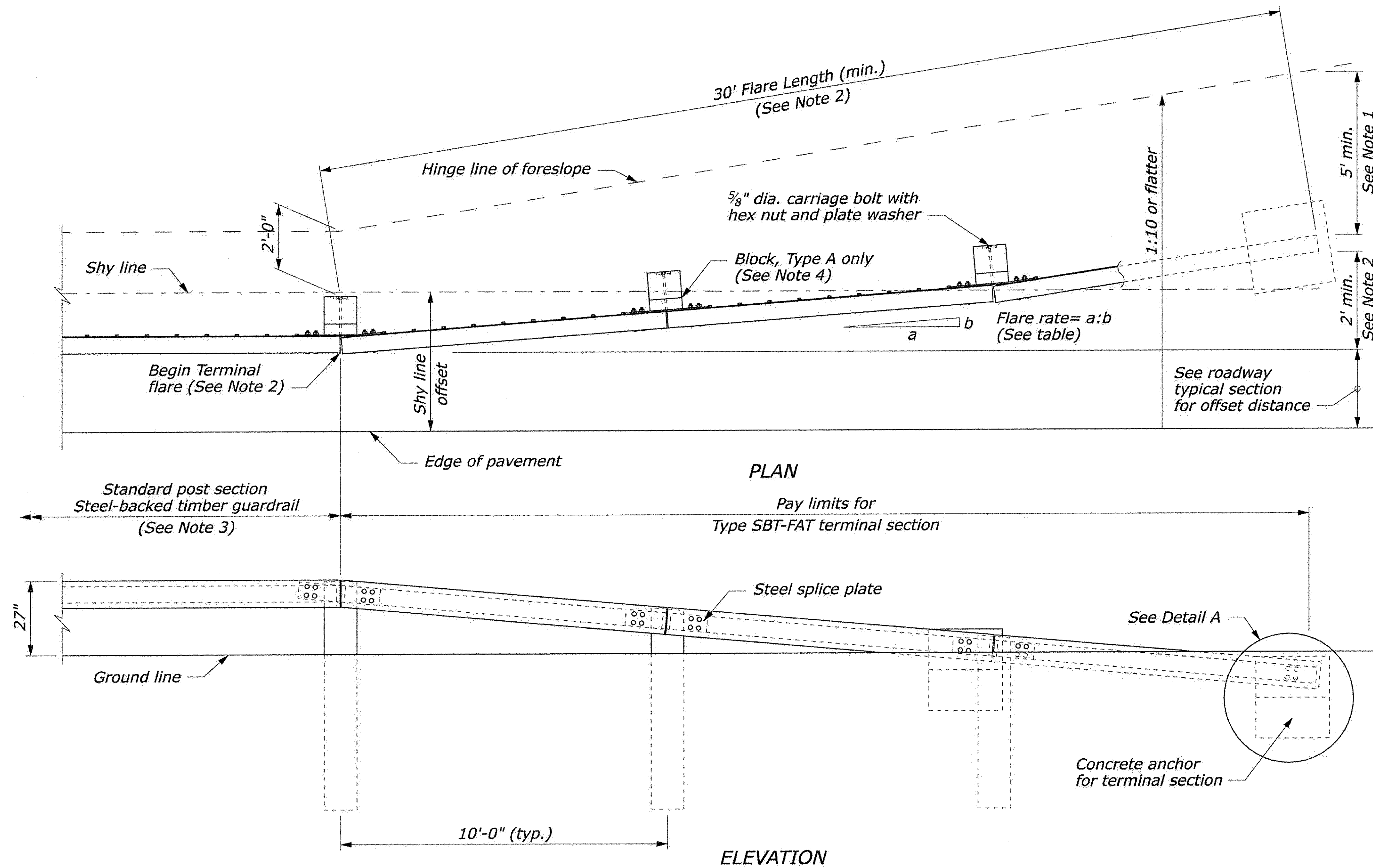


**NOTE:**

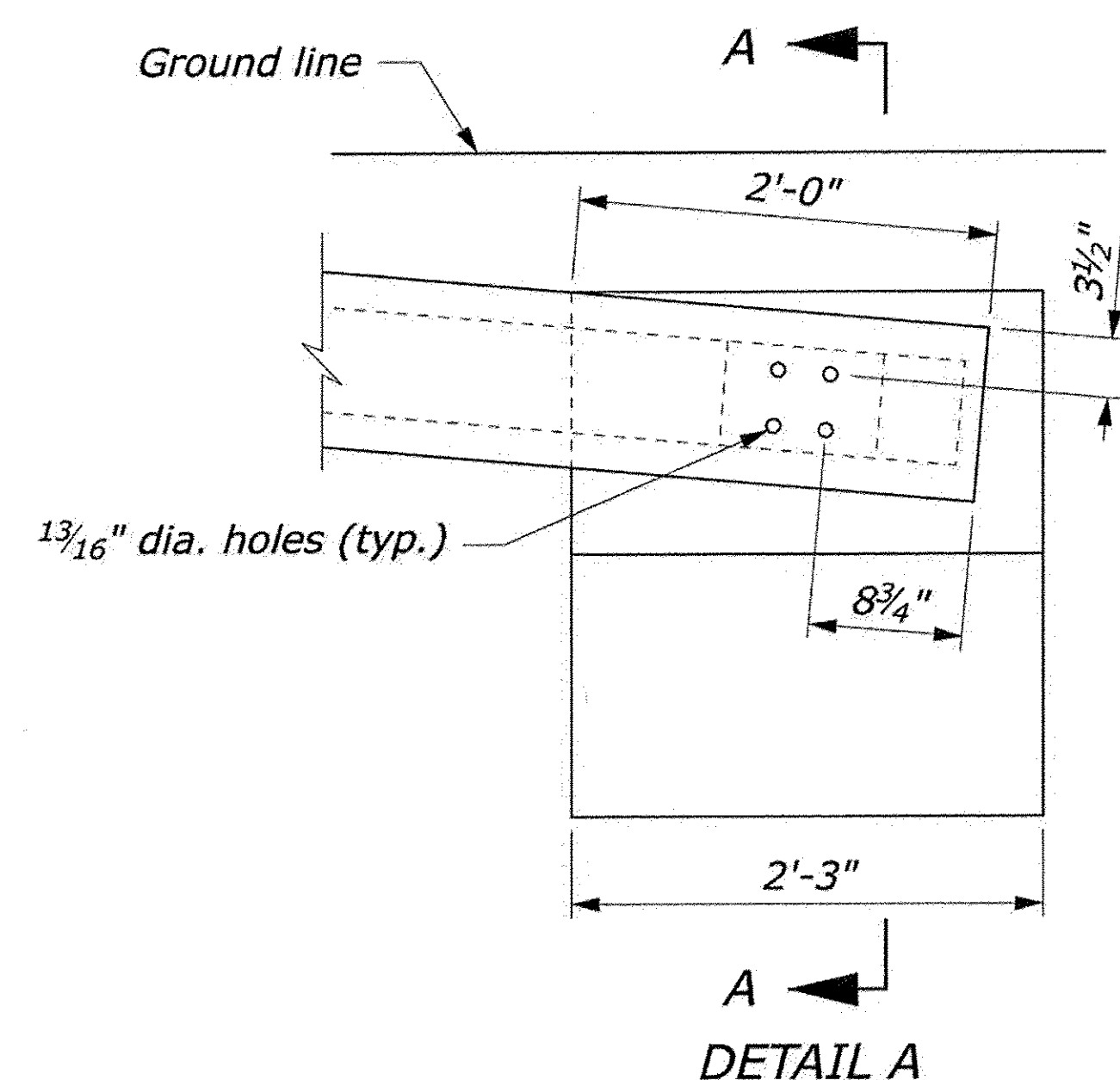
1. Extend the fill widening a minimum of 5 feet behind the guardrail, unless otherwise directed by the CO.
2. The guardrail flare shown in the plan view is the minimum length and rate required. As directed by the CO, flare the guardrail so that the terminal section is outside the clear zone. If the terminal section cannot be located outside the clear zone, it should be flared as far as practical from the road at the maximum rate indicated on the Guardrail Flare Rates table.
3. See Standard 617-60, Steel-Backed Timber Guardrail, Type SBTA and SBTB, for timber, structural steel, and hardware details.
4. On the Type A, blocked-out guardrail, include the blocks in terminal section, except on the concrete anchor. For the Type B, non-blocked-out guardrail, no blocks are included.

Note: CO = Resident Engineer

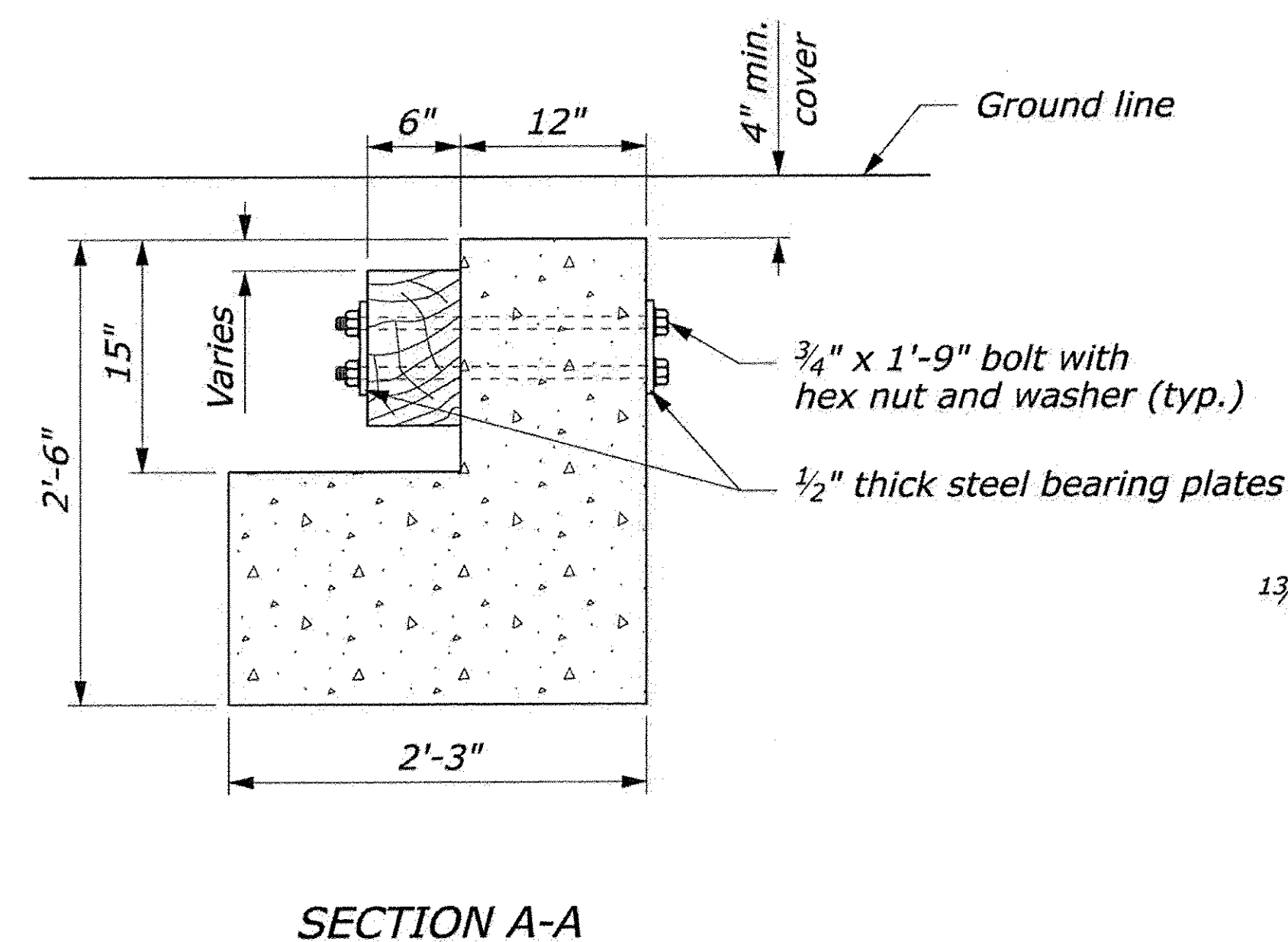


**APPROACH & DEPARTURE FLARE WITH FLARED ANCHOR TERMINAL (FAT)**

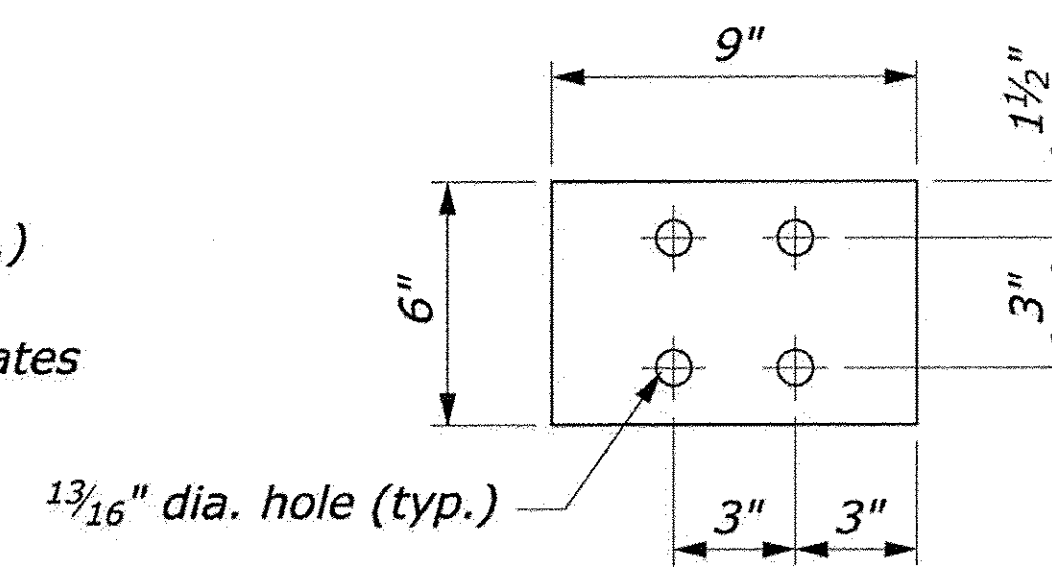
Design Speed (mph)	Shy line offset (ft)	Flare rate inside shy line (a:b)	Flare rate outside shy line (a:b)
60	8.0	26:1	14:1
50	6.5	21:1	11:1
40	5.0	16:1	8:1
30 and less	3.5	13:1	7:1



**CONCRETE ANCHOR**



**SECTION A-A**



**STEEL BEARING PLATE**

NO SCALE  
Sheet 59 of 68

Woodstock BHO 1444(52)  
Woodstock ST 1444(58)

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION  
FEDERAL LANDS HIGHWAY

U.S. CUSTOMARY STANDARD  
**STEEL-BACKED TIMBER GUARDRAIL  
TERMINAL SECTION  
TYPE SBT-FAT**

STANDARD APPROVED FOR USE 1/1990  
REVISED: 4/1994 6/2005  
DRAFT: 9/2007

STANDARD  
617-61

01-Oct-2007 06:33 AM F:\StandDraw\61761.dgn [US Customary]