

INDEX OF SHEETS

STATE OF VERMONT
AGENCY OF TRANSPORTATION



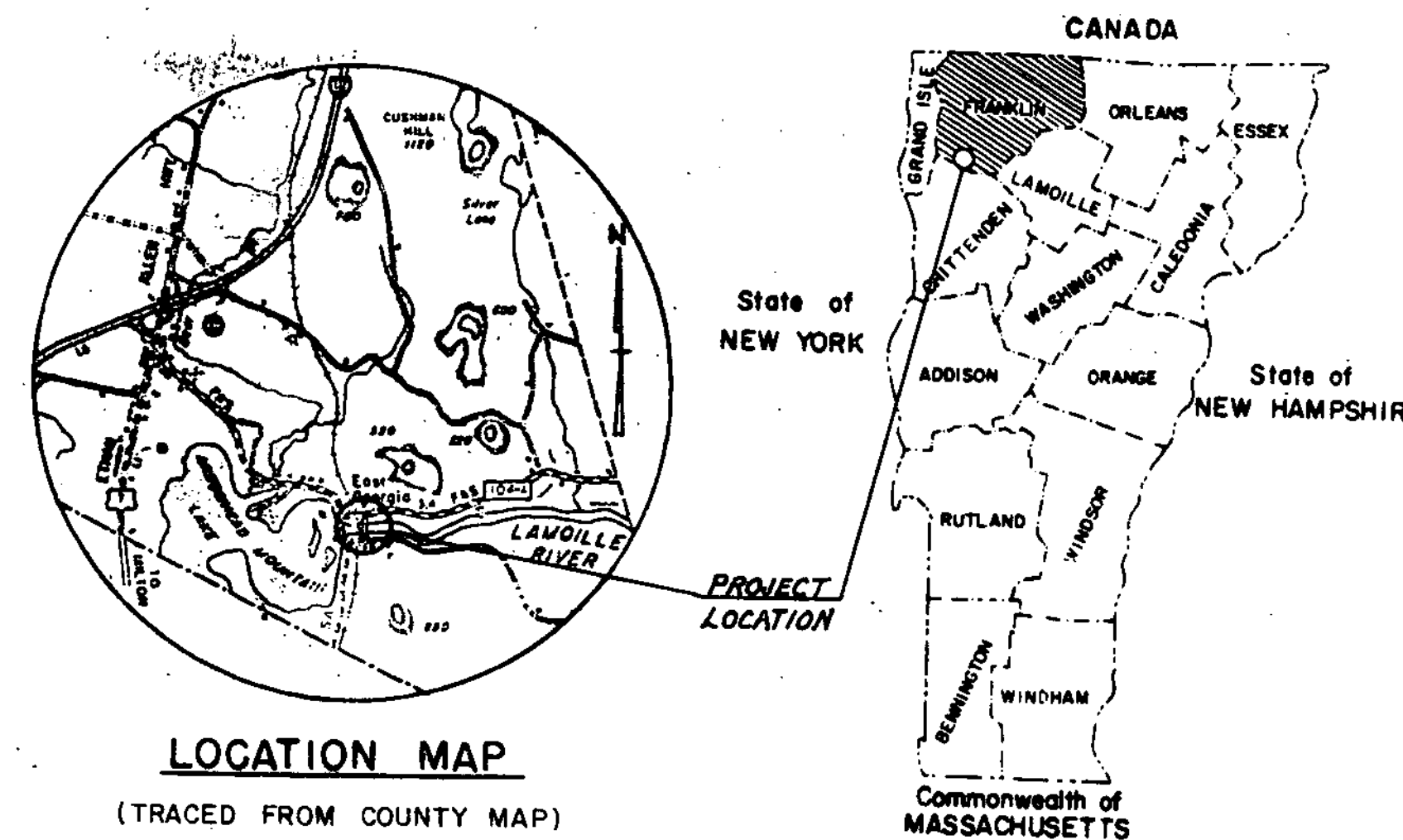
PROPOSED IMPROVEMENT
BRIDGE PROJECT

TOWN OF GEORGIA
COUNTY OF FRANKLIN

ROUTE NO: TH #1 CLASS 2 BRIDGE NO: 10

PROJECT LOCATION: BEGINNING AT A POINT APPROX. .055 MI. IN A WESTERLY DIRECTION FROM THE REVISED INTERSECTION OF TH#1 & TH#36 IN GEORGIA & EXTENDING ALONG TH#1 FOR APPROX. 0.147 MI. TO THE REVISED INTERSECTION OF TH#1 & VT. RTE 104A
PROJECT DESCRIPTION: REMOVAL OF EXISTING STRUCTURE; CONSTRUCTION OF A NEW 3-SPAN CONTINUOUS PLATE GIRDER BRIDGE W/ COMPOSITE CONCRETE SLAB DECK, CONCRETE ABUTMENTS, PIERS, & WINGWALLS; ROADWAY APPROACHES & RELATED CHANNEL WORK.

LENGTH OF STRUCTURE: 322.00 FEET
LENGTH OF PARTICIPATION ROADWAY: 458.68 FEET
LENGTH OF NON-PARTICIPATION ROADWAY: -- FEET
LENGTH OF PROJECT: 780.68 FEET



SCALE: 1" = 1 MILE

10. THE PIERS ARE DESIGNED FOR A MAXIMUM FOOTING PRESSURE OF 6.5 KSF.
11. THE MINIMUM CLEAR COVER FOR REINFORCING STEEL SHALL BE AS SHOWN ON THE DETAIL SHEETS.
12. THE COST OF ON-PROJECT SIGNS AND BARRICADES REQUIRED SHALL BE SUBSIDIARY TO THE ITEM 'MAINTENANCE OF TRAFFIC FOR BRIDGE PROJECTS'.
13. TRAFFIC SHALL BE MAINTAINED ON TOWN HIGHWAY NO. 1 VIA THE EXISTING BRIDGE AND ALSO ON T.H. #36 DURING CONSTRUCTION OF THE NEW BRIDGE UNDER THE ITEM 'MAINTENANCE OF TRAFFIC FOR BRIDGE PROJECT'.
14. STOP SIGNS SHALL BE ERRECTED AT THE INTERSECTION OF T.H. #1 AND VT. RTE. 104A; AND THE INTERSECTION OF T.H. #36 AND T.H. #1 AS DIRECTED BY THE ENGINEER. THIS WORK TO BE PAID UNDER THE ITEMS 'TRAFFIC SIGNS, TYPE A' AND 'TRAFFIC SIGN POSTS, TYPE A'.
15. THE MAIL BOXES (5) AT REVISED STATION 85+35+/- LT. AND THE 'ARROWHEAD RD #1' SIGN AT REVISED STATION 84+75+/- RT. SHALL BE REMOVED AND RESET AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED SUBSIDIARY TO ALL OTHER ITEMS IN THIS PROJECT.
16. FLENNING BRACKETS OR SIMILAR FALSEWORK SHALL BE SPACED AT A MAXIMUM OF FOUR (4) FEET.
17. SEE ADDITIONAL NOTES PERTAINING TO BEARINGS AND BRIDGE SEATS ON SHEETS 20 & 21.
18. DRIP PLATES ARE TO BE PLACED ON GIRDERS 1 & 4 IN THE LOCATIONS AS SHOWN ON SHEET 17 AND ACCORDING TO DETAIL 'C' ON STANDARD SHEET SCB-D7-71.
19. THERE ARE TWO (2) PILE LOADING TESTS INCLUDED IN THE ESTIMATE WITH ADDITIONAL PILE LOADING TESTS TO BE USED AS DEEMED NECESSARY BY THE RESIDENT ENGINEER.
20. THE ABUTMENTS ARE DESIGNED TO BE ON FRICTION PILES WITH A DESIGN LOAD CAPACITY OF 90 KIPS PER PILE.
21. A CONCRETE SEAL BELOW THE BOTTOM OF THE PIER FOOTING SHALL BE AT THE CONTRACTOR'S OPTION. IF THE CONTRACTOR ELECTS TO USE A CONCRETE SEAL, IT SHALL MEET THE REQUIREMENTS OF SECTION 530 AND THE COST SHALL BE SUBSIDIARY TO THE UNIT PRICE BID FOR ITEM 530.10.
22. THE BAILEY BRIDGE BEING USED FOR THE SOUTHERLY END SPAN OF THE EXISTING BRIDGE IS THE PROPERTY OF THE VERMONT AGENCY OF TRANSPORTATION, DISTRICT #8, AND SHALL REMAIN THE PROPERTY OF THE STATE OF VERMONT. THE DISTRICT TRANSPORTATION ADMINISTRATOR OF DISTRICT #8 IN ST. ALBANS SHALL BE NOTIFIED AT LEAST 2 (TWO) WEEKS IN ADVANCE OF THE DESIRED REMOVAL BY THE CONTRACTOR SO THAT DISTRICT #8 MAY PLAN TO REMOVE THE BAILEY BRIDGE.
23. ALL CONCRETE ABOVE THE BOTTOM OF THE FOOTING SHALL BE PLACED IN THE DRY.
24. ALL GIRDERS SHALL BE BLAST CLEANED ACCORDING TO VERMONT SPECIFICATION 504.47(b)2.

LIST OF STANDARDS

50. STD. DWG.	E-3	12/15/78 R
51. STD. DWG.	E-6	4/1/80 R
52. STD. DWG.	E-7	2/2/83 R
53. STD. DWG.	E-7A	4/8/82 R
54. STD. DWG.	E-5C	7/24/84
55. STD. DWG.	E-29	8/25/81 R
56. STD. DWG.	G-1	6/5/84 R
57. STD. DWG.	G-1d	6/5/84 R
58. STD. DWG.	SB-R6-82	6/18/82 R
59. STD. DWG.	SCB-D1-75	9/14/81 R
60. STD. DWG.	SCB-D4-76	10/12/83 R
61. STD. DWG.	SCB-D6-73 DET 840	1/3/79 R

Date MAY 31 1985
 Contractor Winterset Inc.
 Signature [Signature]
 Title President
 Transportation Secretary's Signature [Signature]

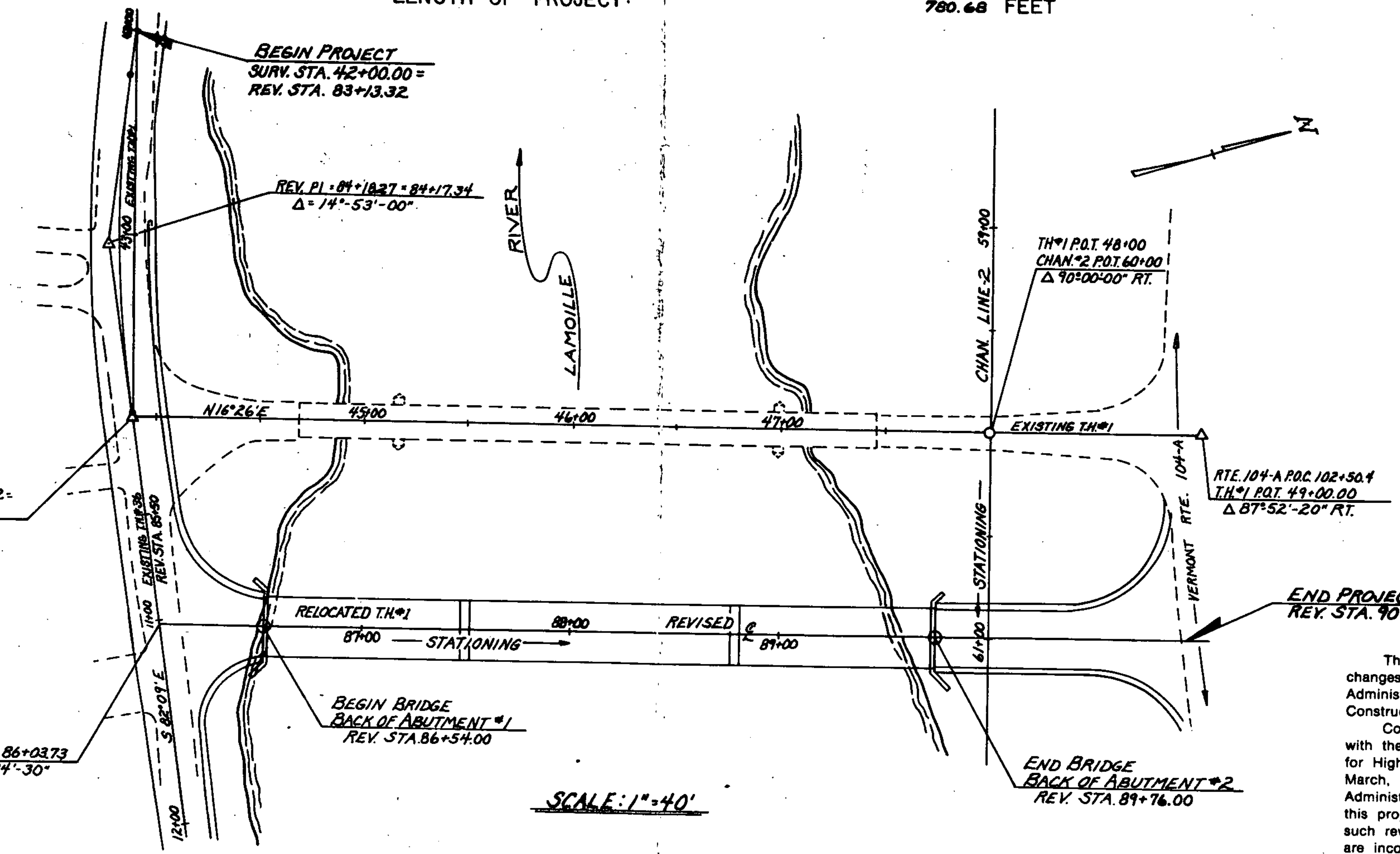
GENERAL NOTES

1. THE GENERAL NOTE PERTAINING TO SPECIFICATIONS, MATERIALS, AND CONSTRUCTION IS SHOWN ON STANDARD DRAWING SCB-D1-75. OTHER GENERAL NOTES ON THE STANDARD, NOT OTHERWISE SHOWN OR MODIFIED ON THESE PLANS, ARE NOTES 1, 3, 4, 5, 7, 8, 9, 10, 11, 12, 13, 14, 15, AND 16.
2. WATER REPELLENT SHALL BE APPLIED TO ALL EXPOSED CONCRETE ON BOTH SUBSTRUCTURE AND SUPERSTRUCTURE, EXCEPT THE UNDERSIDE OF THE SLAB BETWEEN DRIP BEADS.
3. IN ALL HORIZONTAL CONSTRUCTION JOINTS, SHEAR KEYS SHALL BE FORMED AS DETAILED ON STANDARD DRAWING SCB-D4-73, DETAIL 'B', AND THEY SHALL BE CONTINUOUS FOR THE FULL LENGTH OF THE JOINT. ANY UPWARD KEY SHALL BE PLACED INTEGRALLY WITH THE CONCRETE BELOW THE JOINT.
4. TURF ESTABLISHMENT SHALL BE CONSIDERED SUBSIDIARY TO ALL OTHER ITEMS IN THE CONTRACT. SEE SPECIAL PROVISIONS.
5. IN-STREAM CONSTRUCTION SHALL BE RESTRICTED TO JUNE 1 THROUGH OCTOBER 1, UNLESS SUCH WORK IS PERFORMED WITHIN THE CONFINES OF A FILTER CURTAIN, OR UNLESS THE CONTRACTOR OBTAINS PERMISSION FROM THE AGENCY OF ENVIRONMENTAL CONSERVATION TO DO WORK OUTSIDE THAT TIME FRAME. OPERATIONS DURING SUMMER MONTHS SHALL NOT INCREASE THE BACKGROUND LEVEL OF TURBIDITY MORE THAN 10 JACKSON TURBIDITY UNITS (JTU) AND DURING THE WINTER MONTHS SHALL NOT INCREASE THE BACKGROUND LEVEL OF TURBIDITY AT ALL.
6. ALL CONSTRUCTION SHALL CONFORM TO THE STATE OF VERMONT, AGENCY OF TRANSPORTATION, STANDARD SPECIFICATION 104.11 TO PREVENT MATERIAL AND DEBRIS FROM FALLING INTO THE STREAM AND CAUSING POTENTIAL CONTAMINATION TO THE TOWN OF HILTON WATER SUPPLY AND THE RAW WATER INTAKE FOR THE GEORGIA INDUSTRIAL PARK. SEE SPECIAL PROVISIONS FOR OTHER RESTRICTIONS.
7. THE 'STONE FILL, TYPE IV' SHALL BE PLACED IN FRONT OF THE ABUTMENTS BEFORE THE GIRDERS ARE ERRECTED.
8. AFTER TRAFFIC IS ALLOWED ON THE NEW BRIDGE, THE EXISTING BRIDGE SHALL BE REMOVED UNDER THE ITEM 'REMOVAL OF STRUCTURE'. THE ABUTMENTS SHALL BE REMOVED DOWN TO FINISH GRADE I.E. TOP OF THE STONE FILL. THE PIERS SHALL BE REMOVED DOWN TO THE STREAMBED. THE TRUSS SHALL BECOME THE PROPERTY OF THE CONTRACTOR. THE BEAMS ON THE NORTHERLY SPAN SHALL BECOME THE PROPERTY OF THE TOWN OF GEORGIA. CARE SHALL BE USED DURING REMOVAL OF THE BEAMS NOT TO DAMAGE THEM AND THEY SHALL BE STORED ON THE PROJECT FOR REMOVAL BY THE TOWN.
9. THE STRUCTURE IS DESIGNED FOR AN HS-20-44 LIVE LOAD WITH 1 INCH FUTURE PAVEMENT ALLOWANCE.

CONVENTIONAL SIGNS

COUNTY LINE	---
TOWN LINE	- - - -
LIMITS OF ACCESS	○-○-○-○
POINT OF ACCESS	X
FENCE LINE	—X—X—X—X—
STONE WALL	—X—X—X—X—
TRAVELED WAY	—X—X—X—X—
GUARD RAIL	—X—X—X—X—
RAILROAD	—X—X—X—X—
SURVEY LINE	—X—X—X—X—
CULVERT	—X—X—X—X—
POWER POLE	○
TELEPHONE POLE	○
TREES	○
CONTROL OF ACCESS	///
PROPERTY LINE	—X—X—X—X—
R.O.W TAKING LINE	—X—X—X—X—
SLOPE RIGHTS	○-SR-△
TOP OF CUT	△
TOE OF SLOPE	○

DATUM
VERTICAL NGVD 1927
HORIZONTAL N/A



These plans are subject to such engineering changes as may be required by the Federal Highway Administration or the Director of Engineering and Construction.
 Construction is to be carried on in accordance with these plans and the Standard Specifications for Highway and Bridge Construction dated March, 1976, as approved by the Federal Highway Administration on October 27, 1976 for use on this project, including all subsequent revisions and such revised specifications and special provisions as are incorporated in these plans.

SUBMITTED BY ORDER OF THE STATE TRANSPORTATION BOARD
 APPROVED [Signature] DATE 2-25-85
 DIRECTOR OF ENGINEERING AND CONSTRUCTION

PROJECT GEORGIA PROJECT NO. BRZ 1448(3)
 SHEET 1 OF 61 SHEETS
 S.J.B. 7/21/81