

INDEX OF SHEETS

1. TITLE SHEET
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4. EXISTING BRIDGE VIEWS AND DETAILS
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6. BRIDGE REPAIR DETAILS

STANDARDS

E-100 5-26-89R
 E-101 10-30-87
 E-102 1-23-89R
 E-107a 9-10-87

RECORD PLANS

CONTRACTOR WRIGHT CONSTRUCTION INC.
 MT. HOLLY, VT.
 RESIDENT ENGINEER GILBERT H. NEWBURY
 CONSTRUCTION BEGAN 7/22/91
 CONSTRUCTION COMPLETED 11/20/91
 RECORD PLANS BY JAMES MARCELL

I hereby certify that all the construction required by this set of drawings has been accomplished as indicated herein.

By *[Signature]* Resident Engineer
 Date 4-30-93

GENERAL NOTES

NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found on microfilm in Central Files.

1. THE CIRCLED REFERENCE SYSTEM USED IN THESE PLANS WORKS AS FOLLOWS: (4/53) MEANS DETAIL 4 ON SHEET 3.

SEE SHEET 2 FOR OTHER GENERAL NOTES

HYDRAULIC DATA

Q10 = 563.7
 Q50 = 566.0
 Q100 = 567.0

CONVENTIONAL SIGNS

- COUNTY LINE ————
- TOWN LINE - - - - -
- LIMITS OF ACCESS ————
- POINT OF ACCESS ———— X
- FENCE LINE ———— X
- STONE WALL ————
- TRAVELED WAY ————
- GUARD RAIL ————
- RAILROAD ————
- SURVEY LINE ————
- CULVERT ————
- POWER POLE ————
- TELEPHONE POLE ————
- TREES ————
- CONTROL OF ACCESS ————
- PROPERTY LINE ————
- R.O.W. TAKING LINE ————
- SLOPE RIGHTS ———— SR
- TOP OF CUT ————
- TOE OF SLOPE ————

DATUM
 VERTICAL N/A
 HORIZONTAL N/A

STATE OF VERMONT
 AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT
 BRIDGE PROJECT

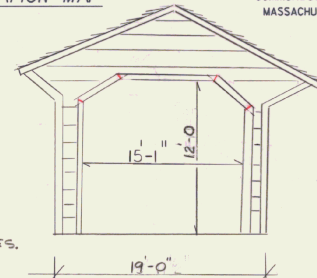
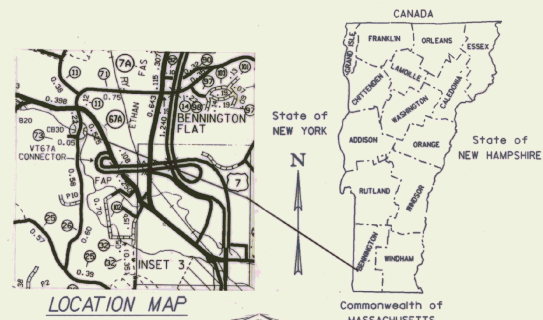
TOWN OF BENNINGTON
 COUNTY OF BENNINGTON

ROUTE NO : TH 26 CL2 BRIDGE NO : 30

PROJECT LOCATION : BEGINNING ON TH 26 IN THE TOWN OF BENNINGTON, APPROXIMATELY 0.22 MILES SOUTH OF THE TH 26 - VT RTE. 67-A INTERSECTION, EXTENDING NORTHEASTLY ALONG TH 26 FOR 0.024 MILES.

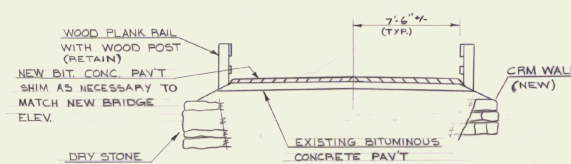
PROJECT DESCRIPTION : REPAIR EXISTING COVERED BRIDGE, REPAIR AND REPLACEMENT OF DECAYED AND DAMAGED MATERIAL

LENGTH OF STRUCTURE : 82.25 FEET.
 LENGTH OF PARTICIPATION ROADWAY : 40.00 FEET.
 LENGTH OF NON-PARTICIPATION ROADWAY : 0.0 FEET.
 LENGTH OF PROJECT : 122.25 FEET.

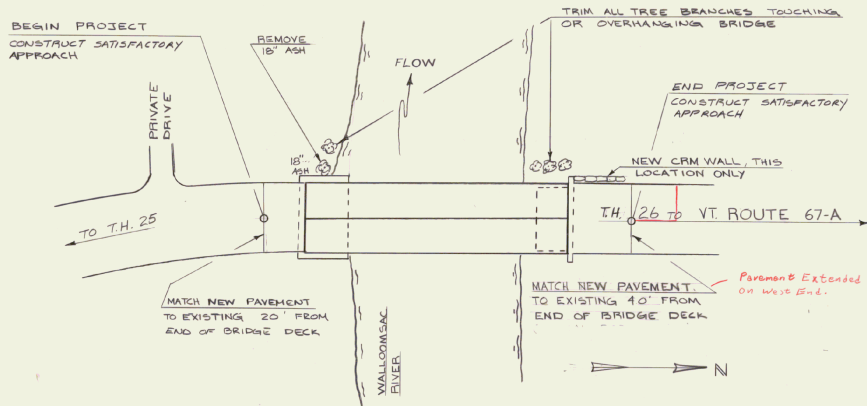


TYPICAL BRIDGE SECTION (NTS)

~ EXISTING BRIDGE DATA ~
 TOWN LATTICE TRUSS, WOOD COVERED BRIDGE
 70' CLEAR SPAN, 90' OVERALL
 BUILT 1889
 14' ROADWAY
 15'- TO STREAMBED



TYPICAL ROADWAY SECTION (LOOKING SOUTH)



PLAN VIEW SKETCH

SILK COVERED BRIDGE

SUBMITTED BY ORDER OF THE STATE TRANSPORTATION BOARD
 APPROVED: *[Signature]* DATE 4/12/93
 DIRECTOR OF PLANNING & PRECONSTRUCTION

TH 26 BR 30 OVER
 WALLOOMSAC RIVER

PROJECT BENNINGTON
 PROJECT NO. TH 3602

SHEET 1 OF 6 SHEETS

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE CHIEF ENGINEER
 CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1990, FOR USE ON THESE PROJECTS, INCLUDING ALL SUBSEQUENT PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

BRIDGE QUANTITY SHEET

GENERAL NOTES

NO.	ITEM	UNIT					GRAND TOTAL
204.25	STRUCTURE EXCAVATION	CY	50				50
204.30	GRANULAR BACKFILL FOR STRUCTURES	CY	30				30
301.15	SUBBASE OF GRAVEL	CY	23				23
406.25	BITUMINOUS CONCRETE PAVEMENT	TON	12				12
502.10	SHORING SUPERSTRUCTURE	LS.	1				1
506.60	STRUCTURAL STEEL	lb.	940				940
513.20	STRUCTURE PAINTING (MODIFIED)	LS.	1				1
522.20	STRUCTURAL LUMBER AND TIMBER - UNTREATED -	MFBM	0.5				0.5
522.25	STRUCTURAL LUMBER AND TIMBER - TREATED	MFBM	7.7				7.7
522.30	NON-STRUCTURAL LUMBER - UNTREATED -	MFBM	3				3
529.20	PARTIAL REMOVAL OF STRUCTURE	EACH	1				1
602.15	CEMENT RUBBLE MASONRY	CY	30				30
621.75	REMOVING AND RESET GUARD RAIL	LF	50				50
635.10	MOBILIZATION	LS.	1				1
665.15	REMOVE EXISTING ROOF	S.F.	2340				2340
665.20	CEDAR SHINGLE ROOF	S.F.	2340				2340

GENERAL NOTES (CONT.)

- ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE AGENCY OF TRANSPORTATION'S STANDARD SPECIFICATIONS FOR CONSTRUCTION, DATED 1990, AND ITS LATEST REVISIONS, AND THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, DATED 1989, AND ITS LATEST REVISIONS.
- THE EXISTING SUPERSTRUCTURE SHALL BE JACKED AND SHORED. MAINTAIN TRUSSES IN EXISTING CONDITION MINIMUM FOR STRAIGHTNESS AND PLUMB. THIS WORK SHALL BE PAID FOR AS ITEM 502.10, "SHORING SUPERSTRUCTURE".
- DIMENSIONS OF EXISTING MATERIALS ARE APPROXIMATE AND SHALL BE CONFIRMED BY THE CONTRACTOR PRIOR TO THE START OF WORK.
- ALL BRIDGE MEMBERS AND CONNECTIONS SHALL BE INSPECTED. DECAYED, DAMAGED AND/OR DISTRESSED MATERIAL SHALL BE REPAIRED. MATERIAL REPLACED SHALL MATCH ORIGINAL SHAPE AND DIMENSIONS AS CLOSELY AS POSSIBLE.
- ALL BOARD SHEATHING SHALL BE REMOVED WITH CARE, AS NECESSARY, TO ALLOW CAREFUL INSPECTION AND REPAIR OF THE BRIDGE, AND PAID FOR AS ITEM 529.20, "PARTIAL REMOVAL OF STRUCTURE". ALL DAMAGED BOARD SHEATHING SHALL BE REMOVED. ALL SHEATHING IN GOOD CONDITION SHALL BE SAVED AND REUSED AS DIRECTED BY THE ENGINEER. NEW BOARDS SHALL BE THE SAME CONFIGURATION AS EXISTING, FASTENED WITH 10# GALVANIZED NAILS IN A MANNER MATCHING EXISTING APPEARANCE. LOOSE SHEATHING SHALL BE REATTACHED SIMILAR TO NEW.
- THE OUTSIDE OF THE BRIDGE, THE SPLASH BOARDING NEAR THE ENTRANCES TO THE BRIDGE, TRIM, AND TRUSS MEMBERS EXPOSED BY THE WINDOW SHALL BE PRIMED AND PAINTED. FINISH COATS OF PAINT SHALL BE FLAT BARN RED WITH WHITE TRIM. PLACED IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS TO MATCH EXISTING COLOR SCHEME. PAINTING SHALL BE DONE AS ITEM 513.20, "STRUCTURE PAINTING (MODIFIED)", IN ACCORDANCE WITH THIS NOTE AND APPLICABLE SPECIFICATIONS IN SECTION 513, "PAINTING".
- THE EXISTING ROOFING SHALL BE REMOVED AS ITEM 665.15 REMOVE EXISTING ROOF. CARE SHALL BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE RIVER. THE ROOF BOARDING AND RAFTERS SHALL BE INSPECTED, AND ANY DISTORBERED OR DAMAGED MEMBERS SHALL BE REPLACED AS DIRECTED BY THE ENGINEER.
- NEW ROOFING SHALL BE RED CEDAR SHINGLES INSTALLED OVER 15-4800-PEST-PAPER, WITH 5" REVEAL, IN ACCORDANCE WITH SPECIAL PROVISIONS, SECTION 665.
- ANY IN-STREAM WORK SHALL BE ACCOMPLISHED DURING THE PERIOD JUNE 1 TO OCTOBER 1. ANY DEVIATION FROM THIS REQUIREMENT SHALL BE APPROVED IN WRITING BY THE AGENCY OF NATURAL RESOURCES.
- ALL STEEL ITEMS AND NAILS SHALL BE GALVANIZED IN ACCORDANCE WITH AASHTO M-111 OR M-232. ALL HARDWARE, EXCEPT NAILS, SHALL BE PAID FOR AS ITEM 506.60.
- THE COST OF NAILS SHALL BE SUBSIDIARY TO LUMBER AND TIMBER ITEMS. ALL NAILING SHALL BE AS SHOWN ON THESE PLANS, OR AS DIRECTED BY THE ENGINEER. NAIL HOLES WILL BE PRE-DRILLED, IF NECESSARY, TO AVOID SPLITTING.
- ALL BOLTS, NUTS AND WASHERS SUPPLIED SHALL BE A COMMON TYPE, MEETING ASTM A-307, GALVANIZED, AND LONG ENOUGH TO ALLOW A 1/2" MINIMUM PROJECTION BEYOND THE NUT IN SNUG POSITION, AND SHALL HAVE SUFFICIENT THREAD LENGTH TO TIGHTEN AS MUCH AS NECESSARY.
- ALL BOLT HOLES SHALL BE 1/16" LARGER THAN THE BOLT. A SPECIAL EFFORT SHALL BE MADE TO DRILL HOLES PERPENDICULAR TO ADJACENT MEMBERS.
- LAG SCREWS/BOLTS SHALL BE A COMMON TYPE MEETING ASTM A-307, AND SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 522.09(a).
- NEW DECK MATERIAL USED TO REPLACE DECK REMOVED TO WORK ON TRUSS MEMBERS SHALL BE 2" X 8" TIMBERS, SURFACED 4 SIDES, PLACED ON EDGE PARALLEL TO ROADWAY CENTERLINE. SPIKING OF TIMBERS SHALL BE AS PER SUBSECTION 522.12(b). EVERY OTHER TIMBER SHALL BE TOE NAILED TO EVERY OTHER FLOOR BEAM WITH 10# GALVANIZED NAILS.
- DECAYED AND/OR DAMAGED AREAS IN THE DECK SHALL BE REPAIRED BY REMOVING UNSOUND MATERIAL TO SOLID WOOD OR 1/2" +/- MINIMUM AND PATCHING IN NEW WOOD. PATCH SHALL BE GLED IN PLACE WITH WATERPROOF EXTERIOR GLUE, AND SHALL BE FLUSH WITH EXISTING DECK SURFACE.
- ALL TREE BRANCHES TOUCHING AND/OR OVERHANGING THE BRIDGE SHALL BE TRIMMED. THE 18" ASH NEXT TO THE SOUTHWEST CORNER OF THE BRIDGE SHALL BE CUT DOWN. ANY DAMAGE TO THE BRIDGE FROM CUTTING AND TRIMMING TREES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. TRIMMING AND CUTTING OF TREES - SUBSIDIARY TO ALL OTHER ITEMS.
- ALL DIRT AND DEBRIS SHALL BE CLEANED FROM ON AND AROUND ALL TRUSS MEMBERS AND BEARING BLOCKS, AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN LUMP SUM BID FOR "PARTIAL REMOVAL OF STRUCTURE".

- REPLACE MISSING STONES AT THE NORTH ABUTMENT AS DRY STONE MASONRY. TO BE PAID FOR AS ITEM 602.15, "CEMENT RUBBLE MASONRY".
- ALL NEW LUMBER AND TIMBER SHALL BE SURFACED OR ROUGH SAWN TO NOMINAL DIMENSIONS (1/4" TOLERANCE) TO MATCH EXISTING MATERIAL, IN ACCORDANCE WITH AASHTO M-168.
- ROOF BOARDING, WALL SHEATHING, COLLAR TIES AND CURB CAN BE ANY NATIVE WOOD SPECIES, TO BE PAID FOR AS ITEM 522.30, "NON-STRUCTURAL LUMBER - UNTREATED".
- RIDGE BEAMS, RAFTERS, TIE BEAMS AND TOP CHORD BRACING SYSTEM SHALL BE MINIMUM GRADE NO. 1 EASTERN SPRUCE, NO. 2 SOUTHERN PINE, OR NO. 2 DOUGLAS FIR, TO BE PAID FOR AS ITEM 522.20, "STRUCTURAL LUMBER AND TIMBER - UNTREATED".
- TOP AND BOTTOM CHORD MATERIAL, LATTICE MEMBERS AND DECK MATERIAL SHALL BE MINIMUM GRADE NO. 1 SOUTHERN PINE OR NO. 1 DOUGLAS FIR, TREATED AS DIRECTED IN THE SPECIAL PROVISIONS, AND PAID FOR AS ITEM 522.25, "STRUCTURAL LUMBER AND TIMBER - TREATED".
- REMOVE ENOUGH ROOFING, SIDING AND FLOOR DECK TO ALLOW COMPLETE INSPECTION OF TRUSSES, THE RESIDENT ENGINEER AND THE CONTRACTOR SHALL INSPECT EXPOSED MEMBERS AND DETERMINE REPAIRS NECESSARY. REPAIR METHODS WILL BE AS SHOWN ON THESE PLANS. TRUSS REPAIR SCHEME SHALL BE DETAILED BY THE CONTRACTOR, WITH INPUT FROM THE RESIDENT ENGINEER. SCHEME SHALL BE SUBMITTED TO THE STRUCTURES ENGINEER FOR REVIEW AND APPROVAL.
- BEARING BLOCKS SHALL BE WHITE OAK HEARTWOOD, TREATED WITH 2 COATS OF CUPRENOL OR EQUIVALENT SURFACE WOOD PRESERVATIVE TREATMENT AS RECOMMENDED BY THE PAINT MANUFACTURER. TO BE PAID FOR AS ITEM 522.25, "STRUCTURAL LUMBER AND TIMBER - TREATED".
- TIMBER QUANTITIES ARE ESTIMATED ASSUMING THAT A PERCENTAGE OF THE TOTAL AMOUNT FOR EACH ITEM IS REPLACED.

ITEM ESTIMATED % REPLACEMENT

NON-STRUCTURAL LUMBER (UNTREATED)	
SIDING	50% (0.85 MFBM)
SPLASH BOARDING	50% (0.22 MFBM)
PORTALS	25% (0.05 MFBM)
ROOF BOARDING	87% (1.57 MFBM)
COLLAR TIES	60% (0.28 MFBM)
CURB	0%

STRUCTURAL LUMBER AND TIMBER (UNTREATED)

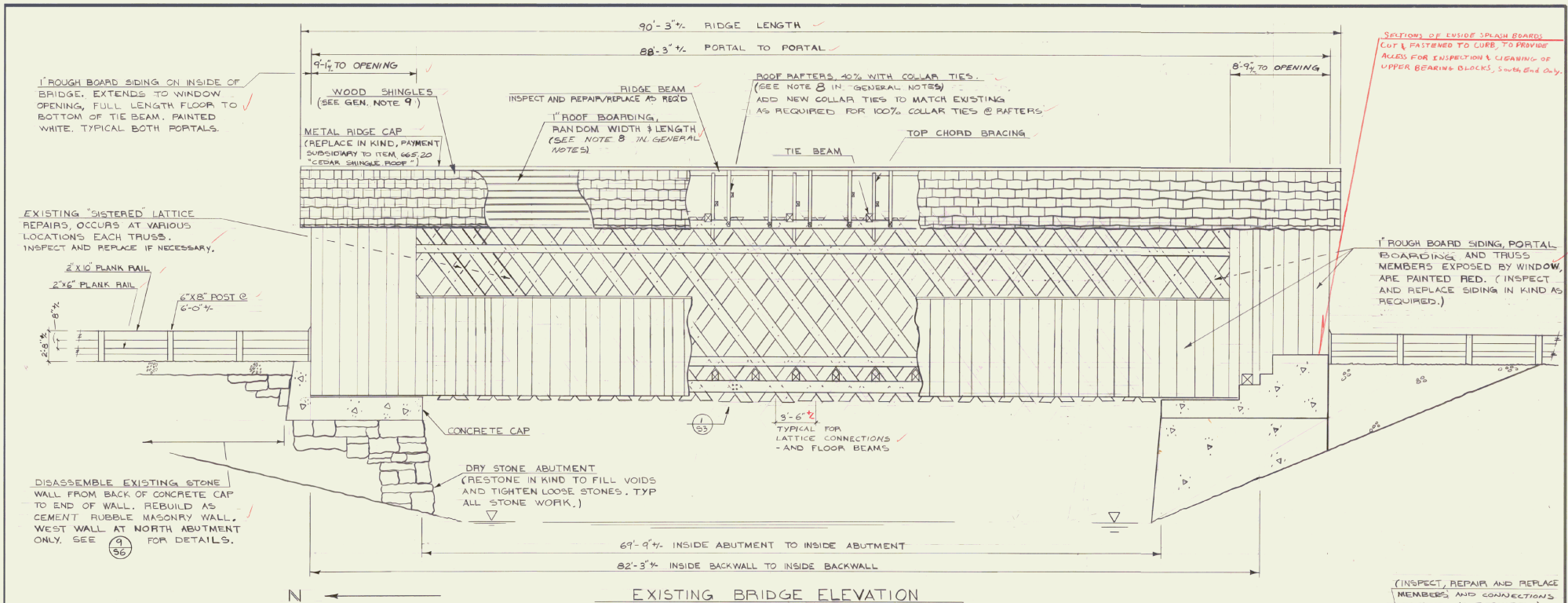
ROOF RAFTERS	50% (0.20 MFBM)
RIDGE BEAM	50% (0.04 MFBM)
TIE BEAMS	23% (0.19 MFBM)

STRUCTURAL LUMBER AND TIMBER (TREATED)

FLOOR DECK	44% (3.94 MFBM)
FLOOR BEAMS	0%
BOTTOM CHORD	25% (0.98 MFBM)
TOP CHORD	44% (1.36 MFBM)
LATTICE	19% (1.00 MFBM)
BEARING BLOCKS	100% (0.39 MFBM)

STATE OF VERMONT AGENCY OF TRANSPORTATION

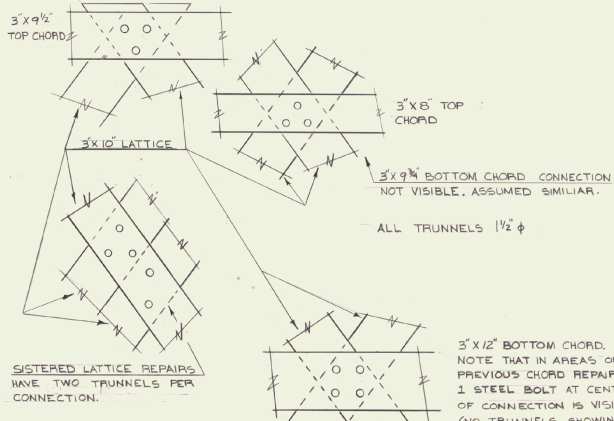
Town of BENNINGTON	Bridge No. 30
Highway No. TH 26	Log Sta. Surv. Sta.
QUANTITIES, GENERAL NOTES, LIST OF SHEETS SILK ROAD COVERED BRIDGE	
Designed By G. NEWBURY	Drawn By G. NEWBURY
Checked By J.B. MCCARTHY 12/90	Date R. GLENDEAN Date 1/92
PROJECT NO. BENNINGTON TH 3802	
I.G.C. Info. PPF-BRONTYSHT	
Bridge Sheet No. 2	Sheet 2 of 6



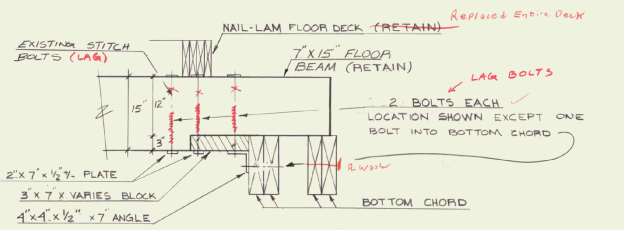
EXISTING BRIDGE ELEVATION

(LOOKING EAST)

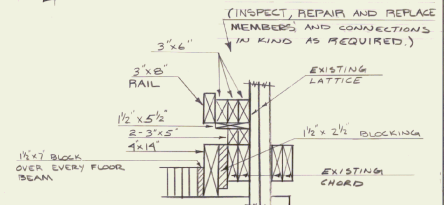
SCALE 1/4" = 1'-0"



(1) TRUNNEL PATTERNS (EXISTING)
SCALE: 1" = 1'-0"



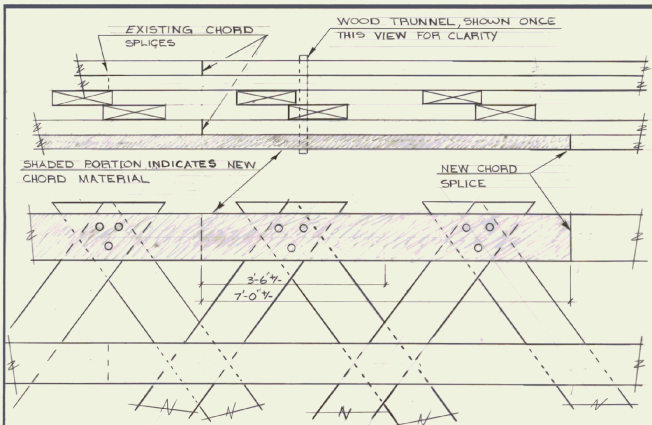
(2) FLOOR BEAM DETAILS (EXISTING)
SCALE 1" = 1'-0"



(3) EXISTING CURB DETAIL
SCALE 3/4" = 1'-0"

STATE OF VERMONT AGENCY OF TRANSPORTATION	
Town Of BENNINGTON	Bridge No. 30
Highway No. TH. 26	Log Sta.
	Surv. Sta.
SILK ROAD COVERED BRIDGE OVER WALLOOBSAC RIVER - EXISTING BRIDGE DETAILS	
Designed By G. NEWBURY	Drawn By G. NEWBURY
Checked By J. B. MCCARTHY 12/90	Date P. GENDRON Date AUGUST 1992
PROJECT BENNINGTON	PROJECT NO. TH 3802
L&C info.	
Bridge Sheet No. 3	Sheet 3 of 6

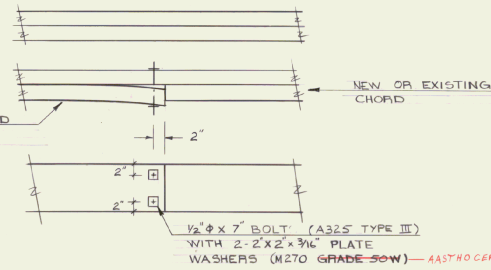
BR-6



NEW CHORD MATERIAL WILL BE CONNECTED TO OTHER CHORDS AND LATTICE BY WOOD TRUNNELS USING EXISTING PATTERN. TRUNNELS WILL EXTEND THROUGH ALL 4 CHORD MEMBERS AND BOTH (2) LATTICE MEMBERS.

INSPECT CHORDS. REPLACE ALL DECAYED AND/OR DAMAGED CHORD MATERIAL. WHEN ADJACENT CHORDS IN THE SAME LINE ARE TO BE REPLACED, JOINTS SHOULD BE ELIMINATED WHEN POSSIBLE BY USING LONGER MEMBERS. HOLD NEW CHORD SPLICES 7'-0 1/2\"/>

FULL BEARING IS REQUIRED AT ALL SPLICES.



WHEN EXISTING CHORD MATERIAL IS SOUND AND SPLICE IS OUT OF LINE, REPAIR AS FOLLOWS: DETERMINE CAUSE OF THE MISALIGNMENT, SUCH AS PROBLEMS WITH NEARBY LATTICE MEMBERS, DOWELS, ETC. REPAIR THESE AREAS FIRST. IF CHORD STILL OUT OF LINE, CLAMP BACK INTO PLACE AND INSTALL BOLTS AND WASHERS SHOWN ABOVE. AVOID CRUSHING OF WOOD. NEW CHORD MEMBERS SHALL BUTT TIGHT AGAINST EXISTING CHORD MEMBERS.

- ALL TRUNNELS SHALL BE INSPECTED FOR DAMAGE, DECAY OR DISTRESS. INSPECTION SHALL INCLUDE VISUAL, TAPPING WITH A HAMMER AND TWISTING WITH PLIERS.
- ALL DAMAGED, DECAYED, DISTRESSED AND/OR LOOSE TRUNNELS SHALL BE REPLACED. ALL NEW TRUNNELS SHALL BE WHITE OAK AND THE SAME DIAMETER AS THE HOLES TO PROVIDE A TIGHT FIT.
- NEW TRUNNELS SHALL MATCH EXISTING IN APPEARANCE AND MAY BE WAXED (EXAMPLE: PARAFIN OR OTHER) TO AID INSERTION.

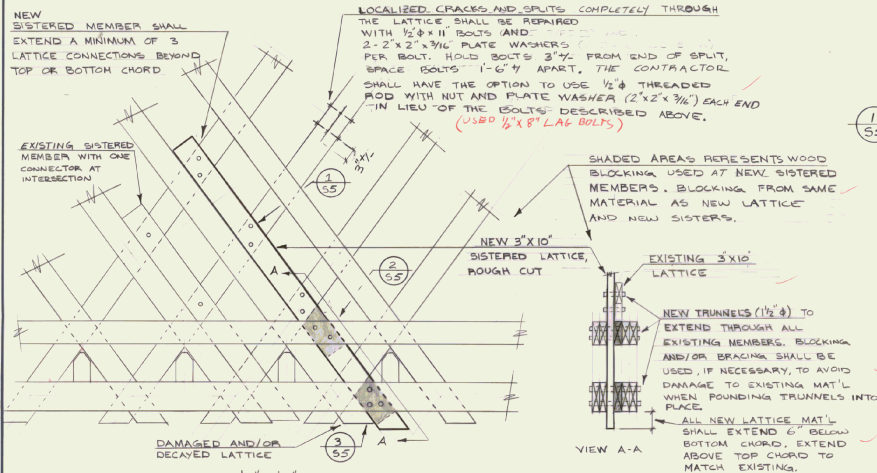
TRUNNEL REPAIRS

6 TOP CHORD SPLICE REPAIR

SCALE 1"=1'-0"

4 TOP CHORD REPLACEMENT DETAILS

SCALE 1"=1'-0"

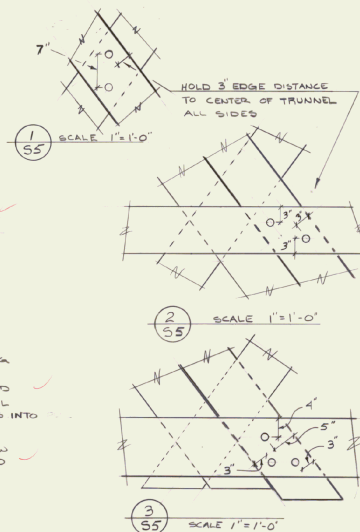


NEW SISTERED MEMBER SHALL EXTEND A MINIMUM OF 3 LATTICE CONNECTIONS BEYOND TOP OF BOTTOM CHORD.

LOCALIZED CRACKS AND SPLITS COMPLETELY THROUGH THE LATTICE SHALL BE REPAIRED WITH 1/2\"/>

SHADED AREAS REPRESENTS WOOD BLOCKING USED AT NEW SISTERED MEMBERS. BLOCKING FROM SAME MATERIAL AS NEW LATTICE AND NEW SISTERS.

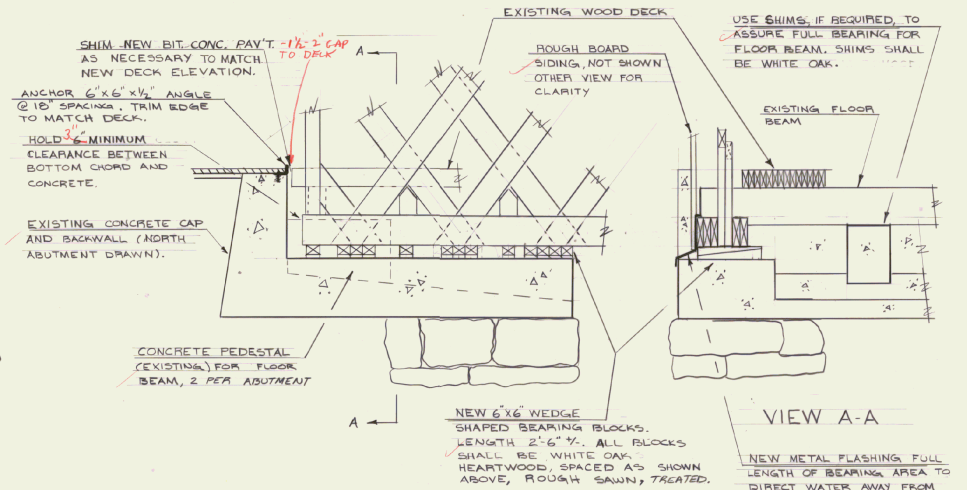
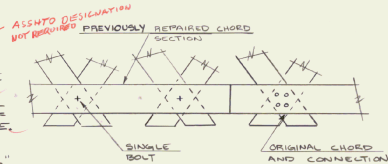
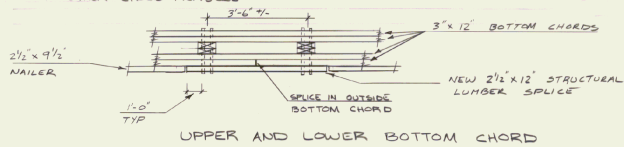
NEW TRUNNELS (1/2\"/>



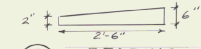
- ALL LATTICE MEMBERS SHALL BE INSPECTED. DAMAGED, DECAYED OR DISTRESSED MEMBERS SHALL BE REPAIRED AS DETAILED IN THE FOLLOWING NOTES & DETAILS.
- THE PREFERRED LATTICE REPAIR METHOD SHALL BE TO SISTER AN ADDITIONAL MEMBER NEXT TO THE AFFECTED LATTICE AS SHOWN IN THE DETAILS TO THE LEFT.
- IF THE LATTICE MEMBER SHOWS DISTRESS AT BOTH ENDS OR ALONG A MAJORITY OF ITS LENGTH, IT SHALL BE REMOVED AND REPLACED.
- SEVERELY DAMAGED, DECAYED OR DISTRESSED LATTICE MEMBERS SHALL BE REPLACED IN ENTIRETY USING EXISTING TRUNNEL PATTERNS.
- REPAIR DAMAGE TO EXISTING SISTERS BY REPLACEMENT WITH NEW SISTERS ACCORDING TO DETAILS SHOWN HERE. USE EXISTING TRUNNEL PATTERNS EXCEPT WHERE ONLY ONE CONNECTOR WAS USED. A MINIMUM OF 2 TRUNNELS PER CONNECTION SHALL BE USED.
- SKETCHES SHOWN HERE FOR SISTERING TO THE BOTTOM CHORD ALSO PERTAIN TO LATTICE REPAIRS AT THE TOP CHORD (NOT DRAWN). USE EXISTING TRUNNEL PATTERNS WHERE SISTERS MEET THE TOP CHORDS.
- NEW LATTICE MATERIAL SHALL MATCH EXISTING IN SIZE AND APPEARANCE.

STATE OF VERMONT	
AGENCY OF TRANSPORTATION	
Town Of BENNINGTON	Bridge No. 30
Highway No. T.H. 26	Log Sta.
	Surv. Sta.
SILK ROAD COVERED BRIDGE OVER	
WALLOONSAC RIVER - GENERAL REPAIR DETAILS	
Designed By G. NEWBURY	Drawn By G. NEWBURY
Checked By J.B. MCCARTHY Date 12/70	Bridge Design Supervisor P. GENDRON Date 1/70
PROJECT BENNINGTON	PROJECT NO. TH 3802
L.C. Info.	
Bridge Sheet No. 5	Sheet 5 of 6

- REMOVE ENOUGH DECK, GUARD RAIL AND SIDING TO ALLOW INSPECTION OF BOTTOM CHORDS. REPLACE IN KIND AFTER COMPLETION OF TRUSS WORK. RETAIN EXISTING FLOOR BEAMS.
- DECAYED, DAMAGED AND/OR DISTRESSED CHORD MEMBERS SHALL BE REPLACED.
- NEW CHORD MATERIAL LENGTHS SHALL BE AS DIRECTED BY THE ENGINEER. EMPHASIS WILL BE ON USING LONGER MEMBERS TO REDUCE JOINTS--ESPECIALLY NEAR ABUTMENTS. HOLD NEW JOINTS 7'-0" FROM EXISTING JOINTS.
- CONNECT NEW CHORD MATERIAL WITH NEW TRUNNELS USING EXISTING TRUNNEL PATTERNS. SEE SHEET 5 FOR NEW TRUNNEL INFORMATION.
- STEEL PLATE WASHERS, ANGLES AND BOLTS LOCATED AT ENDS OF FLOOR BEAMS THAT NEED TO BE REMOVED FOR CHORD REPAIRS SHALL BE REPLACED WITH NEW M270, GRADE 36 STEEL (GALVANIZED) AND A307 TYPE I BOLTS (GALVANIZED), SAME SIZE AS EXISTING.
- EXISTING CHORD SECTIONS THAT SHOW ONE BOLT AT CHORD TO LATTICE CONNECTIONS (FROM A PREVIOUS REPAIR) SHALL BE REMOVED AND NEW TRUNNELS USED IN ORIGINAL PATTERNS. REMOVE BOLT, GLUE ENOUGH FITTING, DOWEL IN HOLE, FULL WIDTH, WITH WATERPROOF GLUE.
- THE 2 1/2" x 9/16" NAILER ON THE OUTSIDE OF THE LOWER BOTTOM CHORD SHALL BE REPLACED WITH 2 1/2" x 12" STRUCTURAL LUMBER. IN THE VICINITY OF SPLICES IN THE OUTSIDE 3" x 12" CHORD MEMBER, THE NEW 2 1/2" x 12" PIECES SHALL BE CONNECTED WITH NEW TRUNNELS IN EXISTING PATTERN TO OTHER CHORD MEMBERS.

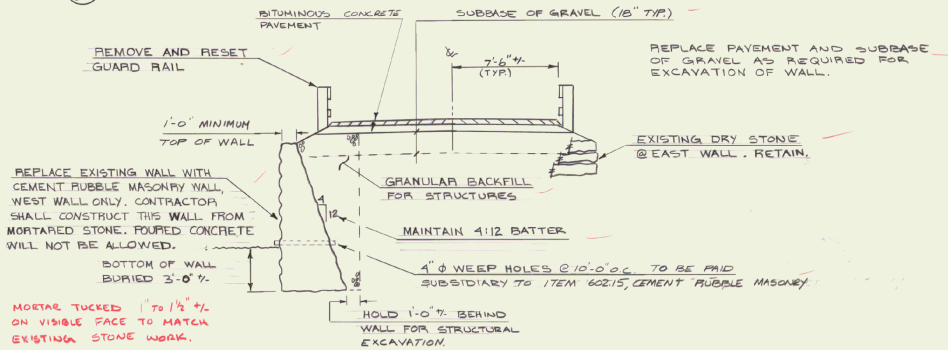


- REPLACE ALL BEARING BLOCKS AS DIRECTED ABOVE TO ASSURE FULL BEARING OF TRUSS CHORD MEMBERS
- SHIM FLOOR BEAM SUPPORTED ON CONCRETE PEDESTALS, IF REQUIRED FOR FULL BEARING
- REPLACE MISSING STONES AT NORTH ABUTMENT TO FILL VOIDS AND TIGHTEN ALL LOOSE STONES
- MAINTAIN 2" MINIMUM TOE HEIGHT ON WEDGE BLOCKS



8 BEARING AREA DETAILS
SCALE 1/2" = 1'-0"

7 BOTTOM CHORD REPAIR DETAILS



9 CRM WALL, NORTH ABUTMENT
NOT TO SCALE

STATE OF VERMONT AGENCY OF TRANSPORTATION	
Town Of BENNINGTON	Bridge No. 30
Highway No. T.H. 26	Log Sta.
	Surv. Sta.
SILK ROAD COVERED BRIDGE OVER WALLOONSAC RIVER - BRIDGE REPAIR DETAILS	
Designed By G. NEWBURY	Drawn By G. NEWBURY
Checked By J.B. Mc CARTHY	Bridge Design Supervisor R. GENDRON
Date 12/90	Date 1/90
PROJECT BENNINGTON	PROJECT NO. TH 3802
L&C info.	
Bridge Sheet No. 6	Sheet 6 of 6