

**NOTE: THE REQUIREMENTS OF ITEM 900.675 – PRECAST PRECOMPRESSED CONCRETE/STEEL COMPOSITE SUPERSTRUCTURE – AND SECTION 510 (EXCEPTING SUBSECTIONS 510.07, 510.09, & 510.12) OF THE VAOT 2006 STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL APPLY TO THIS WORK.**

**DESCRIPTION OF FABRICATING FACILITIES FOR UNIT PRODUCTION:**

- ALL CONCRETE TO BE SUPPLIED BY THE FORT MILLER CO., INC. AUTOMATED BATCH PLANT, AN NPCA & PCI CERTIFIED FACILITY.
- CONCRETE TO BE DELIVERED TO FORM FROM BATCH PLANT USING A MIXER TRUCK AND PLACED USING CONCRETE BUCKETS.
- ALL CONCRETE TO BE CONSOLIDATED USING INTERNAL VIBRATION.
- QUALITY CONTROL EQUIPMENT: A) COMPRESSION TESTER: TEST MARK INDUSTRIES MODEL CM-5000 DB DATE OF LAST CALIBRATION: SEPTEMBER 17, 2009  
B) HIGH PRESSURE AIR METER – FORNEY PRESS-AIR METER MODEL LA-316

**CONCRETE DATA**

STRIPPING STRENGTH: 21 MPa (MINIMUM)  
UNIT INVERTING STRENGTH: 35 MPa (MINIMUM)  
28-DAY STRENGTH: 35 MPa (MINIMUM)  
**MIX DESIGN (YIELD: 1.0 CUBIC METER):** MIX CODE SP206  
CEMENT: 380 Kg LaFARGE NORTH AMERICA I/II  
FLY ASH: 95 Kg  
COARSE AGGREGATE: 996 Kg ASTM C33 SIZE 67  
FINE AGGREGATE: 694 Kg ASTM C33  
WATER CONTENT: 160 Kg  
HIGH RANGE WATER REDUCER: 2.8 L ASTM C494  
AIR ENTRAINING AGENT: 300 ml ASTM C260  
UNIT WEIGHT: 2,324 kg/m<sup>3</sup> (APPROX.)  
28-DAY STRENGTH: 35 MPa (MIN.)  
STRIPPING STRENGTH: 21.0 MPa (MIN.)  
WATER-CEMENT RATIO: 0.34 (APPROX.)  
AIR CONTENT: 5.0% TO 9.0%

**PRIMARY MATERIAL SOURCE:**

CEMENT: LAFARGE NORTH AMERICA I/II NYSDOT BRAND CODE 05  
FLY ASH: HEADWATER RESOURCES NYSDOT BRAND CODE 18  
COARSE AGGREGATE: PECKHAM MATERIALS; GREENFIELD, NY NYSDOT SOURCE 1-39R TEST 09AR27  
FINE AGGREGATE: PECKHAM MATERIALS; WEST FORT ANN, NY NYSDOT SOURCE 1-85F TEST 09AF85  
HIGH RANGE WATER REDUCER: BASF ADMIXTURES, GLENIUM 3030SNS NYSDOT BRAND CODE 4036  
AIR ENTRAINING AGENT: BASF ADMIXTURES; MB AE 90 NYSDOT BRAND CODE 1037

**CONCRETE TESTING/QUALITY ASSURANCE**

- THE CONCRETE STRENGTH SHALL BE DETERMINED FROM CONCRETE TEST CYLINDERS (100mmØ X 200mm) MADE IN CONFORMANCE WITH AASHTO T23, (1) SET PER LOT. (1 SET = 6 CYLS. MIN., 1 LOT = 1 BRIDGE UNIT MAX). ALL CYLINDERS SHALL BE TESTED IN CONFORMANCE WITH AASHTO T23 ON AN APPROVED TESTING MACHINE. ALL CYLINDERS SHALL BE MADE AND TESTED IN THE PRESENCE OF THE AGENCY INSPECTOR. ALL CYLINDERS USED TO TEST FOR CONCRETE STRENGTH SHALL BE CURED IN THE SAME MANNER AS THE UNITS THAT THEY REPRESENT AND IN CONFORMANCE WITH AASHTO T23.
- THE MANUFACTURER SHALL CAST A SUFFICIENT NUMBER OF CONCRETE TEST CYLINDERS TO FULFILL THE CONCRETE STRENGTH TEST REQUIREMENTS AS STATED IN SECTION 'C' BELOW.
- TESTING FOR CONCRETE COMPRESSIVE STRENGTH.  
THE STRENGTH REQUIREMENTS FOR EACH UNIT SHALL BE CERTIFIED BY THE MANUFACTURER, AS FOLLOWS, BEFORE THE UNIT IS ACCEPTED FOR STRENGTH.
  - LIFTING STRENGTH: TWO CYLINDERS FROM EACH UNIT SHALL BE TESTED IN IMMEDIATE SUCCESSION TO VERIFY LIFTING STRENGTH. THE STRENGTH OF EACH CYLINDER SHALL BE AT LEAST 95 PERCENT OF THE REQUIRED LIFTING STRENGTH. THE AVERAGE STRENGTH OF THE TWO CYLINDERS SHALL BE EQUAL TO OR GREATER THAN THE REQUIRED LIFTING STRENGTH AS SPECIFIED HEREIN (21 MPa).
  - 28-DAY STRENGTH: TWO CYLINDERS FROM EACH UNIT SHALL BE TESTED IN IMMEDIATE SUCCESSION AT 28 DAYS OF AGE TO VERIFY THE REQUIRED 28-DAY STRENGTH OF THE CONCRETE. THE AVERAGE STRENGTH OF THE TWO CYLINDERS SHALL BE EQUAL TO OR GREATER THAN THE REQUIRED 28-DAY STRENGTH. IF THIS REQUIREMENT IS NOT MET, ANY REMAINING CYLINDERS REPRESENTING THE UNIT SHALL BE TESTED AT 28 DAYS OF AGE. THE AVERAGE STRENGTH OF ALL CYLINDERS REPRESENTING THE 28-DAY STRENGTH OF ANY ONE UNIT SHALL BE EQUAL TO OR GREATER THAN THE REQUIRED 28-DAY STRENGTH (35 MPa).
  - OPTION: THE MANUFACTURER MAY ELECT TO TEST TWO CYLINDERS FROM EACH UNIT IN IMMEDIATE SUCCESSION, PRIOR TO THE 28-DAY AGE LIMIT. IF THIS OPTION IS EXERCISED, SUFFICIENT CYLINDERS SHALL BE MADE TO ENSURE THAT AT LEAST TWO CYLINDERS ARE AVAILABLE FOR THE 28 DAY TEST. EACH CYLINDER SHALL HAVE A STRENGTH OF AT LEAST 95% OF THE REQUIRED 28-DAY STRENGTH. THE AVERAGE STRENGTH OF THE TWO CYLINDERS MUST BE EQUAL TO OR GREATER THAN THE REQUIRED 28-DAY STRENGTH. IF THESE REQUIREMENTS ARE MET, THE CYLINDER TEST AT 28-DAYS OF AGE SHALL BE WAIVED.
- THE FOLLOWING QUALITY CONTROL TESTS SHALL BE PERFORMED BY THE MANUFACTURE, IN THE PRESENCE OF THE INSPECTOR, FROM THE SAME CONCRETE SAMPLE AS THAT USED FOR THE CONCRETE COMPRESSIVE STRENGTH REQUIREMENTS: 1) COMPRESSIVE STRENGTH AASHTO T23 2) AIR CONTENT ASTM C231 3) TEMPERATURE 4) UNIT MASS ASTM C138 5) WATER-CEMENT RATIO AASHTO T318 6) SLUMP ASTM C143

**INSPECTION VERMONT AGENCY OF TRANSPORTATION**

MATERIALS FURNISHED & WORK PERFORMED UNDER SECTION 510 SHALL BE INSPECTED BY THE AGENCY. ADVANCE NOTIFICATION OF AT LEAST ONE WEEK SHALL BE PROVIDED BY THE FABRICATOR TO THE AGENCY'S ENGINEER AND STRUCTURAL CONCRETE ENGINEER INDICATING THE FABRICATION START DATE.

**REINFORCING STEEL**

- BAR REINFORCEMENT IN CONFORMANCE WITH VAOT 713.01 (ASTM A615M GRADE 420, CONFORMING TO AASHTO M 31M INCL. SUPPLEMENTARY REQUIREMENTS) AND EPOXY COATED (WHERE REQUIRED) IN CONFORMANCE WITH VAOT 713.07 AS SUPPLIED BY:
- NUCOR STEEL AUBURN – AUBURN, NY (SUPPLIER)
  - GERDAU AMERISTEEL SAYREVILLE STEEL MILL DIVISION – SAYREVILLE, NJ (SUPPLIER)
  - CORROSION CONTROL, INC. – AUBURN, NY (EPOXY COATING)

**STRUCTURAL STEEL**

- STRUCTURAL STEEL SHALL MEET THE MATERIAL REQUIREMENTS OF VAOT 714.01-714.05.
- STRUCTURAL STEEL SHALL CONFORM TO AASHTO M270M GRADE 345W AND SHALL AND BE FABRICATED IN CONFORMANCE WITH VAOT SECTION 506.
- STRUCTURAL STEEL FABRICATION DRAWINGS PREPARED BY STS STEEL INC., SCHENECTADY, NY & SUBMITTED UNDER SEPARATE COVER SHALL BE USED IN CONJUNCTION WITH THESE PRECAST CONCRETE FABRICATION DRAWINGS.

**PRODUCTION SCHEDULE**

UNITS SHALL BE FABRICATED ON A FIVE DAY SCHEDULE. NUMBER OF UNITS TO BE PRODUCED PER WEEK SHALL BE (1)±

**UNIT MARKING**

EACH UNIT SHALL BE CLEARLY AND PERMANENTLY LABELED ON THE UNDERSIDE OF THE DECK (IN THE VICINITY OF THE END DIAPHRAGM) WITH THE FOLLOWING INFORMATION:

THE FORT MILLER CO., INC.  
FMC JOB # 11579 MARK NO.: \_\_\_\_\_  
DATE CAST: \_\_\_\_\_ LOAD RATING: HS25-44

**FABRICATION PROCEDURE**

- SIDE BULKHEADS AND REINFORCING STEEL ARE PLACED ON THE DECK FORM.
- STRUCTURAL STEEL FRAMES ARE SUPPORTED (FROM ABOVE) OVER THE DECK FORM & ADJUSTED TO THE CORRECT LOCATION & ELEVATION.
- CROSS-FORM TRANSFER BEAMS ARE INSTALLED AT APPROX. 2.44m – 3.05m INTERVALS TO SUPPORT THE DECK FORM. REINFORCING STEEL IS HUNG FROM THE STRUCTURAL STEEL FRAME.
- TRANSFER BEAMS ARE ADJUSTED TO OBTAIN THE REQ'D VERTICAL DISTANCE BETWEEN THE STEEL FRAME AND THE DECK, WITH THE APPROPRIATE REINFORCEMENT CLEARANCE.
- FINAL ELEVATION CHECKS ARE MADE AT FIELD BEARING LOCATIONS. BEAM DEFLECTIONS ARE CHECKED AND DEFLECTION STOPS ARE SET.
- CONCRETE IS PLACED, VIBRATED AND SCREED TO THE PROPER LEVEL. BEAM DEFLECTIONS ARE CHECKED AND ADJUSTED TO CONFORM TO REQ'D DESIGN DEFLECTIONS.
- THE BRIDGE UNIT AND TEST CYLINDERS ARE CURED ACCORDING TO PROCEDURE OUTLINED BELOW.
- AT THE POINT THAT 2 CONSECUTIVE CYLINDER BREAKS INDICATE THAT CONCRETE STRENGTH HAS REACHED 21 MPa, TRANSFER BEAMS ARE REMOVED AND THE UNIT IS LIFTED FROM FORM. THE FINAL CURING PHASE CONTINUES.
- AT THE POINT THAT 2 CONSECUTIVE CYLINDER BREAKS INDICATE THAT CONCRETE STRENGTH HAS REACHED 35 MPa (AND A MINIMUM OF 72 HRS. HAVE ELAPSED SINCE CONCRETE PLACEMENT), THE UNIT MAY BE INVERTED TO THE UPRIGHT POSITION. THE FINAL CURING PHASE CONTINUES UNTIL THE CONCRETE HAS ATTAINED THE SPECIFIED 28-DAY COMPRESSIVE STRENGTH (35 MPa).

**CONCRETE PLACEMENT**

CONCRETE SHALL NOT BE DEPOSITED IN THE FORMS UNTIL THE AGENCY REPRESENTATIVE HAS APPROVED PLACEMENT OF REINFORCEMENT AND INSERTS. THE CONCRETE SHALL BE VIBRATED INTERNALLY, EXTERNALLY, OR A COMBINATION THEREOF TO THE REQUIRED CONSOLIDATION. THE VIBRATING SHALL BE DONE WITH CARE AND IN SUCH A MANNER THAT:

- CONCRETE IS UNIFORMLY CONSOLIDATED.
- DISPLACEMENT OF REINFORCEMENT AND INSERTS IS AVOIDED.
- ACCEPTABLE SURFACE FINISHES ARE PRODUCED.

**CURING (NON-ACCELERATED CURE / SATURATED COVER):**

- INITIAL CURING PHASE  
THE INITIAL CURING PHASE FOR EACH UNIT SHALL BE THAT PERIOD BEGINNING FROM THE TIME THAT EACH UNIT IS COMPLETELY FINISHED AND CONTINUING UNTIL THE FINAL CURING PHASE COMMENCES. DURING THIS PHASE THE ENCLOSURE (FORM) TEMPERATURE SHALL BE MAINTAINED AT APPROXIMATELY THE CONCRETE PLACEMENT TEMPERATURE WITH ARTIFICIAL HEAT APPLIED AS NECESSARY. TO PREVENT DAMAGE TO THE TOP-IN-FORM SURFACE, THE PLASTIC CONCRETE OF EACH UNIT CAST SHALL BE ALLOWED TO ATTAIN INITIAL SET BEFORE THE FINAL CURING PHASE BEGINS. THE INITIAL CURING PHASE SHALL BE APPROXIMATELY 1-4 HOURS DURATION BUT SHALL NOT EXCEED 8 HOURS.
- FINAL CURING PHASE  
THE FINAL CURING PHASE MAY BEGIN AT ANY TIME AFTER THE INITIAL CONCRETE SET HAS BEEN ACHIEVED. EACH UNIT SHALL BE COVERED WITH HEAVY, WATER-SATURATED BURLAP (OR OTHER MATERIAL ACCEPTABLE TO THE ENGINEER IN CHARGE). THE BURLAP SHALL BE KEPT SATURATED AND THE CONCRETE SURFACE TEMPERATURE NOT ALLOWED TO FALL BELOW 20°C. THESE CONDITIONS SHALL BE MAINTAINED UNTIL DESIGN STRENGTH (35 MPa) HAS BEEN ATTAINED. UNITS MAY THEN BE EXPOSED TO AMBIENT TEMPERATURES LESS THAN 20°C
- FALL/WINTER CURING  
ALL CONCRETE PLACEMENT WILL TAKE PLACE IN HEATED PRODUCTION BUILDINGS WHERE AMBIENT AIR TEMPERATURES ARE ABOVE 7°C. SHOULD THE NECESSITY ARISE FOR PLACING CONCRETE AT A TEMPERATURE LESS THAN 7°C, A SEPARATE CURING PROCEDURE WILL BE SUBMITTED FOR APPROVAL.

**SURFACE FINISHES**

- TOP OF DECK FINISH TO BE COARSE SANDBLAST FORMALINER EQUIVALENT TO SCOTT SYSTEMS FLEX-LINER SANDBLAST NO. 3
- UNDERSIDE OF DECK TO RECEIVE A MEDIUM BROOM FINISH.
- INTERIOR JOINT KEYWAYS TO RECEIVE A MEDIUM SANDBLAST PRIOR TO SHIPPING
- FASCIA TO RECEIVE A FORMICA FORM FINISH. FASCIA INSERTS, FOR PRECAST CURB FORMWORK, TO BE PATCHED PRIOR TO APPLICATION OF FINAL RUB FINISH.

**DIMENSIONAL TOLERANCES**

- GEOMETRY OF CONCRETE DECK
  - LENGTH (EACH UNIT), ±20mm (ADJACENT UNIT LENGTHS SHALL NOT VARY BY MORE THAN 20mm)
  - WIDTH, ±10mm
  - DECK THICKNESS, +10mm, -6mm
  - DEVIATION FROM DIAGONALS, ±20mm (HORIZONTAL)
  - DEVIATION FROM END SQUARENESS OR SKEW, ±20mm (HORIZONTAL)
  - STRINGER SPACING, ±13mm (WITHIN A UNIT)
  - HORIZONTAL ALIGNMENT, ±10mm (DEVIATION FROM STRAIGHT LINE PARALLEL TO THE CENTERLINE OF THE UNIT)
  - INSERT LOCATION, ±10mm
- REINFORCING
  - SPACING, ±50mm (NON-CUMULATIVE)
  - COVER, (EA. MAT) ±10mm
- BEAM CAMBER
  - INVERTED (AT TIME OF CASTING), ±6mm (MEASURED AT TENSION FLANGE, MID-SPAN) P.M.R.
  - UPRIGHT DESIGN CAMBER, ±10mm
- FIELD INSTALLATION
  - VERTICAL DEVIATION BETWEEN UNITS, PRIOR TO GROUTING, SHALL NOT EXCEED 10mm (AFTER FINISHING OPERATIONS ARE COMPLETE)
  - JOINT WIDTH BETWEEN UNITS SHALL BE 20mm ±13mm

**NOTE TO CONTRACTOR:**

THIS SHOP DRAWING REPRESENTS OUR INTERPRETATION OF THE PLANS AND SPECIFICATIONS, AND OUR CONTRACT REQUIREMENTS FOR THIS PROJECT. PRIOR TO THE MANUFACTURE OF ANY ITEM FOR THIS PROJECT, ALL DIMENSIONS, METHODS OF CONSTRUCTION AND EXISTING CONDITIONS MUST BE CHECKED, CORRECTED AND/OR APPROVED BY OUR CUSTOMER. NO ITEM WILL BE SCHEDULED FOR PRODUCTION UNTIL WE HAVE BEEN NOTIFIED IN WRITING THAT OUR DRAWINGS HAVE BEEN APPROVED FOR FABRICATION. APPROVAL DELAYS WILL RESULT IN FABRICATION DELAYS. ANY ITEM THAT IS FABRICATED IN ACCORDANCE WITH APPROVED SHOP DRAWINGS THAT DOES NOT FIT THE CUSTOMER'S REQUIREMENTS WILL BE REMADE AND SHIPPED TO THE PROJECT ONLY AT THE CUSTOMER'S EXPENSE, AND ONLY AFTER RECEIPT OF A PURCHASE ORDER TO COVER THE ADDED EXPENSE. WE ASSUME NO RESPONSIBILITY FOR THE ALTERING OF OUR PRODUCTS TO ACCOMMODATE OTHER TRADES UNLESS REQUIRED INFORMATION IS FURNISHED AND SHOWN ON OUR SHOP DRAWINGS AT THE TIME THEY ARE APPROVED FOR FABRICATION BY OUR CUSTOMER.

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SHEET No.	DRAWING No.	TITLE	REVISION No.
1	B1	PRODUCTION NOTES	1
2	B2	BRIDGE LAYOUT	0
3	B3	UNIT S1 DETAILS	0
4	B4	UNIT S2 DETAILS	0
5	B5	PRECAST CURB DETAILS	0
6	B6	LIFTING	0

**UNIT REJECTION CRITERIA**

- INDIVIDUAL PRECAST UNITS MAY BE REJECTED FOR ANY OF THE FOLLOWING REASONS:
- FRACTURES OR CRACKS PASSING THROUGH THE DECK.
  - CAMBER THAT DOES NOT MEET THE REQUIREMENTS IN THESE APPROVED FABRICATION DRAWINGS.
  - HONEYCOMBED, OPEN TEXTURE.
  - DIMENSIONS NOT WITHIN THE ALLOWABLE TOLERANCES SPECIFIED HEREIN.
  - SEPARATION OF THE CONCRETE DECK FROM THE STEEL STRINGERS.
  - DEFECTS THAT INDICATE PROPORTIONING, MIXING, AND MOLDING NOT IN COMPLIANCE WITH THE SPECIFICATIONS.
  - DAMAGED ENDS WHERE SUCH DAMAGE WOULD PREVENT MAKING A SATISFACTORY JOINT.
  - UNITS WITH CRACK(S) WITHIN ANY PART OF THE CONCRETE THIS GREATER THAN 0.8mm IN WIDTH.
  - SIGNIFICANT DAMAGE TO THE UNITS DURING TRANSPORTATION, ERECTION, OR CONSTRUCTION AS DETERMINED BY THE ENGINEER.
  - UNITS NOT FABRICATED IN CONFORMANCE WITH THE CONTRACT DOCUMENTS.

**REPAIRS**

UNITS THAT CONTAIN MINOR DEFECTS CAUSED BY MANUFACTURE OR HANDLING MAY BE REPAIRED AT THE MANUFACTURING SITE. REPAIR PROCEDURES SHALL BE IN ACCORDANCE WITH THE APPROVED QUALITY CONTROL PLAN AND REQUIRE APPROVAL BY THE ENGINEER. MINOR DEFECTS ARE DEFINED AS HOLES, HONEYCOMBING, OR SPALLS WHICH ARE 150mm OR LESS IN DIAMETER AND DO NOT PENETRATE DEEPER THAN 25mm INTO CONCRETE. SURFACE VOIDS OR "BUGHOLES" THAT ARE LESS THAN 16mm IN DIAMETER AND LESS THAN 6mm DEEP NEED NOT BE REPAIRED. REPAIRS SHALL BE MADE USING AN OVERHEAD AND VERTICAL CONCRETE REPAIR MATERIAL SATISFACTORY TO THE ENGINEER. THE REPAIR MATERIAL SHALL BE CURED AS SPECIFIED BY THE MANUFACTURER. THE ENGINEER SHALL APPROVE FINAL REPAIRS.

**CRACKING**

CRACK WIDTHS LESS THAN 0.3mm SHALL BE SEALED WITH A PENETRATING SEALER USING AGENCY APPROVED MATERIALS AND PROCEDURES. CRACK WIDTHS MEASURING 0.3mm TO 0.8mm SHALL BE EPOXY INJECTED USING AGENCY APPROVED MATERIALS AND PROCEDURES. AT THE ENGINEER'S DISCRETION, CRACKED MEMBERS SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL EXPENSE TO THE OWNER.

**PRE-ASSEMBLY**

THE UNITS SHALL BE PRE-ASSEMBLED AT THE FABRICATOR'S PLANT TO ASSURE PROPER MATCH BETWEEN UNITS BEFORE SHIPMENT TO THE PROJECT SITE, TO THE SATISFACTION OF THE AGENCY. A REPRESENTATIVE FOR THE CONTRACTOR WILL BE PRESENT TO WITNESS THE PRE-ASSEMBLY AND TO SECURE GEOMETRIC CONTROL INFORMATION FOR COORDINATION OF WORK AT THE PROJECT SITE.

**SHIPPING**

UNITS SHALL NOT BE SHIPPED UNTIL THE MINIMUM 28-DAY CONCRETE STRENGTH IS ATTAINED AND THEY HAVE RECEIVED THE STAMP OF APPROVAL BY THE AGENCY. A 48-HOUR ADVANCE NOTICE OF THE LOADING AND SHIPPING SCHEDULE SHALL BE PROVIDED. THE UNITS SHALL BE SECURED ON A VEHICLE IN ORDER THAT NO FATIGUE CRACKING WILL OCCUR DURING TRANSPORT. UNITS SHALL BE SHIPPED ON WOOD DUNNAGE LOCATED AS CLOSE TO BEARING AS POSSIBLE. UNITS SHALL BE SECURED WITH CHAINS AT A MINIMUM OF TWO LOCATIONS PER END. CHAIN GUARDS WILL BE EMPLOYED TO PROTECT VULNERABLE CONCRETE EDGES FROM DAMAGE.

**SHIP LOOSE**

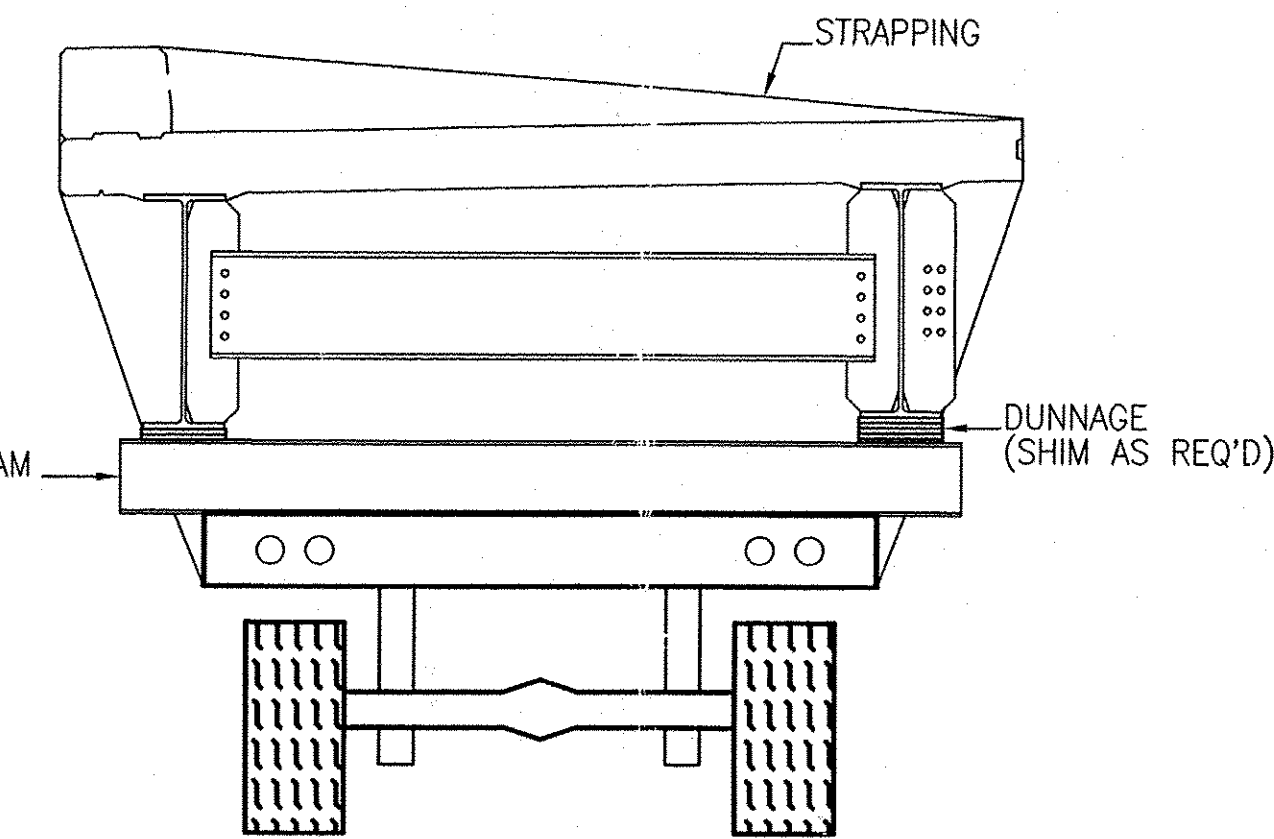
- NOTE: ITEMS DESIGNATED AS "TO BE RETURNED" SHALL BE RETURNED VIA COMMON CARRIER BY AND AT THE SOLE EXPENSE OF THE CONTRACTOR.
- (6) 21T COFFEE POT LIFTING SLINGS (ITEM 5362)
  - TECHNICAL SERVICE (ITEM 7026)
  - (24) 22mmØ x 57mm ASTM A325 TYPE 3 BOLT
  - (24) 22mmØ ASTM A563 GR D33 HEX NUTS
  - (24) 22mmØ ASTM F436 TYPE 3

**TABLE OF UNITS**

UNIT MK#	QTY REQ'D	MASS (kg)	VOLUME (m <sup>3</sup> )	AREA (m <sup>2</sup> )
S1	1	61900	21.4	64.0
S2	1	61900	21.4	64.0

**WARNING:**

THE VOLUMES & WEIGHTS LISTED ARE BASED ON DESIGN THICKNESSES. ACTUAL VALUES CAN & DO VARY IN ACCORDANCE WITH ALLOWABLE TOLERANCES. IT IS NECESSARY TO SIZE ALL LIFTING EQUIPMENT TAKING THIS FACT INTO CONSIDERATION !



**SHIPPING DETAIL**

VAOT PROJECT FH 010-1(2): ITEM NO. 900.675

NO.	DATE	BY	REVISIONS
△	6-25-10	MAW	SHIP LOOSE, CONCRETE TESTING, SHIPPING DETAIL

	THE FORT MILLER Co., Inc. P.O. BOX 98 SCHUYLERVILLE, NY 12871 (518) 695-5000 (518) 695-4970 FAX	F.M. JOB NO. 11579
	PROJECT: TH 18 OVER SOUTH BRANCH MIDDLEBURY RIVER TOWN OF RIPTON, COUNTY OF ADDISON, VT	SHEET NO. 1
SUBJECT: PRODUCTION NOTES	DATE: 6-1-10	DRN. BY: M. WERN
CONTRACTOR: CCS CONSTRUCTORS, LLC	SCALE: NONE	CHK. BY: S. HARRIGAN
ENGINEER/ARCHITECT: VERMONT AGENCY OF TRANSPORTATION	DWG. NO. B1	

Wendy Haugeto  
WENDY HAUGETO  
STATE OF VERMONT PE LICENSE 66451  
AREA ENGINEERING, INC.  
186 HOLLOW ROAD  
SKILLMAN, NJ 08558