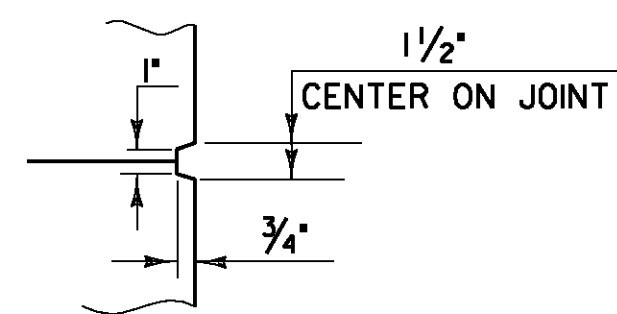
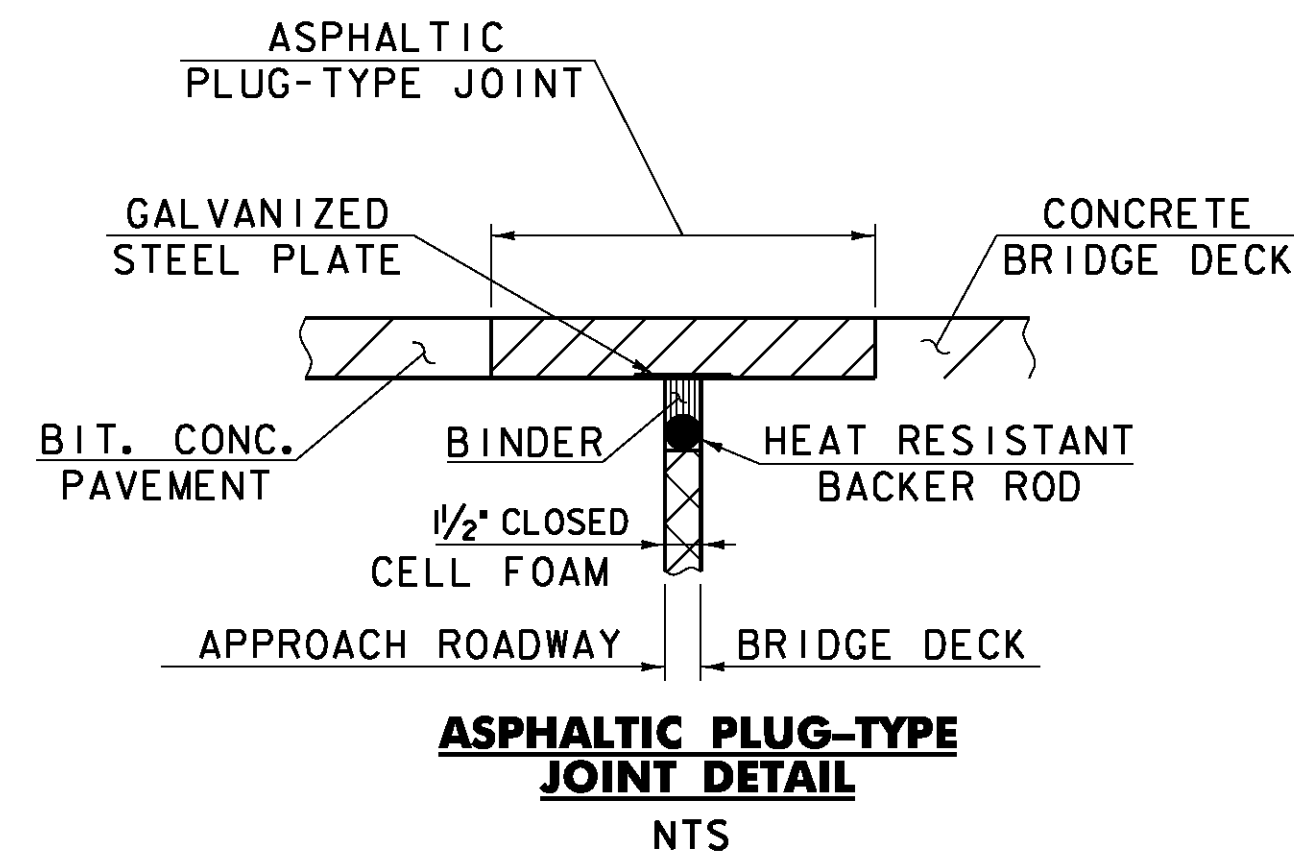


**NOTE:**

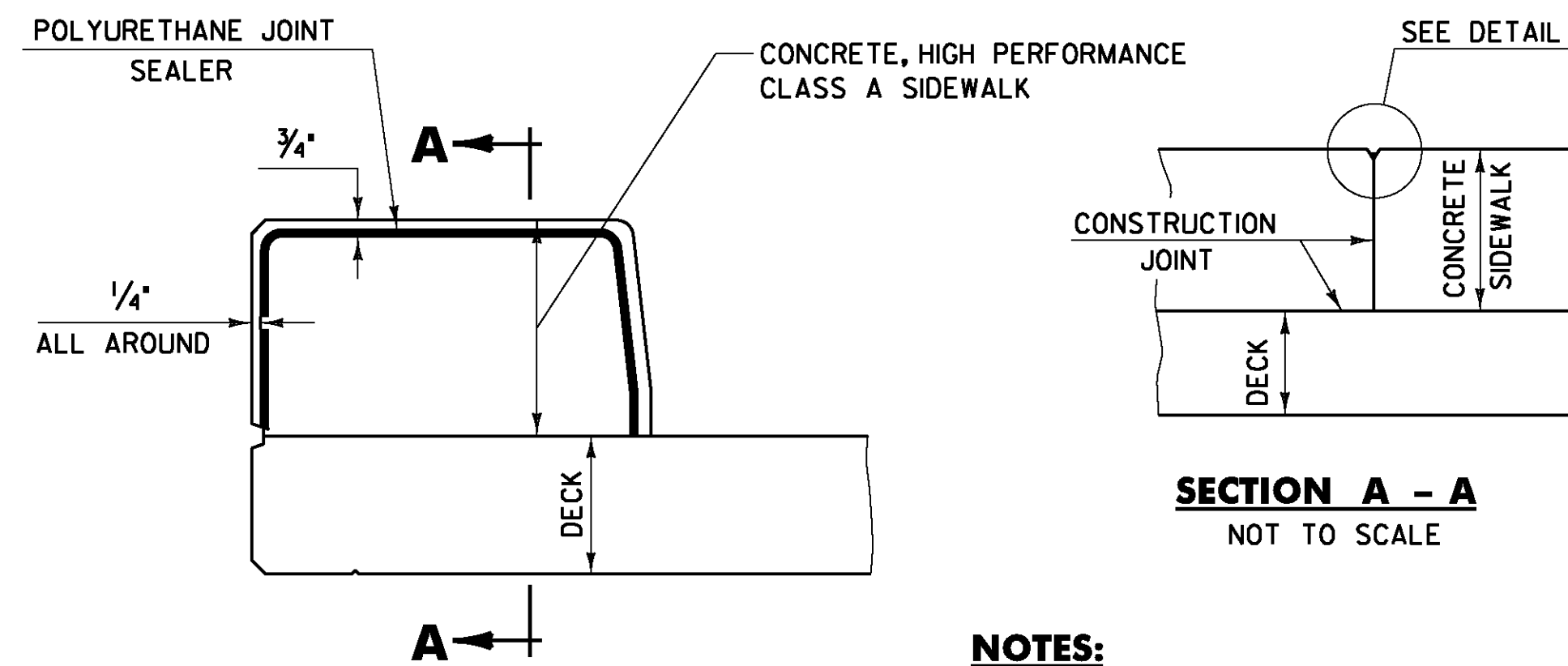
PAYMENT FOR WATERSTOP WILL BE MADE INCIDENTAL TO THE ADJACENT CONCRETE PAY ITEM.



**SCORE MARK DETAIL**  
NOT TO SCALE



**ASPHALTIC PLUG-TYPE JOINT DETAIL**  
NTS



**TYPICAL SECTION THROUGH CONCRETE SIDEWALK CONSTRUCTION JOINT**  
NOT TO SCALE

**NOTES:**

1. CONSTRUCTION JOINTS THROUGH CONCRETE SIDEWALKS SHALL BE SPACED MAXIMUM 15'-0" CENTER TO CENTER AND SHALL BE 1'-6" MINIMUM FROM THE CENTER OF THE NEAREST BRIDGE RAIL POST. CONCRETE SHALL BE PLACED IN ALTERNATING SECTIONS WITH A MINIMUM OF 48 HOURS DELAY BETWEEN ADJACENT POURS.
2. LONGITUDINAL REINFORCING SHALL PASS THROUGH CONCRETE SIDEWALK CONSTRUCTION JOINTS.
3. THE CONTRACTOR MAY ELIMINATE THE CURB JOINTS AND MAKE EACH CURB IN ONE PLACEMENT USING AN APPROVED SHRINKAGE REDUCING ADMIXTURE AS NOTED IN THE SPECIAL PROVISIONS. THE COST OF ANY SUCH ADMIXTURE SHALL BE INCIDENTAL TO THE CONCRETE PAY ITEM.

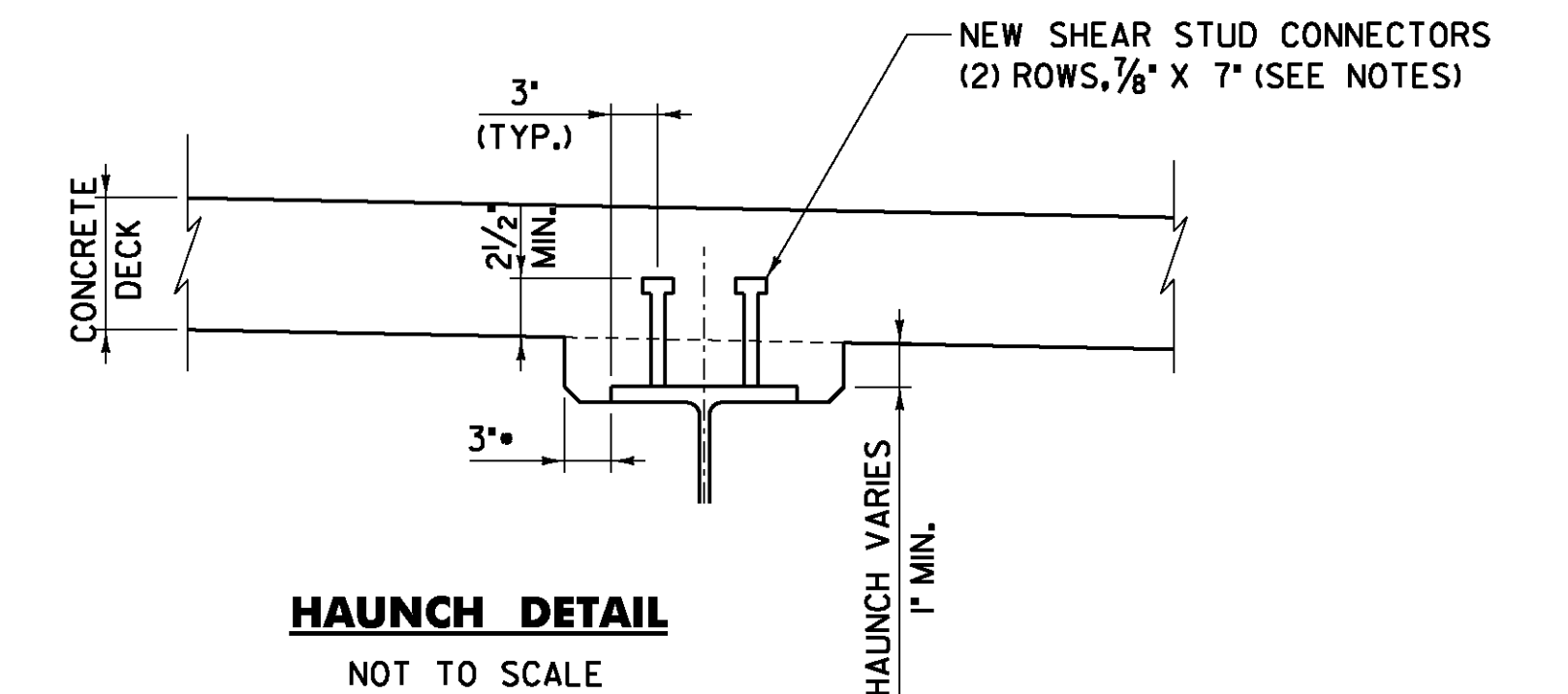
**ASPHALTIC PLUG JOINT NOTES**

**1. INSTALLATION**

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
- B. EXCAVATE THE JOINT AS SHOWN ON THE PLANS WITH SAWS AND PNEUMATIC HAMMER OR A HAMMER AND CHISEL.
- C. BLAST CLEAN THE JOINT AREA OF DEBRIS AND ASPHALT. THOROUGHLY DRY THE JOINT AREA WITH HOT COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
- D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
- E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" +/- OF BINDER ABOVE THE ROD.
- F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
- G. PLACE 1/4" THICK BY 8" WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE APPROACH SLAB IS COVERED WITH A STONE BASE OR BITUMINOUS PAVEMENT AND VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
- H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
- I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
- J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
- K. PROTECTED JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 125 DEG F +/-.

**2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL):**

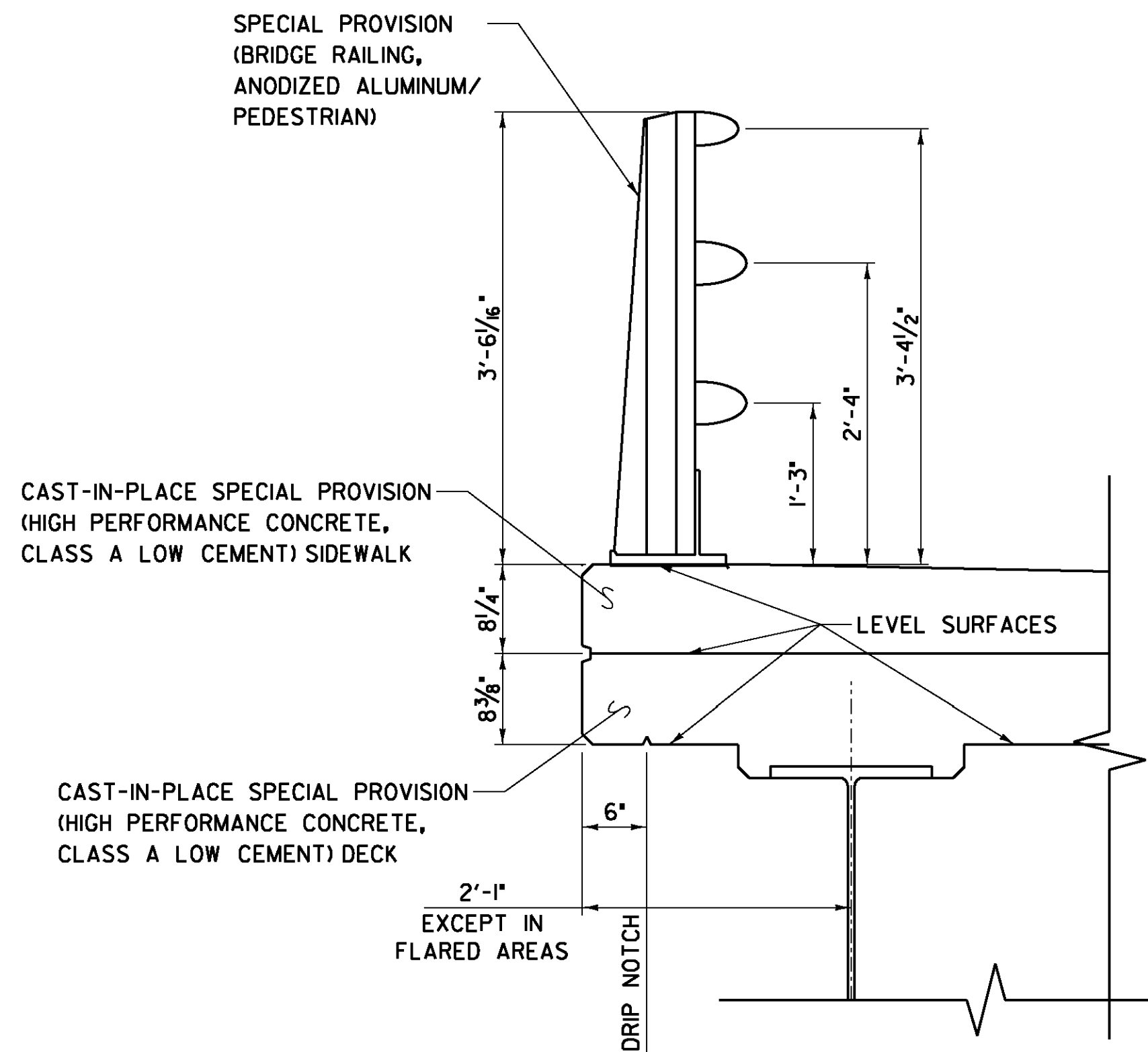
- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 50 DEG F AND RISING.
- B. THE ROAD SURFACE IS SUFFICIENTLY DRY.
- C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.



**HAUNCH DETAIL**  
NOT TO SCALE

**NOTES:**

- CONTRACTOR SHALL SUPPLY LONGER SHEAR STUDS WHERE NEEDED TO COMPLY WITH MINIMUM EMBEDMENT OF 2 1/2" INCHES ABOVE THE BOTTOM OF THE DECK.
- THE 3" HORIZONTAL SECTION MAY BE ELIMINATED FOR FORMING SYSTEMS DESIGNED FOR THE CONSTRUCTION OF VERTICAL HAUNCHES. ANY VOIDS RESULTING FROM FORMING SYSTEM ELEMENTS SHALL BE FILLED WITH JOINT SEALER, POLYURETHANE MEETING THE REQUIREMENTS OF SECTION 524. THE COST OF THE JOINT SEALER, POLYURETHANE SHALL BE INCIDENTAL TO THE ADJACENT CONCRETE.

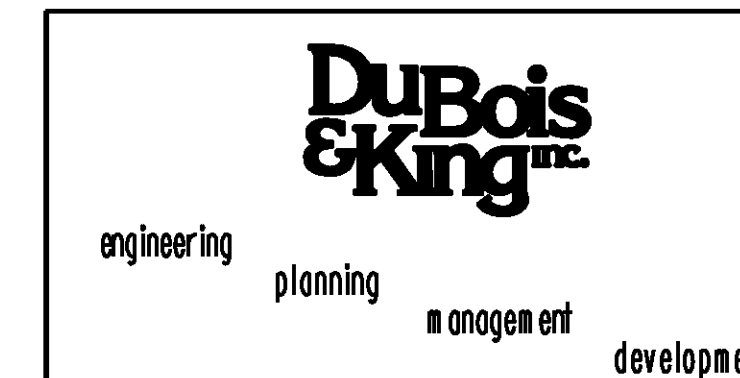


**TYPICAL RAIL SECTION**

SCALE: 1" = 1' - 0"  
SEE BRIDGE AND APPROACH RAILING DETAILS SHEETS FOR ADDITIONAL INFORMATION.

**STATE OF VERMONT AGENCY OF TRANSPORTATION**

Town Of	BARRE CITY	Bridge No.	7
Highway No.	PROSPECT ST.	Log Sta.	
		Surv. Sta.	
<b>PROSPECT ST. OVER STEVENS BRANCH</b>			
<b>DECK DETAILS</b>			
Designed By	A.P. GUYETTE	Drawn By	A.P. GUYETTE
Checked By	Date	Bridge Design Supervisor	
E. P. DETRICK	1/09	J. W. TUCKER	Date 1/09
PROJECT	BARRE CITY	PROJECT NO.	BHF 6000 (15) S
I.G.C. info.			
Bridge Sheet No.		Sheet	24 of 56



PLOTTED 2/2/2009