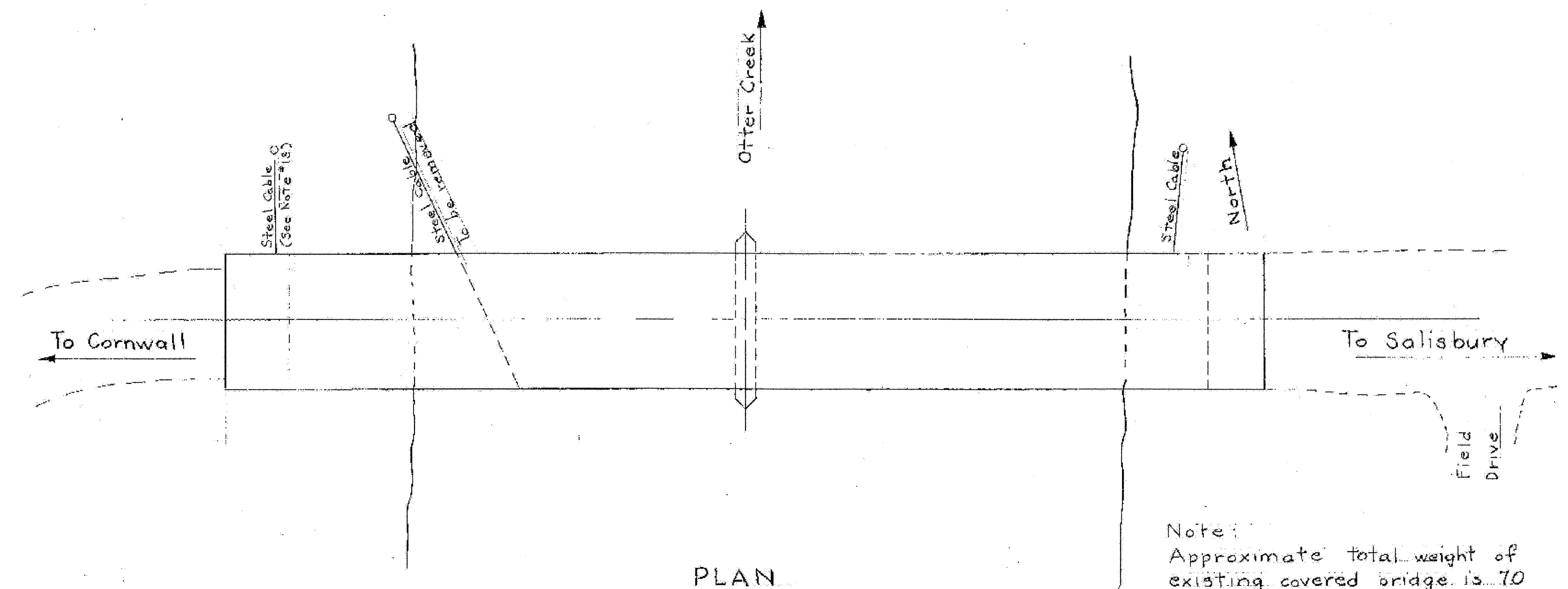


General Notes

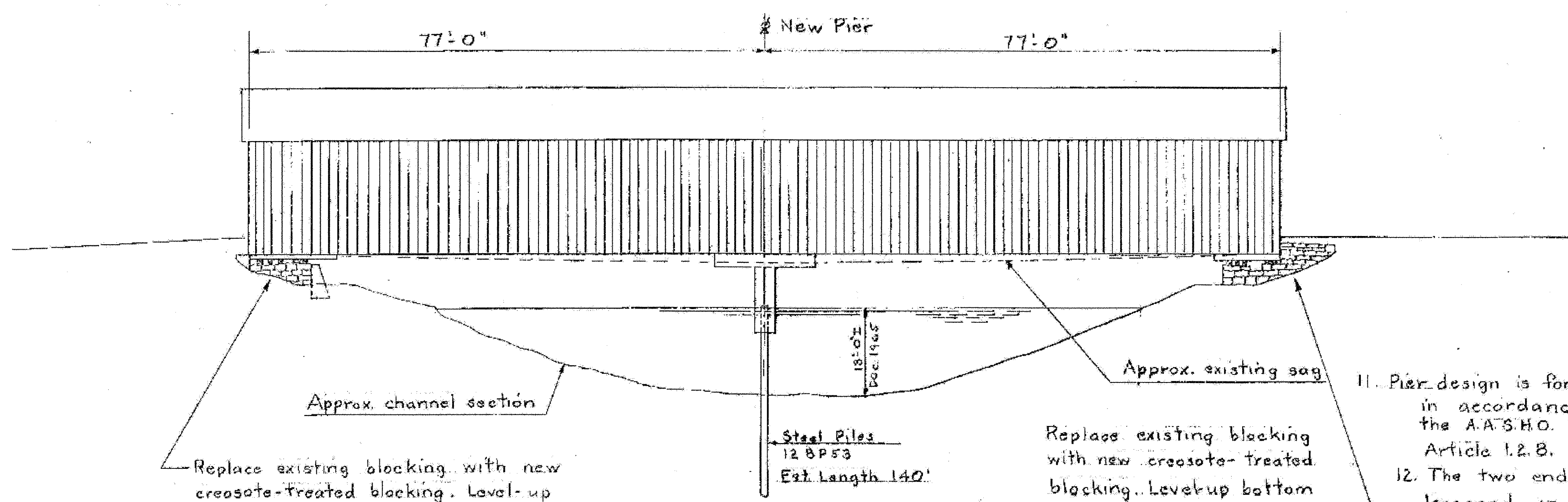
1. All materials and construction shall conform to the state of Vermont, Department of Highways, Standard Specifications, for Road and Bridge Construction, dated April 1964 and the AASHTO Standard Specifications dated 1961.
2. All sheathing used to repair existing superstructure shall be unpainted, weathered boards of a color compatible with the adjacent boards.
3. All new structural timber is to be of Eastern Spruce, 1200f Structural Grade, or an approved equivalent as specified in Item 509.02 of the Standard Specifications.
4. All new structural timber is to be treated with creosote oil, with a minimum net retention of 6 lbs./cuft empty cell.
5. The cost of nails shall be included in the unit price bid for Item 509, Structural Timber.
6. The cost of bolts and other hardware used for truss repair shall be paid for under Item 404-A Structural Steel.
7. All connections shall be checked before and after jacking. Damaged pins or bolts are to be removed and replaced using equivalent materials and size. The cost of replacing up to 150 wood pins is to be paid for under Item 509, Structural Timber. Payment for pins will be on the basis of 10 board feet per pin. The cost of additional wood pins will be paid for as Extra Work or by Supplementary Agreement.
8. Steel piles (12BP53) are to be driven to a min. safe bearing capacity of 35 tons/pile.
9. The bridge shall be kept open to traffic as much as possible during construction.
10. The Contractor shall submit details for jacking the superstructure to the State for approval.



PLAN

Note:
Approximate total weight of existing covered bridge is 70 tons.

Replace missing, broken, or damaged boards and planks on the sides of the bridge, flooring and portals.
Re-nail all loosened pieces.



ELEVATION

Replace existing blocking with new creosote-treated blocking. Level-up bottom chords of truss. Face up front of abutment with concrete, repair and point up loose masonry on the sides.

Replace existing blocking with new creosote-treated blocking. Level-up bottom chords of truss. Repair and point-up loose masonry.

11. Pier design is for H10-44 loading applied in accordance with the provisions of the A.A.S.H.O. Standard Specifications, Article 1.2.8.
12. The two end guy cables are to be loosened or removed prior to jacking, leveling, and plumbing the superstructure and are to be re-fastened and adjusted when this work is completed.
13. For other notes not listed on this sheet see BR-454 &
14. The number of wood pins to be used for truss repair will be determined by the Engineer.

STATE OF VERMONT DEPARTMENT OF HIGHWAYS	
TOWN OF <u>SALISBURY-Cornwall</u>	
ROAD NO. <u>S.A. #1</u> BRIDGE NO. <u>8</u>	
<u>S.A. #1 OVER OTTER CREEK</u>	
<u>PLAN & ELEVATION</u>	
SALISBURY-CORNWALL BHO 1445 (27)	
SHEET 57 OF 60 FOR INFORMATION PURPOSES ONLY	