

Table 6H-2. Meaning of Symbols on Typical Application Diagrams

| | | | |
|--|--|--|--------------------------------------|
| | Arrow board | | Shadow vehicle |
| | Arrow board support or trailer (shown facing down) | | Sign (shown facing left) |
| | Changeable message sign or support trailer | | Surveyor |
| | Channelizing device | | Temporary barrier |
| | Crash cushion | | Temporary barrier with warning light |
| | Direction of temporary traffic detour | | Traffic or pedestrian signal |
| | Direction of traffic | | Truck-mounted attenuator |
| | Flagger | | Type 3 barricade |
| | High-level warning device (Flag tree) | | Warning light |
| | Longitudinal channelizing device | | Work space |
| | Luminaire | | Work vehicle |
| | Pavement markings that should be removed for a long-term project | | |

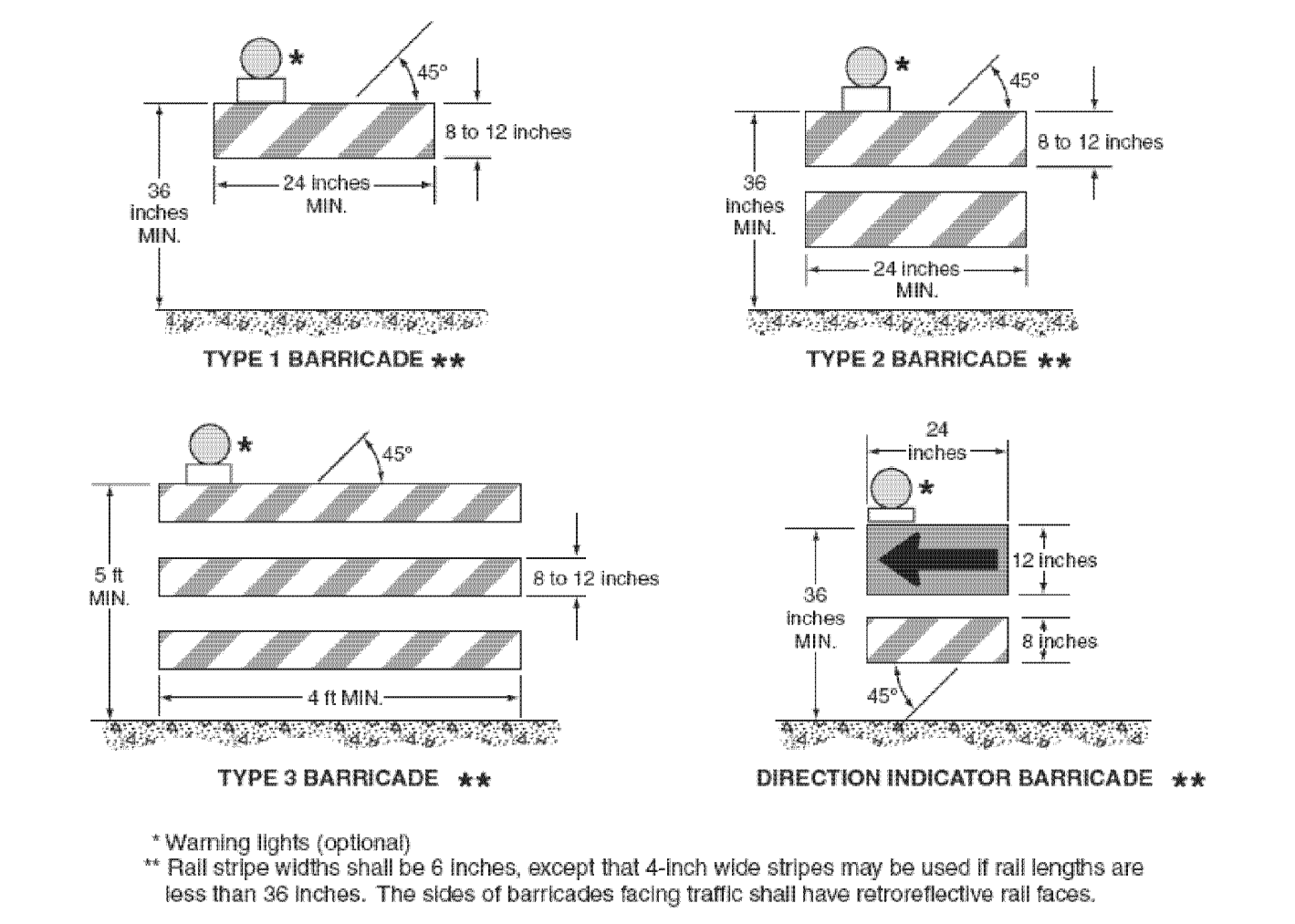
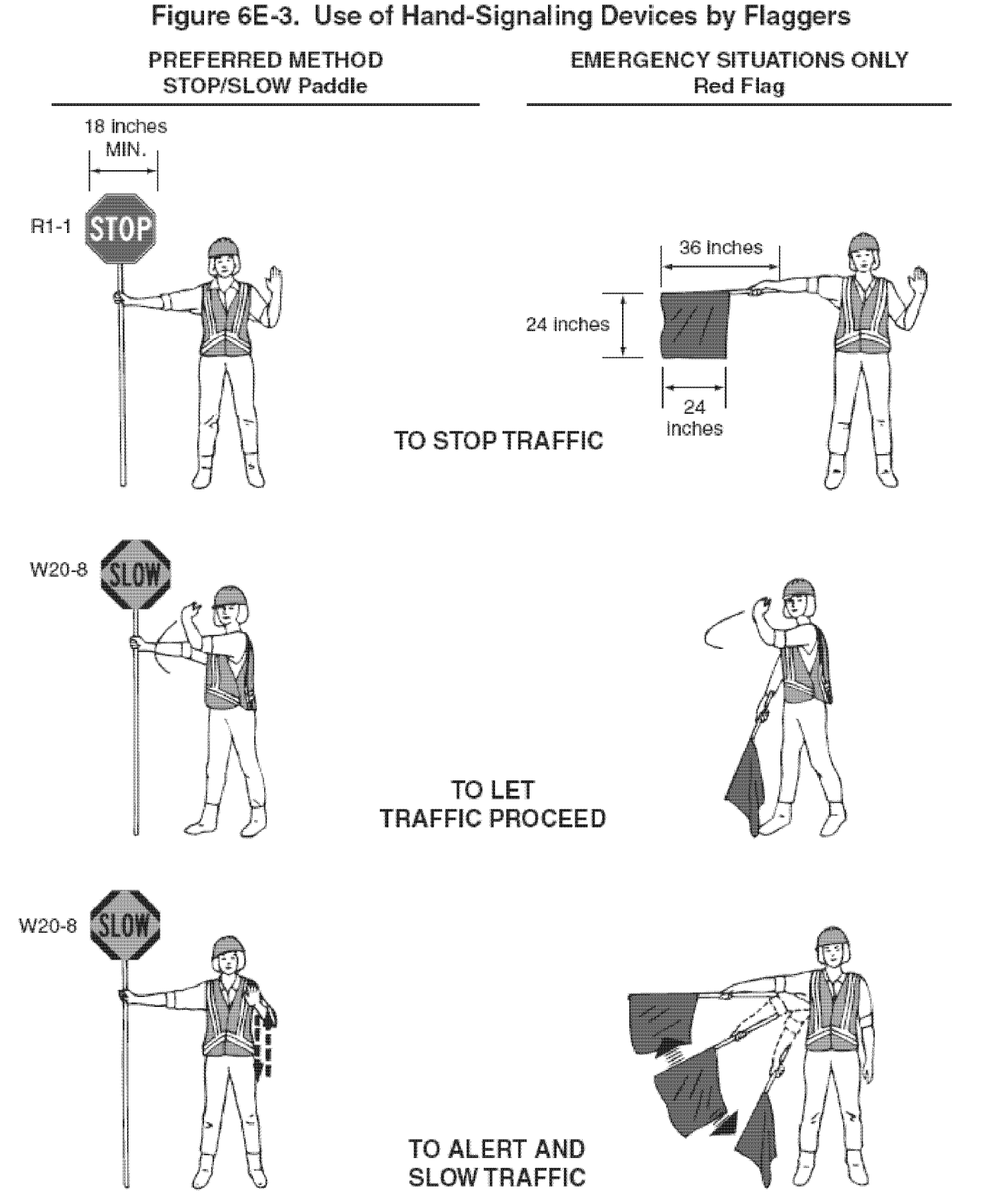
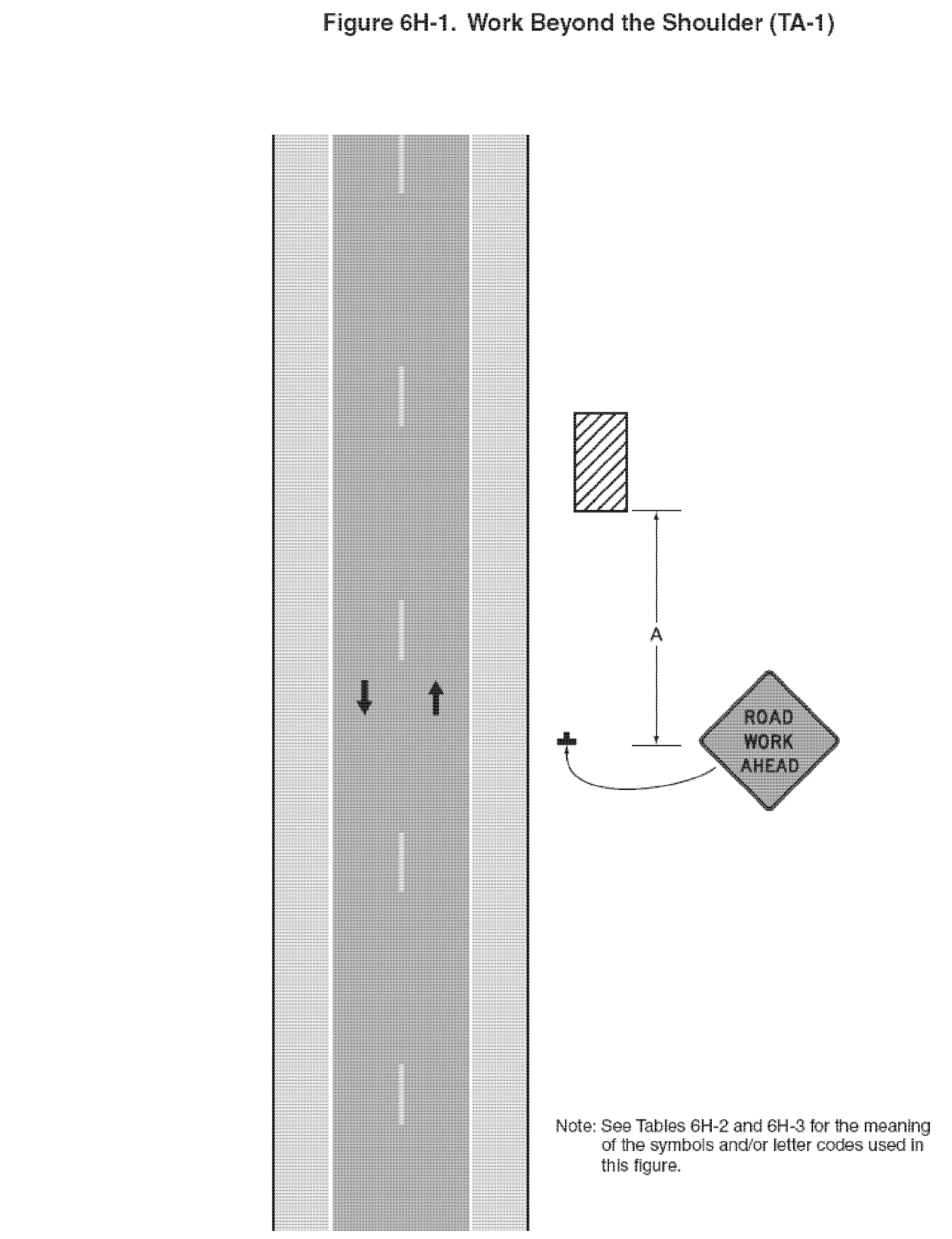
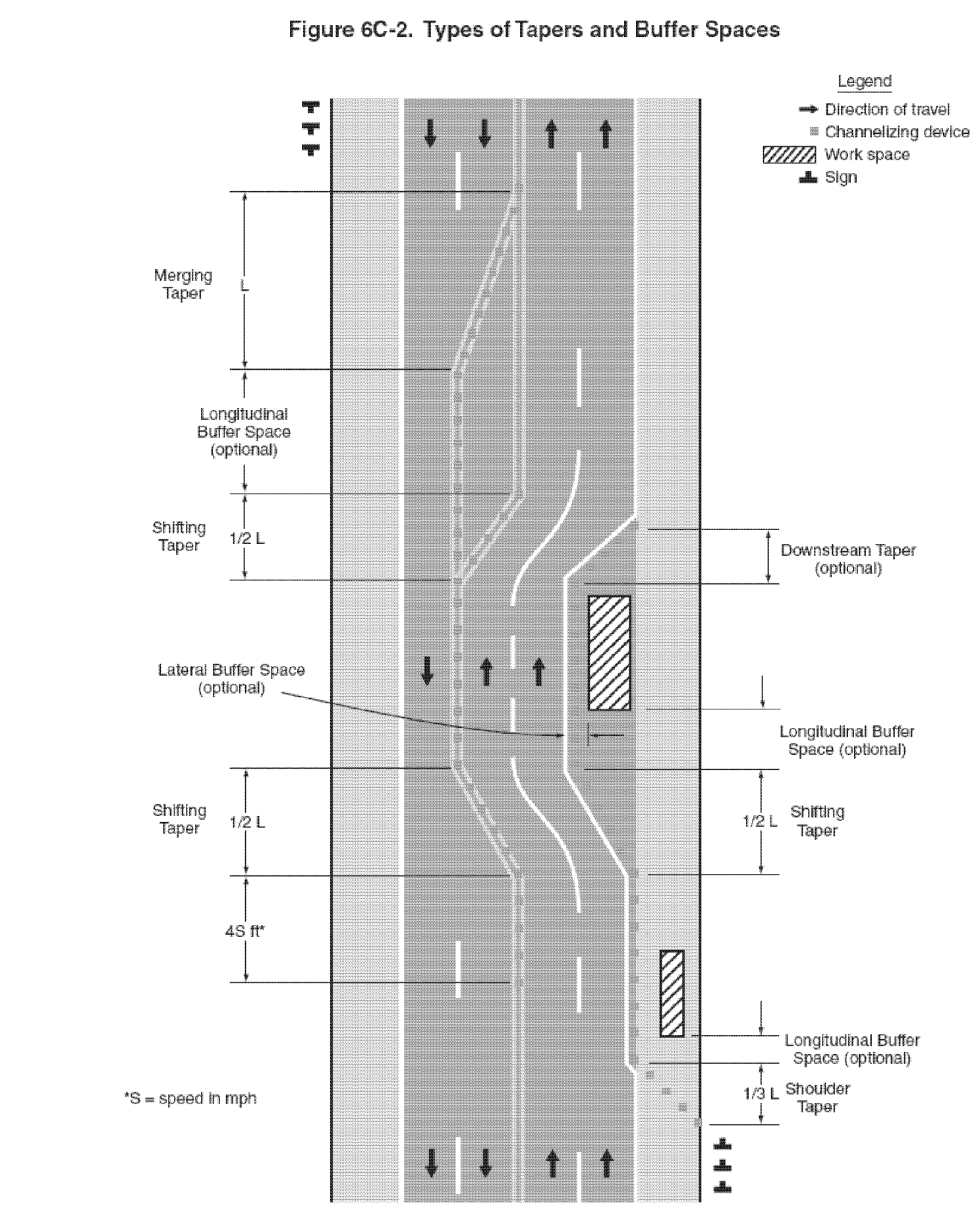
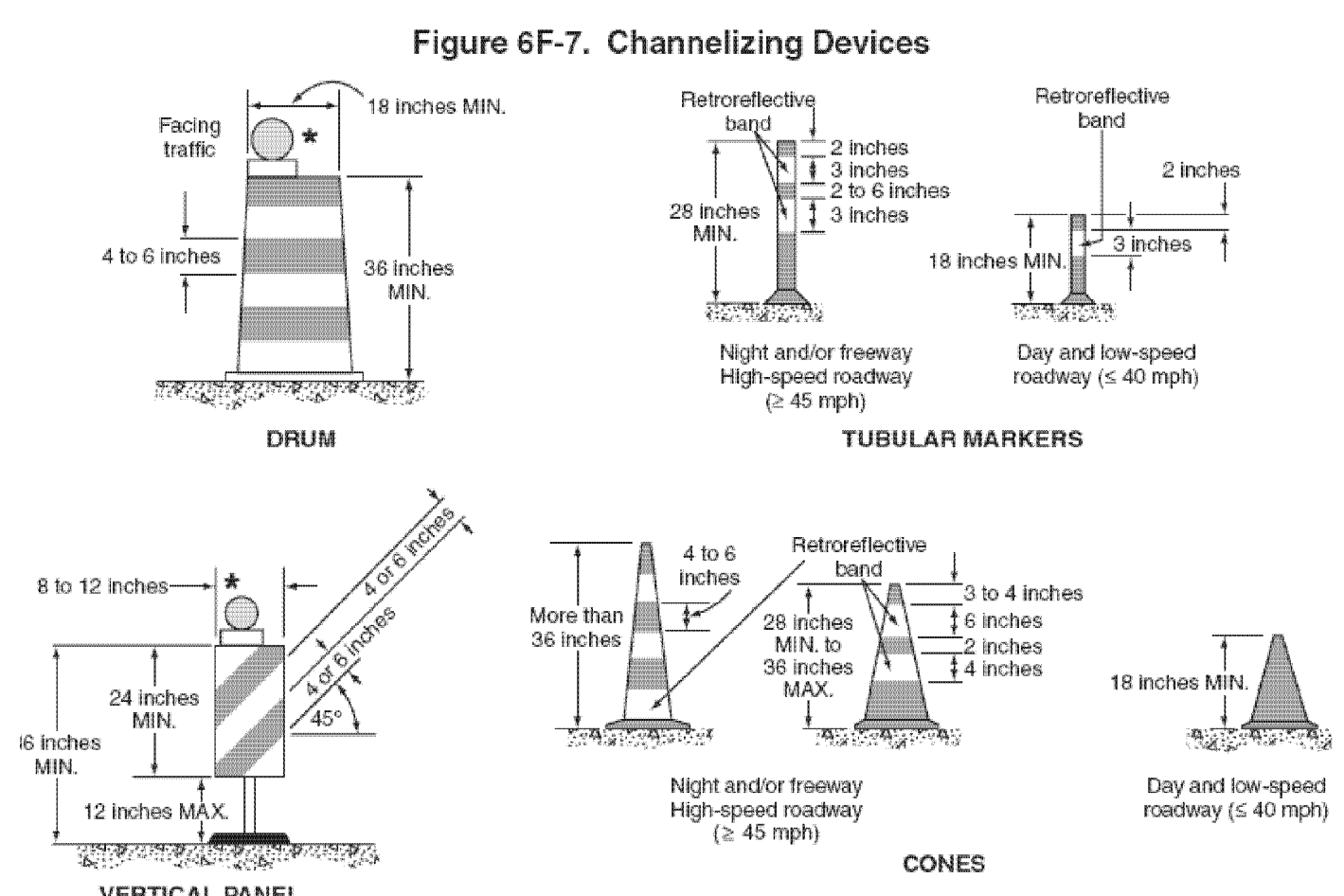


Table 6C-3. Taper Length Criteria for Temporary Traffic Control Zones

| Type of Taper | Taper Length |
|---------------------------------|-----------------------------------|
| Merging Taper | at least L |
| Shifting Taper | at least 0.5 L |
| Shoulder Taper | at least 0.33 L |
| One-Lane, Two-Way Traffic Taper | 50 feet minimum, 100 feet maximum |
| Downstream Taper | 50 feet minimum, 100 feet maximum |

Note: Use Table 6C-4 to calculate L

Table 6H-3. Meaning of Letter Codes on Typical Application Diagrams

| Road Type | Distance Between Signs** | | |
|----------------------|--------------------------|------------|------------|
| | A | B | C |
| Urban (low speed)* | 100 feet | 100 feet | 100 feet |
| Urban (high speed)* | 350 feet | 350 feet | 350 feet |
| Rural | 500 feet | 500 feet | 500 feet |
| Expressway / Freeway | 1,000 feet | 1,500 feet | 2,640 feet |

* Speed category to be determined by highway agency
** The column headings A, B, and C are the dimensions shown in Figures 6H-1 through 6H-46. The A dimension is the distance from the transition or point of restriction to the first sign. The B dimension is the distance between the first and second signs. The C dimension is the distance between the second and third signs. (The "first sign" is the sign in a three-sign series that is closest to the TTC zone. The "third sign" is the sign that is furthest upstream from the TTC zone.)

- TEMPORARY TRAFFIC CONTROL NOTES**
- TEMPORARY TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH THESE PROJECT PLANS, APPLICABLE VTRANS E-SERIES STANDARD DRAWINGS, AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), DATED 2009, AND ITS LATEST REVISIONS, OR AS DIRECTED BY THE ENGINEER. WHERE CONFLICTS EXIST, THE MUTCD SHALL GOVERN. IF THE CONTRACTOR DOES NOT WISH TO FOLLOW THE TEMPORARY TRAFFIC CONTROL PROVIDED, CONTRACTOR MAY SUBMIT AN ALTERNATE PROPOSAL IN ACCORDANCE WITH SUBSECTION 641.02
 - WHEN WORKING IN THE VICINITY OF EXISTING PEDESTRIAN FACILITIES, ADA PEDESTRIAN ACCOMMODATIONS SHALL BE MAINTAINED AT ALL TIMES. PAYMENT SHALL BE INCLUDED IN THE UNIT BID PRICE FOR CONTRACT ITEM 641.10 TRAFFIC CONTROL.
 - THE CONTRACTOR SHALL PROVIDE ACCESS THROUGH THE WORK ZONE FOR EMERGENCY VEHICLES AT ALL TIMES.
 - THE CONTRACTOR SHALL CONDUCT THE WORK AT ALL TIMES IN SUCH A MANNER AND IN SUCH SEQUENCE SO AS TO ENSURE THE LEAST INTERFERENCE WITH TRAFFIC OCCURS.
 - SIGNS SHALL ONLY BE VISIBLE TO THE MOTORIST WHEN THE MESSAGE IS PERTINENT, I.E. A "FLAGGER AHEAD" SIGN SHALL ONLY BE VISIBLE TO THE MOTORIST WHEN THE FLAGGER IS ACTUALLY PRESENT PERFORMING THEIR DUTIES.
 - PAYMENT FOR CONSTRUCTION SIGNING WILL BE MADE UNDER CONTRACT ITEM 641.10.
 - SIGNS SHALL BE MOVED AS WORK AREA MOVES. AT NO TIME SHALL THE FLAGGER SIGN BE GREATER THAN 750 FEET IN ADVANCE OF THE FLAGGER STATION

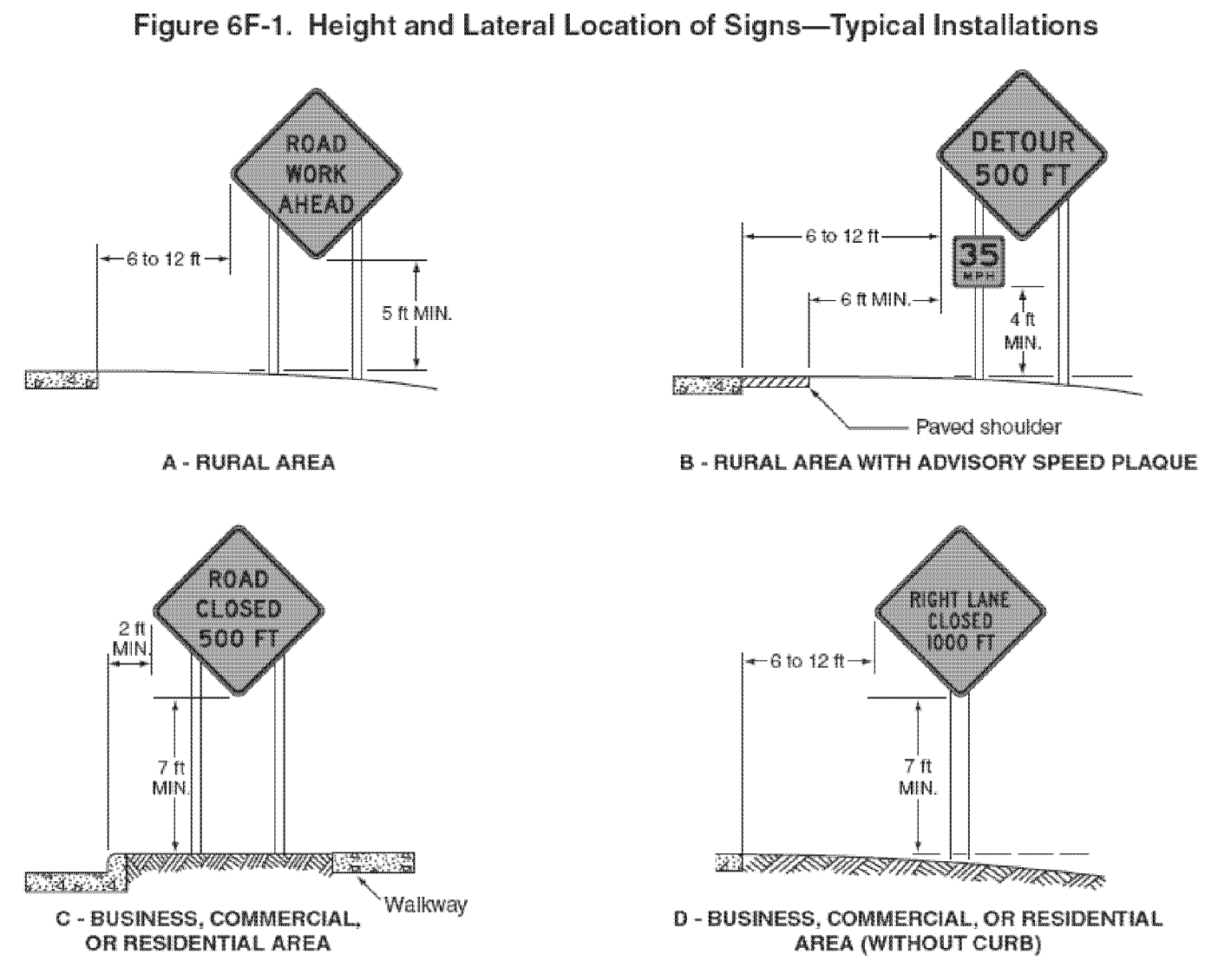


Table 6C-4. Formulas for Determining Taper Length

| Speed (S) | Taper Length (L) in feet |
|----------------|--------------------------|
| 40 mph or less | $L = \frac{WS^2}{60}$ |
| 45 mph or more | $L = WS$ |

Where: L = taper length in feet
W = width of offset in feet
S = posted speed limit, or off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed in mph

Table 6C-2. Stopping Sight Distance as a Function of Speed

| Speed* | Distance |
|--------|----------|
| 20 mph | 115 feet |
| 25 mph | 155 feet |
| 30 mph | 200 feet |
| 35 mph | 250 feet |
| 40 mph | 305 feet |
| 45 mph | 360 feet |
| 50 mph | 425 feet |
| 55 mph | 495 feet |
| 60 mph | 570 feet |
| 65 mph | 645 feet |
| 70 mph | 730 feet |
| 75 mph | 820 feet |

* Posted speed, off-peak 85th-percentile speed prior to work starting, or the anticipated operating speed