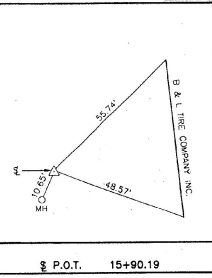
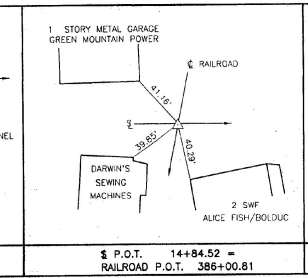
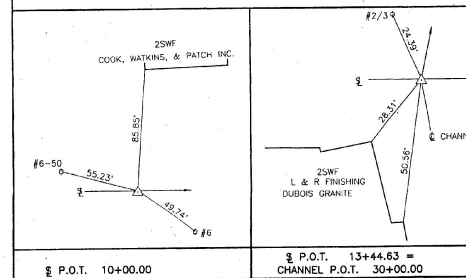


- REMOVING AND RESETTING FENCE**  
STA. 43+20 TO STA. 43+45 LT  
(SEE NOTE 3)
- REMOVING AND RESETTING PIPE RAIL**  
STA. 43+44 TO STA. 43+46 LT.  
(SEE NOTE 3)
- CAST-IN-PLACE CONCRETE RETAINING WALL**  
STA. 44+28 TO STA. 44+52 LT. R.04'
- VERTICAL GRANITE CURB**  
STA. 42+50 TO STA. 43+26 RT  
STA. 43+42 TO STA. 43+47 RT  
STA. 43+15 TO STA. 43+36 LT (R=25')  
STA. 43+36 TO STA. 43+47 LT  
STA. 44+28 TO STA. 44+60 RT  
STA. 44+28 TO STA. 44+56, 15' LT  
STA. 44+56, 15' LT TO STA. 44+56, 19' LT (R=2)  
STA. 44+28 TO STA. 44+56, 19' LT
- CHANGING ELEV. OF MANHOLE**  
STA. 42+85 RT  
STA. 42+87 LT  
STA. 44+60 RT
- PORTLAND CEMENT SIDEWALK**  
STA. 42+50 TO STA. 43+47 RT - 5' WIDE  
STA. 44+28 TO STA. 44+78 RT - VARIES  
(SEE ROW SHEET 38 OF 6)
- PORTLAND CEMENT CONCRETE ISLAND**  
STA. 44+28 TO STA. 44+58 LT - 4' WIDE  
(SEE ROW SHEET 38 OF 6)
- ALUMINUM APPROACH RAILING**  
STA. 43+22 TO STA. 43+47 LT  
STA. 44+28 TO STA. 44+53 LT
- CONSTRUCT DRIVE**  
STA. 43+10 LT - 17' PAVED  
STA. 43+28 RT - 11' GRAVEL (SKEWED)  
STA. 44+67 LT - 22' PAVED  
STA. 44+67 RT - 12' GRAVEL
- NOTE:**  
CONSTRUCTION OF DRIVES ALONG RAILROAD TRACKS MUST MAINTAIN A MINIMUM CLEARANCE OF 8'-6" BETWEEN NEW CONSTRUCTION AND C OF RAILS.
- CONSTRUCT CONCRETE WALK**  
STA. 43+09 TO STA. 43+14 RT.

- NOTES:**
- CONCRETE WALK AND CONCRETE ISLAND TO BE PAID FOR UNDER ITEM 618.10 PORTLAND CEMENT SIDEWALK.
  - THE INTENT FOR THE CATCH BASIN AT STA. 43+17 LT IS TO CONNECT THE EXISTING SYSTEM FROM THE MANHOLE AT STA. 42+92 LT AND REMOVE THE EXISTING PIPE FROM STA. 43+17 LT TO THE OUTLET. REMOVAL OF THIS PORTION OF THE PIPE SHALL BE SUBSIDIARY TO THE INSTALLATION OF THE NEW 24" PIPE.
  - UPON COMPLETION OF THE DRAINAGE SYSTEM INSTALLATION BETWEEN STATIONS 43+00 AND 43+50 THE CONTRACTOR SHALL CLEAN CULVERT IN PLACE (ITEM 601.995) FROM MANHOLE AT STA. 42+92 LT, 19' TO SYSTEM OUTLET AT STA. 43+50 LT, 26'.
  - TWO INCH THICK RIGID INSULATION SHALL BE PLACED OVER AND DOWN THE SIDES OF BOTH THE 6" AND 16" DIAMETER WATERLINES AT THE LOCATIONS WHERE THESE PIPES HAVE LESS THAN 6 FEET OF COVER.

- NOTES:**
- CONSTRUCTION OF DRIVES ALONG RAILROAD TRACKS MUST MAINTAIN A MINIMUM CLEARANCE OF 8'-6" BETWEEN NEW CONSTRUCTION AND C OF RAILS.
  - GRADE CROSS SLOPE OF DRIVE AT STA. 43+28 RT TO DRAIN TOWARDS RIVER
  - REMOVING AND RESETTING FENCE (STA. 43+20 TO STA. 43+45 LT. AND REMOVING AND RESETTING PIPE RAIL (STA. 43+44 TO STA. 43+46 LT. SUBSIDIARY TO ITEM 204.20 TRENCH EXCAVATION.



**STATE OF VERMONT AGENCY OF TRANSPORTATION**

CITY OF	BARRE	BRIDGE NO.	B-10
STREET NAME	BLACKWELL STREET	LOG STA.	SURV. STA.
BLACKWELL STREET BRIDGE OVER STEVENS BRANCH			
<b>ROW PLAN</b>			
DESIGNED BY	SWB	DRAWN BY	GNS
CHECKED BY	DATE	BRIDGE DESIGN SUPERVISOR	DATE
PROJECT		BARRE CITY	PROJECT NO.
			BRM 6000(13)
B&C DRAWING NO.		920388B/20	DATE
			10/07/94
ROW SHEET NO.			SHEET 71 OF 46

**BETTIGOLE ANDREWS & CLARK, INC.**  
Consulting Engineers