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GENERAL NOTES

1. ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO VERMONT AGENCY OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAY AND BRIDGE CONSTRUCTION, DATED MARCH 1976 AND ITS LATEST REVISIONS AND THE AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, DATED 1977 AND ITS LATEST REVISIONS.
2. THIS STRUCTURE HAS A POSTING RATING OF 8 TONS.
3. THE CONTRACTOR SHALL BE ALLOWED TO CLOSE THE ROAD TO TRAFFIC. THE SELECTMEN SHALL BE GIVEN 7 DAYS WRITTEN NOTICE OF INTENT TO CLOSE THE ROAD.
4. COST OF SIGNS AND BARRICADES REQUIRED SHALL BE SUBSIDIARY TO CONTRACT ITEMS.
5. ALL DIMENSIONS SHOWN ON THE PLANS ARE APPROXIMATE AND SHOULD BE FIELD MEASURED AND ADJUSTMENTS MADE ON THE PLANS, AS NECESSARY.
6. THE EXISTING DECK SYSTEM SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH SECTION 202, PAYMENT TO BE INCLUDED IN PARTIAL REMOVAL OF STRUCTURE.
7. ALL LUMBER AND TIMBER SHALL CONFORM TO SECTION 611 OF VERMONT AGENCY OF TRANSPORTATION SPECIFICATIONS. ALL UNTREATED LUMBER AND TIMBER SHALL BE ROUGH SAWN TO NOMINAL DIMENSIONS. ALL TREATED LUMBER AND TIMBER SHALL BE DRESSED SQUARE FOUR SIDES (S4S) AND TREATED WITH CHROMATED COPPER ARSENATE (CCA) AS PER SPECIAL PROVISIONS. UNLESS OTHERWISE NOTED, ALL LUMBER AND TIMBER SHALL BE EASTERN SPRUCE OR SOUTHERN PINE, SELECT STRUCTURAL OR NO. 1 GRADE.
8. BOARDING MAY BE REMOVED TO FACILITATE REPAIR WORK. ANY BOARDING REMOVED OR DAMAGED BY THE CONTRACTOR SHALL BE REPLACED AND / OR REBUILT OF ROUGH CUT, RANDOM WIDTH BOARDS IN A MANNER MATCHING EXISTING APPEARANCE, AS DIRECTED BY THE ENGINEER.
9. PIECES SHALL BE PLACED SUCH THAT MAXIMUM VARIATION IN DEPTH AND FINISH GRADE OR ADJACENT PIECES IN LAMINATED DECK SHALL BE 3/16".
10. NEW BRIDGE RUNNER PLANKS SHALL BE OAK (UNTREATED).
11. ALL REPLACED BOARDING SHALL BE FASTENED WITH 6d GALVANIZED NAILS. OTHER NAILING NECESSARY SHALL BE DONE AS SHOWN ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.
12. ALL STRUCTURAL MEMBERS IN THE BRIDGE SHALL BE VISUALLY INSPECTED FOR SURFACE DECAY. ALL AREAS OF SURFACE DECAY SHALL BE CLEANED BY REMOVING UNSOUND WOOD. THESE AREAS SHALL BE TREATED WITH TWO BRUSHED ON COATS OF WOODLIFE PRESERVATIVE OR APPROVED EQUIVALENT. PAYMENT TO BE INCLUDED IN PARTIAL REMOVAL OF STRUCTURE.
13. ALL NAILS, SPIKES, BOLTS, NUTS, WASHERS, LAG BOLTS, AND OTHER HARDWARE SHALL BE ZINC COATED (GALVANIZED) IN ACCORDANCE WITH ASTM A153 WITH PAYMENT TO BE SUBSIDIARY TO LUMBER AND TIMBER ITEMS.
14. ALL FLOOR BEAMS SHALL BE TOE NAILED TO LOWER BOTTOM CHORDS USING 20d GALVANIZED SPIKES.
15. NEW FLOORING SHALL BE LAMINATED 2 x 6's ON EDGE LONGITUDINALLY WITH BUTT JOINTS OCCURRING ONLY AT THE CENTERLINE OF FLOOR BEAMS AND ADJACENT BUTT JOINTS STAGGERED AT LEAST ONE FLOOR BEAM. EVERY OTHER 2 x 6 SHALL BE TOE NAILED TO EVERY OTHER FLOOR BEAM WITH 20d GALVANIZED SPIKES.
16. NOTCH TOP OF FLOOR BEAM AS REQUIRED SO THAT EVERY OTHER FLOOR BEAM WILL EXTEND THROUGH THE OPENINGS IN THE LATTICE WORK AND SET ON THE OUTSIDE MEMBER OF THE LOWER BOTTOM CHORD. THE ALTERNATING FLOOR BEAMS WILL BUTT AGAINST THE LATTICE WORK, AND WILL REST ON THE INSIDE LOWER BOTTOM CHORD ONLY.
17. EXISTING 8" x 8" LONGITUDINAL BEARING BLOCKS AND 3" x 6" TRANSVERSE BEARING BLOCKS SHALL BE REPLACED IF NECESSARY AS DETERMINED BY THE ENGINEER. NEW BEARING BLOCKS SHALL BE YELLOW BIRCH, MAPLE, OR OAK.
18. ALL CONCRETE USED FOR NEW BACKWALLS AND BRIDGE SEATS SHALL BE CONCRETE, CLASS B. ALLOWABLE DESIGN STRESS: f'c 3500 psi, f'e 1400 psi.
19. ALL EXPOSED EDGES OF CONCRETE SHALL BE CHAMFERED 1" x 1".
20. CEMENT RUBBLE MASONRY SHALL BE USED TO FACE ALL NEW EXPOSED CONCRETE IN A MANNER THAT CONCEALS THE CONCRETE WORK AND BLENDS WELL WITH THE EXISTING STONE ABUTMENTS, AS DIRECTED BY THE ENGINEER. LOCAL STONE MAY BE USED (SEE SPECIAL PROVISIONS).
21. PAVE APPROXIMATELY 25' AT EAST APPROACH AND APPROXIMATELY 15' AT WEST APPROACH. PAVEMENT SHALL BE 2" THICK AND DRAIN AWAY FROM BRIDGE AS DIRECTED BY THE ENGINEER. GRADING AND PREPARATION TO BE PAID UNDER BITUMINOUS CONCRETE PAVEMENT (SEE SPECIAL PROVISIONS FOR ALLOWABLE PAVING METHODS).
22. EXCAVATION FOR BACKWALLS SHALL BE PAID AS STRUCTURE EXCAVATION.
23. NEW CONCRETE CURB SHALL HAVE A 1" IN 6" BATTER, AS SHOWN ON PLANS.
24. PROJECT QUANTITIES ARE SHOWN ON SHEET 4 OF 7.
25. REASONABLE CARE SHALL BE USED IN THE REMOVAL OF THE EXISTING FLOOR BEAMS. THESE FLOOR BEAMS SHALL BECOME THE PROPERTY OF THE TOWN OF SHREWSBURY.

CONVENTIONAL SIGNS

- COUNTY LINE
- TOWN LINE
- LIMITS OF ACCESS
- POINT OF ACCESS
- FENCE LINE
- STONE WALL
- TRAVELED WAY
- GUARD RAIL
- RAILROAD
- SURVEY LINE
- CULVERT
- POWER POLE
- TELEPHONE POLE
- TREES
- CONTROL OF ACCESS
- PROPERTY LINE
- ROW TAKING LINE
- SLOPE RIGHTS
- TOP OF CUT
- TOE OF SLOPE

RECORD PLANS & MATERIAL SUPPLIERS  
 CONTRACTOR: BREADLOAF CONSTRUCTION CO., INC.  
 MIDDLEBURY, VERMONT

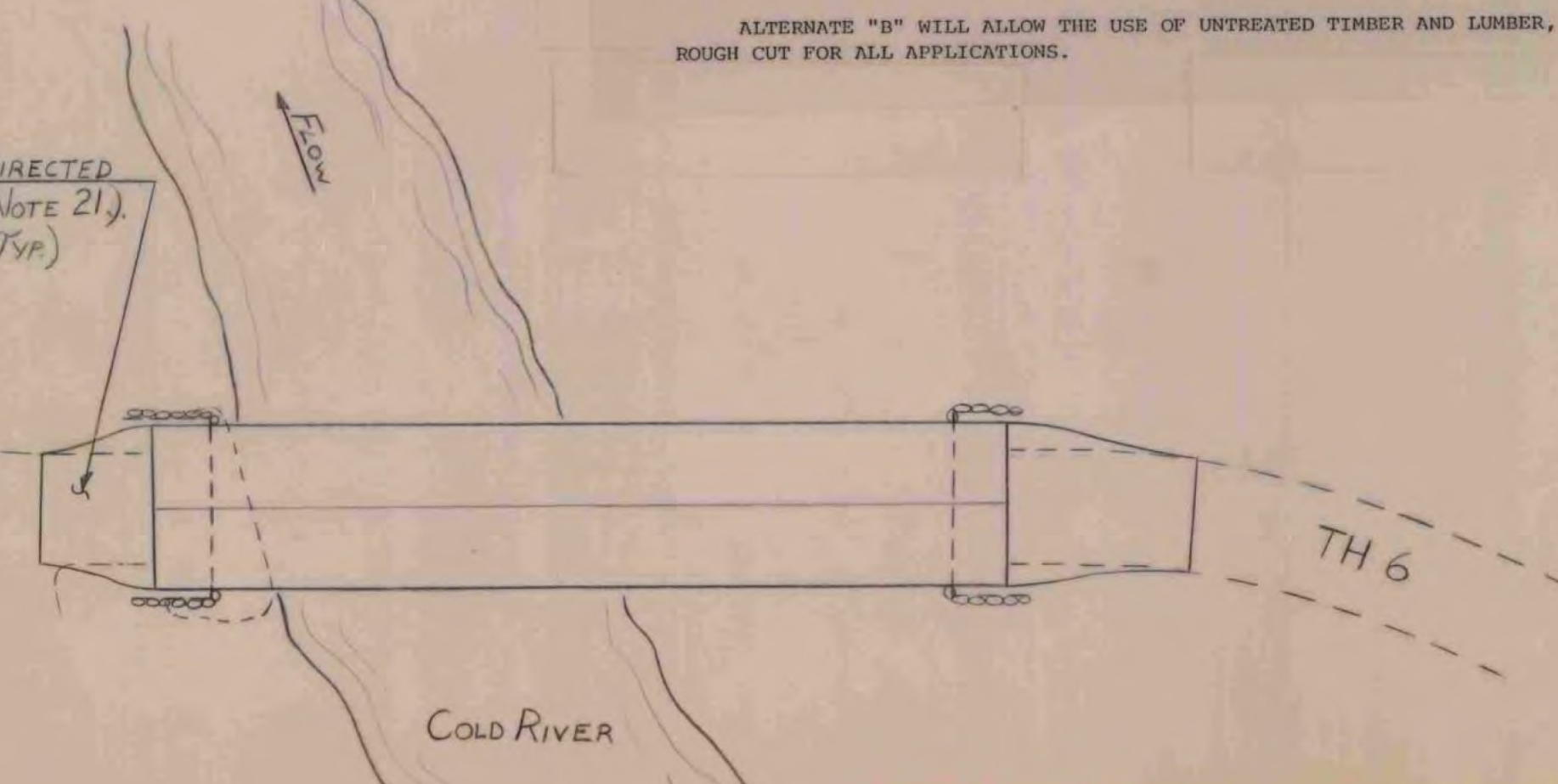
CONTRACT DATED: MAY 30, 1979  
 CONSTRUCTION COMPLETED: MAY 13, 1980  
 CONSTRUCTION ACCEPTED: MAY 20, 1980  
 RESIDENT ENGINEER: VICTOR DWIRE  
 RECORD PLANS: RICHARD P. RUSSELL, JR.

GRANULAR BACKFILL FOR STRUCTURES: J.F. CARRARA & SONS, INC. N. CLARENDON, VERMONT  
 BITUMINOUS CONCRETE PAVEMENT: F.W. WHITCOMB CONST. CORP. WALLINGFORD, VT.  
 CONCRETE, CLASS B: J.F. CARRARA & SONS, INC. N. CLARENDON, VT.  
 REINFORCING STEEL: K-ROSS BUILDING SUPPLY CENTER INC. LEBANON, N.H.  
 GUARD RAIL: LAFAYETTE-SHELDON INC. ESSEX JCT., VT.  
 TREATED & UNTREATED LUMBER & TIMBER: DIAMOND LUMBER RUTLAND, VT.

NOTE: ANY FURTHER INFORMATION CONCERNING FINAL QUANTITIES, AMOUNTS OR OTHER DETAILS RELATIVE TO THIS PROJECT MAY BE FOUND IN EITHER THE FIELD BOOK OR THE ESTIMATE FILE.

PAVE APPROACH AS DIRECTED BY THE ENGINEER (SEE GEN. NOTE 21). (TYP.)

0.10 MILES TO TH 1



PLAN SCALE 1" = 20'-0"

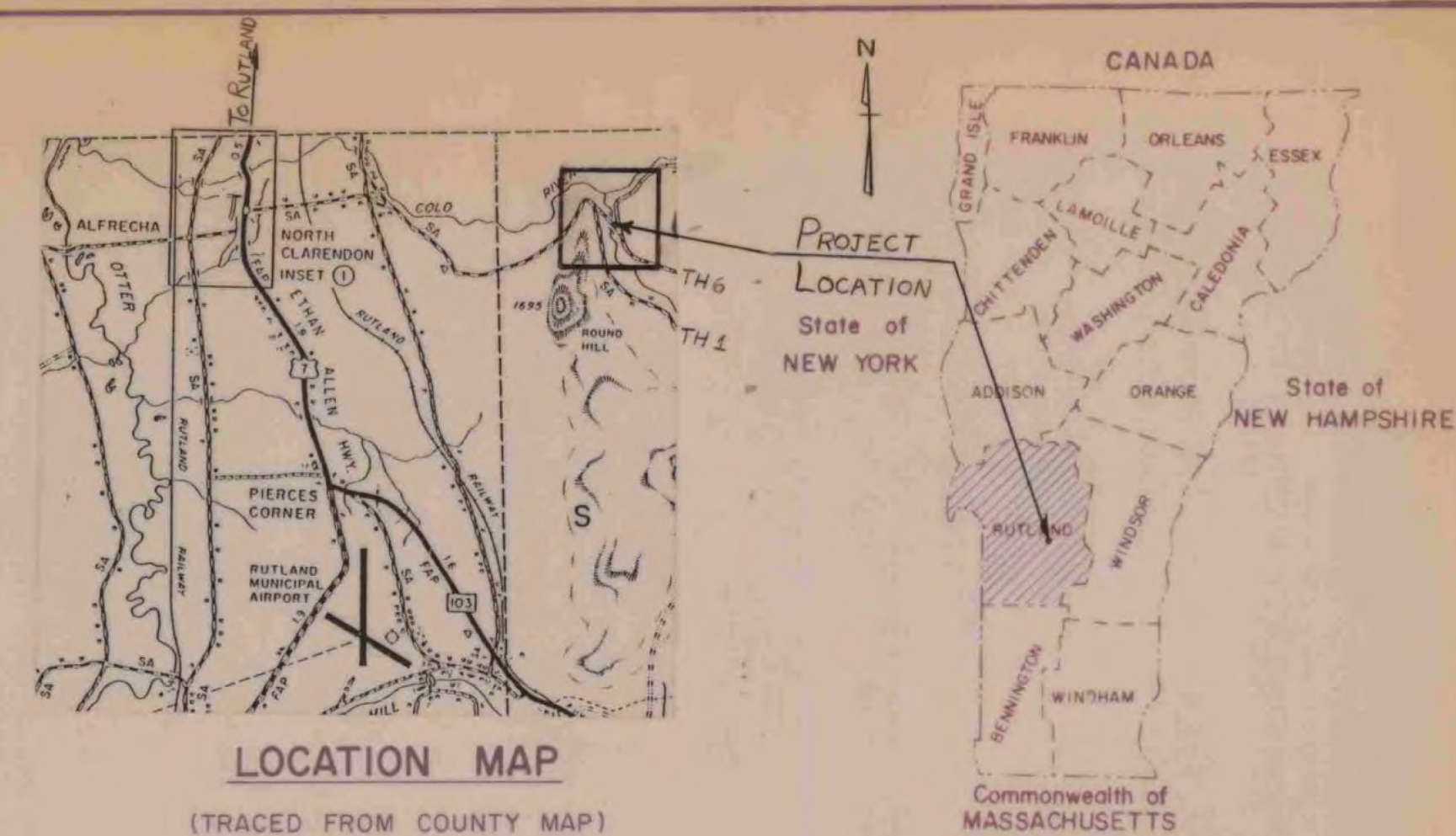
LENGTH OF BRIDGE 115.0 FT.

PROJECT DESCRIPTION:  
 ADD NEW CONCRETE BACKWALLS AND BRIDGE SEATS FACED WITH CEMENT RUBBLE MASONRY. REPLACE FLOOR BEAMS, ADD NEW LAMINATED LONGITUDINAL 2" x 6" DECK AND RUNNER PLANKS. INSTALL NEW 3" PLANK GUARDRAIL. REPLACE DETERIORATED BEARING TIMBERS.

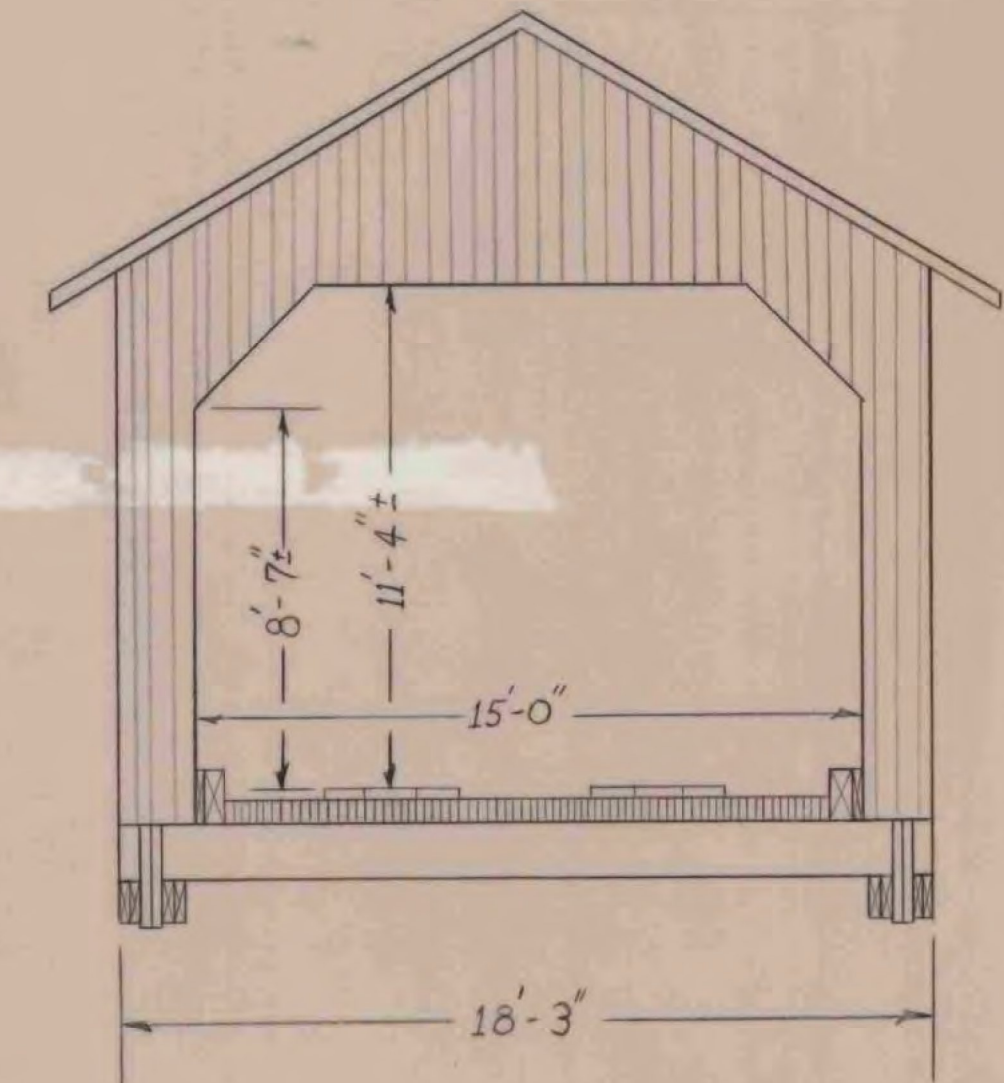
PROJECT LOCATION:  
 SHREWSBURY BRIDGE #34 (BROWN COVERED BRIDGE) OVER THE COLD RIVER ON TH 6, 0.1 MILES WEST OF TH 1.

PROPOSED IMPROVEMENT  
 BRIDGE PROJECT  
 TOWN OF SHREWSBURY  
 COUNTY OF RUTLAND  
 ROUTE NO: TH 6 BRIDGE NO: 34

STATE OF VERMONT  
 AGENCY OF TRANSPORTATION



LOCATION MAP (TRACED FROM COUNTY MAP)



END ELEVATION SCALE 1/4" = 1'-0"

EXPLANATION OF ALTERNATES  
 THE CONTRACTOR SHALL SUBMIT BIDS FOR BOTH ALTERNATE "A" (TREATED LUMBER) AND ALTERNATE "B" (UNTREATED LUMBER). THE STATE WILL SELECT ONE ALTERNATE BASED UPON THE BID RESULTS.  
 WITH BOTH ALTERNATES "A" & "B", ANY REQUIRED BOARDING AND THE RUNNER PLANKS WILL BE UNTREATED LUMBER AND TIMBER.  
 ALTERNATE "A" WILL REQUIRE THAT THE FLOOR BEAMS, DECK, DECK SPACER BLOCKS, TIMBER CURB AND BEARING BLOCKS AND TIMBERS BE TREATED TIMBER AND LUMBER. QUANTITIES SHOWN ARE BASED ON DRESSED (S4S) DIMENSIONS AND ARE THEREFORE SOMEWHAT HIGHER THAN THE CORRESPONDING QUANTITIES SHOWN UNDER ALTERNATE "B", (UNTREATED, ROUGH CUT).  
 ALTERNATE "B" WILL ALLOW THE USE OF UNTREATED TIMBER AND LUMBER, ROUGH CUT FOR ALL APPLICATIONS.

FOR REFERENCE ONLY SHEET 20 OF 23

SUBMITTED BY ORDER OF THE STATE TRANSPORTATION BOARD  
 APPROVED: *A.J. Gage* DATE: OCT 24, 1979  
 DIR. OF ENGINEERING AND CONSTRUCTION

PROJECT NO: SHREWSBURY  
 Cov. Br. 28  
 SHEET 1 OF 7 SHEETS