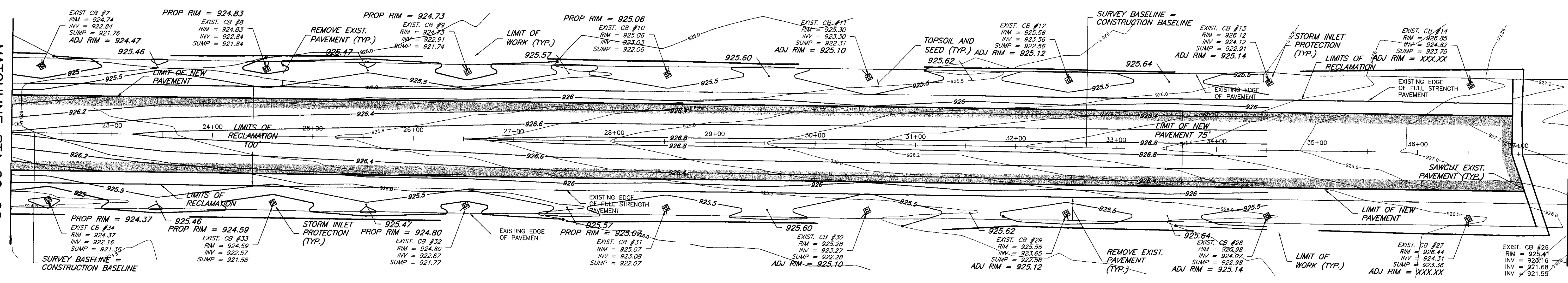


MATCHLINE STA 22+00
SEE SHEET NO. 4

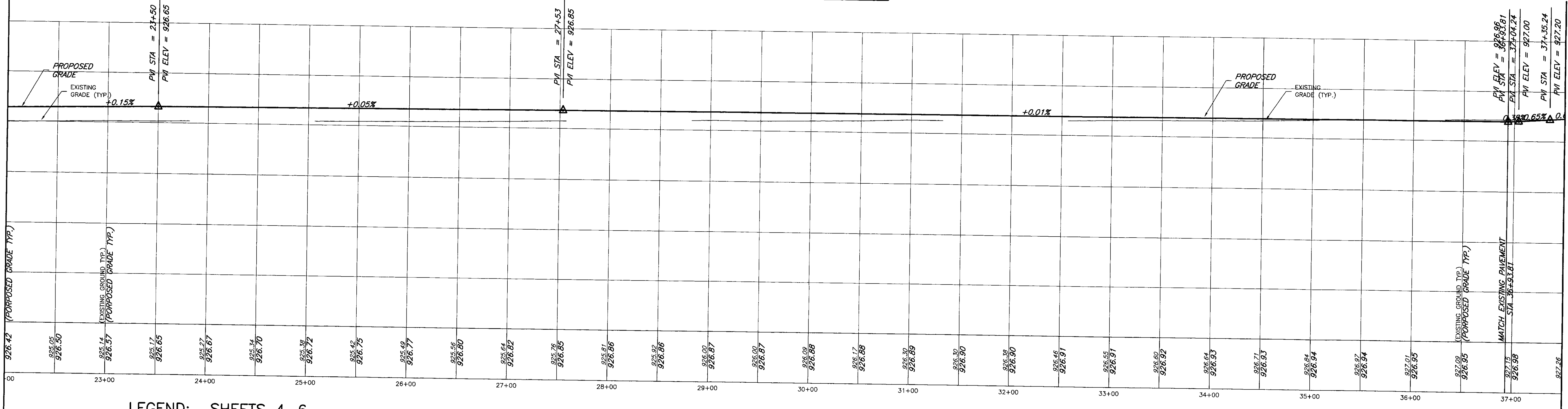
MATCHLINE STA 37+50
SEE SHEET NO. 6



PLAN RUNWAY 5-23

MATCHLINE STA 22+00
SEE SHEET NO. 4

MATCHLINE STA 37+50
SEE SHEET NO. 6

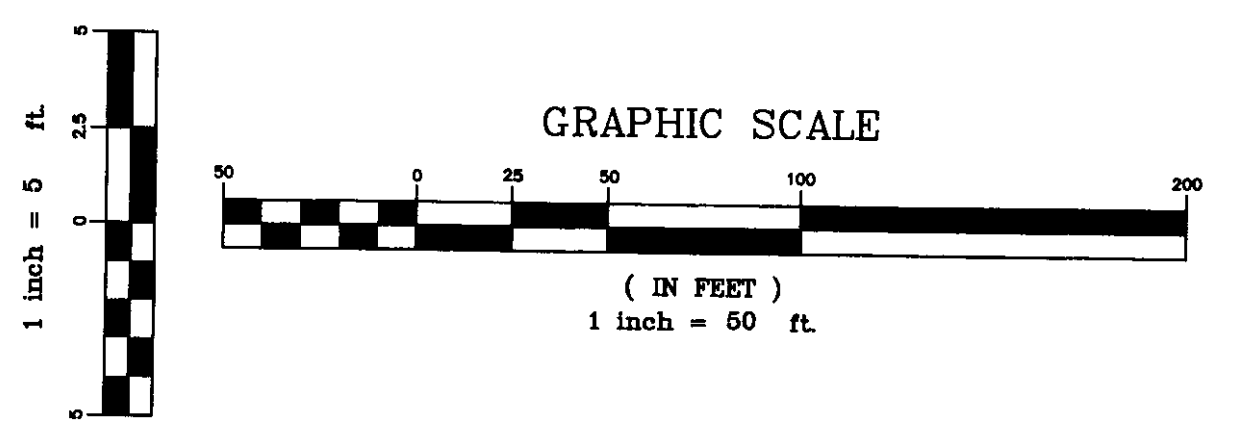


PROFILE RUNWAY 5-23

LEGEND: SHEETS 4-6

- PROPOSED EDGE OF PAVEMENT
- LIMITS OF RECLAMATION
- HAYBALE SILTATION CONTROL
- SILT FENCE
- 923 PROPOSED MINOR CONTOUR
- 925 PROPOSED MAJOR CONTOUR
- LIMIT OF GRADING
- 924.50 SPOT GRADE
- EXISTING EDGE OF PAVED SHOULDER
- EXISTING CATCH BASIN
- 923 EXISTING MINOR CONTOUR
- 925 EXISTING MAJOR CONTOUR
- EXISTING DIRT ROAD

PROP RIM = 925.63 MAINTAIN EXISTING RIM ELEVATION
ADJ RIM = 925.63 ADJUST CATCH BASIN



NOTES: PLAN AND PROFILE SHEETS 4-6

1. FOR TYPICAL SECTIONS AND CONSTRUCTION DETAILS SEE SHEETS 8 AND 9.
2. FOR BENCHMARK DATA SEE GENERAL AND PHASING PLANS, SHEETS 2 AND 3.
3. AFTER THE INSTALLATION OF BITUMINOUS CONCRETE, CONTRACTOR SHALL REESTABLISH PK NAILS AT BEGINNING AND END OF THE RUNWAY CENTERLINE. CONTRACTOR SHALL MAKE ALL NECESSARY TIES TO ENSURE PK NAIL IS PLACED AT ORIGINAL LOCATION.
4. THE LOCATION OF EXISTING UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY ALL UTILITIES AND INVERTS IN THE FIELD.
5. HAY BALES SHALL BE PROPERLY INSTALLED AT ALL CATCH BASINS AND AS SHOWN ON THE PLANS, PRIOR TO CONSTRUCTION. HAY BALES SHALL BE REMOVED AFTER VEGETATION IS ESTABLISHED.

Revisions:	Drawn By:	App'd. By:	Date:

Designed By:	Date:
MCG	8-18-00
Drawn By:	Date:
MCG	8-18-00
Checked By:	Date:
LJG	8-18-00

DRAFT

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RECONSTRUCT RUNWAY 5-23
PLAN AND PROFILE (STA 22+00 TO 37+50)

Drawing No. PP-2