

BRIDGE RAILING, GALVANIZED STEEL TUBING/CONCRETE COMBINATION
 STA. 16+21 TO 17+34, LT
 STA. 16+30 TO 17+42, RT

VERTICAL GRANITE CURB
 STA. 15+26 TO 16+27, LT
 STA. 17+38 TO 18+25, LT

PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH
 STA. 15+26 TO 16+27, LT
 STA. 17+38 TO 18+25, LT

STEEL BEAM GUARDRAIL, GALVANIZED
 STA. 15+65.5 TO 16+03.0, LT
 STA. 15+80.3 TO 16+12.0, RT
 STA. 17+52.0 TO 17+89.5, LT
 STA. 17+60.0 TO 18+75.0, RT

REMOVE AND RESET GUARDRAIL
 STA. 18+75.0 TO 19+00.0
 (TRANSITION TO HEIGHT OF NEW GUARDRAIL)

ANCHOR FOR STEEL BEAM RAIL
 STA. 15+65.5, LT
 STA. 15+80.3, RT
 STA. 17+89.5, LT

REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 15+71 TO 16+01, LT
 STA. 17+32 TO 17+62, LT
 STA. 17+40 TO 18+25, RT

***CONSTRUCT DRIVE**
 STA. 14+98, RT (34' PAVED) (COMMERCIAL)
 STA. 15+08, LT (34.5' PAVED) (RESIDENTIAL)
 STA. 15+66, RT (39.5' PAVED) (COMMERCIAL)

***DRIVE CONSTRUCTION PAID AS ITEMS:**
 203.15 COMMON EXCAVATION
 301.35 SUBBASE OF DENSE GRADED CRUSHED STONE
 900.675 SPECIAL PROVISION (HAND-PLACED BITUMINOUS CONCRETE MATERIAL, DRIVES)

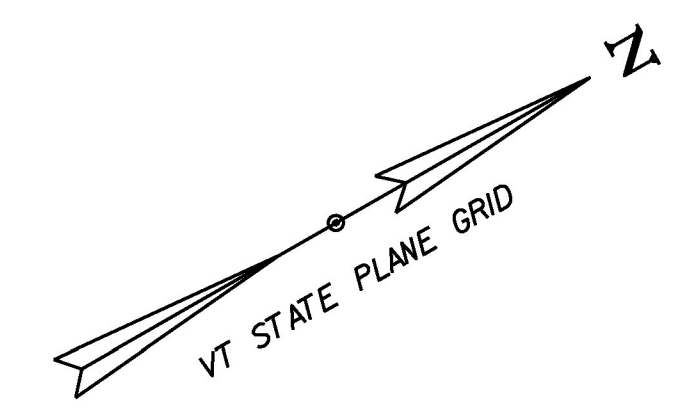
GUARDRAIL APPROACH SECTION, CONC COMB BRIDGE RAILING TL-3
 STA. 16+03 TO 16+21, LT
 STA. 16+12 TO 16+30, RT
 STA. 17+34 TO 17+52, LT
 STA. 17+42 TO 17+60, RT

RELOCATE MAILBOX, SINGLE SUPPORT
 STA. 15+75, LT

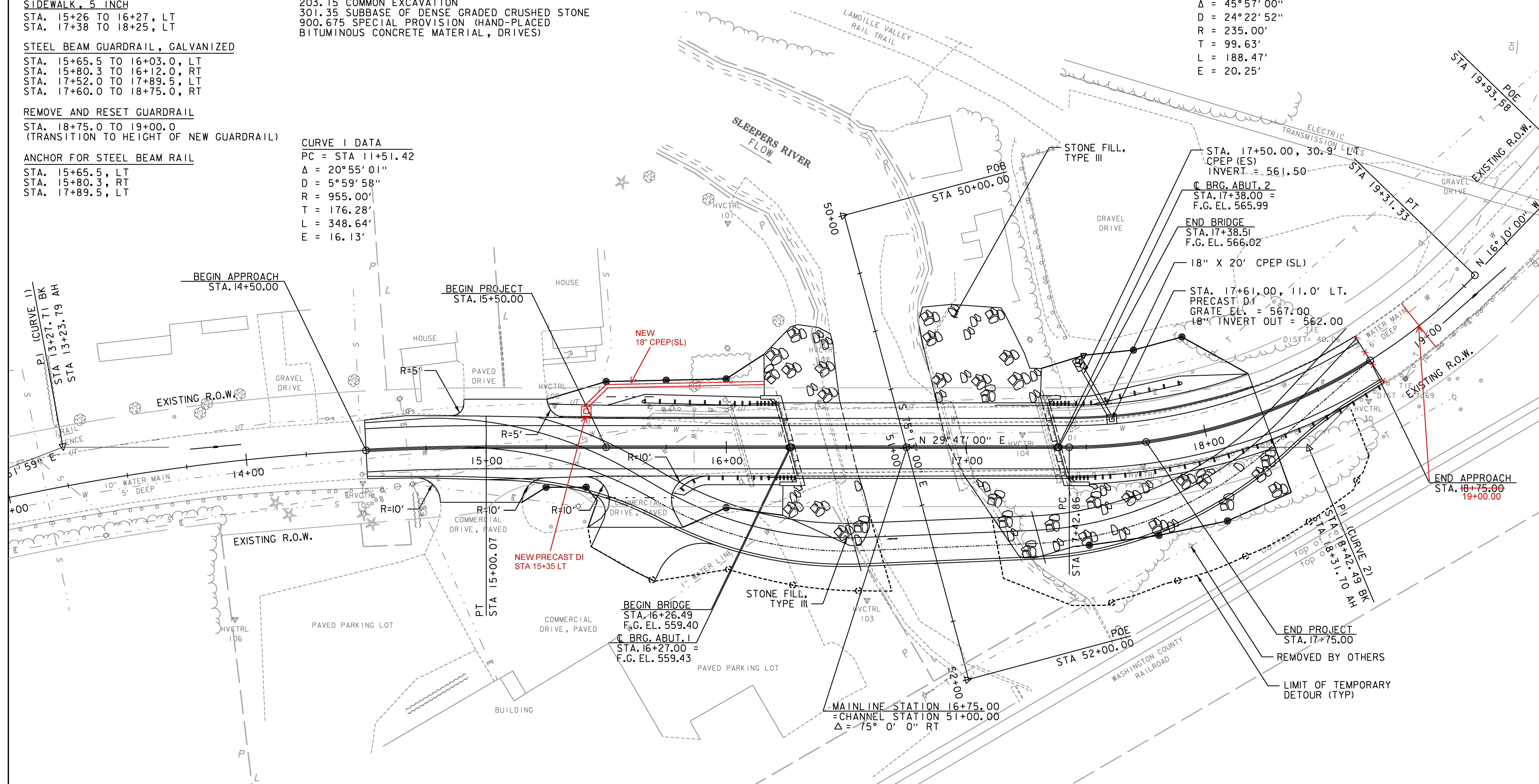
PRECAST REINFORCED CONCRETE DROP INLET WITH CAST IRON GRATE
 STA. 17+61, LT
 15+35, LT

18" CPEP (SL)
 STA. 17+50, LT TO 17+61, LT (20')
 STA. 15+35, LT TO 16+15, LT
 18" CPEPES
 STA. 17+50

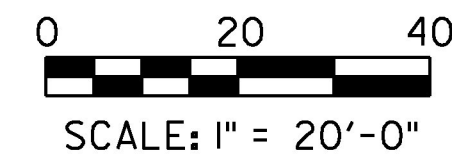
CURVE 2 DATA
 $\Delta = 45^\circ 57' 00''$
 $D = 24^\circ 22' 52''$
 $R = 235.00'$
 $T = 99.63'$
 $L = 188.47'$
 $E = 20.25'$



CURVE 1 DATA
 PC = STA 11+51.42
 $\Delta = 20^\circ 55' 01''$
 $D = 5^\circ 59' 58''$
 $R = 955.00'$
 $T = 176.28'$
 $L = 348.64'$
 $E = 16.13'$



LAYOUT



PROJECT NAME:	ST JOHNSBURY	PLOT DATE:	9/30/2016
PROJECT NUMBER:	BRO 1447(30)	DRAWN BY:	I. MAYNARD
FILE NAME:	z12j164bdr.dgn	CHECKED BY:	G. SANTY
PROJECT LEADER:	M. CHENETTE	SHEET	12 OF 76
DESIGNED BY:	I. MAYNARD		
PLAN LAYOUT SHEET			

