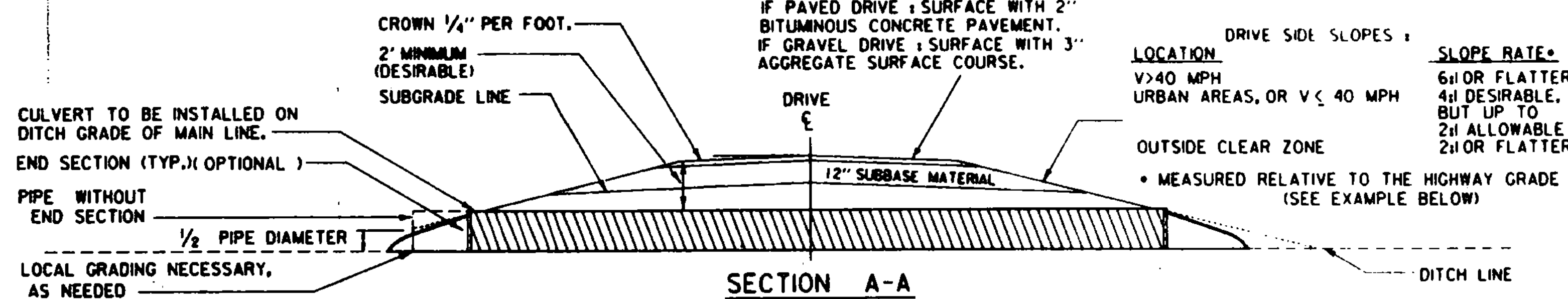
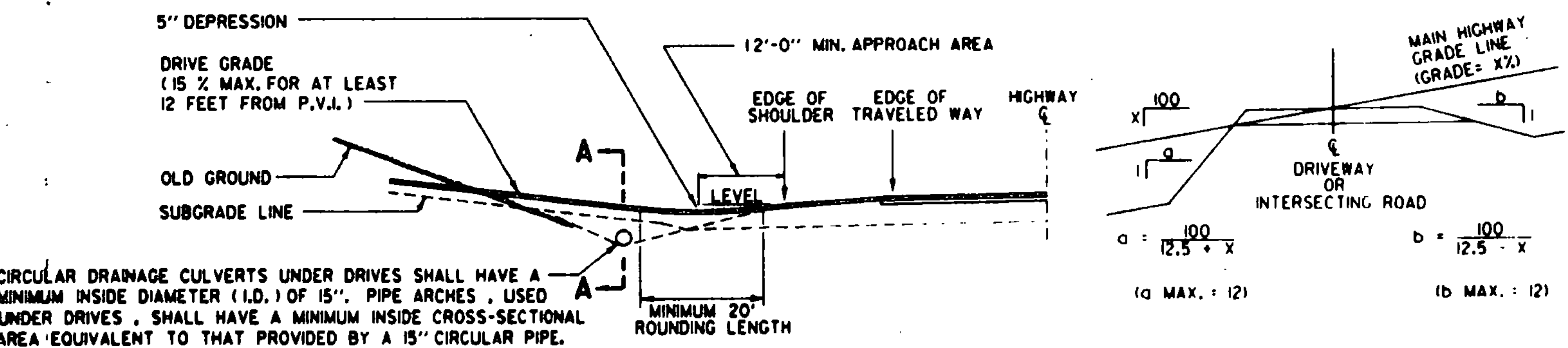


PERSPECTIVE SKETCH OF DRIVE INTERSECTION SHOWING DEPRESSED RAMP

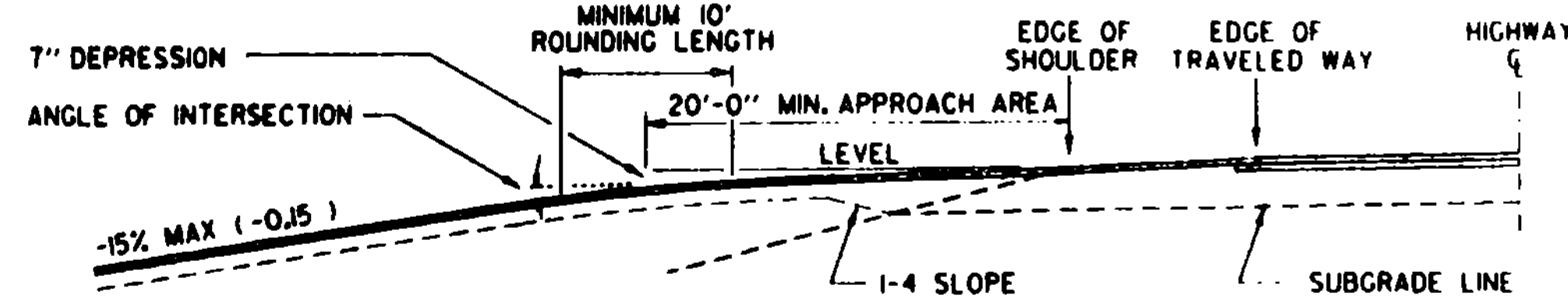


SECTION A-A

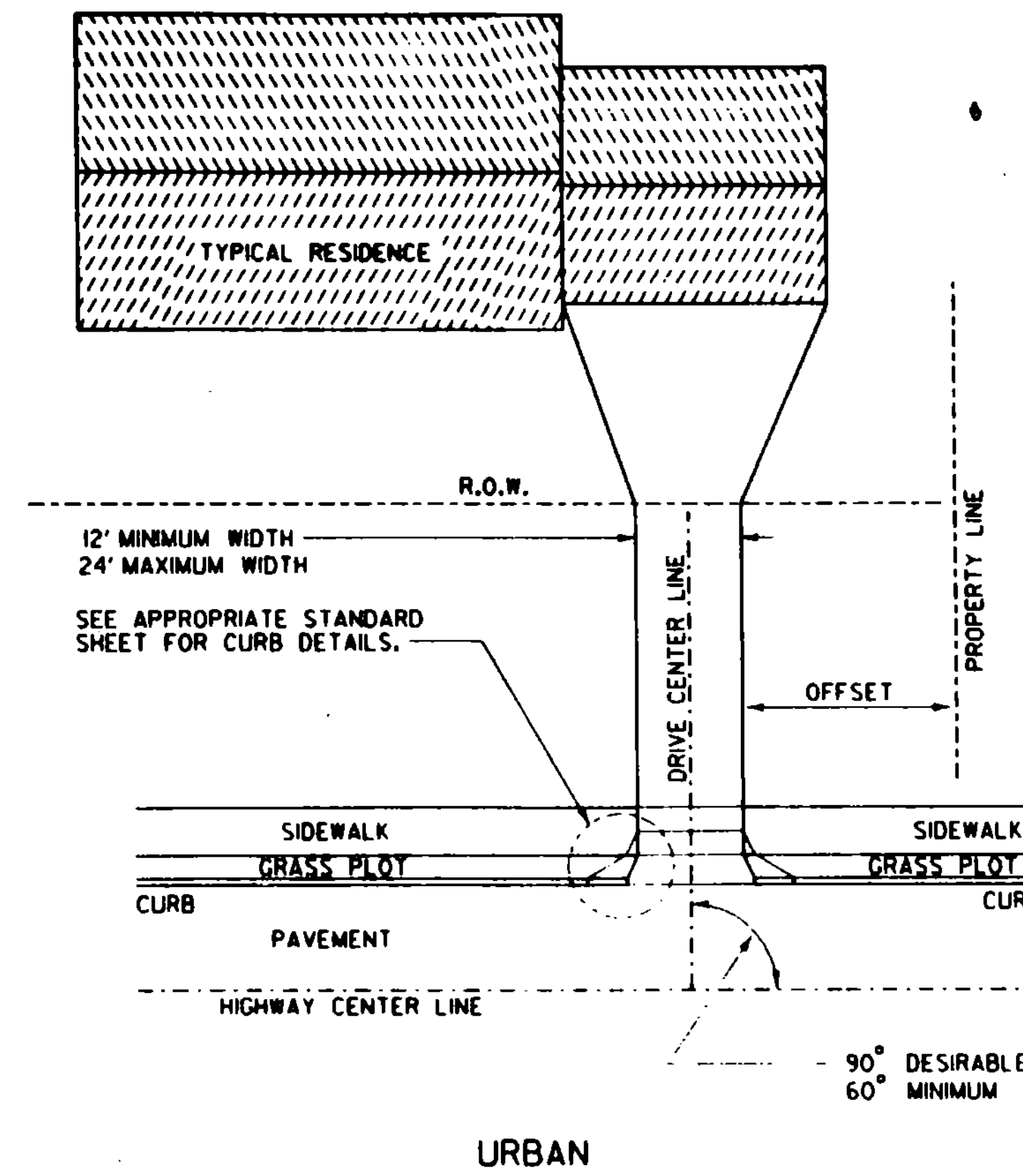


PROFILE OF DRIVEWAY INTERSECTION (CUT SECTION)

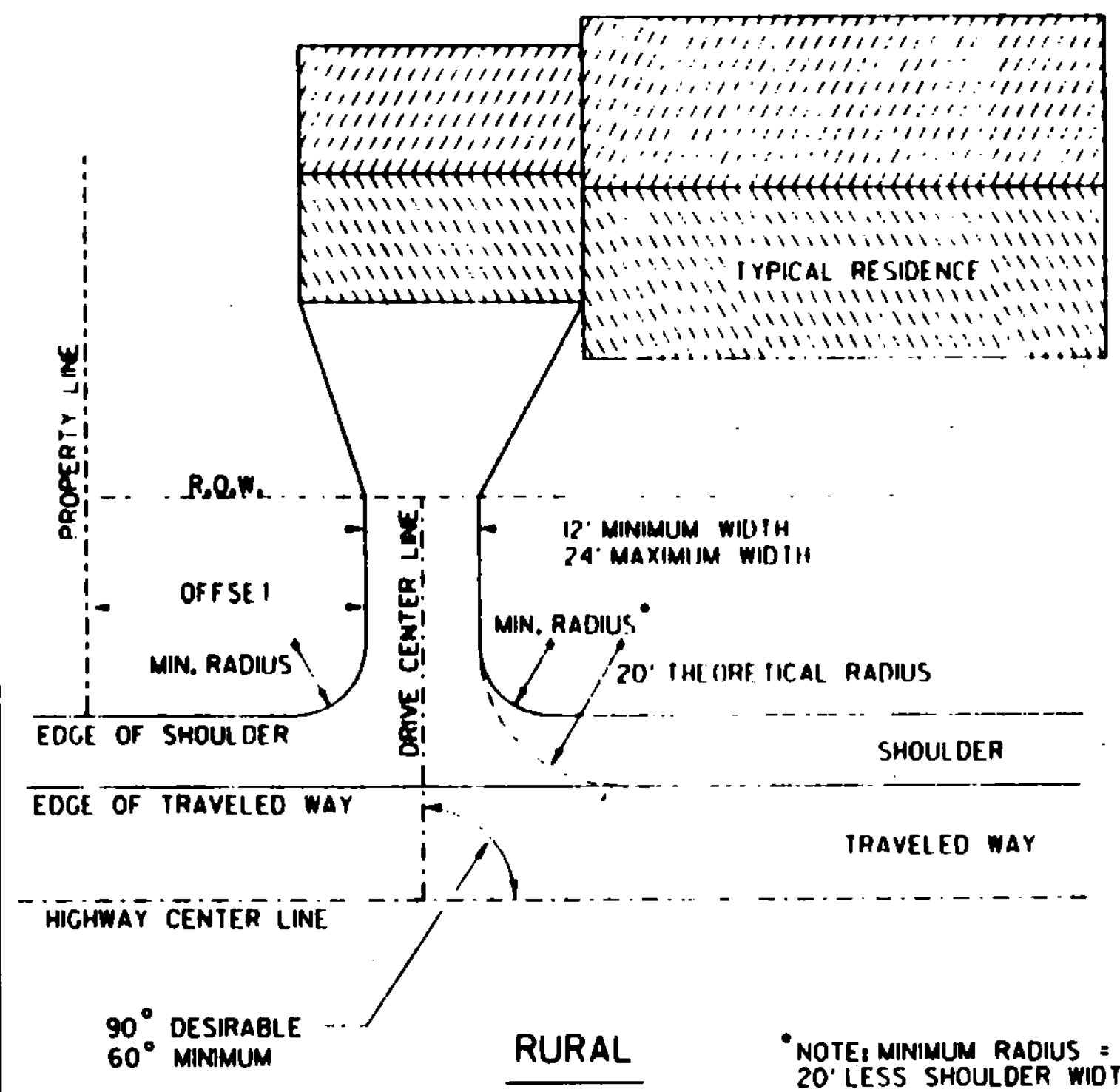
SIDE SLOPE EXAMPLE



PROFILE OF DRIVEWAY INTERSECTION (FILL SECTION)



URBAN



RURAL

NOTE: MINIMUM RADIUS = 20' LESS SHOULDER WIDTH.

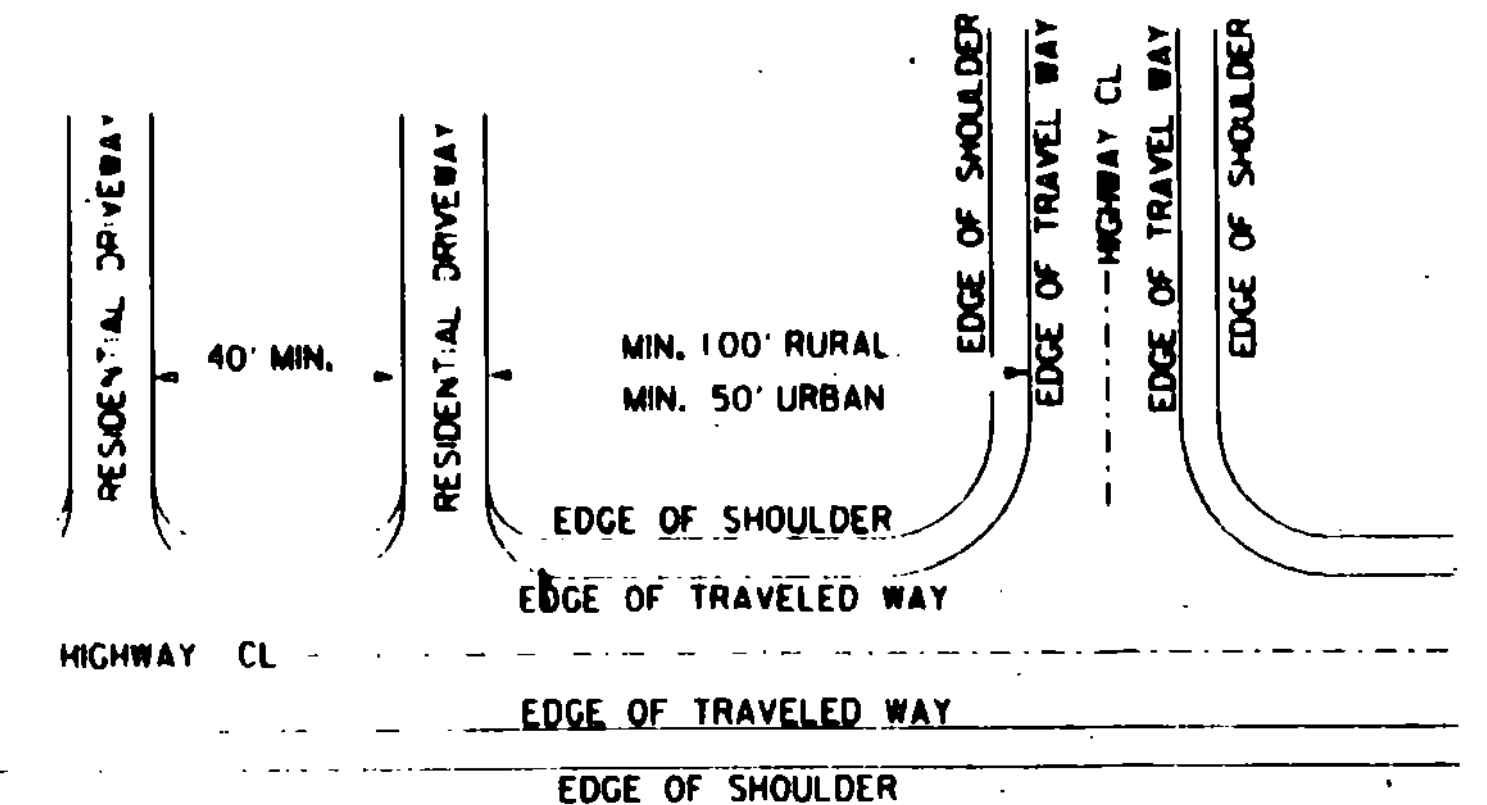
NOTES:

- ALL CONSTRUCTION REQUIRED BY THE PERMIT AND INDICATED ON THIS SHEET, SHALL BE THE RESPONSIBILITY OF THE APPLICANT, AND IS SUBJECT TO THE APPROVAL OF THE VT. AGENCY OF TRANSPORTATION DISTRICT TRANSPORTATION ADMINISTRATOR.
- CORNER SIGHT DISTANCES, EQUAL TO OR GREATER THAN THOSE SHOWN BELOW, SHALL BE PROVIDED IN BOTH DIRECTIONS FOR ALL DRIVES ENTERING ON PUBLIC HIGHWAYS, UNLESS OTHERWISE APPROVED BY THE AGENCY OF TRANSPORTATION. CORNER SIGHT DISTANCE IS MEASURED FROM A POINT ON THE DRIVE AT LEAST 15 FEET FROM THE EDGE OF TRAVELED WAY OF THE ADJACENT ROADWAY AND MEASURED FROM A HEIGHT OF EYE OF 3.5 FEET ON THE DRIVE TO A HEIGHT OF 4.25 FEET ON THE ROADWAY.
- THE OFFSET BETWEEN THE PROPERTY LINE AND THE EDGE OF THE DRIVEWAY MAY BE GOVERNED BY LOCAL ZONING LAWS. DRIVEWAY WIDTH RESTRICTIONS SHOWN PERTAIN ONLY TO THE AREA WITHIN THE HIGHWAY R.O.W..
- DRIVEWAY GRADES STEEPER THAN THOSE SHOWN ARE ALLOWED AS LONG AS A 20' APPROACH AREA IS ACHIEVED FOR THE VEHICLE TO PAUSE BEFORE ENTERING THE HIGHWAY.
- NO PORTION OF ANY DRIVE SHALL BE LOCATED CLOSER THAN 100 FEET IN RURAL AREAS, AND 50 FEET IN URBAN AREAS, FROM THE CLOSEST EDGE OF THE TRAVELED WAY AT THE INTERSECTION OF A HIGHWAY, UNLESS NO OTHER REASONABLE ACCESS IS AVAILABLE AND PRIOR APPROVAL IS GRANTED BY THE AGENCY OF TRANSPORTATION. OTHER CONSIDERATIONS, SUCH AS TRAFFIC SIGNALS, HIGH TRAFFIC VOLUMES OR FUNCTIONAL CLASS OF HIGHWAY SHOULD BE ADDRESSED WHEN DETERMINING THE APPROPRIATE OFFSET DISTANCE.

CORNER SIGHT DISTANCE CHART

DESIGN SPEED (M.P.H.)	MINIMUM (FT)	DESIRABLE (AASHTO) (FT)
25	150	350
30	200	450
35	250	580
40	325	750
45	400	950
50	475	1180

NOTE: ADVANCE WARNING SIGNS WILL BE REQUIRED IF CORNER SIGHT DISTANCES ARE BELOW MINIMUM VALUES.



NOT TO SCALE

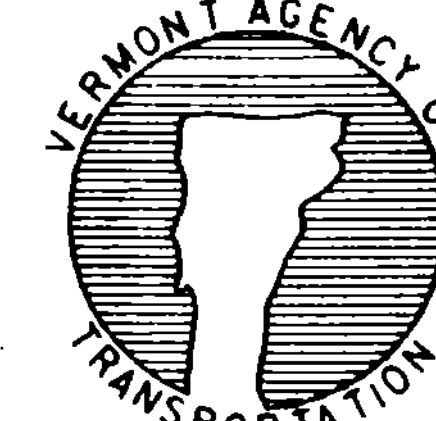
REVISIONS AND CORRECTIONS
3/07/90 ADDED DRIVE SIDE SLOPE RATES

APPROVED 3/12/90
DATE
Richard
CHIEF ENGINEER

APPROVED FOR THIS PROJECT AND/OR DESIGN IMPLEMENTATION FHWA FINAL APPROVAL PENDING.

Stephen B. MacArthur
DIRECTOR OF PLANNING AND PRECONSTRUCTION
Justin
DESIGN ENGINEER

RESIDENTIAL DRIVEWAY



STANDARD B-71A