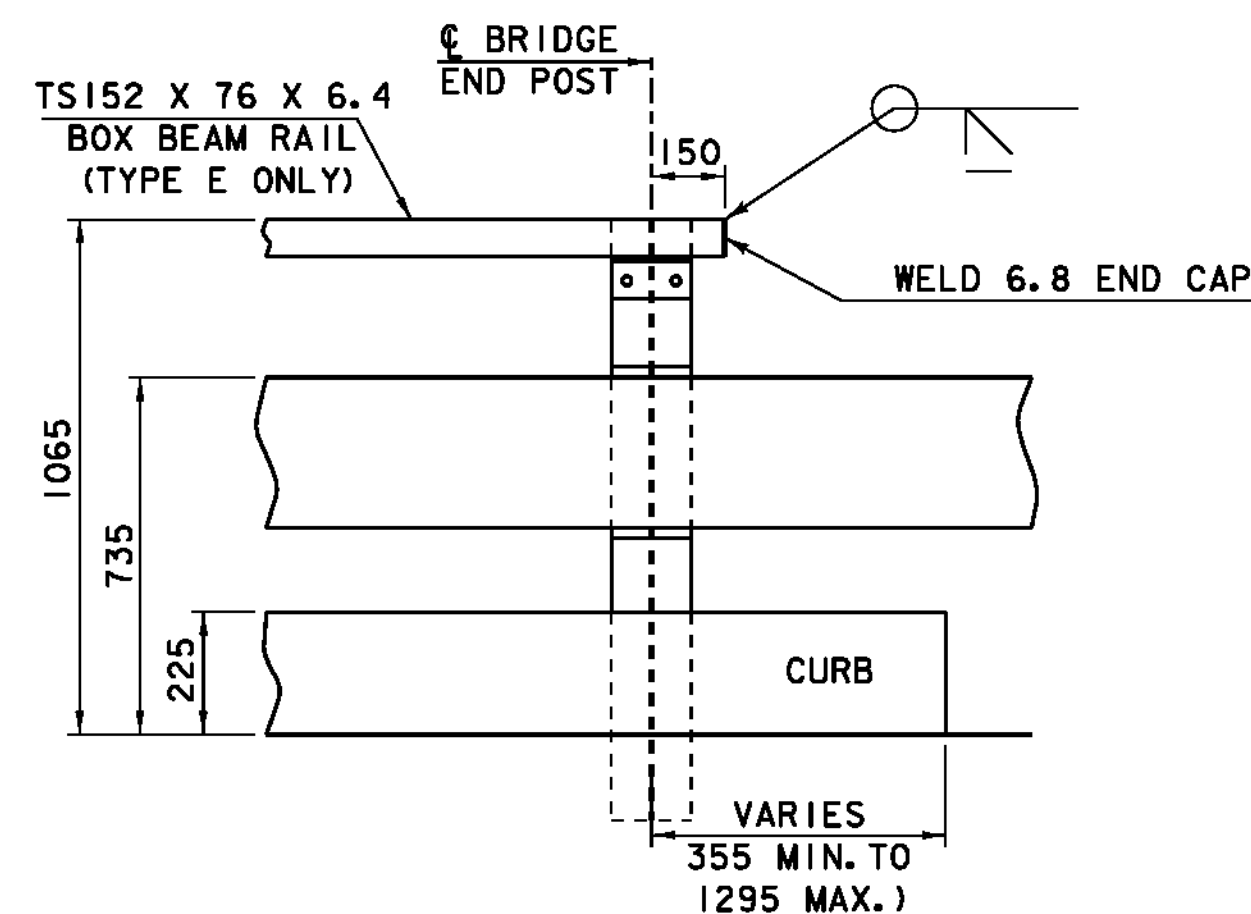
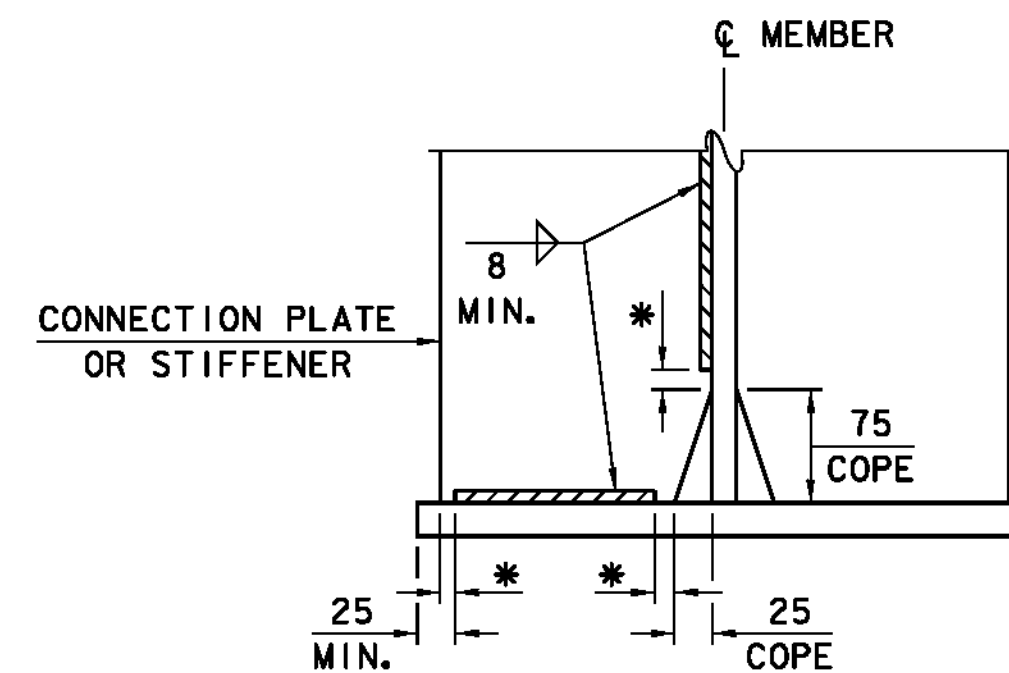


**TRANSVERSE BRIDGE SLAB
CONSTRUCTION JOINT DETAILS**



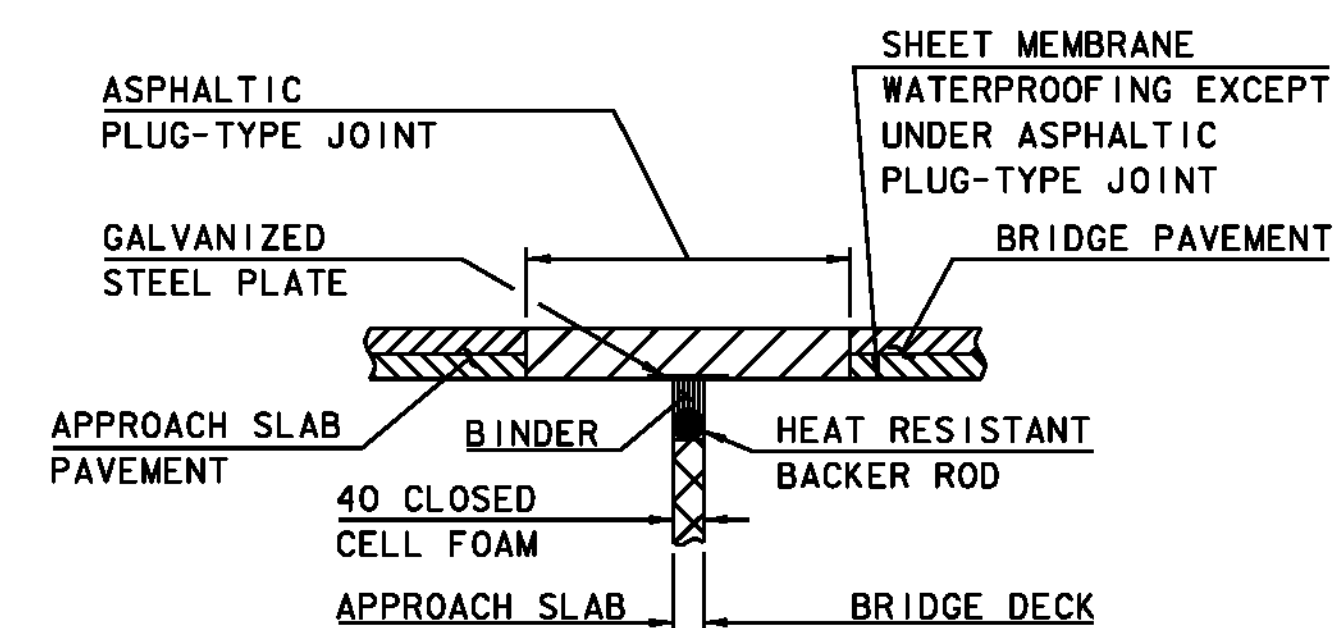
WINGWALL #2 APPROACH RAIL DETAILS

NOTE: GRIND TO 3 RADIUS PRIOR TO GALVANIZING



**WELD TERMINATION
AND COPING DETAIL FOR
STEEL MEMBERS**

* NO WELD FOR 10 MIN., 22 MAX. (EXCEPT MUST MAINTAIN 25 MIN. FROM EDGE OF FLANGE)

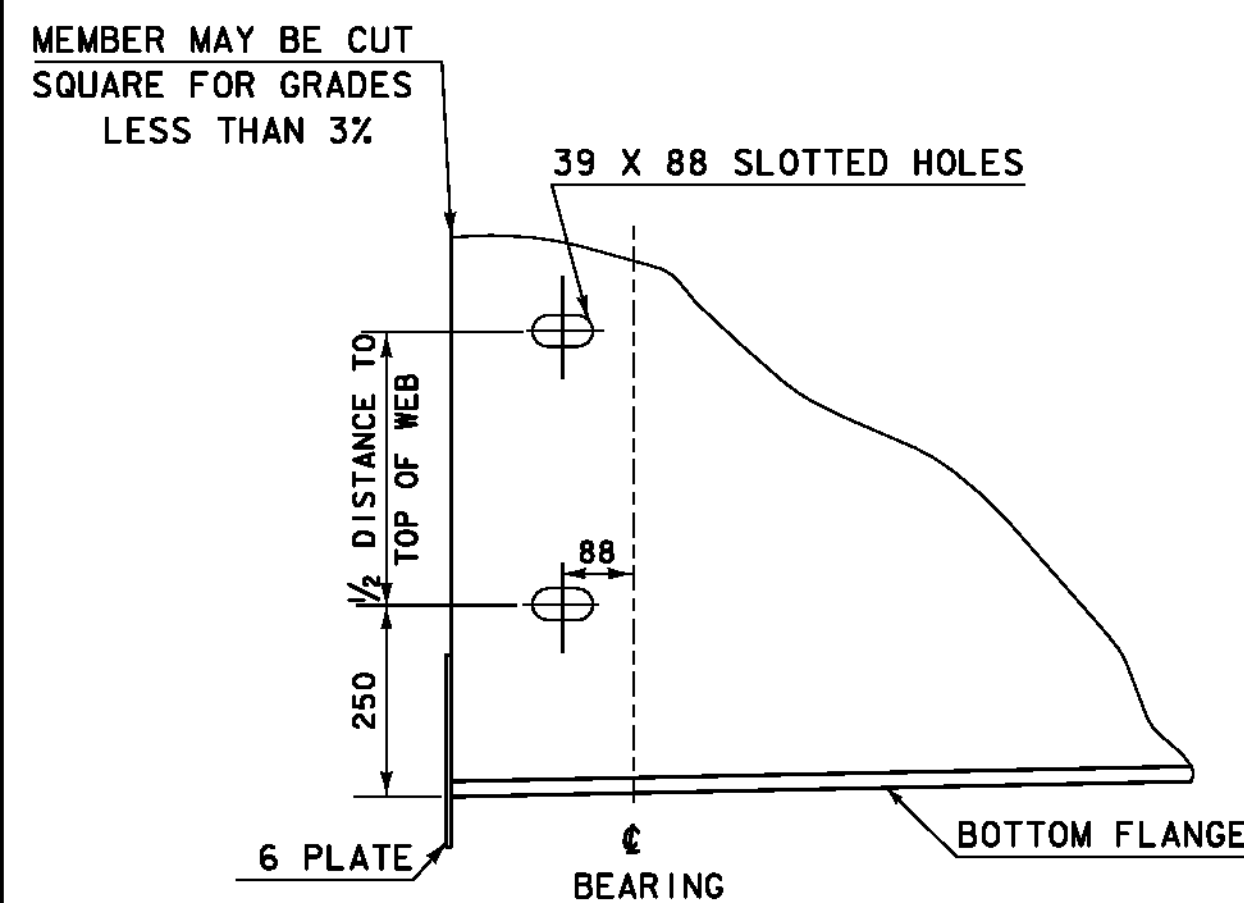


**ASPHALTIC PLUG-TYPE
JOINT DETAIL
(NOT TO SCALE)**

ASPHALTIC PLUG JOINT NOTES

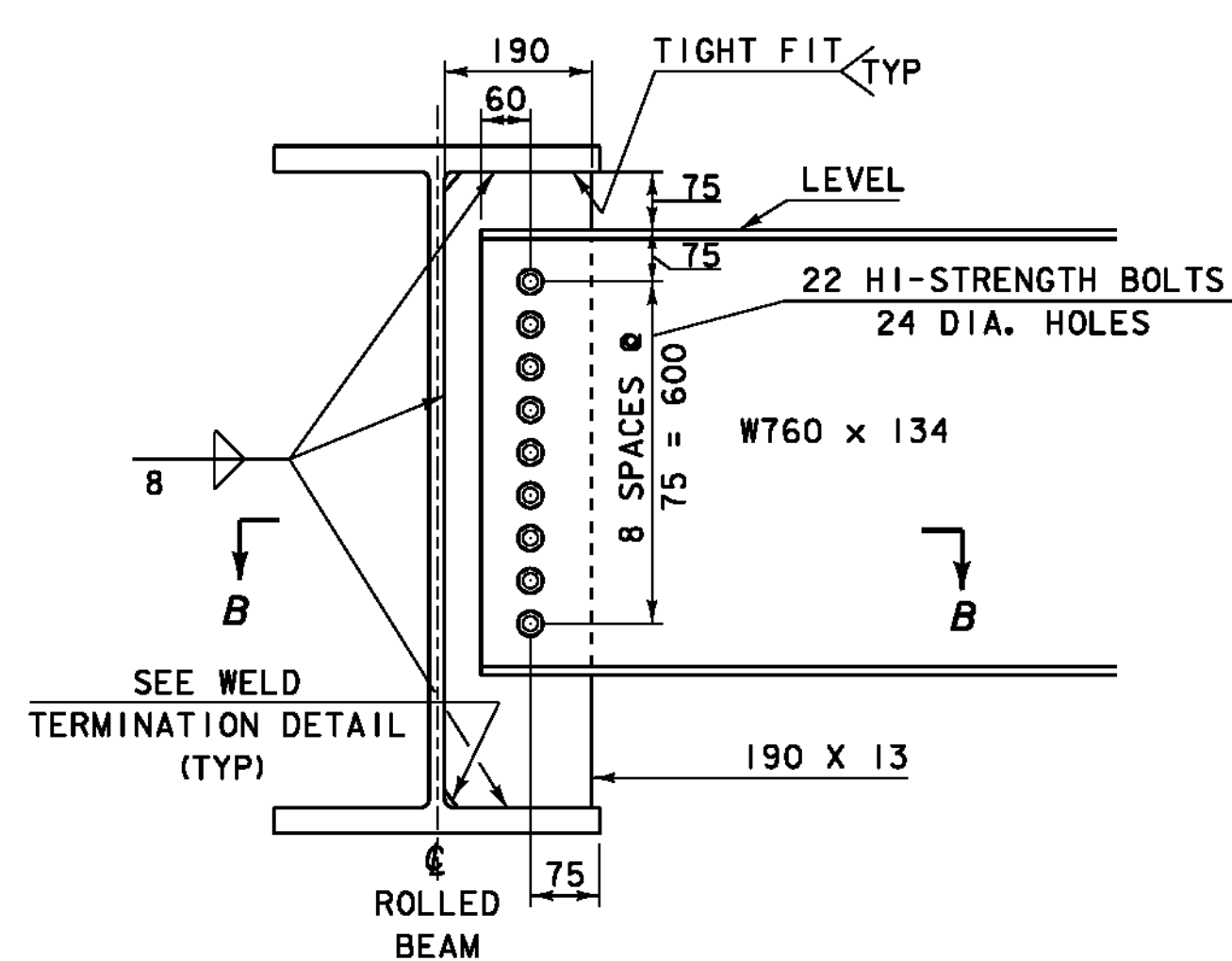
I. INSTALLATION

- A. LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
 - B. REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNEUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
 - C. BLAST CLEAN THE JOINT AREA OF DEBRIS, ASPHALT AND SHEET MEMBRANE. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
 - D. REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
 - E. PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 25 +/- OF BINDER ABOVE THE ROD.
 - F. HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
 - G. PLACE 8 THICK BY 200 WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRE-STAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
 - H. HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
 - I. INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
 - J. IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
 - K. ONCE THE JOINT REACHES 82 DEG C (180 DEG F) +/-, WATER MAYBE USED TO EXPEDITE THE COOLING PROCESS.
 - L. PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51DEG C (125 DEG F) +/-.
- 2. WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):**
- A. THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
 - B. THE ROAD SURFACE IS DRY.
 - C. WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.



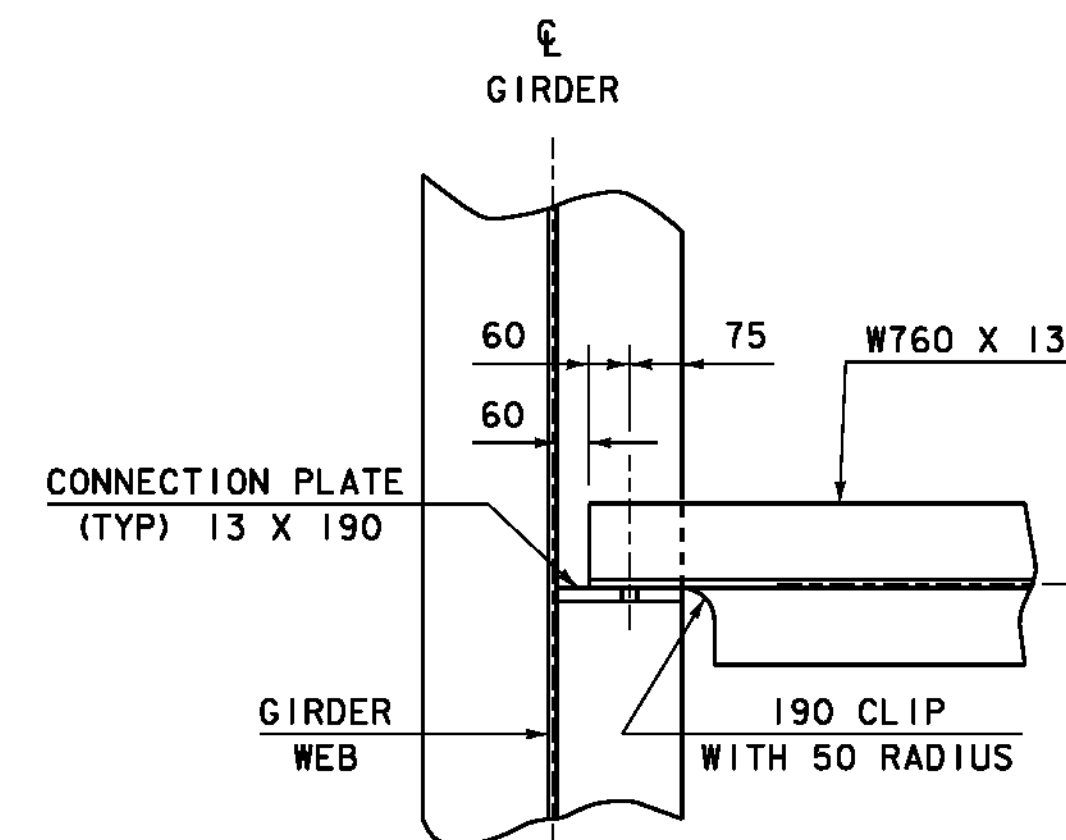
**ELEVATION VIEW END OF
STEEL MEMBER AT ABUTMENT**

SLOTTED HOLES ARE FOR HORIZONTAL REINFORCING BARS IN FRONT FACE OF CURTAIN WALL.



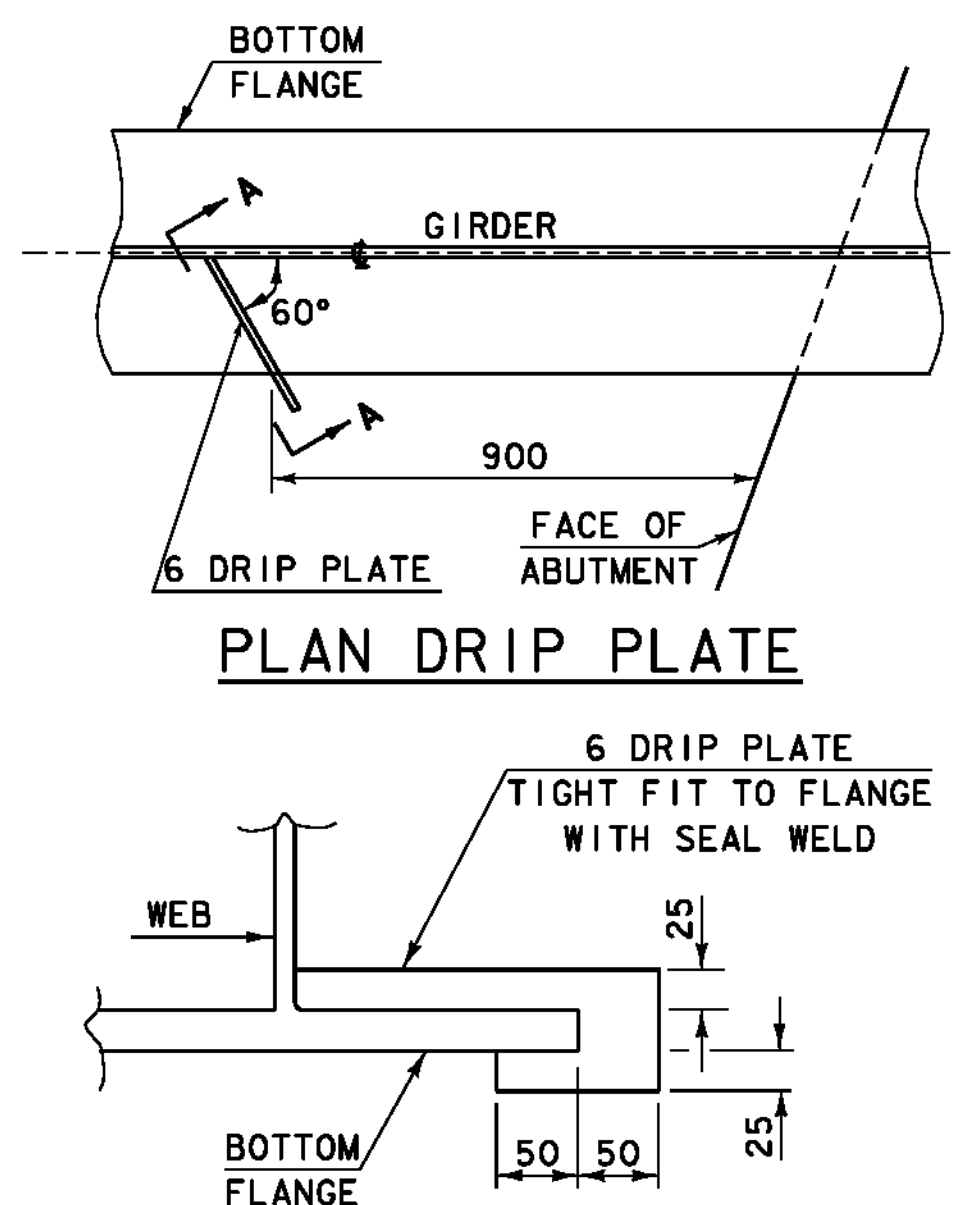
NOTE: HI-STRENGTH BOLTS, NUTS AND WASHERS SHALL CONFORM TO AASHTO M164M.

INTERMEDIATE DIAPHRAGMS



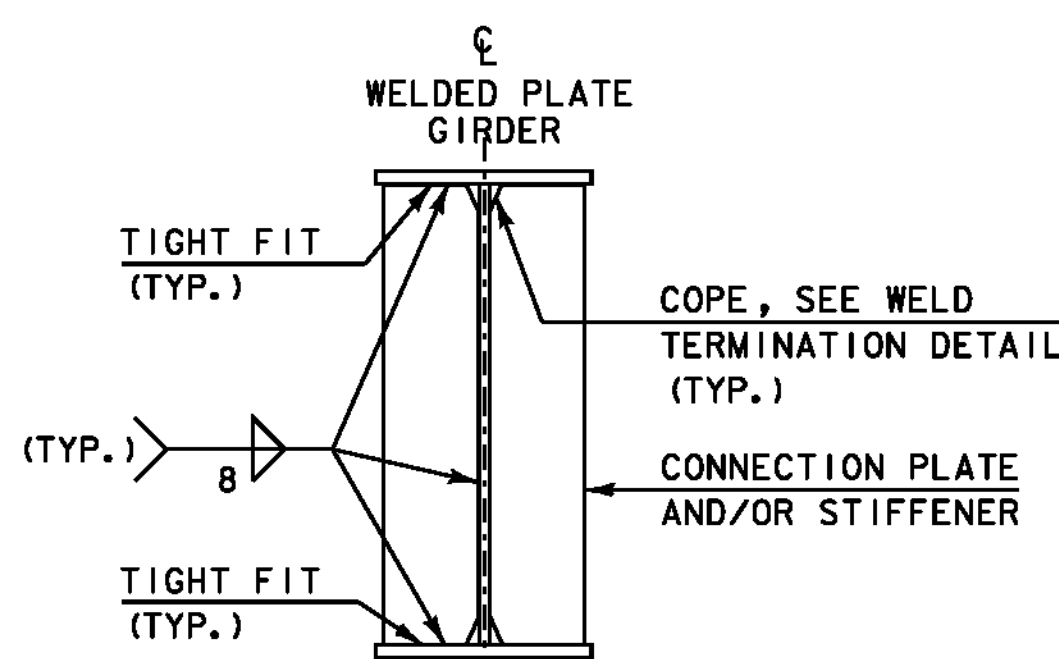
ALL CONNECTIONS SHALL BE MADE USING 22 DIAMETER, AASHTO M164M BOLTS. ALL HOLES IN THE DIAPHRAGMS SHALL BE 24 DIAMETER, ALL HOLES IN THE CONNECTION PLATES SHALL BE 24 DIAMETER.

SECTION B - B



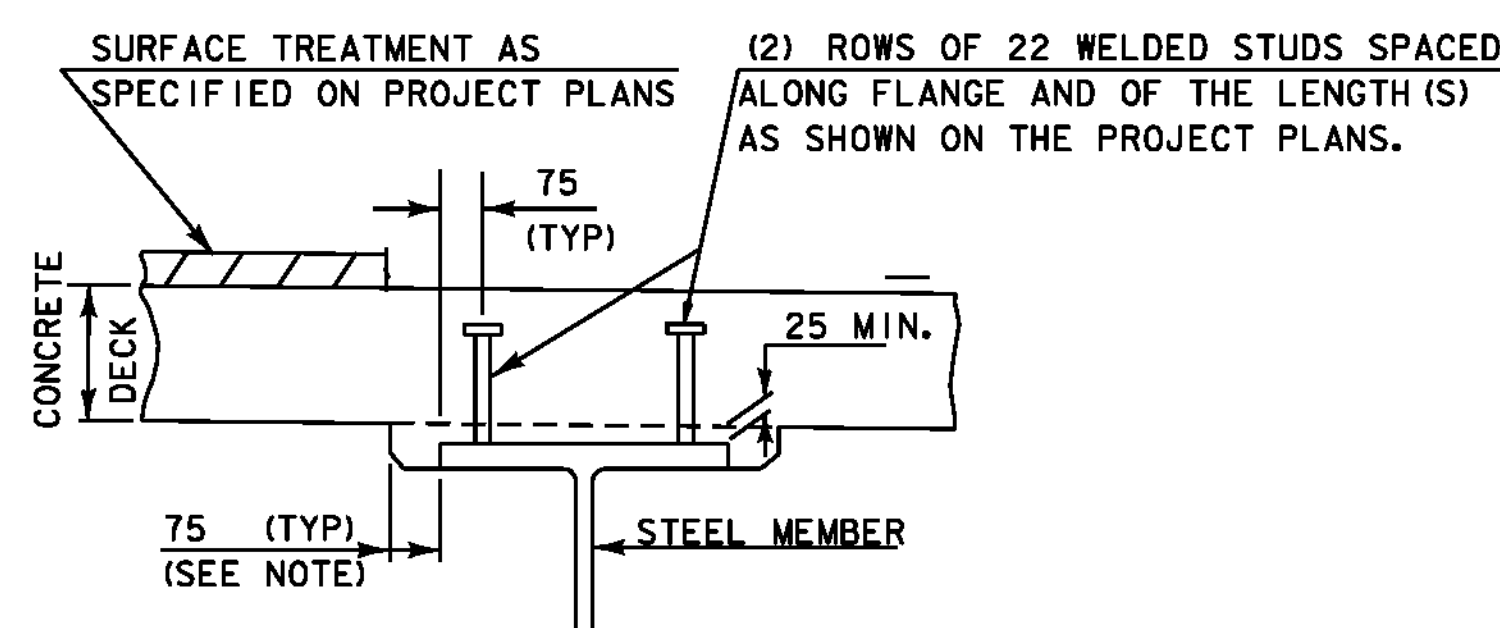
SECTION A-A

NOTE: DRIP PLATES SHALL BE PLACED ON OUTSIDE EDGE OF FASCIA GIRDERS ON THE HIGH SIDE OF ALL PIERS AND ABUTMENTS OR AS INDICATED ON PROJECT PLANS.



**INTERMEDIATE CONNECTION PLATES
AND/OR STIFFENERS FOR WELDED
PLATE GIRDERS**

(NOT TO SCALE)



NOTE: THE 75 HORIZONTAL SECTION MAY BE ELIMINATED FOR FORMING SYSTEMS DESIGNED FOR THE CONSTRUCTION OF VERTICAL HAUNCHES. ANY VOIDS RESULTING FROM FORMING SYSTEM ELEMENTS SHALL BE FILLED WITH JOINT SEALER, POLYURETHANE MEETING THE REQUIREMENTS OF SECTION 524. THE COST OF THE JOINT SEALER, POLYURETHANE SHALL BE INCIDENTAL TO THE ADJACENT CONCRETE.

DETAIL "A"

HAUNCH AND SHEAR CONNECTOR DETAILS

PROJECT: READSBORO	PROJECT NO.: BRO 1441 (25)
DESIGN FILE NAME: 94J070Structures\sj070sup.dgn	PLOT DATE: 05-JAN-2010
IPARM FILE NAME: 94J070girder.i	DRAWN BY: D.D. BEARD
DESIGNED BY: H. I. SALLS	CHECKED BY: R. S. YOUNG
SQUAD LEADER: C. P. WILLIAMS	SHEET: 21 OF 60
BRIDGE DETAIL SHEET	