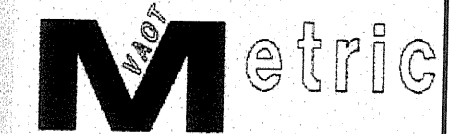


GENERAL NOTES



1. ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE AGENCY OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR CONSTRUCTION, DATED 2001, AND ITS LATEST REVISIONS, AND THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, SEVENTEENTH EDITION, AND ITS LATEST REVISIONS.
2. BRIDGE IS DESIGNED FOR MS 22.5 LIVE LOAD WITH AN ALLOWANCE FOR 80mm FUTURE PAVEMENT. LOAD RATING DOES NOT REFLECT FUTURE PAVEMENT.
3. IN-STREAM CONSTRUCTION SHALL BE RESTRICTED TO JUNE 1 TO OCTOBER 1, UNLESS THE CONTRACTOR OBTAINS WRITTEN PERMISSION FROM THE AGENCY OF NATURAL RESOURCES TO DO WORK OUTSIDE OF THAT TIME FRAME.
4. THE CONTRACTOR SHALL REVIEW AND UNDERSTAND ALL APPLICABLE ENVIRONMENTAL PERMITS AND INSURE THAT ALL CONDITIONS ARE MET.
5. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PREVENT SILTATION OR POLLUTION, ESPECIALLY THE DISCHARGE OF RAW CONCRETE, INTO ANY WATERCOURSE.
6. THE EXISTING SUPERSTRUCTURE SHALL BE REMOVED UNDER THE PAY ITEM 529.20 "PARTIAL REMOVAL OF STRUCTURE." THIS INCLUDES EXISTING BEAMS AND DECKING. THE REMOVED COMPONENTS WILL BECOME THE PROPERTY OF THE CONTRACTOR. THESE COMPONENTS MAY CONTAIN LEAD. THE CONTRACTOR SHALL INDEMNIFY AND HOLD THE STATE, ITS OFFICERS AND EMPLOYEES HARMLESS CONCERNING THE CONTRACTOR'S USE OR DISPOSITION OF THESE COMPONENTS.
7. PAYMENT FOR REMOVAL OF EXISTING BITUMINOUS PAVEMENT ON THE BRIDGE SHALL BE MADE UNDER THE ITEM 529.10, "REMOVAL OF BRIDGE PAVEMENT". THE MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF PROPERLY AT AN OFFSITE LOCATION.
8. THE EXISTING ABUTMENTS SHALL BE REMOVED UNDER THE PAY ITEM 529.20, "PARTIAL REMOVAL OF STRUCTURE". THE EXISTING ABUTMENTS SHALL BE REMOVED ONLY DOWN TO AN ELEVATION SUCH THAT THE STONE FILL, TYPE IV, MAY BE PLACED. SEE CHANNEL SECTIONS FOR LIMITS OF STONE FILL.
9. THE COST OF ON-PROJECT CONSTRUCTION SIGNS AND BARRICADES REQUIRED SHALL BE PAID FOR UNDER THE ITEM 641.10 "TRAFFIC CONTROL."
10. ALL DIMENSIONS ARE HORIZONTAL OR VERTICAL AND ARE GIVEN AT 20 DEGREES C UNLESS OTHERWISE NOTED.
11. DURING CONSTRUCTION, TRAFFIC WILL BE MAINTAINED ON THE EXISTING STRUCTURE.
12. ACCESS TO DRIVES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED. WHEN THE CONTRACTOR MUST TEMPORARILY RESTRICT ACCESS TO THE DRIVES, NOTICE TO THE PROPERTY OWNERS SHALL BE GIVEN IN ADVANCE BY THE CONTRACTOR. THIS WORK SHALL BE INCIDENTAL TO ITEM 641.10 "TRAFFIC CONTROL."
13. THE REMOVAL AND/OR RESETTING OF TRAFFIC SIGNS, AS DEEMED NECESSARY BY THE RESIDENT ENGINEER, WILL BE PAID FOR UNDER TO THE ITEM 641.10 "TRAFFIC CONTROL."
14. NOT USED.
15. STONE FILL, TYPE IV SHALL BE PLACED IN FRONT OF THE ABUTMENTS BEFORE THE DECK IS POURED.
16. PAY ITEM 210.10 "CLEARING AND GRUBBING", INCLUDES REMOVAL OF ALL INDIVIDUAL TREES SPECIFIED FOR REMOVAL ON THE PROJECT PLANS.
17. PAYMENT FOR REMOVAL AND RESETTING OF FENCING AT STATION 3+568 LT WILL BE MADE UNDER PAY ITEM 620.50 "REMOVING AND RESETTING FENCE".
18. PAYMENT FOR REMOVAL OF THE CONCRETE PIPE AT STATION 3+577 WILL MADE UNDER THE PAY ITEM 204.20 "TRENCH EXCAVATION OF EARTH".
19. A WATER QUALITY AND QUANTITY TEST WILL BE PERFORMED FOR THE WELL LOCATED AT STA 3+662.5 LT PRIOR TO AND IMMEDIATELY AFTER CONSTRUCTION. THIS WORK WILL BE PERFORMED BY AGENCY STAFF AND/OR DESIGNEES.
20. REMOVAL OF THE LAID POLE BARRIER BETWEEN STATIONS 3+539 - 3+556.5 LT WILL BE PAID UNDER ITEM 621.80 "REMOVAL AND DISPOSAL OF GUARDRAIL" (MOD.).
21. ANY TIMBER, INCLUDING FIREWOOD, TO BE CLEARED BETWEEN STATIONS 3+650 - 3+710 & 4+022 - 4+050 RT SHALL BE RETAINED BY THE PROPERTY OWNERS. THE TREES SHALL BE CUT, LIMBED, LEFT LOG LENGTH AND MOVED OUT OF THE RIGHT-OF-WAY TO A NEARBY AREA ON THE PROPERTY TO BE DETERMINED BY THE RESIDENT ENGINEER AND PROPERTY OWNER. PAYMENT FOR THIS WORK WILL BE INCIDENTAL TO PAY ITEM 201.10 "CLEARING AND GRUBBING".
22. STONE REMOVED FROM THE STONE WALLS BETWEEN STATIONS 3+575 RT - 3+582 RT & 3+666 LT - 3+674.5 LT SHALL BE RETAINED BY THE PROPERTY OWNER. IT SHALL BE STOCKPILED ON THE OWNERS PROPERTY, THE LOCATION TO BE DETERMINED BY THE RESIDENT ENGINEER AND PROPERTY OWNER. PAYMENT FOR THIS WORK WILL BE MADE UNDER PAY ITEM 620.55 "REMOVAL OF EXISTING FENCE" (MOD).

23. THERE WILL BE 7 - (2.5 METER TO 4 METER HIGH) EVERGREEN TREES PLANTED BETWEEN STATIONS 3+664 LT - 3+686 LT. THE LOCATION OF THESE TREES SHALL BE DETERMINED BY THE RESIDENT ENGINEER AND THE PROPERTY OWNER WITH CONSIDERATION GIVEN TO THE LOCATION OF THE DRILLED WELL AND WATERLINE AT STATION 3+662.5 LT. THIS WORK WILL BE PAID FOR UNDER THE ITEM 656.20, "EVERGREEN TREES (WHITE PINE) (2.5M - 4.0M)."

CONCRETE

24. THE HEIGHT OF FILL BEHIND ABUTMENTS WILL BE LIMITED TO THE BRIDGE SEAT ELEVATION UNTIL THE DECK HAS BEEN POURED AND THE CURING PERIOD IS UP.
25. THE KEY IN CONCRETE CONSTRUCTION JOINTS SHALL BE MONOLITHIC AND CONTINUOUS FOR THE FULL LENGTH OF THE JOINT, ANY UPWARD KEY SHALL BE PLACED INTEGRALLY WITH THE CONCRETE BELOW THE JOINT.
26. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 25 mm BY 25 mm UNLESS A TOOLED RADIUS IS CALLED OUT.
27. JOINTS AND SCORE MARKS IN CONCRETE SHALL BE CONSTRUCTED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
28. ALL REINFORCING STEEL SHALL BE DETAILED AND FABRICATED USING PROCEDURES AND TOLERANCES IN ACCORDANCE WITH APPLICABLE PUBLICATIONS OF THE CONCRETE REINFORCING STEEL INSTITUTE (CRSI).
29. REINFORCING PLACEMENT TOLERANCES SHALL BE:
SPACING +/- 25 mm
CLEARANCE +/- 5 mm
30. MINIMUM COVER FOR REINFORCING STEEL SHALL BE 50 mm ALONG THE BACK FACES OF WALLS AGAINST EARTH, 40 mm ALONG THE BOTTOM SURFACE OF THE DECK AND 80 mm ELSEWHERE, UNLESS OTHERWISE NOTED.
31. SURFACES OF BRIDGE SEATS UNDER BEARING DEVICES SHALL BE LEVEL, OTHER BRIDGE SEAT AREAS SHALL BE SLOPED 4.0 % TOWARDS MID-SPAN. THE ENTIRE BRIDGE SEAT SURFACE SHALL BE SMOOTHED WITH A MAGNESIUM FLOAT FINISH.
32. DECK POUR IS TO BE DONE IN ONE POUR WITHIN A MAXIMUM OF EIGHT HOURS. IF CIRCUMSTANCES BEYOND THE CONTRACTOR'S CONTROL PREVENT THIS FROM BEING ACCOMPLISHED, A MINIMUM DELAY OF NINETY-SIX (96) HOURS BETWEEN THE COMPLETION OF ONE DAY'S POUR AND THE BEGINNING OF ANY OTHER POUR WILL BE ADHERED TO. SEE 'DECK PLAN' SHEET FOR DETAIL OF TRANSVERSE BRIDGE SLAB CONSTRUCTION JOINT.
33. WATER REPELLENT (MOD. - SILANE) SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES EXCEPT THE UNDERSIDE OF THE DECK BETWEEN THE DRIP BEADS.
34. NO CONCRETE IN THE ABUTMENTS OR WINGWALLS SHALL BE PLACED ABOVE THE BRIDGE SEAT ELEVATIONS UNTIL THE BEAMS/GIRDERS HAVE BEEN PROFILED AND THE FINISHED GRADE OF THE DECK HAS BEEN DETERMINED.
35. ALL SUBSTRUCTURE CONCRETE SHALL BE CONCRETE, HIGH PERFORMANCE CLASS B UNLESS OTHERWISE NOTED.
36. THE DECK SHALL BE CONCRETE, HIGH PERFORMANCE CLASS A, BROOM FINISHED PERPENDICULAR TO THE DIRECTION OF TRAFFIC. APPLYING A BROOM FINISH WILL NOT BE PAID SEPARATELY, BUT WILL BE MADE INCIDENTAL TO THE PAY ITEM 501.33 "CONCRETE, HIGH PERFORMANCE CLASS A".

STRUCTURAL STEEL

37. NOT USED.
38. ANY CONNECTIONS THAT ARE NOT DETAILED ON THE PLANS SHALL BE DETAILED BY THE FABRICATOR AND SUBMITTED TO THE STRUCTURES ENGINEER FOR APPROVAL.
39. ALL WELDING SHALL CONFORM TO THE PROVISIONS OF VERMONT SPECIFICATION 506.10.
40. ALL MEMBERS MARKED (CVN) MUST MEET THE CHARPY V-NOTCH TESTING REQUIREMENTS AS INDICATED IN SECTION 714 OF THE VERMONT AGENCY OF TRANSPORTATION SPECIFICATIONS FOR CONSTRUCTION.
41. ANY HOLES IN THE WEBS OF THE FASCIA BEAMS/GIRDERS THAT ARE NOT OTHERWISE FILLED, SHALL BE FILLED WITH EITHER BUTTON HEAD OR HEX HEAD BOLTS. THESE BOLTS SHALL BE TIGHTENED IN ACCORDANCE WITH VERMONT SPECIFICATION 506.19.
42. FIELD CONNECTIONS SHALL BE MADE WITH 22 mm DIAMETER AASHTO DESIGNATION M 164M TYPE III BOLTS IN 24 mm DIAMETER HOLES, EXCEPT CONNECTION BETWEEN LATERAL BRACING AND BOTTOM FLANGES WHICH REQUIRE 20 DIAMETER BOLTS AND 22 AND 24 DIAMETER HOLES. ALL CONNECTIONS MADE WITHIN PAINTED AREAS SHALL BUT SHALL BE LIMITED TO A MAXIMUM OF 1.2 METER SPACING.

BE MADE WITH M 164M TYPE I GALVANIZED BOLTS. SEE FRAMING PLAN SHEET FOR DETAILS.

43. THE DESIGN OF THE FLEMING BRACKETS SHALL BE LEFT UP TO THE CONTRACTOR BUT SHALL BE LIMITED TO A MAXIMUM OF 1.2 METER SPACING.

44. AFTER THE STEEL HAS BEEN ERRECTED, ELEVATIONS SHALL BE TAKEN ALONG THE TOP OF THE BEAMS/GIRDERS, AS DIRECTED BY THE ENGINEER, FOR USE IN DETERMINING THE FINISHED GRADE.

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PROJECT LEADER: R. WHITCOMB

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DESIGNED BY: D. BONNEAU

CHECKED BY: C. MEUNIER

GENERAL NOTES SHEET

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