

STATE OF VERMONT

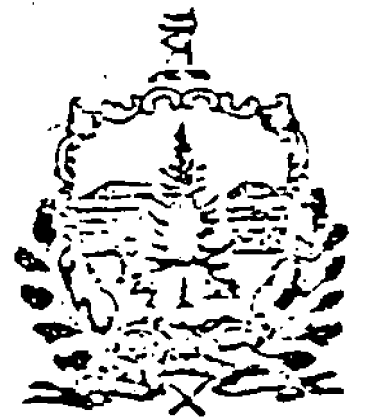
MAY 31 1985 AGENCY OF TRANSPORTATION

Date MAY 31 1985

*Cooley Asphalt Paving*  
Contractor

*Wilfred A. Laprade*  
Signature

President  
Title



CONTRACT PLANS

THESE PLANS DO NOT REFLECT  
CHANGES MADE ON THE PROJECT.

PROPOSED IMPROVEMENT

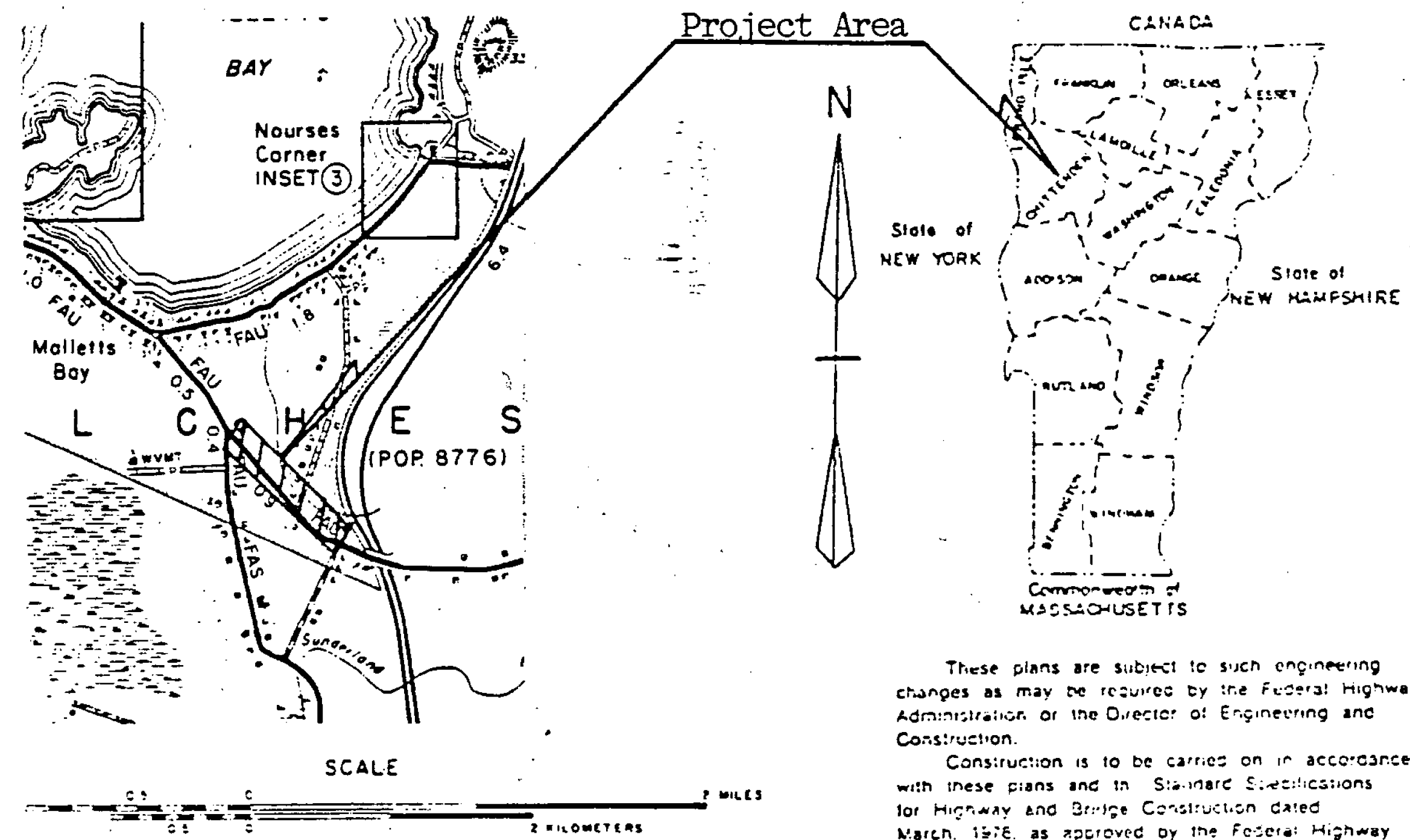
*Anna Champet*  
Transportation Secretary Signature  
RESURFACING PROJECT

TOWN OF: Colchester

COUNTY OF: Chittenden

ROUTE NO: TH #9

ROUTE CLASS: 2



- Index of Sheets
1. Title Sheet
  2. Project Description and Location
  - 3-4. Typical Sections & Design Data
  - 5-6. Project Lengths & Item Quantities
  - 7-9. Plan Detail of Intersections
  - 10-14. Highway Marking Details
  15. Standard Sheet E-4 (03/04/81 R)
  16. Standard Sheet E-6 (04/01/80 R)
  17. Standard Sheet E-8 (06-15-83R)
  18. Standard Sheet E-50 (03-16-82R)

These plans are subject to such engineering changes as may be required by the Federal Highway Administration or the Director of Engineering and Construction.  
Construction is to be carried on in accordance with these plans and in Standard Specifications for Highway and Bridge Construction dated March, 1978, as approved by the Federal Highway Administration on October 27, 1978 for use on this project including all subsequent revisions and such revised specifications and special provisions as are incorporated in these plans.

PROJECT PROCESSED UNDER  
SECONDARY ROAD PLAN

SUBMITTED BY CHIEF OF THE STATE TRANSPORTATION BOARD

APPROVED *Frank Stinch* DATE \_\_\_\_\_  
DIRECTOR OF ENGINEERING AND CONSTRUCTION

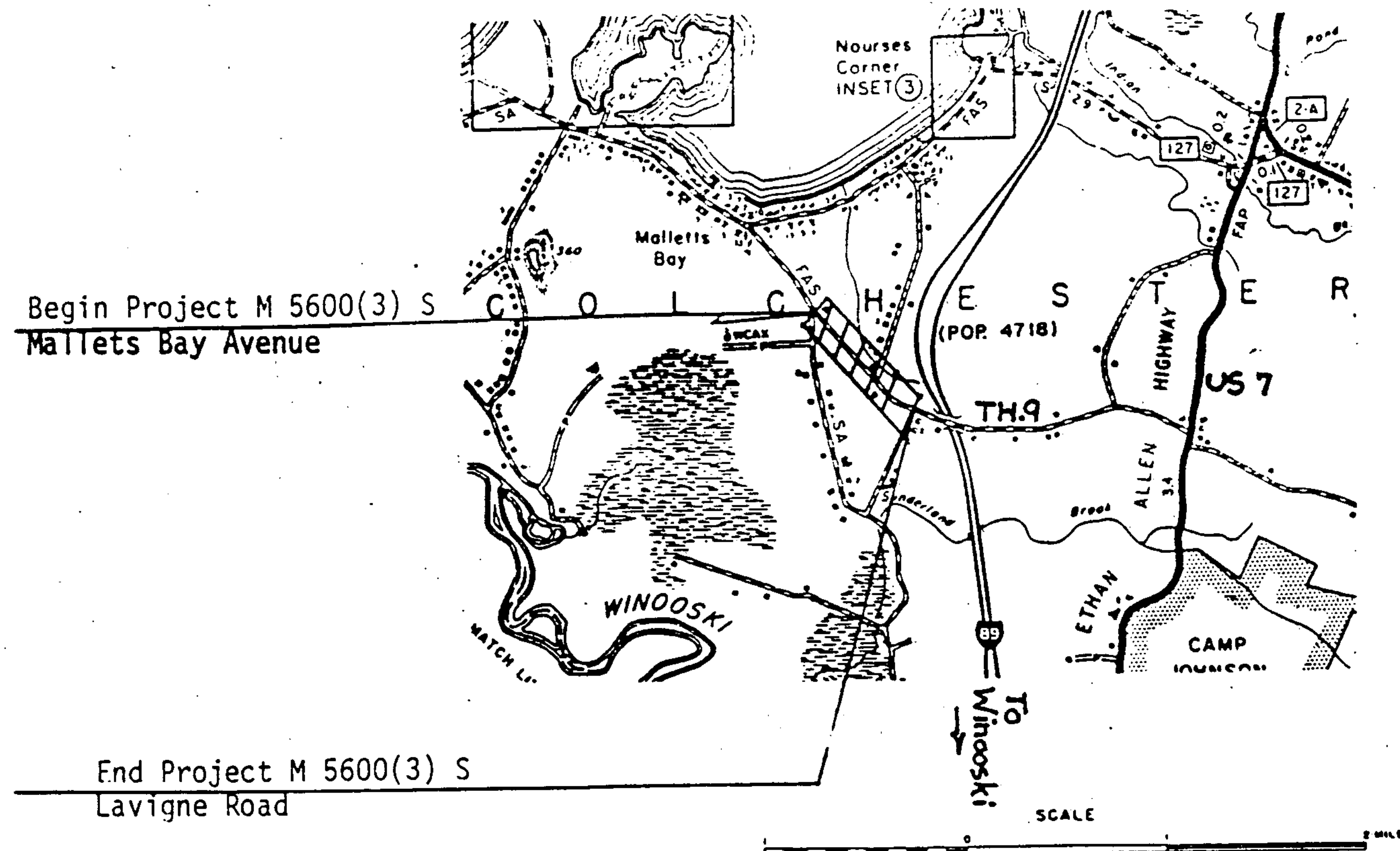
DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED \_\_\_\_\_ DATE \_\_\_\_\_  
DIVISION ADMINISTRATOR

PROJECT NO. M 5600(3) S

SHEET 1 OF 18 SHEETS

# PROJECT DESCRIPTION AND LOCATION



Begin Project M 5600(3) S  
Malletts Bay Avenue

End Project M 5600(3) S  
Lavigne Road

On TH #9, Blakely Road in the Town of Colchester, Beginning at the intersection of Malletts Bay Avenue and extending southeast 0.70 mile to the intersection of Lavigne Road.

Project Length 0.70 mile = 3696 feet.  
ADT - 5890  
V - 35

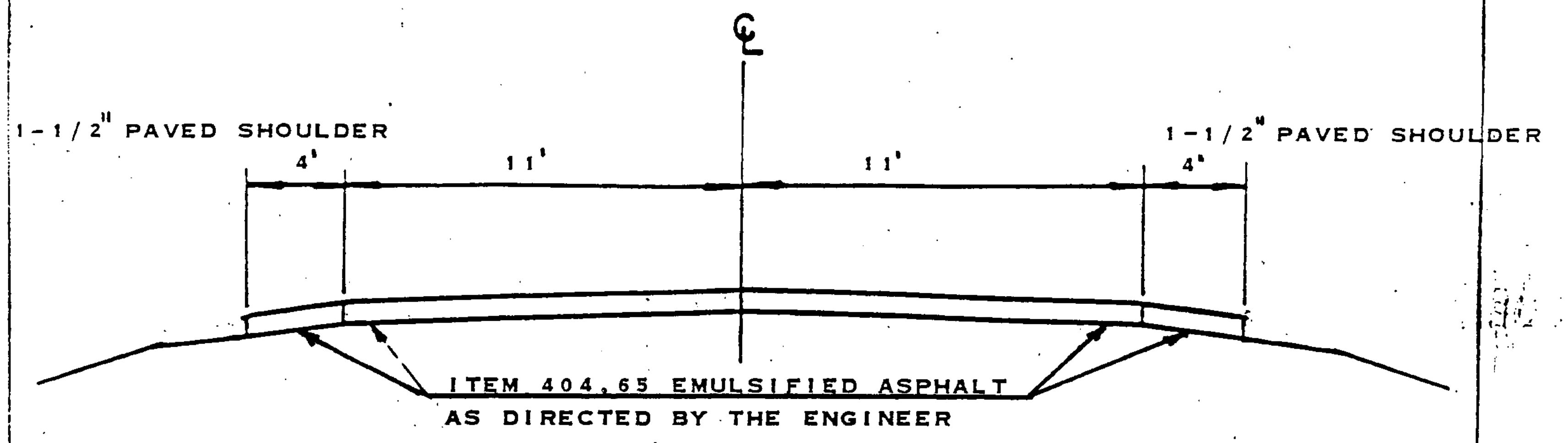
Work to be performed under this project includes leveling with 200 tons/mile and overlaying the existing roadway with 1½" bituminous concrete pavement and constructing 4' shoulders each side paved with 1½" bituminous concrete pavement.

PROJECT Colchester

NO. M 5600(3) S  
SHEET 2 OF 18 SHEETS

# TYPICAL SECTIONS & DESIGN DATA

ITEM 406.25 BITUMINOUS CONCRETE PAVEMENT.  
 LEVELING COURSE 200 TONS/MILE  
 1-1/2" WEARING COURSE TYPE III 22' WIDE ( $\pm 1/4$ ")  
 1-1/2" PAVED SHOULDERS 4' WIDE ( SEE SHEET 4 FOR SHOULDER  
 CONSTRUCTION DETAILS)



BEGIN PROJECT MALLET'S BAY AVENUE MM 0.00  
 END PROJECT LAVIGNE ROAD MM 0.70  
 0.70 MILE OR 3696 FEET

**URBAN GRASS SEED FORMULA**

%WT	LBS/A	
37.50	30	CREEPING RED FESCUE
31.25	25	KENTUCKY BLUEGRASS
<u>31.25</u>	<u>25</u>	PERENNIAL RYE GRASS
100.0	80	

**GENERAL NOTES:**

SEED TO BE APPLIED PER SEEDING FORMULA OR AS DIRECTED BY THE ENGINEER. FERTILIZER-FORMULA 10-20-10 TO BE USED WITH SEED, APPLIED AT A RATE OF 500 LBS/ACRE OR AS DIRECTED BY THE ENGINEER.

HAY MULCH TO BE PLACED ON EARTH SLOPES AT THE RATE OF 2 TONS/ACRE OR AS DIRECTED BY THE ENGINEER.

TOPSOIL TO BE USED WITH SEED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER

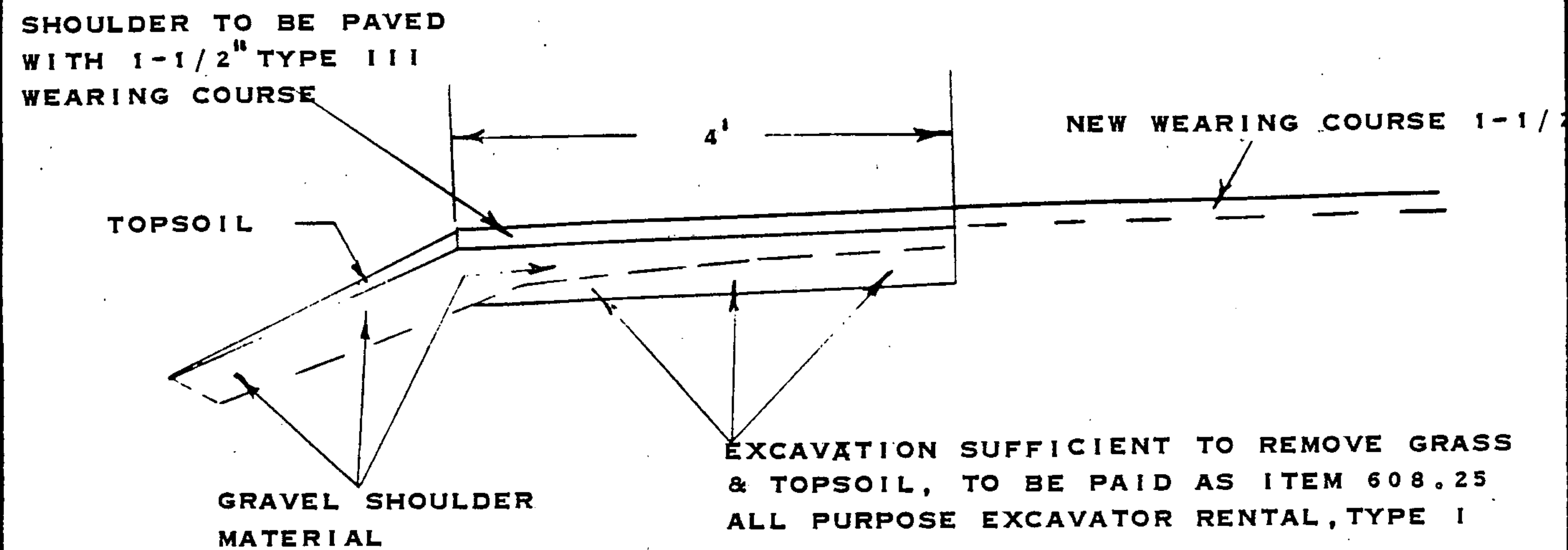
AGRICULTURAL LIMESTONE TO BE APPLIED AT RATE OF 2 TONS PER ACRE OR AS DIRECTED BY THE ENGINEER.

PROJECT COLCHESTER

NO. M 5600 (3)S  
 SHEET 3 OF 18

# TYPICAL SECTIONS & DESIGN DATA

## SHOULDER TYPICAL



1. WHERE SHOULDERS ARE LESS THAN 4' WIDE, THEY SHALL BE WIDENED TO 4' WHERE THE EXISTING DRAINAGE CAN BE MAINTAINED.
2. THE EXCAVATION OF THE EXISTING SHOULDERS SHALL BE ONLY TO THE DEPTH NECESSARY TO REMOVE GRASS AND TOPSOIL.
3. ITEMS 651.10-SEED (URBAN MIX), 651.15-FERTILIZER, 651.20-AGRICULTURAL LIMESTONE, 651.25-HAY MULCH AND 653.10-TOPSOIL, HAVE BEEN INCLUDED IN THE CONTRACT TO RESTORE LAWNS AND THE ROADSIDE TO AN ATTRACTIVE APPEARANCE.
4. MANY LARGE TREES EXIST NEAR THE EDGE OF THE ROAD IN THE VICINITY OF LAVIGNE ROAD, APPROXIMATE MM 0.55-MM0.70. THE TOWN HAS AGREED TO REMOVE THESE TREES PRIOR TO THE START OF THE PROJECT.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MOVING AND RESETTING MAILBOXES WITHIN THE PROJECT LIMITS. ALL ACTIVITIES CONCERNING RELOCATING MAILBOXES SHALL BE DONE IN COOPERATION WITH THE MAIL CARRIER(S) AND THE PROPERTY OWNER(S), AND EVERY EFFORT SHALL BE TAKEN TO KEEP THE INCONVENIENCE TO A MINIMUM. THE COST OF TAKING CARE OF MAILBOXES SHALL BE SUBSIDIARY TO ITEM 406.25 BITUMINOUS CONCRETE PAVEMENT.
6. LIMITS OF RIGHT-OF-WAY IS CONSIDERED TO BE 3 RODS. ALL CONSTRUCTION ACTIVITIES WILL BE CONFINED TO THE EXISTING HIGHWAY RIGHT-OF-WAY.
7. THE ELEVATION OF THE EXISTING MANHOLE AT APPROXIMATE MM 0.68 WILL BE adjusted by Colchester Fire Dist. #3.
8. THE TOWN WILL BE RESPONSIBLE FOR THE REMOVAL AND RESETTING OF THE EXISTING TRAFFIC SIGNS.
9. THE TOWN WILL IMPROVE THE INTERSECTION OF MALLETT'S BAY AVE AND BLACKELY ROAD. THE CONTRACTOR WILL BE RESPONSIBLE FOR OVERLAYING THE EXISTING INTERSECTION WITH 1 1/2" BITUMINOUS CONCRETE PAVEMENT.

PROJECT COLCHESTER

NO. M 5600(3)S

SHEET 4 OF 18 SHEETS

PROJECT LENGTHS AND ITEM QUANTITIES

ITEM DESCRIPTION	OPTION														
	LENGTH	WIDTH	OVERLAY DEPTH	LEVELING COURSE	GRAVEL SHOULDERS	GRAVEL SHOULDERS	EMULSIFIED ASPHALT	BITUMINOUS CONCRETE PAVEMENT	POWER BROOM RENTAL	CHANGE ELEV. OF CB, DI OR MH	UNIFORMED TRAFFIC OFFICERS	FLAGPERSONS	ALL PURP EXCAV RENTAL	TRUCK RENTAL	
ITEM NO	FT.	FT.	IN.	TONS/MI	402.10 CY.	402.11 TON	404.65 CWT	406.25 TON	608.30 HR.	604.40 EA.	630.10 HR.	630.15 HR.	608.25 HR	608.37 HR	
LOCATION															
10.00 MM 0.70	3696	22	1 1/2	200	275	452	25	753	6		65	65	40	15	
DRIVES								140							Level course 200 tons/mile
TH APPROACHES								21							
VED SHOULDERS								90							
OUNDING								274							
								22							
TOTALS	3696				275	452	25	1300	6		65	65	40	15	

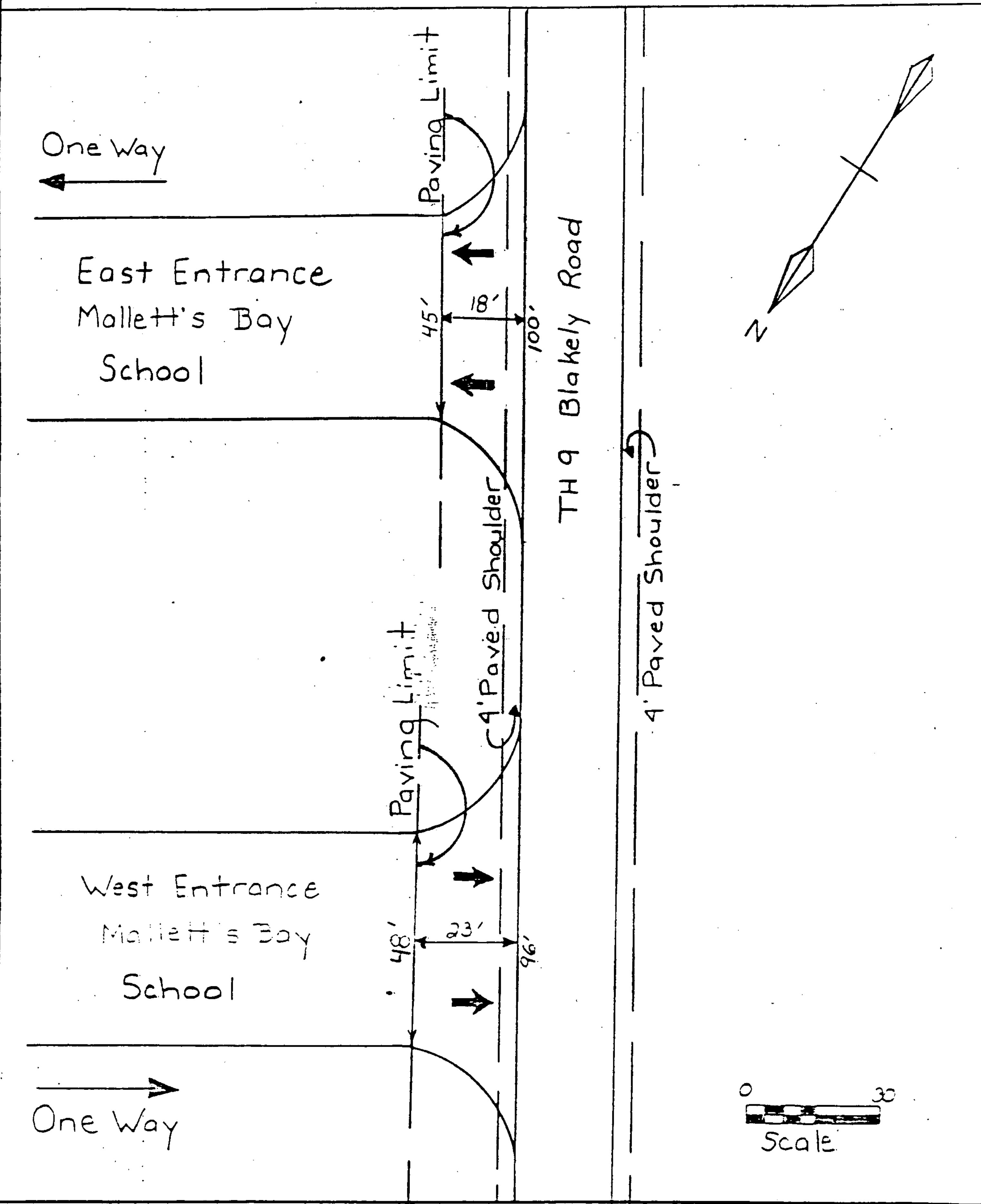
PROJECT COLCHESTER

NO. M. 5600(3)S

SHEET 5 OF 18



# PLAN DETAILS - INTERSECTIONS

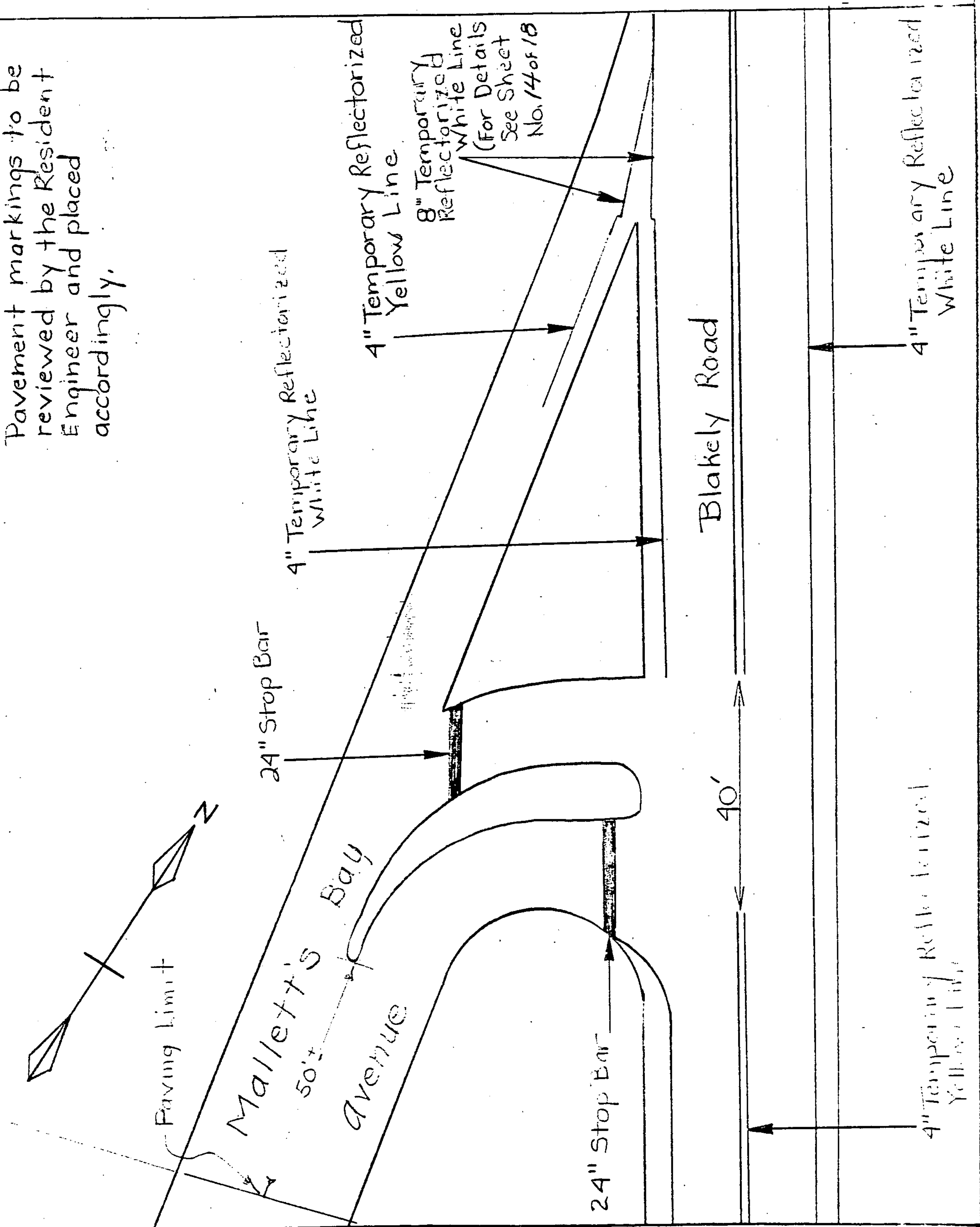


PROJECT Colchester

NO. M 5600(3)S  
SHEET 7 OF 18 SHEETS

Note:

Pavement markings to be reviewed by the Resident Engineer and placed accordingly.

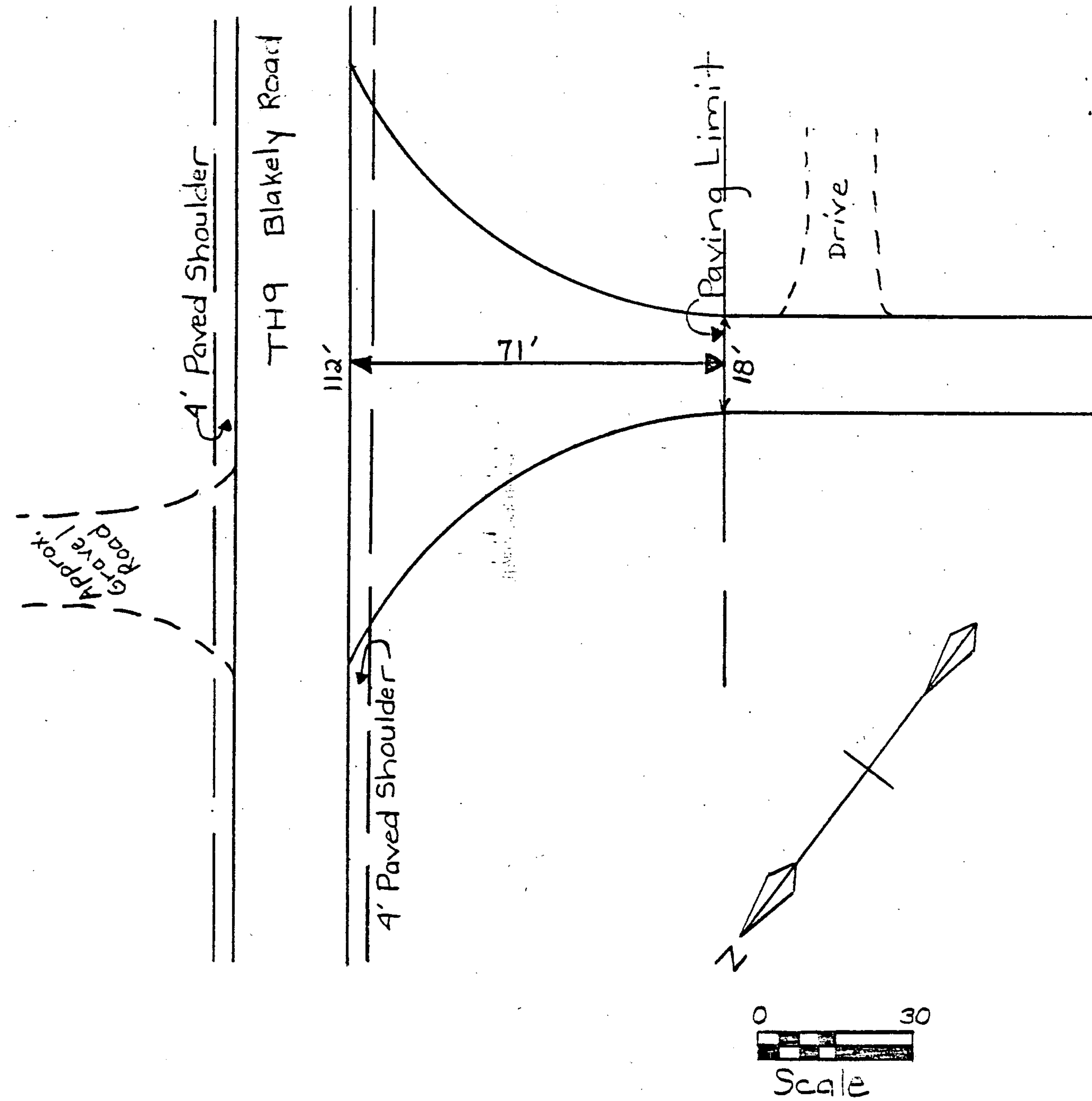


PROJECT Colchester

NO. M 5600(3) B

SHEET 8 OF 18 SHEETS

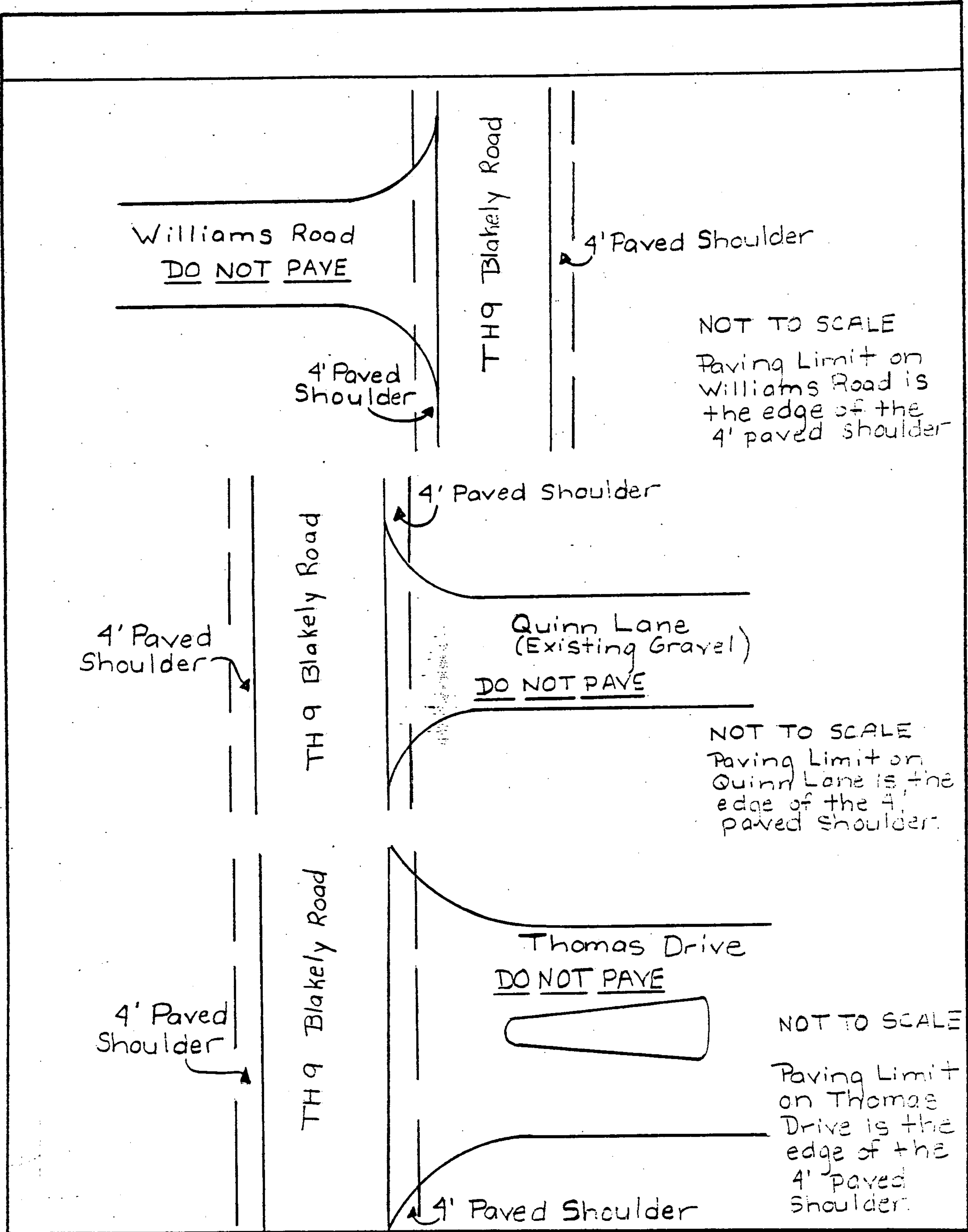
Lavigne Road Approach



PROJECT Colchester

NO. M 5600 (3) S

SHEET 9 OF 18 SHEETS



PROJECT Colchester

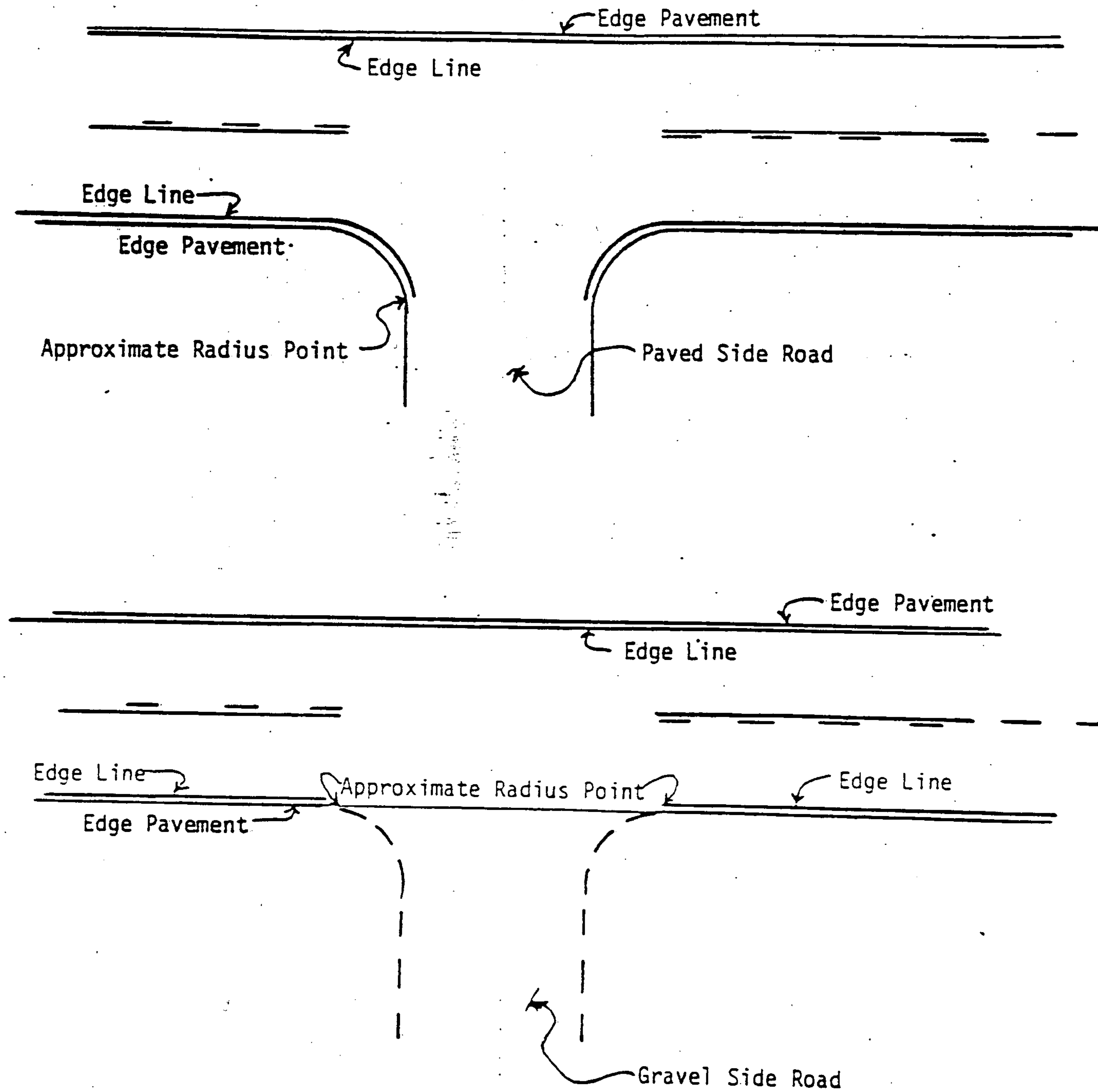
NO. M 5600(3)S

SHEET 10 OF 18 SHEETS

Guideline for Edgeline Application  
on 2-Lane, 2-Way Highways

Edgelines shall be applied to the right and left of centerline  
on highways that have a traffic volume in excess of 1000 vehicles per day.

Treatment at intersecting side roads shall be as shown below:



- A. CENTERLINE AND GORE AREA MARKINGS SHALL BE APPLIED AT THE END OF EACH WORKING DAY. THE FOLLOWING LAYOUT REQUIREMENTS SHALL BE MET:

NO PASSING BARRIER

SOLID STRIPES.

DASHED LINE

10-FOOT SOLID LINE WITH 30-FOOT GAP.

SOLID LINE - (GORE AREAS TO INCLUDE CHANNELIZING LINE AND DASHED LINE)

PER STANDARD SHEET E-50.

EDGE LINES

WHEN SPECIFIED, EDGE LINES ARE NOT REQUIRED UNTIL COMPLETION OF THE PROJECT. ON INTERSTATE PROJECTS, TEMPORARY EDGE LINES SHOULD BE APPLIED WHERE TRAFFIC VOLUMES AND SPEEDS ARE HIGH AND DELAY OF SEVERAL DAYS IS ANTICIPATED.

- B. TEMPORARY MARKINGS MAY CONSIST OF PAINT, TAPE OR RAISED PAVEMENT MARKERS (RPM'S). THE TAPE SHALL BE A RETRO-REFLECTIVE FILM ON A CONFORMABLE METALLIC BACKING THAT CAN BE PAVED OVER. TAPE MAY BE USED ON THE FINAL SURFACE COURSE IF IT WILL NOT INTERFERE WITH THE FINAL MARKING APPLICATION. THE RPM'S SHALL HAVE A SELF-ADHESIVE BACKING EASILY REMOVED BEFORE PAVING AND SHALL CONFORM TO THE FOLLOWING LAYOUT PATTERN: TEMPORARY TAPE MARKINGS WILL BE OFFSET AND REMOVED WHEN PROJECT IS FINISHED AND FINAL CENTERLINE PAINTED. THE TAPE SHALL BE THE TYPE THAT IS REMOVABLE INTACT AND NOT SEPERATE AT ANY TIME.

NO PASSING BARRIER

No RPM'S ALLOWED.

DASHED LINE

FOUR RETRO-REFLECTIVE RPM'S ON 3 1/2 FOOT CENTERS WITH A 30 FOOT GAP.

SOLID LINE - EDGE LINES

INTERSTATE MEDIAN SIDE-RETRO-REFLECTIVE RPM'S ON 4 TO 5 FOOT CENTER.  
DRIVERS RIGHT SIDE-RPM'S NOT ALLOWED.

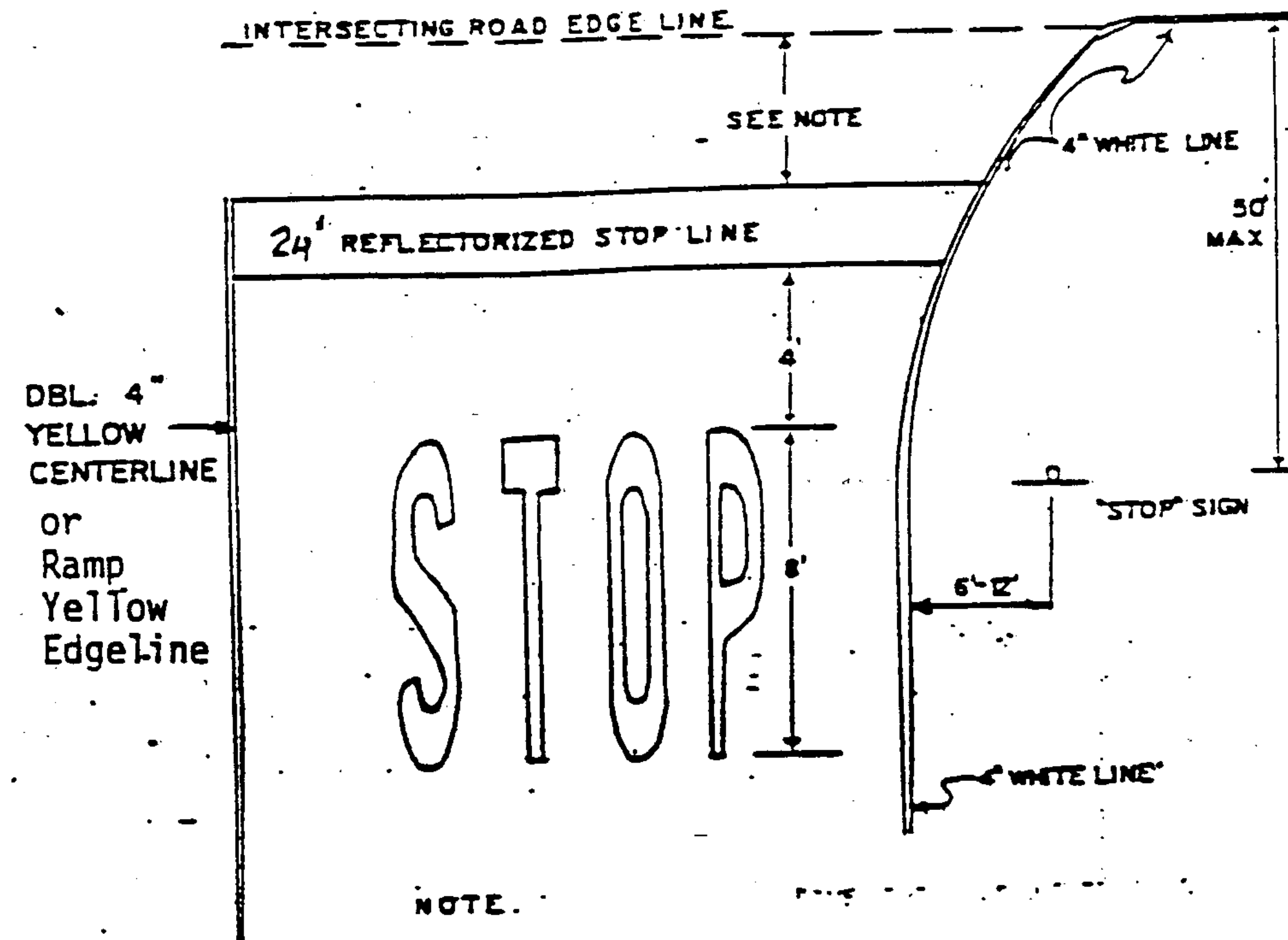
- C. WHEN PAINT IS USED FOR TEMPORARY MARKING, AN ALTERNATE MATERIAL SUCH AS TAPE OR RPM'S SHALL BE ON HAND IN THE EVENT RAIN PREVENTS THE PAINT APPLICATION FROM BEING COMPLETED. ALL PAINT SHALL BE REFLECTORIZED.
- D. PAYMENT FOR PAINT AND TAPE SHALL BE COMPUTED ON A LINEAR FOOT BASIS AS IF PAINT WAS USED. PAYMENT FOR THE RPM'S SHALL BE COMPUTED AS IF AN EQUIVALENT PAINT LINE WAS USED. (FOR EXAMPLE, DASHED LINE PAID AS 10 FEET OF PAINT, SOLID LINE PAID AS THE TOTAL DISTANCE COVERED WITH THE MARKERS).
- E. PRIOR TO ACCEPTANCE, THE PAVEMENT MARKINGS SHALL BE COMPLETED FOR THE ENTIRE PROJECT BY THE CONTRACTOR AS DETAILED ON THE PLANS OR DIRECTED BY THE RESIDENT ENGINEER.

REVISED

01/12/85

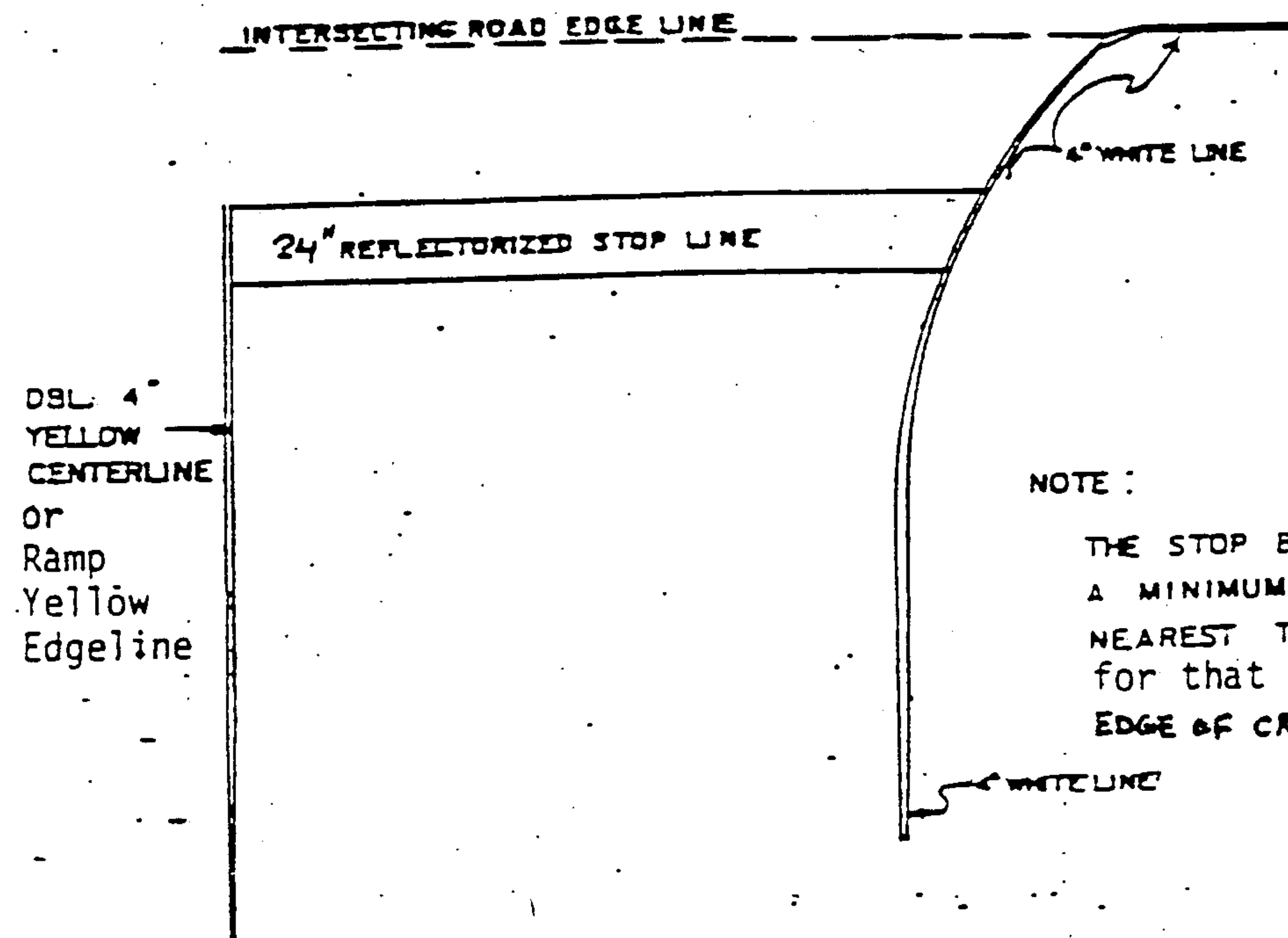
# STOP BAR- DETAILS

## Without Signal



THE STOP LINE SHOULD BE PLACED AT THE DESIRED STOPPING POINT, IN NO CASE MORE THAN 30 FEET OR LESS THAN 4' FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY, OR EDGE OF CROSSWALK

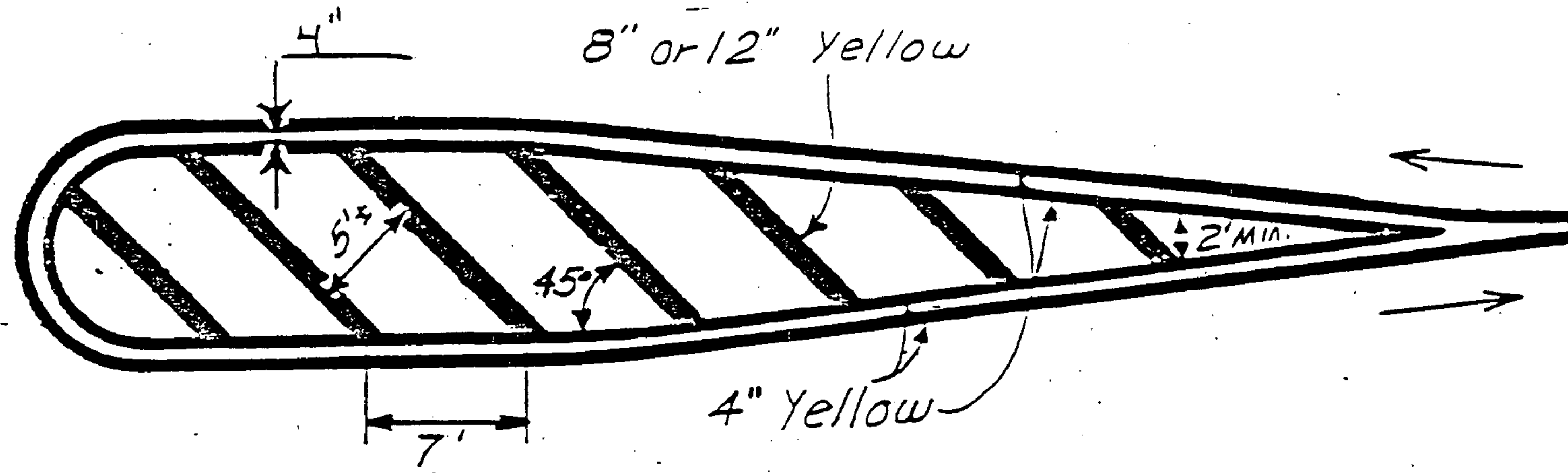
## With Signal



THE STOP BAR SHALL BE PLACED A MINIMUM OF 40' FROM THE NEAREST TRAFFIC SIGNAL FACE for that approach, OR 4' FROM EDGE OF CROSSWALK.

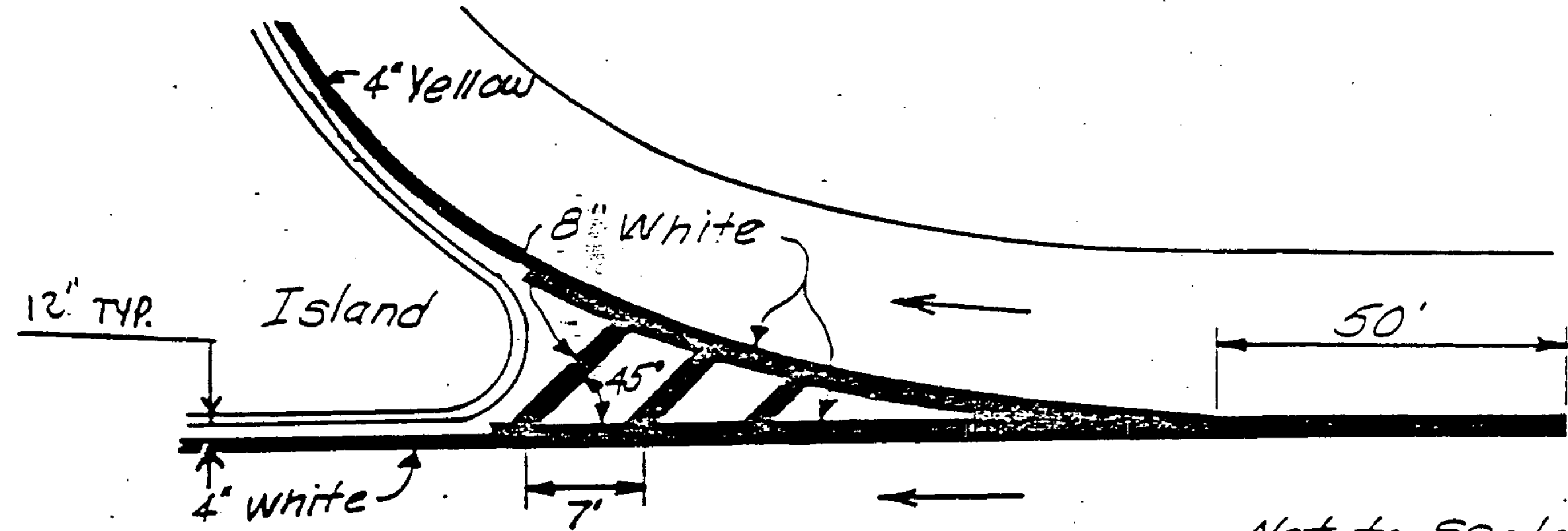
# SPECIAL MARKING DETAILS

## Painted Island Detail



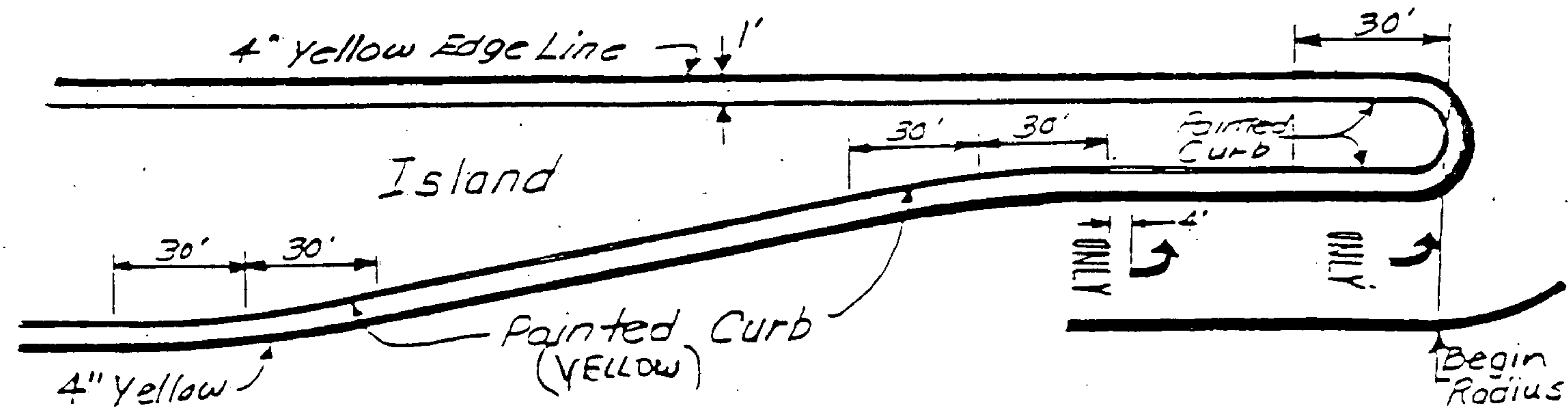
Not to Scale

## Gore Marking Detail - EXIT



Not to Scale

## Turn Lane and Painted Curb Detail



Not to Scale