

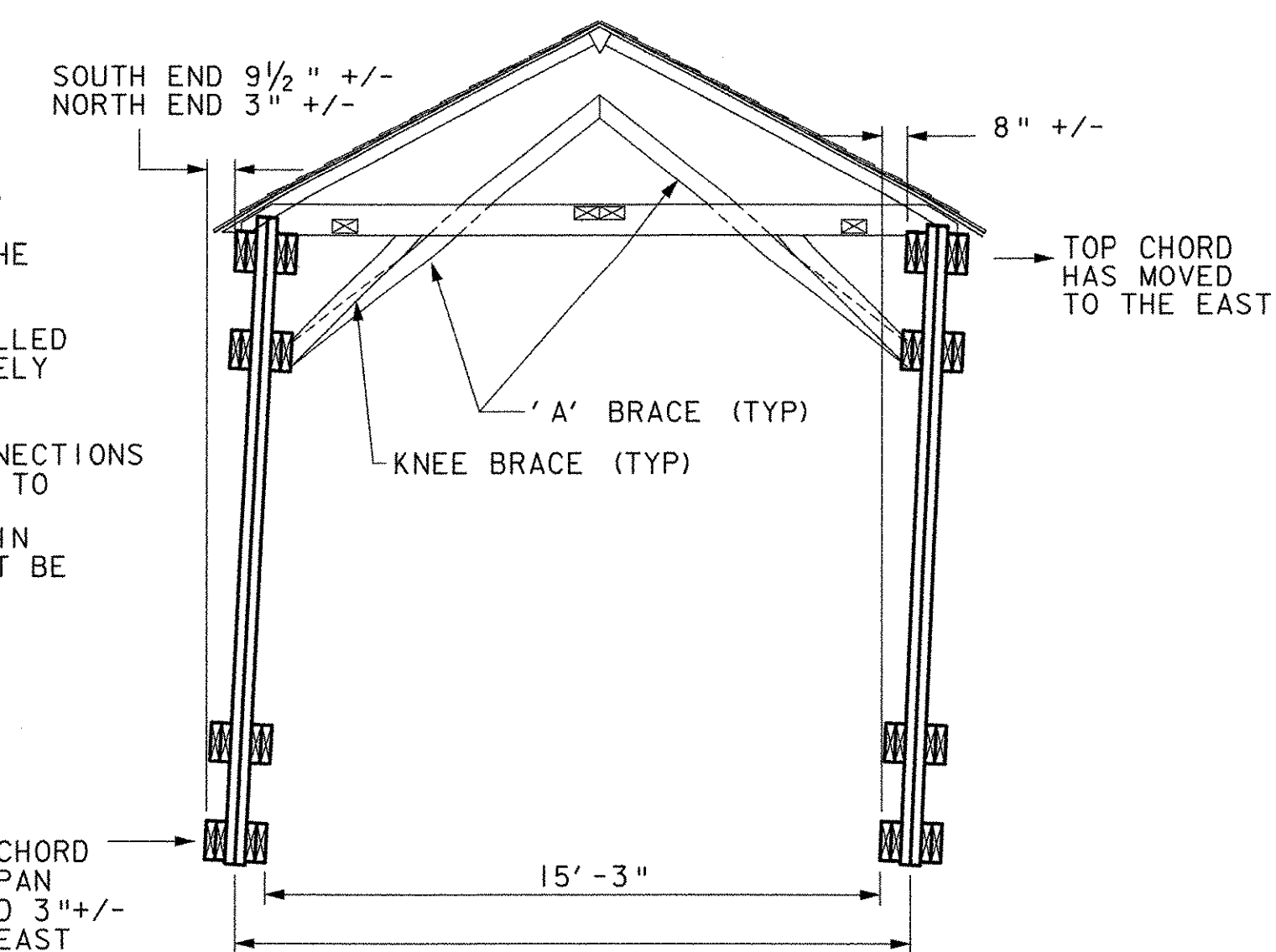
**TYPICAL DECK AT ABUTMENT**

SCALE: 1/2" = 1'

1. PRIOR TO STRAIGHTENING, KNEE BRACES AND 'A' BRACES SHALL BE LOOSENEED OR DISASSEMBLED. MATERIALS TO BE RETAINED FOR REUSE.
2. FOLLOWING THE STRAIGHTENING OPERATIONS, THE KNEE BRACES AND 'A' BRACES SHALL BE REINSTALLED AND THE CONNECTIONS REMADE IN THE MANNER OF THE EXISTING CONNECTION. HARDWOOD SHIMS OR WEDGES SHALL BE USED WHERE NECESSARY TO STRENGTHEN CONNECTIONS.
3. WHERE EXISTING CONNECTIONS USE BOLTS, A NEW HOLE MAY BE DRILLED FOR THE NEW BOLT. THE NEW BOLT SHALL MATCH THE EXISTING AS CLOSELY AS POSSIBLE. ALL ABANDONED HOLES SHALL BE COMPLETELY FILLED WITH A DOWEL.
4. ACTUAL DETAILS TO BE UTILIZED TO STRENGTHEN THE BRACING CONNECTIONS SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL TWO WEEKS PRIOR TO THE COMMENCEMENT OF THE STRAIGHTENING OPERATIONS.
5. THE FINAL POSITION OF THE TRUSSES AT EACH END SHALL BE WITHIN 1" OF VERTICAL AND THE MIDSPAN BOW OF THE BOTTOM CHORD SHALL NOT BE MORE THAN 1/2".

**REBAR SCHEDULE**

PIECES	BAR	LENGTH	LOCATION
16	NO. 8	1'-6"	NORTH AND SOUTH ABUT. DECK SEAT
2	NO. 5	15'-0"	NORTH AND SOUTH ABUT. DECK SEAT
1	WELDED MESH 4 GA.	10 SF	SOUTH ABUT. SPALL REPAIR



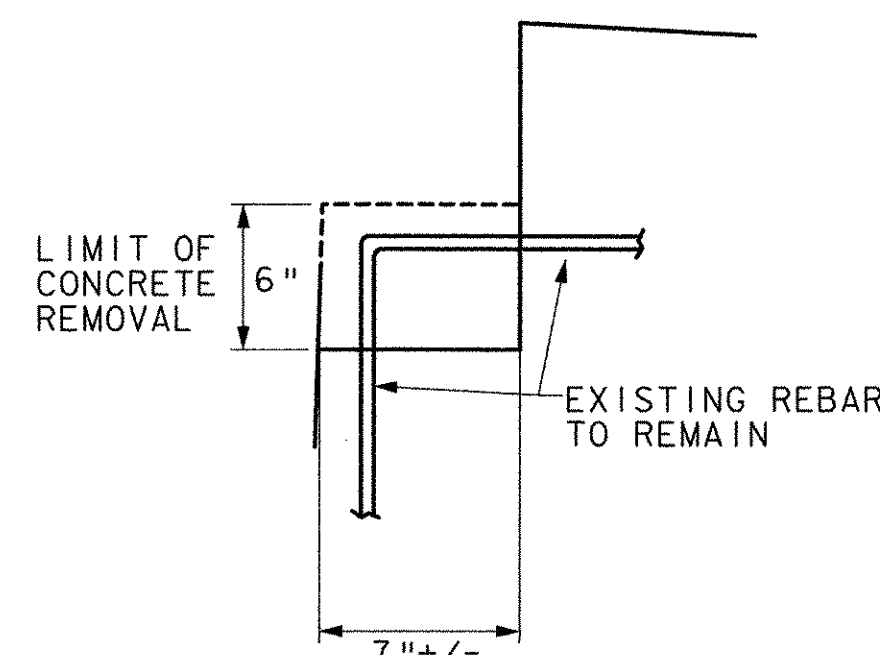
**SOUTH END ELEVATION**

**TRUSS MISALIGNMENT DETAILS**

SCALE: 1/4" = 1'

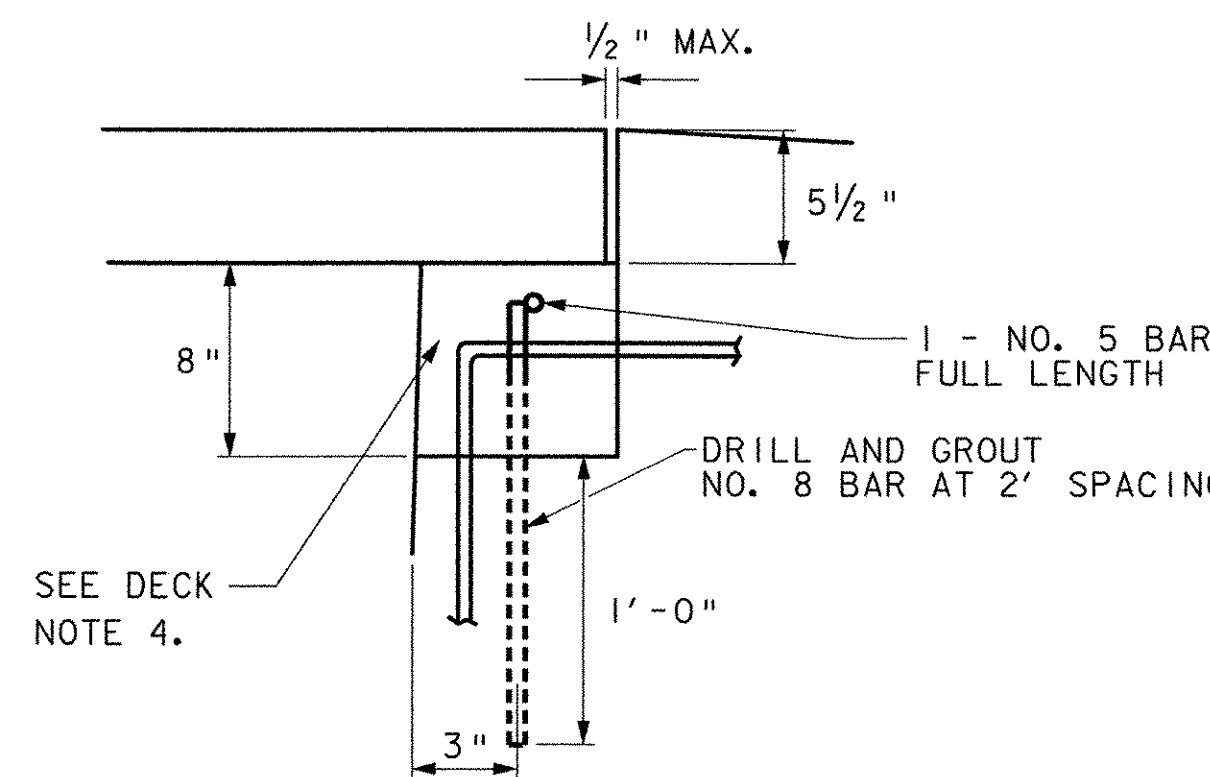
**DECK NOTES:**

1. NAILS SHALL PENETRATE AT LEAST 2-1/2 LAMINATIONS AND SHALL BE COMMON WIRE OR DEFORMED SHANK GALVANIZED NAILS.
2. IF SPLITTING OF LAMINATIONS OCCUR DURING PLACEMENT, LEAD HOLES SHALL BE PREBORED. PREBORE DIAMETERS SHALL NOT EXCEED 3/4 OF THE NAIL SHANK DIAMETER.
3. THE LENGTH OF THE DECK VARIES APPROXIMATELY 1' FROM THE EAST SIDE TO THE WEST SIDE. THE LAMINATIONS MAY BE FIELD CUT TO ADJUST THE LENGTH. ALL LAMINATIONS CUT IN THE FIELD SHALL BE TREATED WITH PRESERVATIVE AND FIRE RETARDANT IN THE AREA OF THE CUT PRIOR TO INSTALLATION.
4. REMOVE EXISTING CONCRETE AT SEAT FOR THE END OF THE DECK. DRILL AND GROUT NO. 8 BAR 1' DEEP AT 2' SPACING. PLACE NO. 5 BAR FULL LENGTH OF DECK SEAT. PLACE CONCRETE TO PROVIDE SEAT FOR NEW DECK. ALL WORK AND MATERIAL TO COMPLETE THIS DETAIL SHALL BE PAID FOR UNDER ITEM 580.14 "REPAIR OF CONCRETE SUBSTRUCTURE, CLASS 11." THE QUANTITY OF THE WORK SHALL BE BASED ON THE VERTICAL DIMENSION ON THE FACE OF THE ABUTMENT.
5. THE MATERIAL FOR THE DECK SHALL BE VISUALLY GRADED SOUTHERN PINE NO. 1 OR BETTER.
6. THE MINIMUM LENGTH OF A SINGLE LAMINATION SHALL BE 4' AND THE MINIMUM LAP LENGTH SHALL BE 2'.
7. DECK LAMINATIONS TO BE TOE-NAILED TO THE FLOORBEAMS. APPROXIMATE SPACING OF TOE-NAILS TO BE 1' ALONG THE FLOORBEAM.



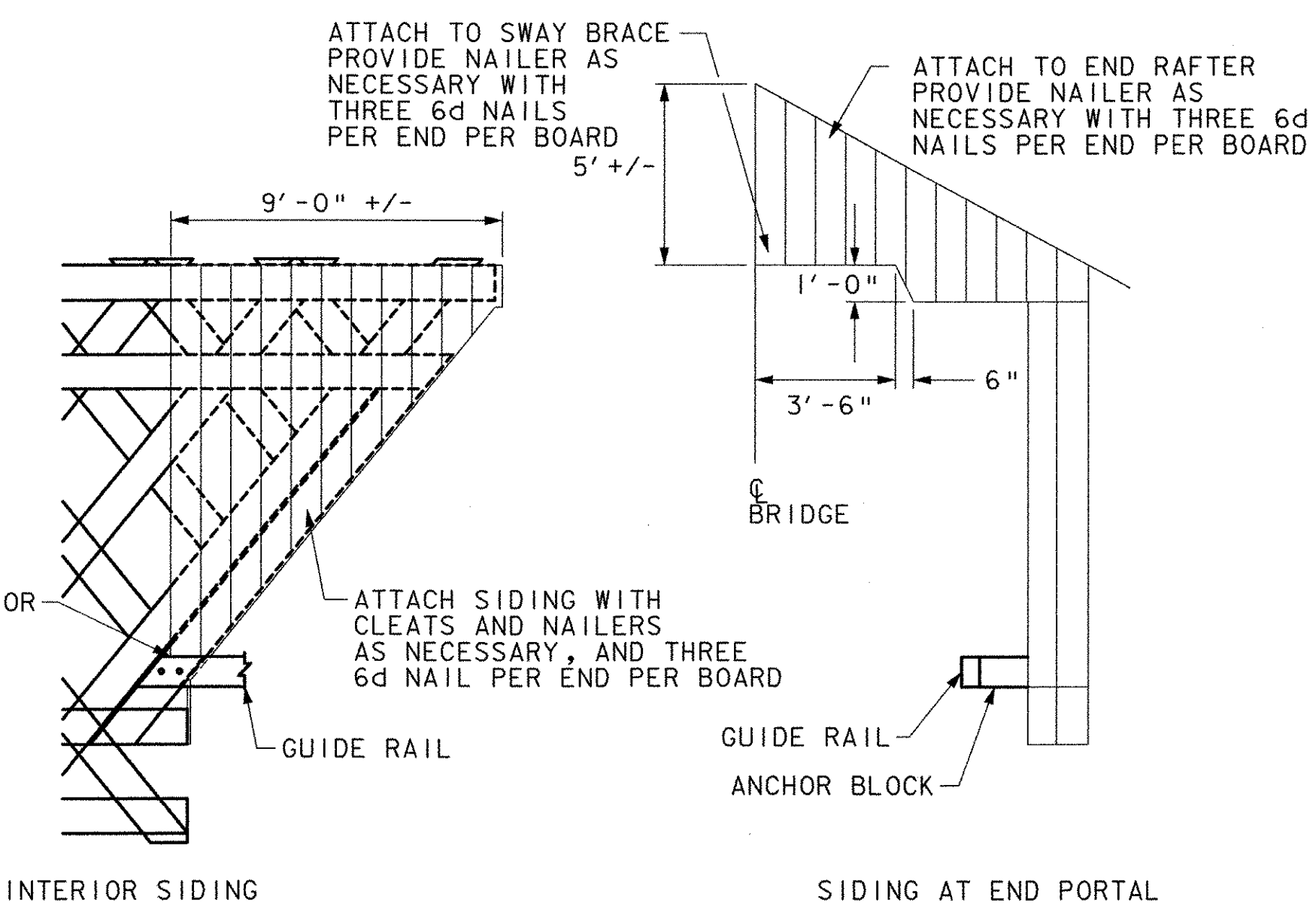
**TYPICAL CONCRETE REMOVAL AT SEAT FOR END OF DECK**

SCALE: 1 1/2" = 1'



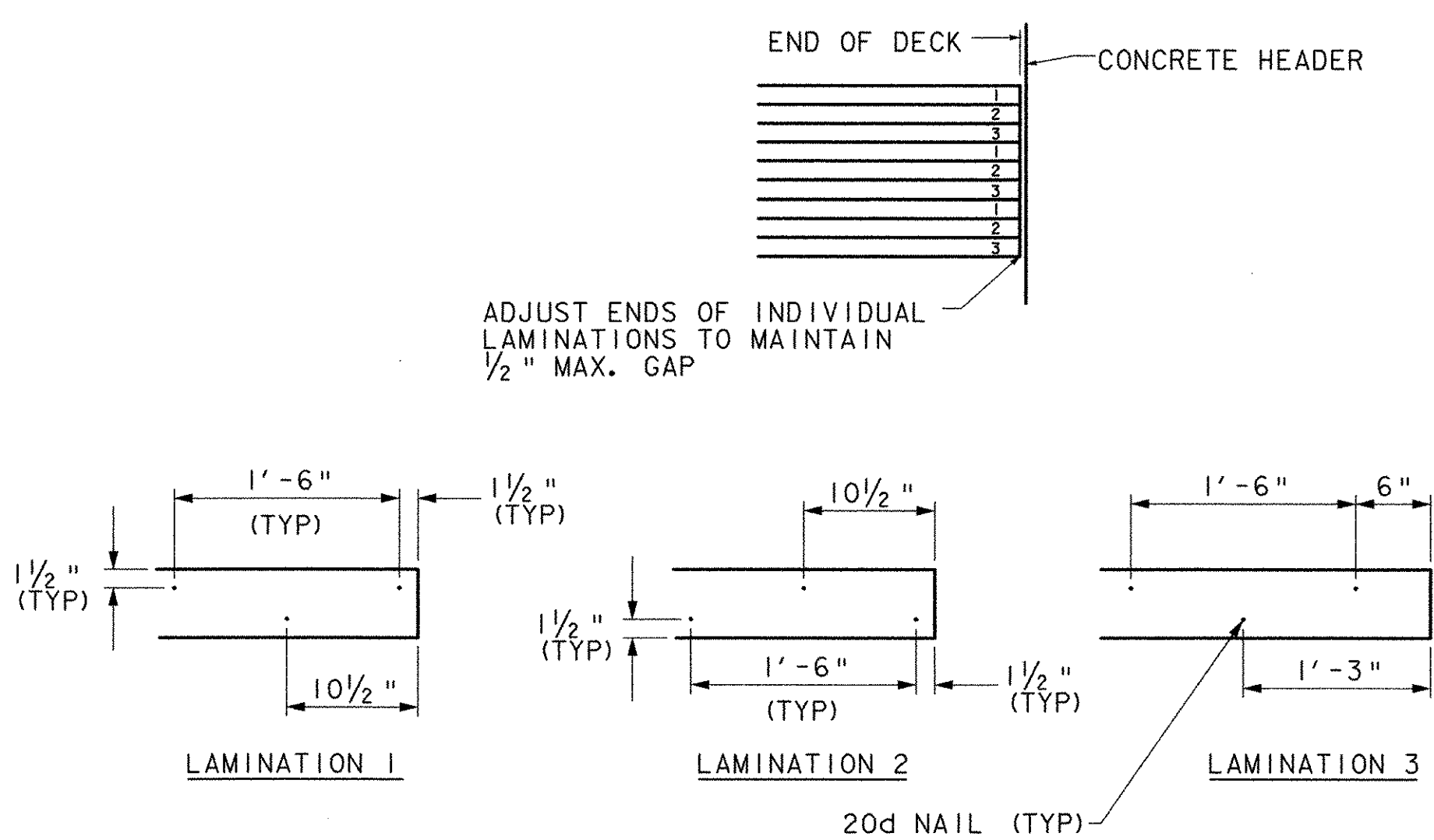
**REBUILD SEAT FOR END OF DECK**

SCALE: 1 1/2" = 1'



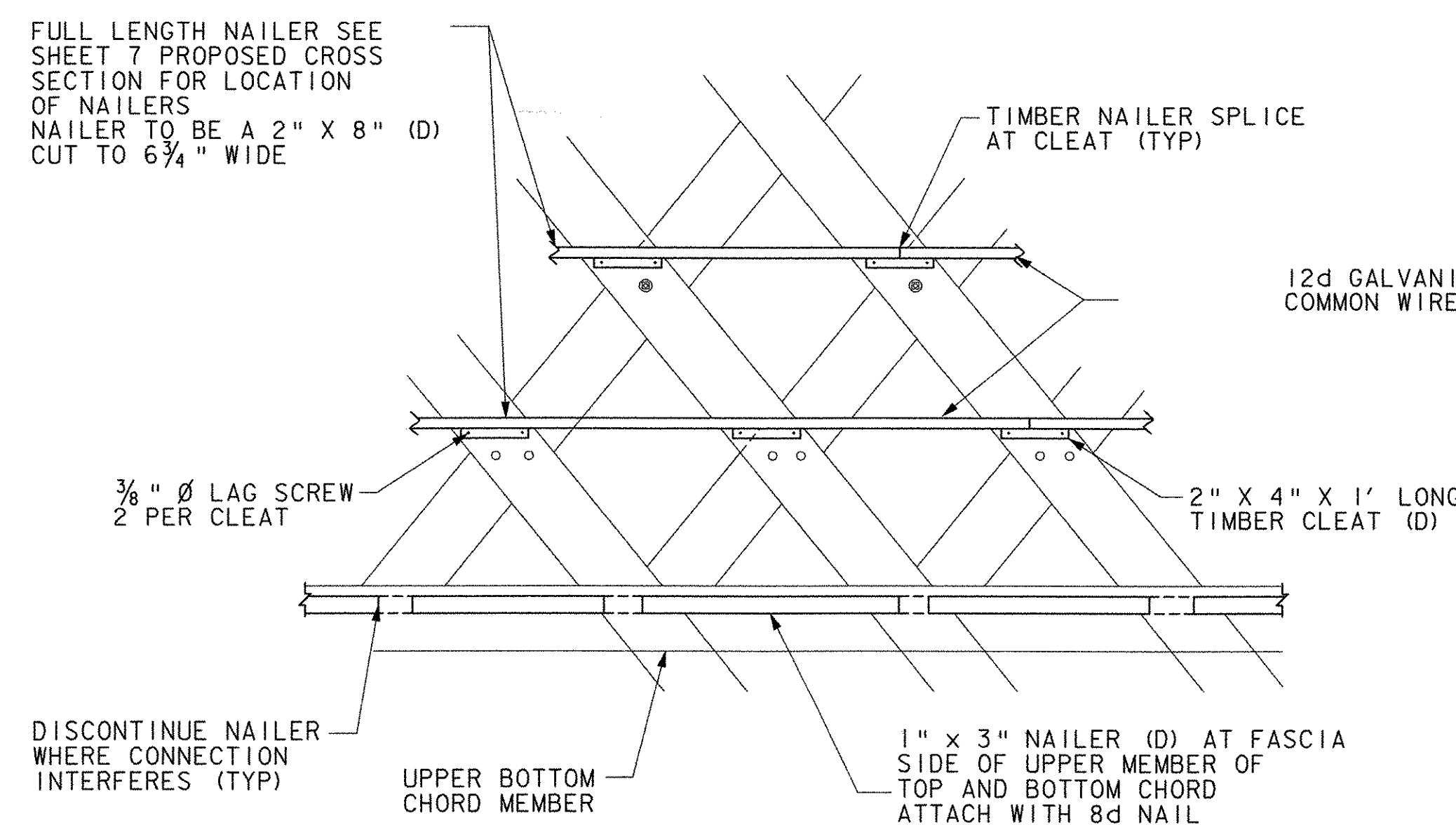
**INTERIOR AND END PORTAL SIDING DETAILS**

SCALE: 1/4" = 1'



**LAMINATION PLACEMENT**

SCALE: 1" = 1'



**TYPICAL CLEAT AND NAILER DETAILS**

SCALE: 1/2" = 1'

**CLEAT AND NAILER INSTALLATION**

SCALE: 1 1/2" = 1'



**STATE OF VERMONT AGENCY OF TRANSPORTATION**

Town Of	PITTSFORD, VT	Bridge No.	31
Highway No.	ELM STREET (TH13)	Log Sta.	
		Surv. Sta.	
COOLEY COVERED BRIDGE REHABILITATION			
MISCELLANEOUS DETAILS I			
Designed By	J. MESSIER	Drawn By	J. MESSIER
Checked By	Date	Bridge Design Supervisor	Date
	E. ALLEN RANDALL 01/03/03		
PROJECT	PITTSFORD	PROJECT NO.	BHO 1443 (36)
I.G.C. Info.			
Bridge Sheet No.		Sheet	9 of 15

03 JAN 2003 01:20:14 Cooley Bridge (cd) dgm\com\edg