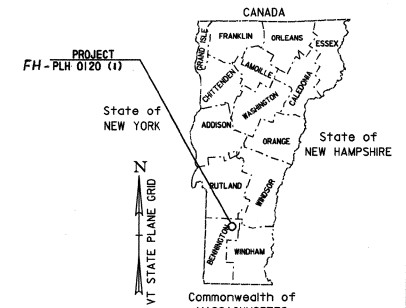


STATE OF VERMONT AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT TOWN OF PERU COUNTY OF BENNINGTON FOREST HIGHWAY #3



INDEX OF SHEETS

1	TITLE SHEET
2	PROJECT TYPICAL SHEET
3-4	QUANTITY SHEET
5	ITEM DETAIL SUMMARY SHEET
6-8	LAYOUT SHEETS
9	TRAFFIC SIGN SUMMARY SHEET
10	MISCELLANEOUS DETAILS & CONSTRUCTION APPROACH SIGNING

VAOT STANDARDS

E-106M	REINFORCED CONCRETE D.J. WITH GRATE	06-13-97
D-16M	CAST IRON GRATE, TYPE B	06-13-97
E-10M	TRAFFIC SIGNS, MINOR MAINTENANCE	06-13-97
E-100CM	CONSTRUCTION APPROACH SIGNS	06-13-97
E-101M	CONSTRUCTION SIGN DETAILS	06-13-97
E-102M	CONSTRUCTION SIGN DETAILS	06-13-97
E-102AM	CONSTRUCTION SIGN DETAILS	06-13-97
E-103M	MAINLINE TRAFFIC CONTROL, DIVIDED HIGHWAY, ONE LANE CLOSED	06-13-97
E-106M	TRAFFIC CONTROL MISCELLANEOUS DETAILS	06-13-97
E-107M	DELINEATION, BARRICADES AND DETOURS FOR CONSTRUCTION AREAS	06-13-97
E-107AM	BREAKAWAY BARRICADE DETAILS	06-13-97
E-108M	CONSTRUCTION ZONE LONGITUDINAL DROP OFFS	06-13-97
E-110M	MAJOR MAINTENANCE OPERATION LANE CLOSURE	06-13-97
E-12K	STANDARD SIGN PLACEMENT CONVENTIONAL ROAD	06-13-97
E-138M	MILE MARKER DETAILS STATE AND TOWN HIGHWAYS	06-13-97
E-143M	REGULATORY SIGN DETAILS	06-13-97
E-160M	FLANGED CHANNEL STEEL SIGN POST	06-13-97
E-164M	SQUARE STEEL SIGN POST	06-13-97
E-19M	PAVEMENT MARKING DETAILS	06-13-97
E-193M	PAVEMENT MARKING DETAILS	06-13-97
G-1dM	STEEL BEAM GUARD RAIL (40 MPH & LESS) HEAVY DUTY STEEL BEAM GUARD RAIL STEEL BEAM MEDIAN BARRIER ANCHOR FOR STEEL BEAM RAIL	06-13-97
G-17cM	MODIFIED ECCENTRIC LOADER TERMINAL WITH WOOD POSTS (MELT)	06-13-97
G-17bM	MODIFIED ECCENTRIC LOADER TERMINAL WITH WOOD POSTS (MELT)	06-13-97

BEGINNING IN THE TOWN OF PERU
ON FOREST HIGHWAY #3 AT MILE MARKER 0.341 AND EXTENDING EASTERLY
ALONG FOREST HIGHWAY #3 FOR A DISTANCE OF 2.622 KILOMETERS
TO MILE MARKER 1.970 IN THE TOWN OF PERU.

MILE MARKER TO MILE MARKER DATA FOREST HIGHWAY #3 PERU STA. 0+548.00 (MM 0.341) TO STA. 3+170.00 (MM 1.970)	LENGTH (METERS) 2622.00	LENGTH (MILES) 1.629
LENGTH OF ROADWAY LENGTH OF PROJECT	2622.00 METERS 2622.00 METERS	1.629 MILES 1.629 MILES

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RESURFACING THE EXISTING HIGHWAY,
SHIM/LEVELING, PROVIDING A WEARING COURSE, AND REPLACING SEVERAL DRAINAGE CULVERTS.
PAVEMENT MARKINGS, GUARDRAIL AND INCIDENTAL ITEMS ARE ALSO INCLUDED.

RECORD PLANS

CONTRACTOR: LANE CONSTRUCTION - MERIDAN, CT

RESIDENT ENGINEER: S. HAZEN

CONSTRUCTION BEGAN: APRIL 10, 1999

CONSTRUCTION COMPLETE: JUNE 9, 1999

RECORD PLANS BY: S. HAZEN & KEVIN NORTH

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.

BY: *[Signature]* FOR: *[Signature]* RESIDENT ENGINEER

DATE: 4/01/03

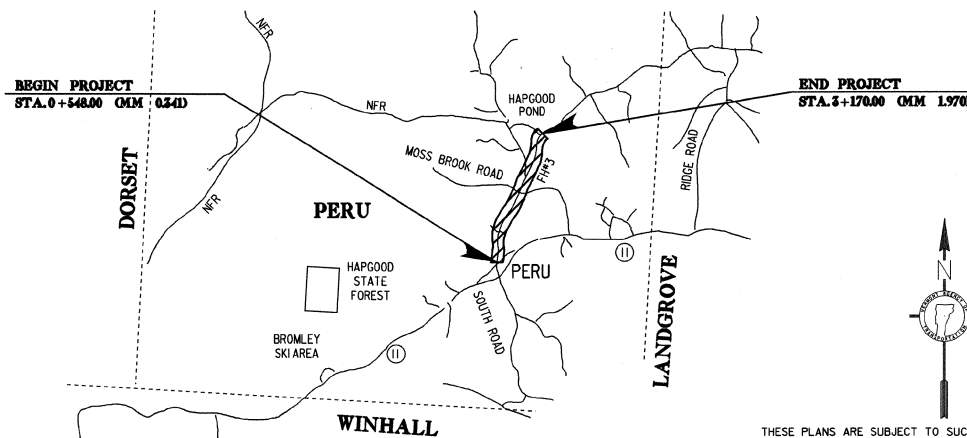
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found on microfilm in Central Files.

TRAFFIC DATA

FOREST HIGHWAY #3 (PERU MM 0.341 TO MM 1.970)

1999 ADT =
1999 DHV =
2009 ADT =
2009 DHV =

1996 ~ 2000 CUM. ESALS



CONVENTIONAL SIGNS

COUNTY LINE	---
TOWN LINE	- - -
LIMITS OF ACCESS	○-○-○
POINT OF ACCESS	X
FENCE LINE	-x-x-
STONE WALL	o-o-o-o
TRAVELED WAY	o-o-o-o
GUARD RAIL	o-o-o-o
RAILROAD	
SURVEY LINE	---
CULVERT	---
POWER POLE	⊕
TELEPHONE POLE	⊕
TREES	⊕
CONTROL OF ACCESS	///
PROPERTY LINE	---
R.O.W. TAKING LINE	SR
SLOPE RIGHTS	△
TOP OF CUT	△
TOE OF SLOPE	○

STATE OF VERMONT
SEAL OF SAULS F. WOOD
PROFESSIONAL ENGINEER

PLANS PREPARED BY

M FARLAND-JOHNSON, INC.
BINGHAMTON N.Y.

BY: *Charles F. Wood Jr.* 9-23-98

DATUM
VERTICAL _____
HORIZONTAL _____

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE CHIEF ENGINEER. CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1995, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON AUGUST 21, 1995 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

Metric

SUBMITTED BY ORDER OF THE
STATE TRANSPORTATION BOARD

APPROVED: _____ DATE: 4/01/03
DIRECTOR OF PROJECT DEVELOPMENT

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____ DATE: _____
DIVISION ADMINISTRATOR

PROJECT: PERU
FH-PLH 0120(1)

SHEET 1 OF 10 SHEETS

NOTES

1. THE PAVEMENT WEARING COURSE SHALL BE TYPE III, THE LEVELING COURSE SHALL BE TYPE IV. FOR ESTIMATING PURPOSES, LEVELING HAS BEEN CALCULATED AT A DEPTH OF 15mm. ASPHALT CEMENT USED IN THE BITUMINOUS CONCRETE PAVEMENT SHALL BE 58-34.
2. COLD PLANING TO BE COMPLETED ACCORDING TO TYPICAL OR AS NOTED OTHERWISE ON THE PLANS. ALL COLD PLANED AREAS SHALL BE TACKED WITH EMULSIFIED ASPHALT.
3. EMULSIFIED ASPHALT TO BE APPLIED ON EXISTING PAVEMENT, BETWEEN ALL COURSES OF PAVEMENT, AND ON COLD PLANED AREAS AT THE RATE OF 0.07 L/m² OR AS DIRECTED BY THE ENGINEER.
4. BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 5mm. (TOTAL THICKNESS EXCLUDING LEVELING)
5. ALL DRIVES SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER. A NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 406.25. ESTIMATED QUANTITIES OF THE ABOVE ITEMS HAVE BEEN INCLUDED TO PAY FOR THIS WORK.
6. THE EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 75mm OR AS DIRECTED BY THE ENGINEER. EXCAVATION WILL BE PAID FOR AS ALL-PURPOSE EXCAVATOR OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH SUBBASE OF CRUSHED GRAVEL (FINE GRADED). EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT, AS DIRECTED BY THE ENGINEER.
7. 0.9m OF BACKING IS REQUIRED BEHIND FACE OF GUARD RAIL WITH 1.8m POSTS. IF THIS CANNOT BE OBTAINED, THEN 2.4m POSTS SHALL BE USED.
8. PIPE INLET AND OUTLET AREAS, AND DITCH CLEANING THROUGH PROJECT SHALL BE PERFORMED AT LOCATIONS AND AS DIRECTED BY THE ENGINEER. PAYMENT SHALL BE MADE UNDER APPLICABLE EQUIPMENT RENTAL ITEMS).
9. AN ESTIMATE OF EARTH BORROW HAS BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING MELT FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 75mm DEPTH OF AGGREGATE SHOULDER MATERIAL UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE QUANTITIES INCLUDED REFLECT 20 CUBIC METERS OF EARTH BORROW AND 5 TONS OF AGGREGATE SHOULDER FOR EACH GUARD RAIL TERMINAL.

RURAL AREAS - SEED MIXTURE

% WT	kg/ha	NAME	PUR %	GERM %
37.1	26.0	CREEPING RED FESCUE	98	85
37.1	26.0	TALL FESCUE	95	90
5.7	4.0	RED TOP	95	90
14.4	10.0	BIRDSFOOT TREFOLI	98	85
5.7	4.0	ANNUAL RYE GRASS	95	85
100.0	70.0			

SEED MIXTURE:

SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.

SEED:

TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.

FERTILIZER:

FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 560 kg/ha. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA)

AGRICULTURAL LIMESTONE:

TO BE APPLIED AT THE RATE OF 4500kg/ha, OR AS DIRECTED BY THE ENGINEER.

HAY MULCH:

TO BE PLACED ON EARTH SLOPES AT THE RATE OF 4500kg/ha, OR AS DIRECTED BY THE ENGINEER.

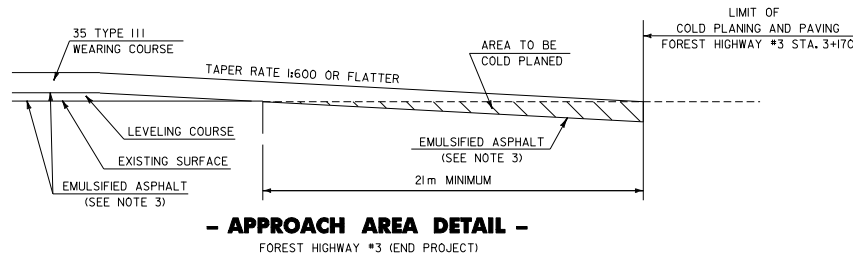
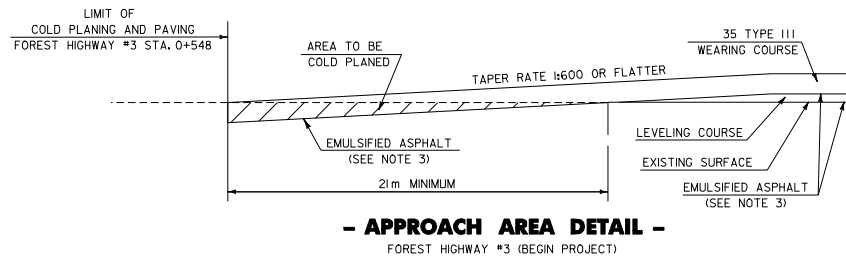
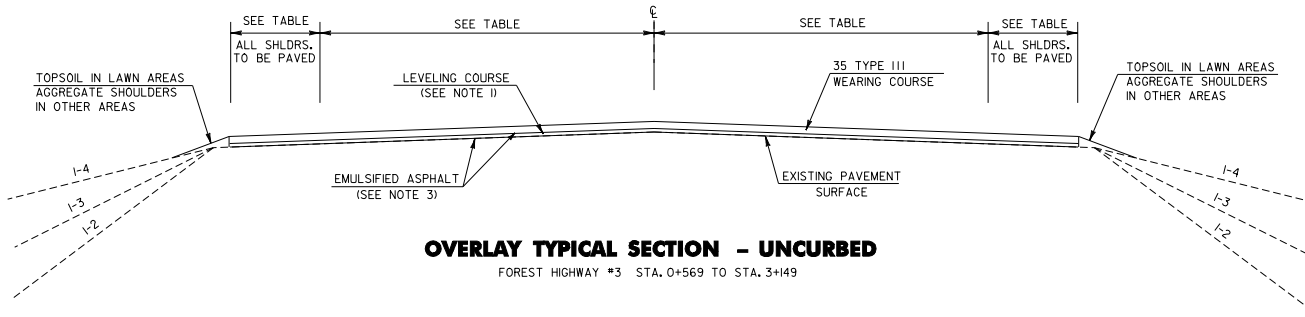
TOPSOIL:

TO BE USED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

MARKER POSTS:

TO BE PLACED AS INDICATED OR AS DIRECTED BY THE ENGINEER.

ALL DIMENSIONS ARE IN MILLIMETERS UNLESS OTHERWISE NOTED



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IN DPR**

TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING ±/km	LEVELING ±	NOTES
PERU - FOREST HIGHWAY #3	0+548	1+970	0.6m - 3.0m - 3.0m - 0.6m	38 35	0.259	368.6	OVERLAY WITH 35mm TYPE III & LEVELING COURSE
	1+970	2+010	(0.6m - 1.6m) - 3.0m - 3.0m - (0.6m - 1.6m)	38 35	0.295	11.8	OVERLAY WITH 35mm TYPE III & LEVELING COURSE
	2+010	2+050	(1.6m - 0.6m) - 3.0m - 3.0m - (1.6m - 0.6m)	38 35	0.295	11.8	OVERLAY WITH 35mm TYPE III & LEVELING COURSE
	2+050	3+170	0.6m - 3.0m - 3.0m - 0.6m	38 35	0.259	368.6	OVERLAY WITH 35mm TYPE III & LEVELING COURSE

PROJECT TYPICAL SHEET	PROJECT:	PERU	PROJECT NO.:	FH-PLH-0120(001)
	DESIGN FILE NAME:	\\vaot+cadd\flingcabnet\98e074\Construction\9	PLOT DATE:	01-APR-2003 1h
	IPARM FILE NAME:	pc074ty.i	SURVEY DATE:	
	SURVEYED BY:	ACT, JAV	DRAWN BY:	ACT
SQUAD LEADER:	JAV	SHEET:	2 OF 10	

**FOREST HIGHWAY #3
STA. 0+548 = MM 0.341
BEGIN PROJECT FH-PLH 0120(001)**

TEMPORARY AND DURABLE 100mm WHITE LINE
TEMP. STA. 1+08 TO STA. 1+560
STA. 0+550.0 TO STA. 1+560.0 SOLID LT. AND RT. (EDGE LINE)
(WITH EDGE LINE BREAK FOR ADAMS LANE)

TEMPORARY AND DURABLE 100mm YELLOW LINE
STA. 0+550.0 TO STA. 1+560.0 SOLID LT. AND RT.
(WITH CENTERLINE BREAK FOR ADAMS LANE)

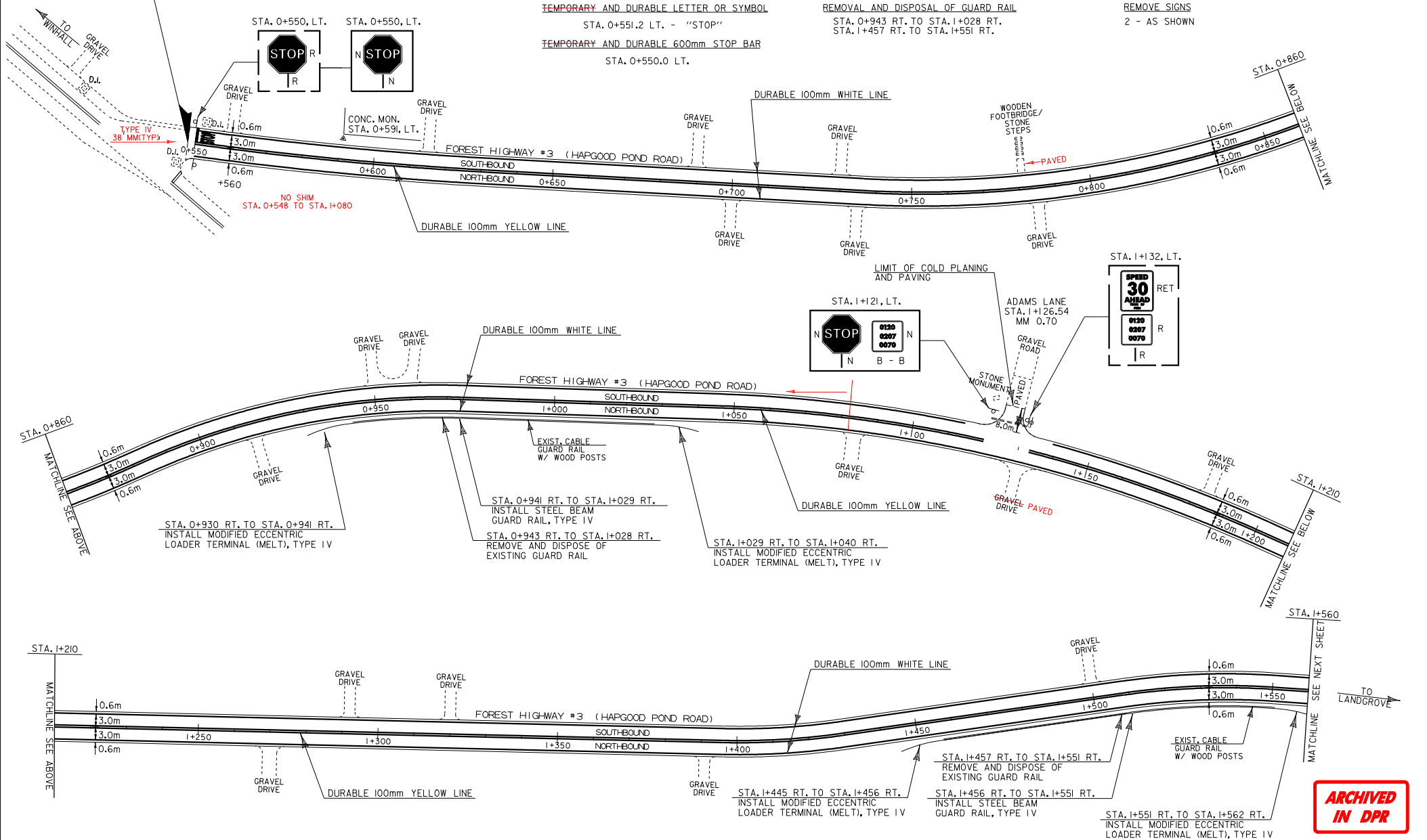
MODIFIED ECCENTRIC LOADER TERMINAL, TYPE IV
STA. 0+930 RT. TO STA. 0+941 RT.
STA. 1+029 RT. TO STA. 1+040 RT.
STA. 1+445 RT. TO STA. 1+456 RT.
STA. 1+551 RT. TO STA. 1+562 RT.

STEEL BEAM GUARD RAIL, TYPE IV
STA. 0+941 RT. TO STA. 1+029 RT.
STA. 1+456 RT. TO STA. 1+551 RT.

TEMPORARY AND DURABLE LETTER OR SYMBOL
STA. 0+551.2 LT. - "STOP"
TEMPORARY AND DURABLE 600mm STOP BAR
STA. 0+550.0 LT.

REMOVAL AND DISPOSAL OF GUARD RAIL
STA. 0+943 RT. TO STA. 1+028 RT.
STA. 1+457 RT. TO STA. 1+551 RT.

REMOVE SIGNS
2 - AS SHOWN



LEGEND
N = NEW
R = REMOVE
R&S = REMOVE AND SALVAGE
S = SALVAGE SIGN
RET = RETAIN
B - B = BACK TO BACK

**ARCHIVED
IN DPR**

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVING PROJECT LAYOUT	PROJECT:	PERU	PROJECT NO.:	FH-PLH-0120(001)
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	IPARM FILE NAME:	_pc02740.dwg	SURVEY DATE:	
	SURVEYED BY:	ACT, JAV	DRAWN BY:	ACT
	SQUAD LEADER:	JAV	SHEET:	6 OF 10

TEMPORARY AND DURABLE 100mm WHITE LINE

STA. 1+560.0 TO STA. 2+610.0 SOLID LT. AND RT. (EDGELINES)
(WITH EDGELINE BREAKS FOR SIDE ROADS)

TEMPORARY AND DURABLE 100mm YELLOW LINE

STA. 1+560.0 TO STA. 2+610.0 SOLID LT. AND RT.
(WITH CENTERLINE BREAKS FOR SIDE ROADS)

STEEL BEAM GUARD RAIL, TYPE IV

STA. 2+214 RT. TO STA. 2+283 RT.
STA. 2+278 LT. TO STA. 2+328 LT.

MODIFIED ECCENTRIC LOADER TERMINAL, TYPE IV

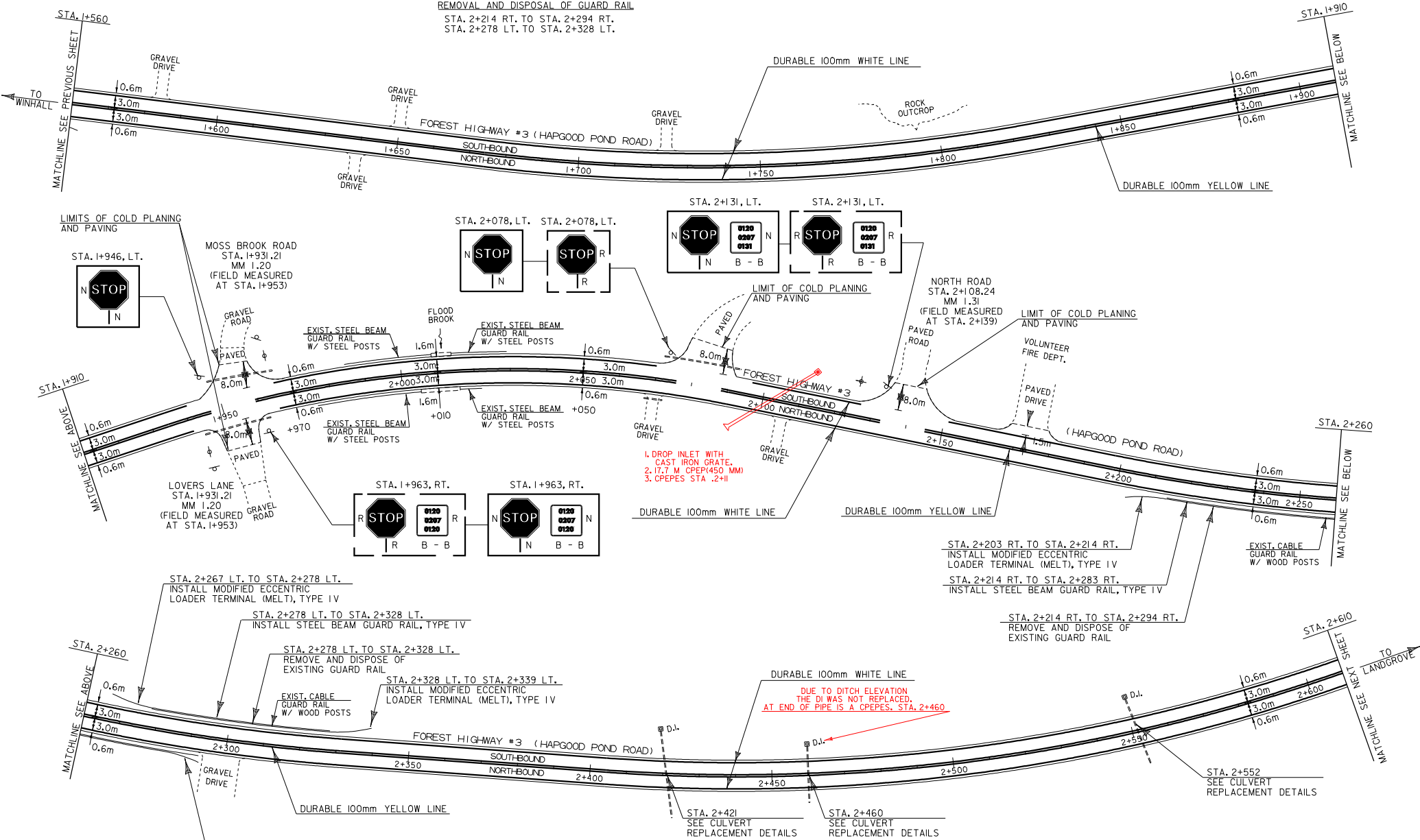
STA. 2+203 RT. TO STA. 2+214 RT.
STA. 2+328 LT. TO STA. 2+339 LT.
STA. 2+267 LT. TO STA. 2+278 LT.
STA. 2+283 RT. TO STA. 2+294 RT.

REMOVE SIGNS

5 - AS SHOWN

REMOVAL AND DISPOSAL OF GUARD RAIL

STA. 2+214 RT. TO STA. 2+294 RT.
STA. 2+278 LT. TO STA. 2+328 LT.



1. DROP INLET WITH CAST IRON GRATE.
2. 17.7 M CPEP (450 MM)
3. CPEPES STA. 2+11

DUE TO DITCH ELEVATION THE DI WAS NOT REPLACED. AT END OF PIPE IS A CPEPES, STA. 2+460

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- LEGEND**
- N = NEW
 - R = REMOVE
 - R&S = REMOVE AND SALVAGE
 - S = SALVAGE SIGN
 - RET = RETAIN
 - B - B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVING PROJECT LAYOUT	PROJECT:	PERU	PROJECT NO.:	FH-PLH-0120(001)
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	IPARM FILE NAME:	pa012021	PLOT DATE:	01-APR-2003 1h
	SURVEYED BY:	ACT, JAV	SURVEY DATE:	
SQUAD LEADER:	JAV	DRAWN BY:	ACT	
			SHEET:	7 OF 10

TEMPORARY AND DURABLE 100mm WHITE LINE

STA. 2+610.0 TO STA. 3+170.00 SOLID LT. AND RT. (EDGE LINE)
(WITH EDGE LINE BREAK FOR HAPGOOD POND ENTRANCE)

TEMPORARY AND DURABLE 100mm YELLOW LINE

STA. 2+610.0 TO STA. 3+170.00 SOLID LT. AND RT.
(WITH CENTERLINE BREAK FOR HAPGOOD POND ENTRANCE)

REMOVAL AND DISPOSAL OF GUARD RAIL

STA. 2+944 RT. TO STA. 3+138 RT.
STA. 2+976 LT. TO STA. 3+141 LT.

STEEL BEAM GUARD RAIL, TYPE IV

STA. 2+944 RT. TO STA. 3+138 RT.
STA. 2+975 LT. TO STA. 3+141 LT.

MODIFIED ECCENTRIC LOADER TERMINAL, TYPE IV

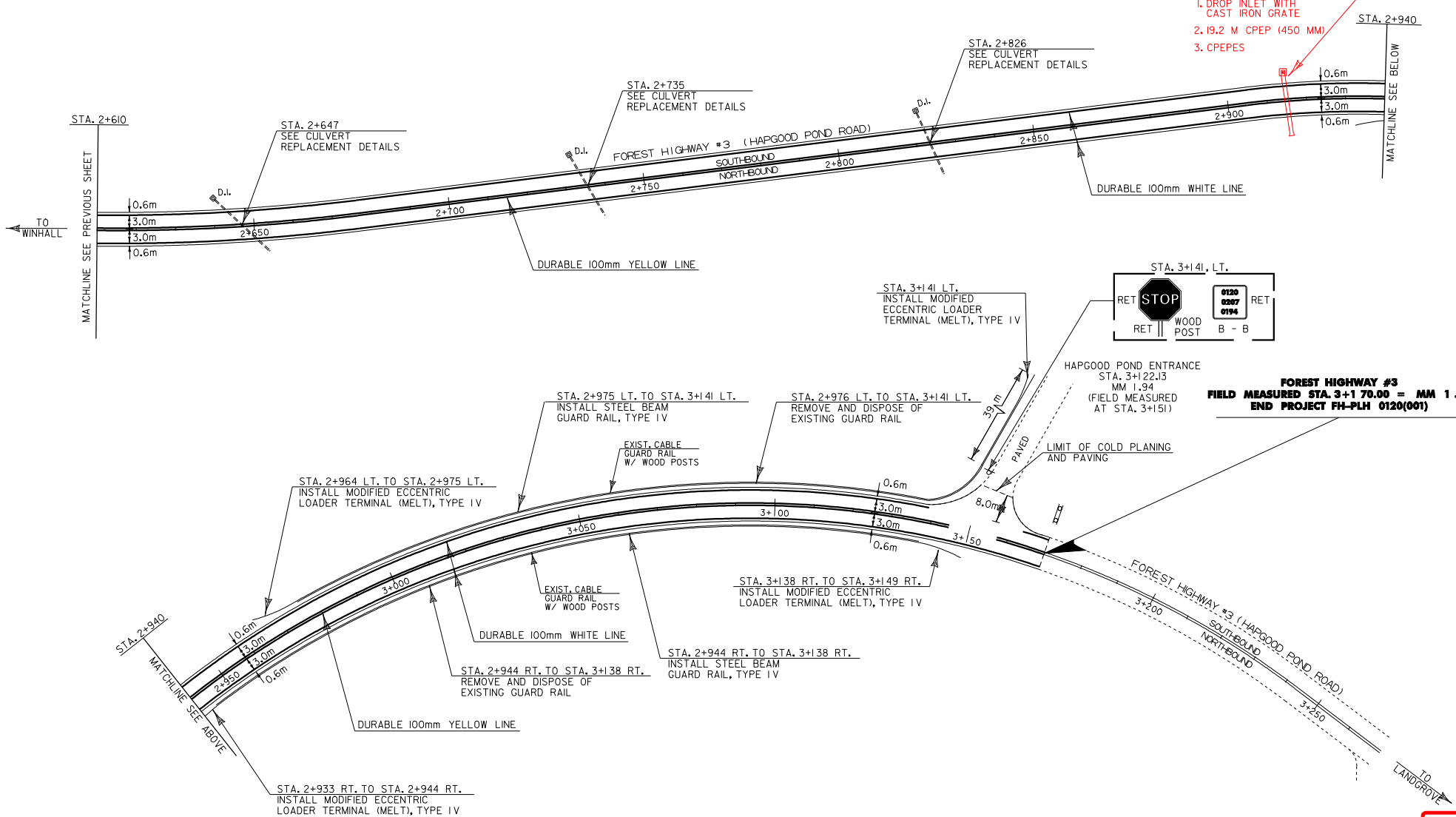
STA. 2+933 RT. TO STA. 2+944 RT.
STA. 2+964 LT. TO STA. 2+975 LT.
STA. 3+138 RT. TO STA. 3+149 RT.
~~STA. 3+141 LT.~~

ANCHOR FOR STEEL BEAM GUARD RAIL

STA. 3+141LT.

STA. 2+934

1. DROP INLET WITH CAST IRON GRATE
2. 19.2 M CPEP (450 MM)
3. CPEPES



FOREST HIGHWAY #3
FIELD MEASURED STA. 3+170.00 = MM 1.97
END PROJECT FH-PLH 0120(001)

LEGEND

- N = NEW
- R = REMOVE
- R&S = REMOVE AND SALVAGE
- S = SALVAGE SIGN
- RET = RETAIN
- B - B = BACK TO BACK

PAVING PROJECT LAYOUT

PROJECT:	PERU	PROJECT NO.:	FH-PLH-0120(001)
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IPARM FILE NAME:	pa074103.d	SURVEY DATE:	
SURVEYED BY:	ACT, JAV	DRAWN BY:	ACT
SQUAD LEADER:	JAV	SHEET:	8 OF 10

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DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

