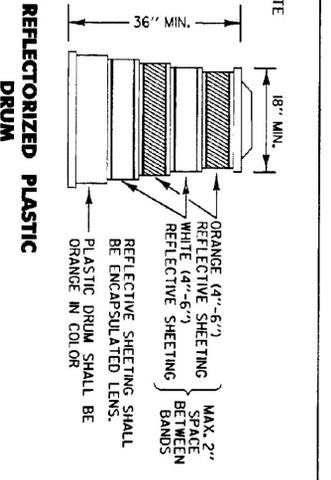


ADVANCED WARNING SIGN PACKAGE FOR COLD PLANED (SCARIFIED) SURFACES.

- NOTES:**
- 1) ADVISORY SPEED AS DETERMINED BY THE RESIDENT ENGINEER (40 M.P.H. MINIMUM RECOMMENDED)
 - 2) SIGNS MOUNTED ON FIXED POSTS, (YIELDING TYPE)
 - 3) ALL DISTANCES ARE DESIRABLE MINIMUMS. FIELD CONDITION SHALL CONTROL THE ACTUAL PLACEMENT.
 - 4) THE BUMP SIGN MAY BE ELIMINATED WHEN THERE IS NO BUMP. WHEN THE CONSTRUCTION AREA IS WORKING IN THE ADVANCED WARNING SIGN PACKAGE SHALL BE USED. SEE STD. E-103.



- NOTES:**
- 1.) 28" CONES SHALL BE USED ON ROADWAYS WITH SPEED LIMITS OF 35 MPH OR MORE AND ON ALL ROADWAYS DURING HOURS OF DARKNESS.
 - 2.) CONES MAY BE WEIGHTED TO PREVENT OVERTURNING, HOWEVER THE WEIGHTS SHALL NOT PRESENT A HAZARD IF THE CONE IS STRUCK.
 - 3.) REFLECTIVE SHEETING SHALL BE ENCAPSULATED LENS.



OTHER STDS. REQUIRED: E-101 E-102 E-103 E-107A E-136 E-150 E-175



STANDARD E-106

APPROVED FOR THIS PROJECT AND/OR DESIGN AND EXECUTION. FINAL APPROVAL PENDING.

/trcf/std/std106.dgn : std106.1