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STATE OF VERMONT
AGENCY OF TRANSPORTATION



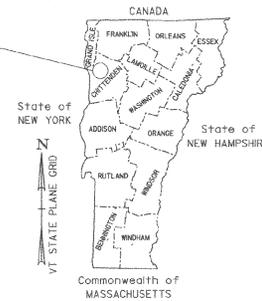
PROPOSED IMPROVEMENT
CLASS I TOWN HIGHWAY
CITY OF WINOOSKI
COUNTY OF CHITTENDEN
U.S. ROUTE 7 & VT ROUTE 15

BEGINNING IN THE CITY OF WINOOSKI ON U.S. ROUTE 7 AT STA. 0+053.11 (MM 0.033) AND EXTENDING NORTHERLY ALONG U.S. ROUTE 7 FOR A DISTANCE OF 387.85 M (0.241 MILE) TO STA. 0+440.96 (MM 0.274), IN ADDITION TO THE WORK ON U.S. ROUTE 7, APPROXIMATELY 51M (0.03 MILE) OF WEST ALLEN STREET HAS BEEN INCLUDED AS APPROACH CONSTRUCTION ALSO BEGINNING IN THE CITY OF WINOOSKI ON VT ROUTE 15 AT STA. 0+009.66 (MM 0.006) AND EXTENDING EASTERLY ALONG VT ROUTE 15 FOR A DISTANCE OF 212.43 M (0.132 MILE) TO STA. 0+222.09 (MM 0.138)

PROJECT DATA	LENGTH (M)	LENGTH (MILES)
CITY OF WINOOSKI, U.S. ROUTE 7 STA. 0+053.11 TO STA. 0+440.96 MM 0.033 TO MM 0.274	= 387.85	0.241
CITY OF WINOOSKI, VT ROUTE 15 STA. 0+009.66 TO STA. 0+222.09 MM 0.006 TO MM 0.138	= 212.43	0.132
TOTAL LENGTH OF PROJECT	= 600.28	0.373
TOTAL LENGTH OF ROADWAY	= 651.28	0.403

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES FULL & PARTIAL DEPTH REPAIR AND DIAMOND GRINDING OF EXISTING PORTLAND CEMENT CONCRETE PAVEMENT ON U.S. ROUTE 7 AND THE WEST ALLEN STREET APPROACH. WORK ALSO INCLUDES THE COLD PLANNING AND RESURFACING OF VT ROUTE 15, WITH BITUMINOUS CONCRETE PAVEMENT, NEW PAVEMENT MARKINGS AND INCIDENTAL ITEMS SHALL BE INCLUDED ON BOTH U.S. ROUTE 7 & VT ROUTE 15.

PROJECT LOCATION
STP 2131(1)S



TRAFFIC DATA

U.S. ROUTE 7 & VT ROUTE 15

LOCATION	ADT		DHW		ESALS
	2000	2010	2000	2010	
U.S. ROUTE 7 SOUTH OF VT 15	22,200	27,650	2,500	3,100	8,850,000 (RIGID ESAL'S)
U.S. ROUTE 7 NORTH OF VT 15	11,700	14,600	1,300	1,650	8,710,000 (RIGID ESAL'S)
VT ROUTE 15 EAST OF U.S. 7	15,300	19,100	1,700	2,150	4,345,000 (RIGID ESAL'S)

RECORD PLANS

CONTRACTOR: F.W. WHITCOMB CONSTRUCTION - WALPOLE, NH
 RESIDENT ENGINEER: R. STANCLIFF
 CONSTRUCTION BEGAN: JULY 24, 2000
 CONSTRUCTION COMPLETE: JUNE 8, 2001
 RECORD PLANS BY: R. STANCLIFF
 I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.
 BY: *R. Stancliff* RESIDENT ENGINEER
 DATE: August 30, 2001
 NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found on microfiche in Central Files.



CONVENTIONAL SYMBOLS

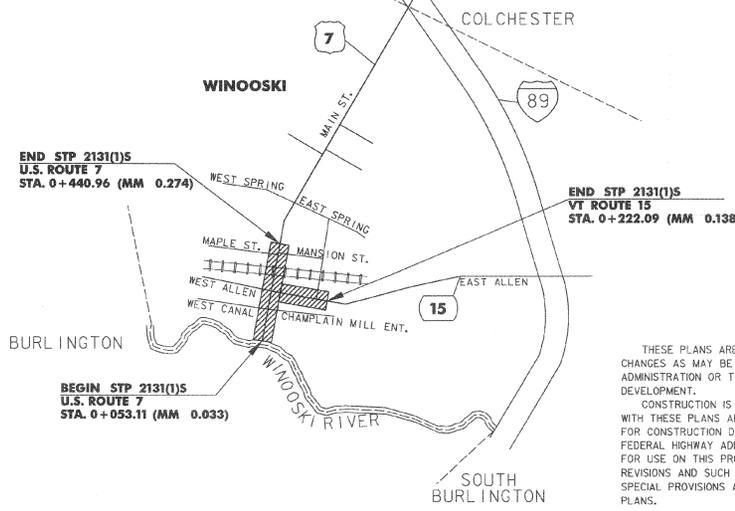
COUNTY LINE	— — — — —
TOWN LINE	— — — — —
LIMITS OF ACCESS	— — — — —
POINT OF ACCESS	X
FENCE LINE	X X X X X
STONE WALL	— — — — —
TRAVELED WAY	— — — — —
GUARD RAIL	— — — — —
RAILROAD	— — — — —
SURVEY LINE	— — — — —
CULVERT	— — — — —
POWER POLE	— — — — —
TELEPHONE POLE	— — — — —
TREES	— — — — —
CONTROL OF ACCESS	— — — — —
PROPERTY LINE	— — — — —
R.O.M. TAKING LINE	— — — — —
SLOPE RIGHTS	— — — — —
TOP OF CUT	— — — — —
TOE OF SLOPE	— — — — —

UTILITY LEGEND	
⊕	= EXISTING HYDRANT
⊙	= EXISTING DI
○	= EXISTING MANHOLE
⊗	= EXISTING TELEPHONE MANHOLE
⊗ELEC	= EXISTING ELECTRIC MANHOLE
⊗SMH	= EXISTING SEWER MANHOLE
WSO	= EXISTING WATER SHUTOFF
GSO	= EXISTING GAS SHUTOFF
♂	= EXISTING MAILBOX
⊠	= EXISTING SIGNAL HEAD
⊠P	= EXISTING PEDESTRAIN SIGNAL HEAD

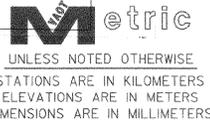
SIGN LEGEND	
N	= NEW
R	= REMOVE
R&S	= REMOVE & SALVAGE
S	= SALVAGE SIGN
RET	= RETAIN
B-B	= BACK TO BACK
⓪	= RETURN TO CITY OF WINOOSKI

SURVEYED BY : D-H	
SURVEYED DATE : 06-02-99	

DATUM	
VERTICAL	—
HORIZONTAL	—



THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROJECT DEVELOPMENT.
 CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1995, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON AUGUST 21, 1995 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.



DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATOR	
APPROVED	DATE
DIRECTOR OF PROJECT DEVELOPMENT	
APPROVED	DATE
PROJECT MANAGER :	
PROJECT NAME :	WINOOSKI
PROJECT NUMBER :	STP 2131(1)S
SHEET 1 OF 51 SHEETS	

NOTES

1. THE BITUMINOUS PAVEMENT WEARING COURSE SHALL BE TYPE III. THE 15 mm LEVELING COURSE SHALL BE TYPE IV UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ALL ASPHALT CEMENT USED IN THE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 64-28.
2. GRASS GROWING ADJACENT TO PAVEMENT OR THROUGH CRACKS IN THE PAVEMENT WHICH MAY HAMPER THE PLACEMENT OF NEW BITUMINOUS CONCRETE SHALL BE REMOVED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. PAYMENT FOR THIS WORK WILL NOT BE MADE DIRECTLY, BUT WILL BE CONSIDERED SUBSIDIARY TO ITEM 406.25 BITUMINOUS CONCRETE PAVEMENT.
3. BITUMINOUS CONCRETE PAVEMENT TOLERANCE \pm 5 mm. (TOTAL THICKNESS EXCLUDING LEVELING)
4. EMULSIFIED ASPHALT SHALL BE APPLIED ON EXISTING BITUMINOUS PAVEMENT SURFACES, BETWEEN ALL COURSES OF PAVEMENT AND ON COLD PLANING SURFACES, AT THE RATE OF 0.12 L/m² OR AS DIRECTED BY THE ENGINEER.
5. SEE SHEET 19 FOR VEHICLE DETECTOR LOOP NOTES.
6. COLD PLANING TO BE COMPLETED ACCORDING TO TYPICAL OR AS NOTED OTHERWISE ON THE PLANS. THE COLD PLANING AND PAVING SHALL MATCH THE EXISTING CONDITIONS AT THE BEGINNING AND END OF CONSTRUCTION AREAS BY THE USE OF A VERTICAL BUTT JOINT. SEE DETAIL ON THIS SHEET. THE COLD PLANING AND PAVING SHALL MATCH THE CONCRETE PAVEMENT SURFACE AFTER THE DIAMOND GRINDING PROCESS, WHICH WILL LOWER THE SURFACE APPROXIMATELY 20 mm.
7. THE DRIVES ON VT ROUTE 15 SHALL RECEIVE A PAVED APRON TO THE EDGE OF SIDEWALK AS DIRECTED BY THE RESIDENT ENGINEER. ANY AND ALL REQUIRED EXCAVATION IN DRIVE AREAS SHALL BE AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 210.10. IF REQUIRED, A NEW DRIVEWAY SUBBASE SHALL BE CONSTRUCTED AND WILL BE PAID FOR UNDER ITEM 301.28. A NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 406.25. ESTIMATED QUANTITIES OF THE ABOVE ITEMS HAVE BEEN INCLUDED TO PAY FOR THIS WORK.
8. THE FOLLOWING SEQUENCE SHALL BE FOLLOWED FOR REPAIR WORK TO BE DONE IN THE PORTLAND CEMENT CONCRETE (P.C.C.) AREAS:
 - A) COMPLETE ALL FULL-DEPTH AND PARTIAL-DEPTH JOINT REPAIRS, SEALING THE JOINTS AS THEY ARE REPAIRED.
 - B) DIAMOND GRIND THE P.C.C. AREAS. NO CONCRETE WORK SHALL BE DONE ON BRIDGE NO. 151.
 - C) RE-SEAL ALL JOINTS IN THE AREAS AFFECTED BY DIAMOND GRINDING. FOR DETAILS, SEE SHEETS 20 & 21.
 - D) APPLY ALL PAVEMENT MARKINGS AS SHOWN ON THE LAYOUT SHEETS.
9. FOR EXISTING PORTLAND CEMENT CONCRETE PAVEMENT JOINT REPAIR DETAILS AND NOTES, SEE SHEETS 20 AND 23.

URBAN AREAS - SEED MIXTURE

% WT	KG/HA	NAME	PUR %	GERM %
42.2	38	CREeping RED FESCUE	98	85
10.0	9	PERENNIAL RYE GRASS	95	90
42.2	38	KENTUCKY BLUE GRASS	85	85
5.6	5	ANNUAL RYE GRASS	95	85
100	90			

SEED MIXTURE:
SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.

SEED:
TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.

FERTILIZER:
FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 560 KG/HA. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA.)

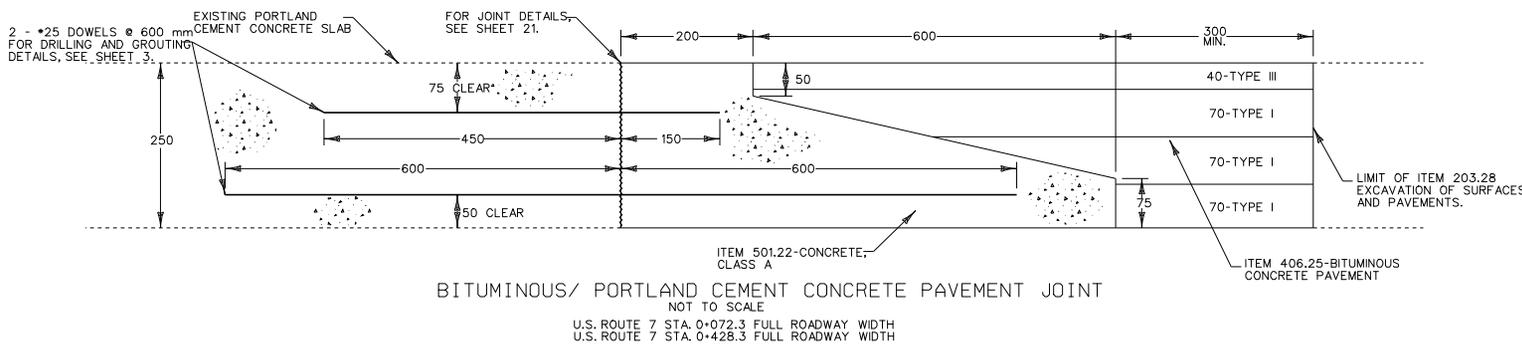
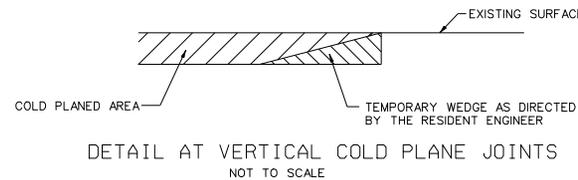
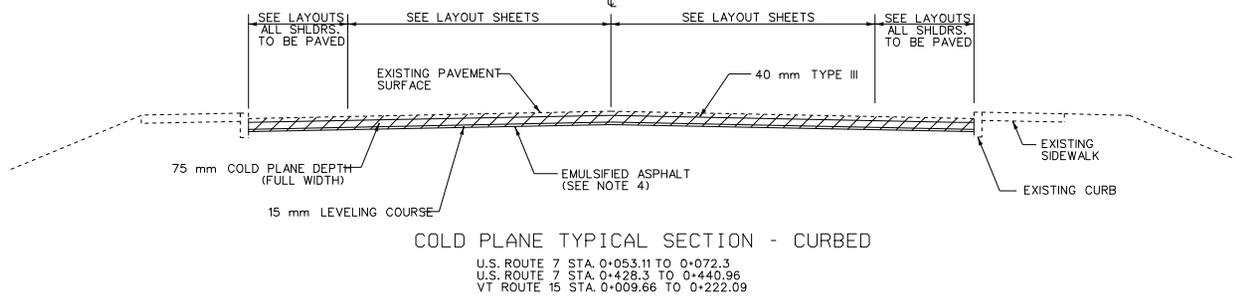
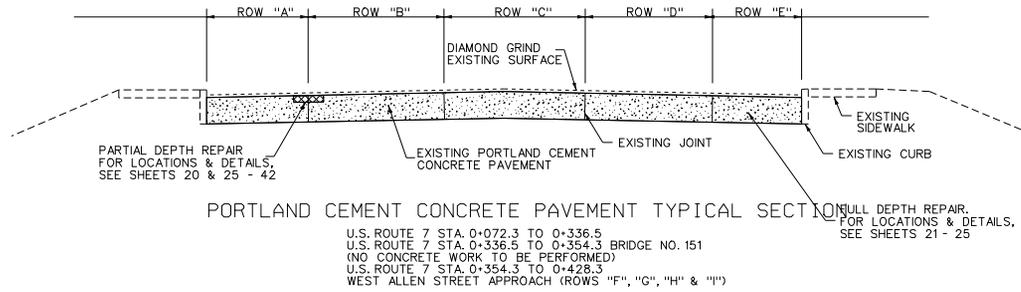
AGRICULTURAL LIMESTONE:
TO BE APPLIED AT THE RATE OF 4500 KG/HA, OR AS DIRECTED BY THE ENGINEER.

HAY MULCH:
TO BE PLACED ON EARTH SLOPES AT THE RATE OF 4500 KG/HA, OR AS DIRECTED BY THE ENGINEER.

TOPSOIL:
TO BE USED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.

NOTE:
ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED

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IN DPR

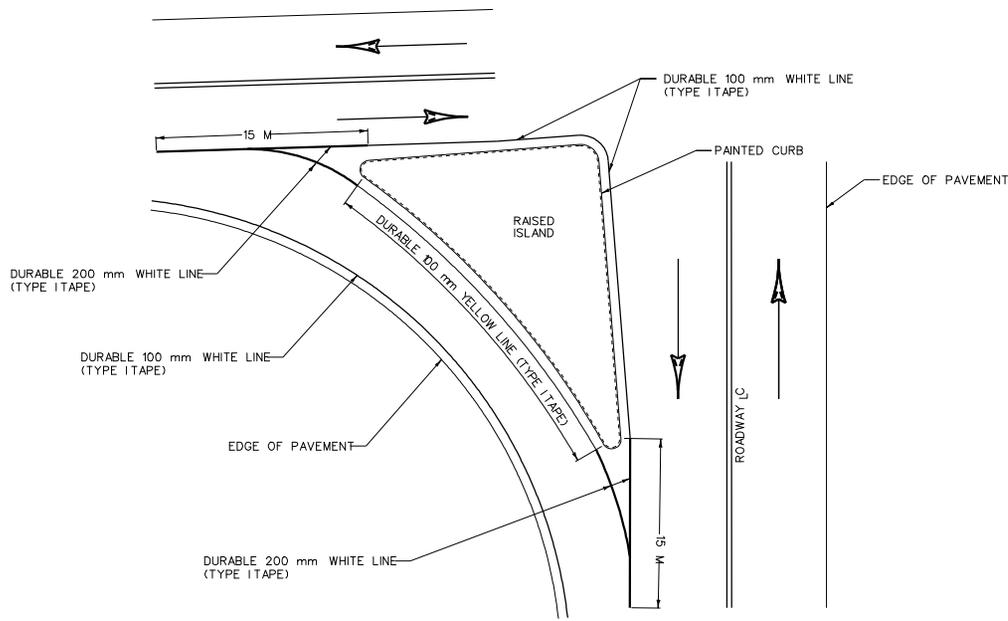


PROJECT PAVING LIMITS

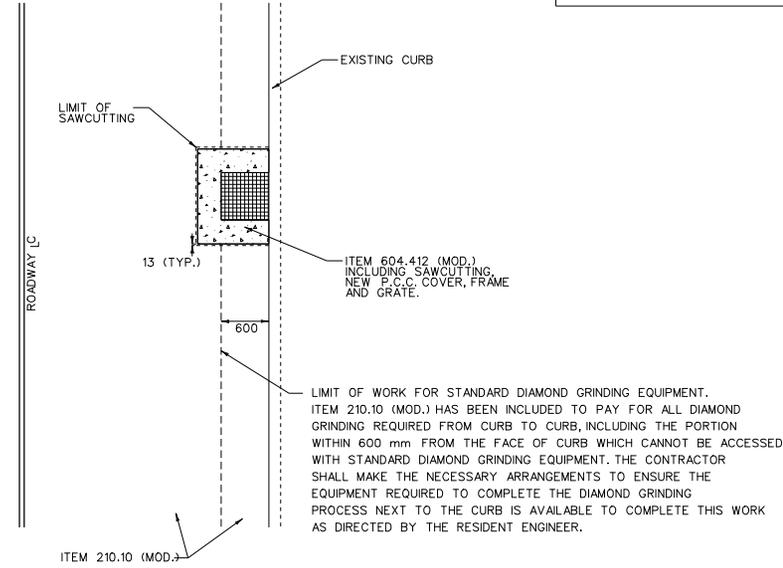
TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TONS	NOTES
WINOOSKI U.S. ROUTE 7	0+053.11	0+072.3	VARIABLES - SEE LAYOUT SHEETS	40 mm	10	COLD PLANE 75 mm. LEVEL WITH 15 mm, THEN PAVE WITH 40 mm TYPE III
WINOOSKI U.S. ROUTE 7	0+072.3	0+336.5	VARIABLES - SEE LAYOUT SHEETS & DETAILS	-	-	REPAIR JOINTS AS SHOWN ON DETAILS AND AS DIRECTED BY THE RESIDENT ENGINEER
WINOOSKI U.S. ROUTE 7	0+336.5	0+354.3	VARIABLES - SEE LAYOUT SHEETS	-	-	BRIDGE NO. 151 - NO CONCRETE WORK TO BE PERFORMED
WINOOSKI U.S. ROUTE 7	0+354.3	0+428.3	VARIABLES - SEE LAYOUT SHEETS & DETAILS	-	-	REPAIR JOINTS AS SHOWN ON DETAILS AND AS DIRECTED BY THE RESIDENT ENGINEER
WINOOSKI U.S. ROUTE 7	0+428.3	0+440.96	VARIABLES - SEE LAYOUT SHEETS	40 mm	7	COLD PLANE 75 mm. LEVEL WITH 15 mm, THEN PAVE WITH 40 mm TYPE III
WINOOSKI VT ROUTE 15	0+009.66	0+222.09	VARIABLES - SEE LAYOUT SHEETS	40 mm	153	COLD PLANE 75 mm. LEVEL WITH 15 mm, THEN PAVE WITH 40 mm TYPE III

PROJECT TYPICALS

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 2 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096pt.i	



PAVEMENT MARKING DETAIL AT RAISED ISLAND LOCATIONS
NOT TO SCALE

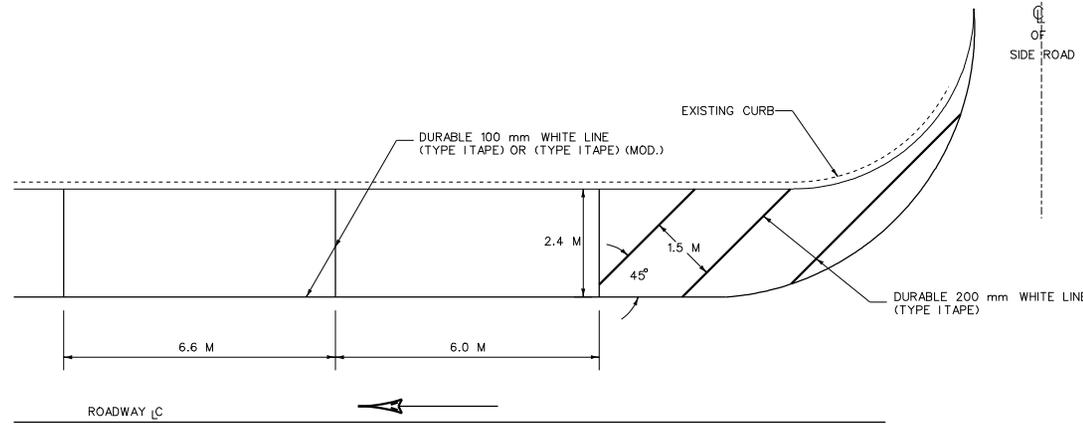
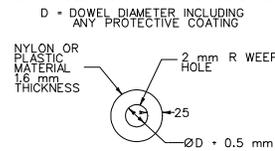


DIAMOND GRINDING & SAWCUTTING DETAIL

DOWEL PLACEMENT SEQUENCE

1. AFTER DRILLING, THE HOLE SHALL BE BLOWN OUT AND ALLOWED TO DRY.
2. THE CEMENT GROUT OR EPOXY SHALL BE PLACED IN THE HOLE, FROM BACK TO FRONT, BY USE OF A FLEXIBLE TUBE.
3. INSERT THE DOWEL INTO THE HOLE WITH A SLIGHT TWISTING MOTION SUCH THAT THE MATERIAL IN BACK IS FORCED UP AND AROUND THE DOWEL.
4. FOR DOWEL BAR ALIGNMENT, NOTE THAT THE DOWELS SHALL BE PLACED AT THE DEPTHS SHOWN ON THE JOINT DETAIL ON SHEET 2, AND PERPENDICULAR TO THE JOINT FACE.
5. INSERT THE GROUT RETENTION DISC OVER THE DOWEL & UP AGAINST THE SLAB FACE. THE DISC SHALL HAVE A TIGHT FIT OVER THE DOWEL AND BE FLUSH AGAINST THE SLAB FACE.
6. ALL MATERIALS AND LABOR ASSOCIATED WITH DRILLING AND GROUTING DOWELS SHALL BE CONSIDERED SUBSIDIARY TO ITEM 507.17.

GROUT RETENTION DISC DETAIL

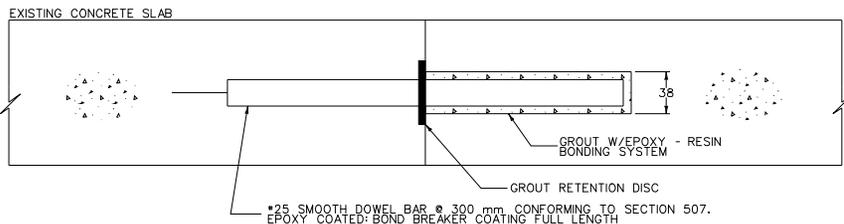


PARKING SPACE DETAIL

NOT TO SCALE

- NOTES:
- 1) ALL EXTERNAL PARKING SPACES ARE 6.0 M UNLESS OTHERWISE NOTED.
 - 2) ALL INTERNAL PARKING SPACES ARE 6.6 M UNLESS OTHERWISE NOTED.
- FOR ACTUAL LOCATIONS, SEE LAYOUT SHEETS.

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DRILLING AND GROUTING DOWEL DETAIL
NOT TO SCALE

NOTE:
ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED

TRAFFIC FLOW →

<p>MISCELLANEOUS DETAILS</p>	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 213(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 3 OF 51
DESIGNED BY: D-H		
IPARM FILE NAME: pd096md.i		

STATE OF VERMONT
AGENCY OF TRANSPORTATION

ITEM DETAIL SUMMARY SHEET



STATION		POS.	203.16	203.28	204.21	301.28	408.30	NEW PIPE			501.22	501.25	507.17	604.40	604.412	604.42	604.47	621.20	621.54	629.20	REMARKS	
BEGIN	END		SOLID ROCK EXCAV. m³	EXCAV. OF SURF. & PAVEMENTS m³	TRENCH EXCAVATION OF ROCK m³	SUBBASE OF CR. GRAVEL T	ONE COURSE CEM. CONC. & PAVEMENT. m²	D	L	TH/CL	EA	m³	m³	EPOXY COATED REINFORCING STEEL KG	CHAN ELEV EA	REHAB DI CLASS 1 (MOD.) EA	CHAN ELEV OF SEWER MH'S EA	CAST IRON GRATE WITH FRAME, TYPE EA	STEEL BEAM G.R. (MOD.) M	MODIFIED ECCENTRIC LOADER TERMINAL EA		ADJUST ELEV OF VALVE BOXES EA
U.S. ROUTE 7																						
0+071.5	0+072.3	LT&RT		2							2		187								CONSTRUCT BITUMINOUS/PCC JOINT DETAIL. FOR DETAILS, SEE SHEET 2.	
0+129.0		LT			11								39								FULL DEPTH CONCRETE REPAIR OF SLAB *S A12, A13 & A14. FOR DETAILS, SEE SHEETS 21-23.	
0+259.0		LT			15								54								FULL DEPTH CONCRETE REPAIR OF SLAB *S F1, F2, G1 & G2. FOR DETAILS, SEE SHEETS 21-23.	
0+428.3	0+429.1	LT&RT		2							2		179								CONSTRUCT BITUMINOUS/PCC JOINT DETAIL. FOR DETAILS, SEE SHEET 2.	
VT ROUTE 15																						
0+009.66	0+222.09	LT&RT				1								17	1	5					18	FOR LOCATIONS OF ITEMS 604.40, 604.42 AND 629.20, SEE SHEET 51.
SUBTOTAL				4	26	1	111				4		459	17	1	5					18	
ROUNDING				1	1	-	9				1		11	-	-	-					-	
PROJECT TOTALS				5	27	1	120				5		470	17	1	5					18	

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PROJECT NAME :	WINOOSKI	PROJECT NO. :	STP 2131(1)s
DESIGN FILE NAME:	pave/99d096/pd096.dgn	PLOT DATE:	08-JAN-2003
IPARM FILE NAME:	PD096ID.1	SURVEY DATE:	
SURVEYED BY:	D-H	DRAWN BY:	D-H
DESIGNED BY:	D-H	SHEET:	7 OF 51

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE)
 STA. 0+053.1 TO 0+072.3, SOLID LT. & RT.
 STA. 0+053.1 TO 0+072.3, DASHED LT. & RT.
 STA. 0+128.9, DOUBLE SOLID LT. (WEST CANAL STREET WITH EDGELINE BREAKS FOR CROSSWALK)
 STA. 0+128.9, SOLID LT. (WEST CANAL STREET PARKING SPACES)
 STA. 0+128.9, DOUBLE SOLID RT. (CHAMPLAIN MILL ENTRANCE "A" WITH EDGELINE BREAKS FOR CROSSWALKS)
 STA. 0+138.0, RT. (FOR DETAILS @ ISLAND SEE SHEET 3)

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE)
 STA. 0+053.1 TO 0+072.3, SOLID LT. & RT.
 STA. 0+128.9, DOUBLE SOLID LT. (WEST CANAL STREET)
 STA. 0+128.9, DOUBLE SOLID RT. (CHAMPLAIN MILL ENTRANCE "A" WITH CENTERLINE BREAK FOR CROSSWALK)
 STA. 0+138.0, RT. (FOR DETAILS @ ISLAND SEE SHEET 3)

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE)
 STA. 0+128.9, LT. & RT.
 TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE) (MOD.)
 STA. 0+136.0, LT. & RT.

TEMPORARY LETTER OR SYMBOL
 STA. 0+101.5, RT. - ARROW
 STA. 0+102.0, RT. - DOUBLE ARROW
 STA. 0+104.8, RT. - ARROW
 STA. 0+128.9, RT. - (2) DOUBLE ARROWS
 STA. 0+128.9, RT. - ARROW
 STA. 0+128.9, RT. - "YIELD"
 STA. 0+156.7, RT. - ARROW
 STA. 0+159.5, LT. - DOUBLE ARROW
 STA. 0+160.0, RT. - ARROW

REMOVING SIGNS AS SHOWN - 5
 ERECTING SALVAGED SIGNS AS SHOWN - 4
 PAINTED CURB
 STA. 0+072.3 TO 0+095.5, LT.
 STA. 0+110.0 TO 0+122.4, LT.
 STA. 0+128.9, RT. (ISLAND)
 STA. 0+133.7 TO 0+141.3, LT.

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE) (MOD.)
 STA. 0+072.3 TO 0+170.0, SOLID LT. & RT.
 (WITH EDGELINE BREAKS FOR SIDE STREETS)
 STA. 0+072.3 TO 0+117.0, DASHED LT. & RT.
 STA. 0+076.0 TO 0+100.0, DOTTED RT.
 STA. 0+090.5 TO 0+110.5, SOLID LT. (PARKING SPACES)
 STA. 0+100.0 TO 0+117.0, SOLID RT. (LANE LINE)
 STA. 0+140.0 TO 0+170.0, SOLID LT. (PARKING SPACES)
 STA. 0+140.0 TO 0+170.0, DASHED RT.
 STA. 0+146.0 TO 0+170.0, DOTTED LT.
 STA. 0+146.0 TO 0+161.5, SOLID LT. (LANE LINE)
 STA. 0+161.5 TO 0+170.0, DOTTED LT. & RT.

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE) (MOD.)
 STA. 0+072.3 TO 0+170.0, SOLID LT. & RT.
 (WITH CENTERLINE BREAK FOR WEST CANAL STREET & CHAMPLAIN MILL ENTRANCE "A")

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE)
 STA. 0+128.9, LT. (NO PARKING ZONE- WEST CANAL STREET)
 STA. 0+138.0, RT. (AT ISLAND)

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE) (MOD.)
 STA. 0+072.3 TO 0+170.0, LT. (NO PARKING ZONES) (FOR DETAILS SEE SHEET 3)

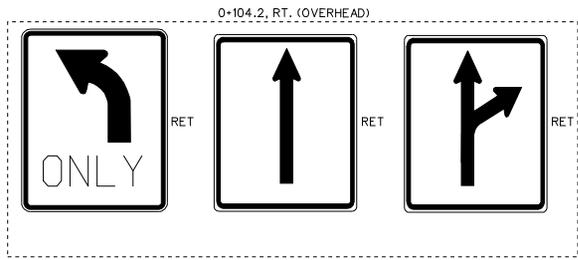
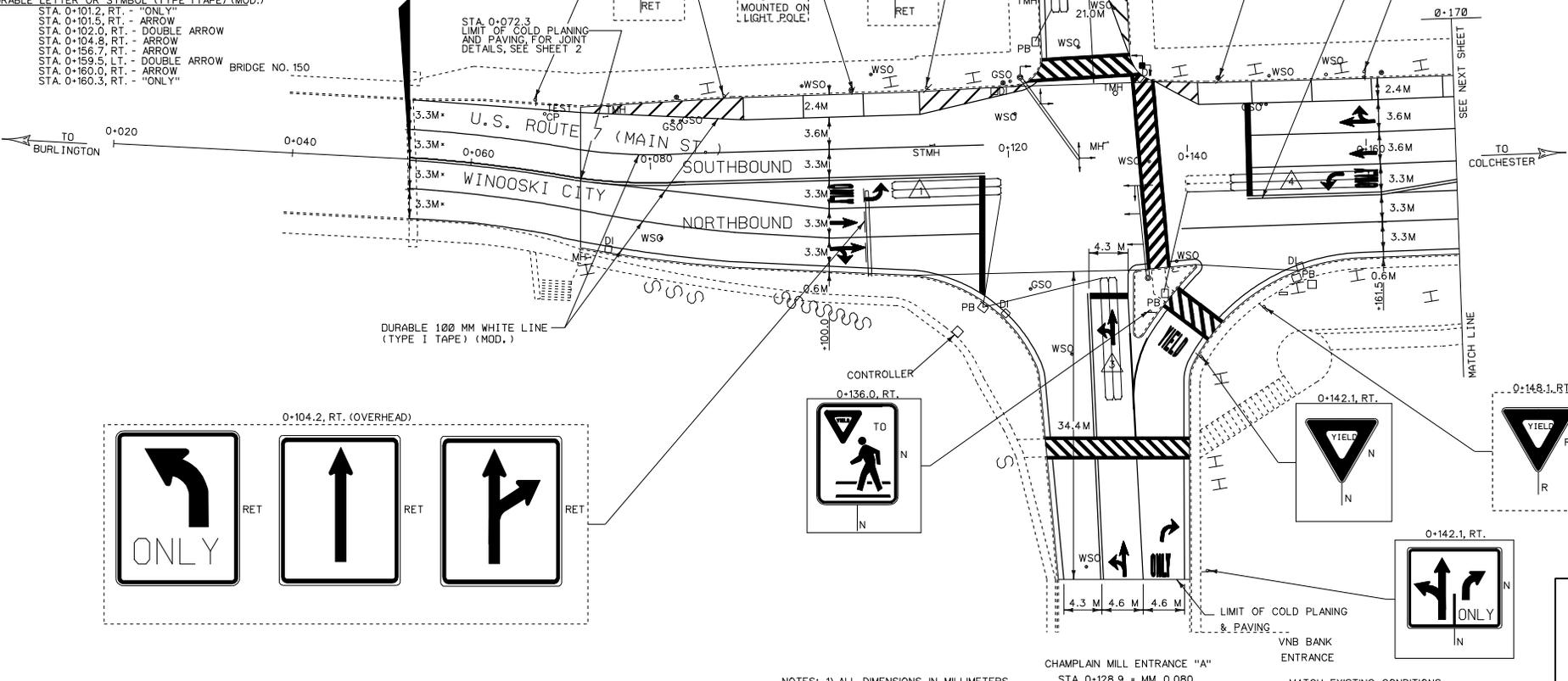
TEMPORARY AND DURABLE 600 mm STOP BAR (TYPE I TAPE)
 STA. 0+128.9, LT. & RT.
 (STOP BARS SHALL BE PLACED A MINIMUM OF 12 M FROM NEAREST SIGNAL HEAD)

TEMPORARY AND DURABLE 600 mm STOP BAR (TYPE I TAPE) (MOD.)
 STA. 0+117.0, RT.
 STA. 0+147.0, LT.
 (STOP BARS SHALL BE PLACED A MINIMUM OF 12 M FROM NEAREST SIGNAL HEAD)

BEGIN STP 2131(1) S
 U.S. ROUTE 7
 WINOOSKI
 STA. 0+053.11 = MM 0.033

DURABLE LETTER OR SYMBOL (TYPE I TAPE)
 STA. 0+128.9, RT. - (2) DOUBLE ARROWS
 STA. 0+128.9, RT. - "ONLY"
 STA. 0+128.9, RT. - ARROW
 STA. 0+128.9, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE I TAPE) (MOD.)
 STA. 0+101.2, RT. - "ONLY"
 STA. 0+101.5, RT. - ARROW
 STA. 0+102.0, RT. - DOUBLE ARROW
 STA. 0+104.8, RT. - ARROW
 STA. 0+156.7, RT. - ARROW
 STA. 0+159.5, LT. - DOUBLE ARROW
 STA. 0+160.0, RT. - ARROW
 STA. 0+160.3, RT. - "ONLY"



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
 2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.
 △ LOOP NO. - FOR DETAILS SEE SHEET 19

* MATCH EXISTING CONDITIONS
 PROJECT NAME: WINOOSKI
 PROJECT NUMBER: STP 2131(1)S
 FILE NAME: pave/99d096/pd096.dgn
 PROJECT LEADER: JLL
 DESIGNED BY: D-H
 IPARM FILE NAME: pd096i01.i
 PLOT DATE: 08-JAN-2003 08
 DRAWN BY: D-H
 CHECKED BY:
 SHEET 8 OF 51

BUILT AS DESIGNED

ARCHIVED IN DPR



TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE ITAPE)
 STA. 0+187.7, DOUBLE SOLID LT. (WEST CENTER STREET WITH EDGELINE BREAKS FOR CROSSWALK)
 STA. 0+187.7, SOLID LT. (WEST CENTER STREET PARKING SPACE)
 STA. 0+240.0, RT. (FOR DETAILS @ ISLAND SEE SHEET 3)
 VT ROUTE 15 STA. 0+009.6 TO 0+040.0, DOUBLE SOLID RT. (WITH EDGELINE BREAKS FOR CROSSWALK)
 VT ROUTE 15 STA. 0+009.6 TO 0+040.0, SOLID RT. (PARKING SPACES) (WITH CENTERLINE BREAK FOR WEST ALLEN STREET)
 VT ROUTE 15 STA. 0+018.5 TO 0+040.0, DOUBLE SOLID RT. (LANE LINES) STA. 0+258.8, DOUBLE SOLID LT. (WEST ALLEN STREET)

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE ITAPE) (MOD.)
 STA. 0+170.0 TO 0+290.0, SOLID LT. & RT.
 STA. 0+170.0 TO 0+240.0, DASHED LT.
 STA. 0+170.0 TO 0+187.5, DOTTED LT. & RT.
 STA. 0+170.0 TO 0+187.5, DASHED RT.
 STA. 0+187.5 TO 0+240.0, SOLID RT. (LANE LINE)
 STA. 0+170.0 TO 0+243.5, SOLID LT. (PARKING SPACES)
 STA. 0+273.0 TO 0+290.0, DASHED LT. & RT.
 STA. 0+258.8, DOUBLE SOLID LT. (WEST ALLEN STREET WITH EDGELINE BREAKS FOR CROSSWALK)
 STA. 0+258.8, SOLID LT. (WEST ALLEN STREET PARKING SPACES)
 STA. 0+240.0 TO 0+258.8, DOTTED LT. & RT.

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE ITAPE)
 STA. 0+187.7, LT.
 STA. 0+240.0, RT.
 VT ROUTE 15 STA. 0+012.0, LT. - RT.

REMOVING SIGNS AS SHOWN - 9

PAINTED CURB
 STA. 0+174.6 TO 0+183.8, LT.
 STA. 0+191.6 TO 0+197.0, LT.
 STA. 0+258.8, RT. (ISLAND)
 STA. 0+243.4 TO 0+252.0, RT.
 STA. 0+258.8, LT. (WEST ALLEN ST.)
 VT ROUTE 15 STA. 0+009.6 TO 0+022.0, RT.

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE ITAPE) (MOD.)
 STA. 0+170.0 TO 0+290.0, SOLID LT. & RT. (WITH EDGELINE BREAKS FOR SIDE STREETS & CROSSWALK)
 STA. 0+170.0 TO 0+240.0, DASHED LT. & RT.
 STA. 0+170.0 TO 0+187.5, DOTTED LT. & RT.
 STA. 0+170.0 TO 0+187.5, DASHED RT.
 STA. 0+187.5 TO 0+240.0, SOLID RT. (LANE LINE)
 STA. 0+170.0 TO 0+243.5, SOLID LT. (PARKING SPACES)
 STA. 0+273.0 TO 0+290.0, DASHED LT. & RT.
 STA. 0+258.8, DOUBLE SOLID LT. (WEST ALLEN STREET WITH EDGELINE BREAKS FOR CROSSWALK)
 STA. 0+258.8, SOLID LT. (WEST ALLEN STREET PARKING SPACES)
 STA. 0+240.0 TO 0+258.8, DOTTED LT. & RT.

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE ITAPE) (MOD.)
 STA. 0+187.7, LT.
 STA. 0+273.0, LT. & RT.

DURABLE LETTER OR SYMBOL (TYPE ITAPE) (MOD.)
 STA. 0+188.0, RT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+258.8, LT. - (2) "ONLY"
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+187.7, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+024.8, RT. - (2) "ONLY"
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE ITAPE)
 STA. 0+187.7, LT. (NO PARKING ZONE - WEST CENTER STREET)
 STA. 0+240.0, RT. (AT ISLAND)
 VT ROUTE 15 STA. 0+013.8 TO 0+021.4 (NO PARKING ZONES FOR DETAILS SEE SHEET 3)

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE ITAPE) (MOD.)
 STA. 0+170.0 TO 0+290.0, LT. & RT. (NO PARKING ZONES) (FOR DETAILS SEE SHEET 3)

TEMPORARY LETTER OR SYMBOL
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE) (MOD.)
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

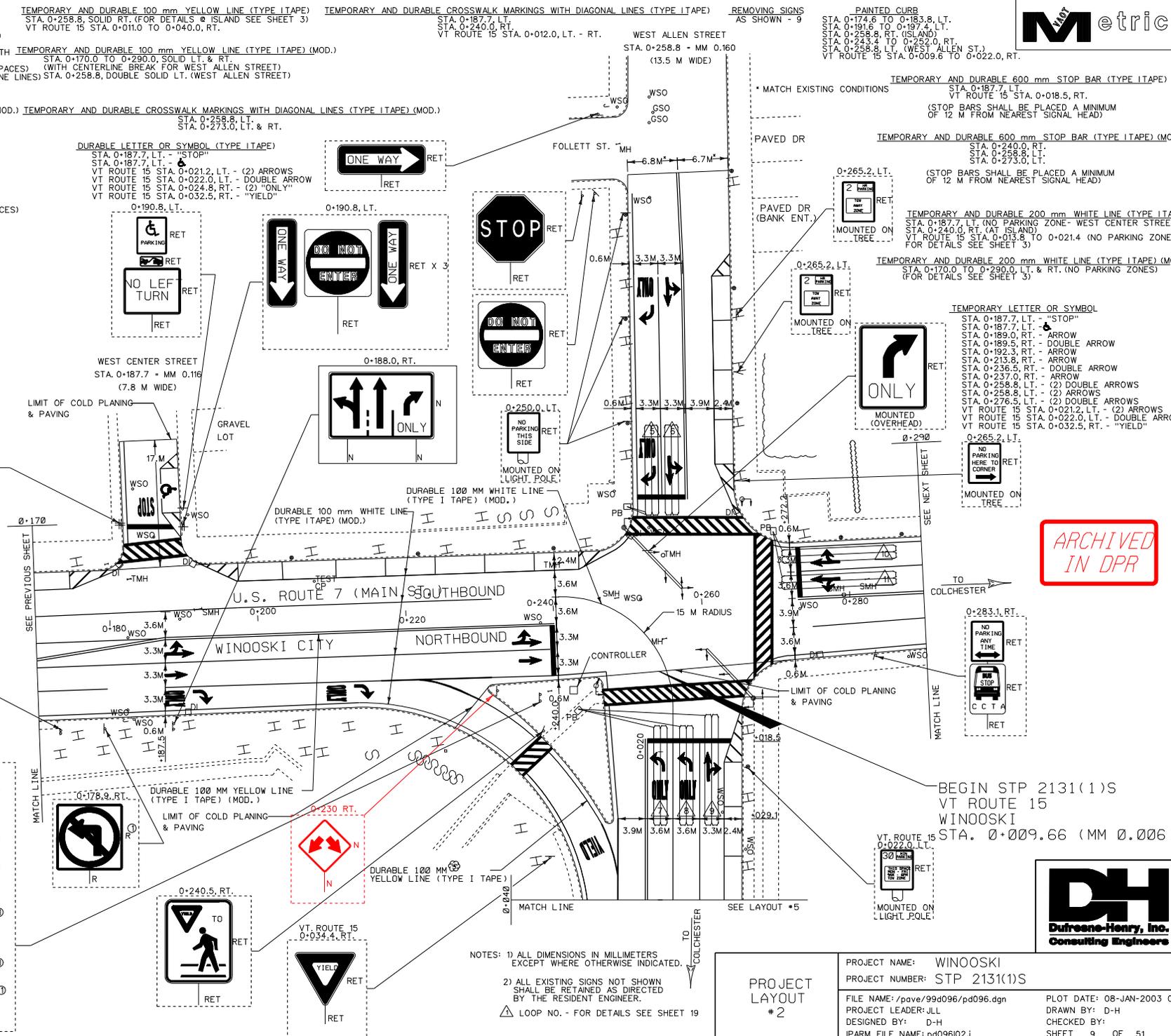
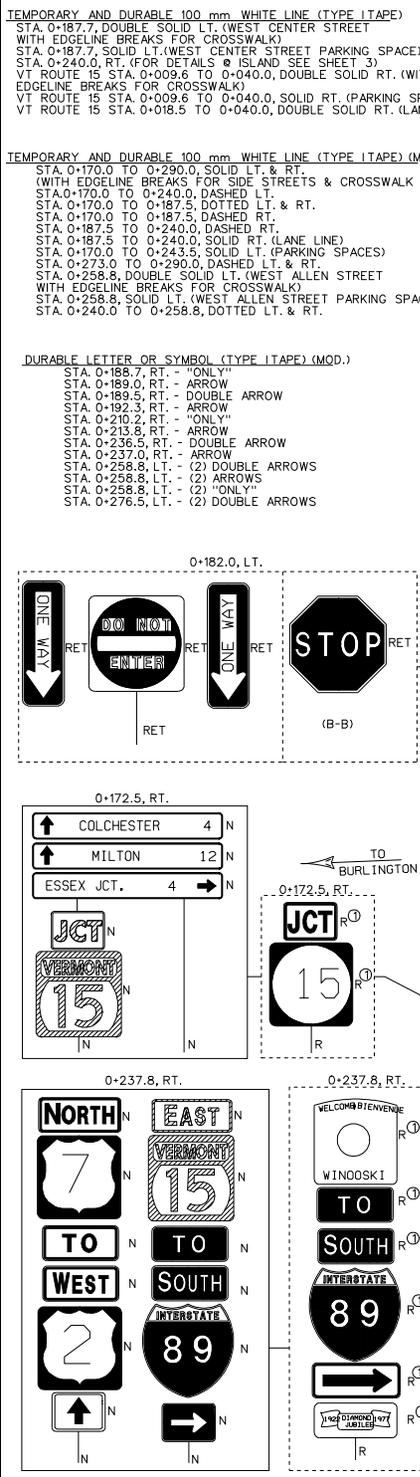
DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"

DURABLE LETTER OR SYMBOL (TYPE ITAPE)
 STA. 0+187.7, LT. - "STOP"
 STA. 0+189.0, RT. - ARROW
 STA. 0+189.5, RT. - DOUBLE ARROW
 STA. 0+192.3, RT. - ARROW
 STA. 0+210.2, RT. - "ONLY"
 STA. 0+213.8, RT. - ARROW
 STA. 0+236.5, RT. - DOUBLE ARROW
 STA. 0+237.0, RT. - ARROW
 STA. 0+258.8, LT. - (2) DOUBLE ARROWS
 STA. 0+258.8, LT. - (2) ARROWS
 STA. 0+276.5, LT. - (2) DOUBLE ARROWS
 VT ROUTE 15 STA. 0+021.2, LT. - (2) ARROWS
 VT ROUTE 15 STA. 0+022.0, LT. - DOUBLE ARROW
 VT ROUTE 15 STA. 0+032.5, RT. - "YIELD"



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NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
 2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.
 3) LOOP NO. - FOR DETAILS SEE SHEET 19

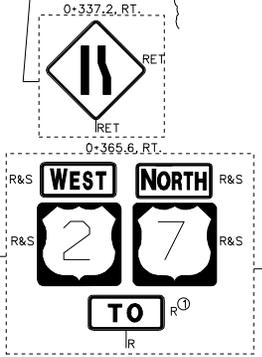
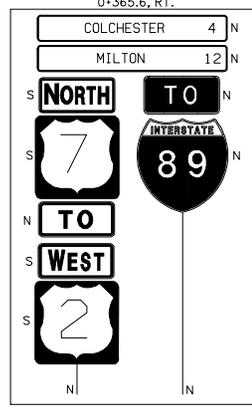
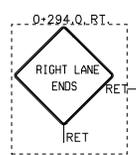
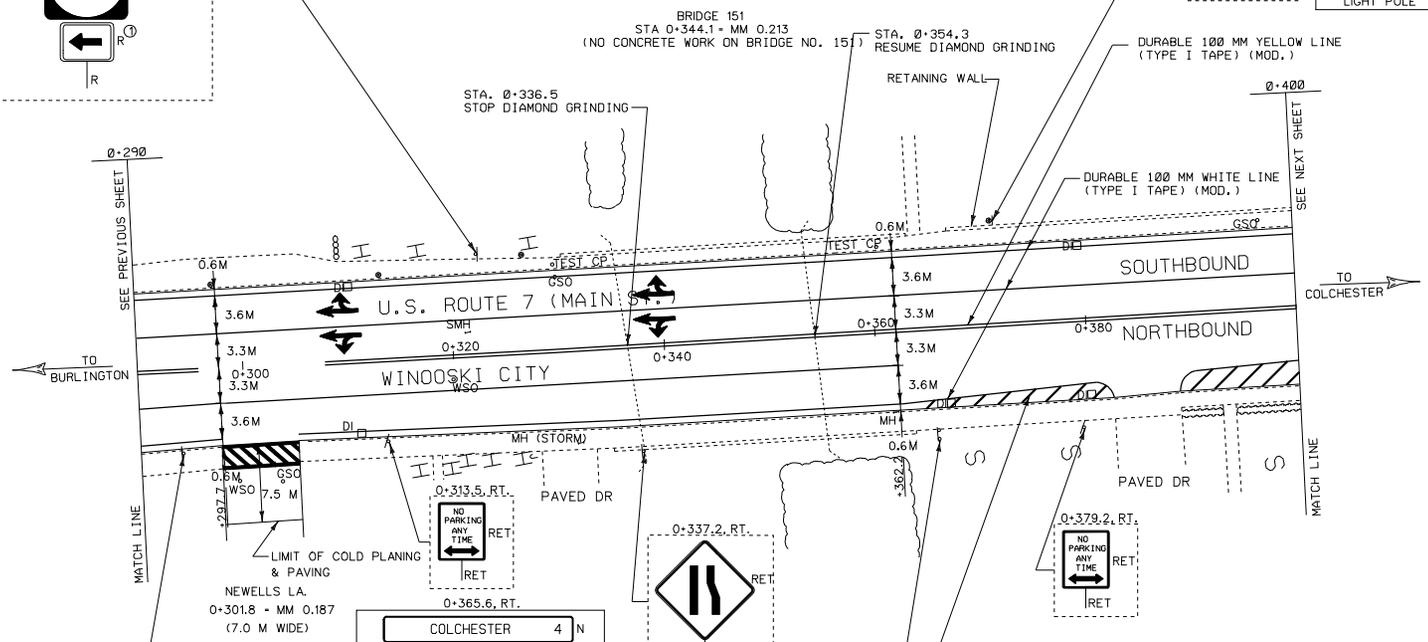
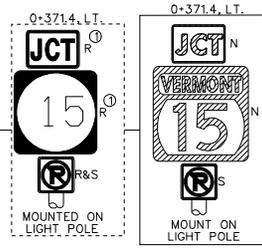
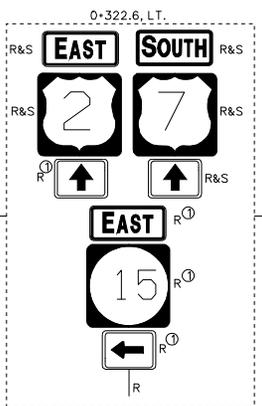
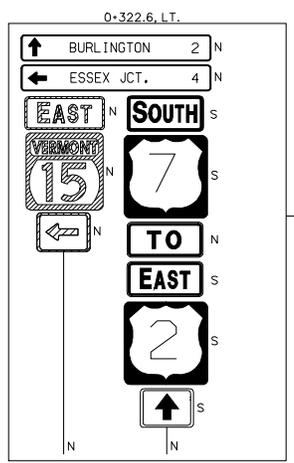
PROJECT LAYOUT #2	PROJECT NAME: WINOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	DESIGNED BY: D-H	SHEET 9 OF 51



TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE) (MOD.) TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE) (MOD.) TEMPORARY & DURABLE LETTER OR SYMBOL (TYPE I TAPE) (MOD.) TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE) (MOD.)
 STA. 0+290.0 TO 0+400.0, SOLID LT. & RT. (WITH EDGELINE BREAKS FOR NEWELLS LA. & PAVED DR) STA. 0+290.0 TO 0+400.0, SOLID LT. & RT. (WITH CENTERLINE BREAK FOR NEWELLS LA.) STA. 0+309.5, LT. - (2) DOUBLE ARROWS STA. 0+338.8, LT. - (2) DOUBLE ARROWS STA. 0+362.0 TO 0+382.0, SOLID RT. (DIAGONAL LINES) STA. 0+389.0 TO 0+400.0, SOLID RT. (DIAGONAL LINES)



TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE) REMOVING SIGNS AS SHOWN - 17 ERECTING SALVAGED SIGNS AS SHOWN - 10



DURABLE 200 MM WHITE LINE (TYPE I TAPE) (MOD.)

BUILT AS DESIGNED

ARCHIVED IN DPR



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
 2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.
 Δ LOOP NO. - FOR DETAILS SEE SHEET 19

PROJECT LAYOUT #3	PROJECT NAME: WINOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	DESIGNED BY: D-H	SHEET 10 OF 51

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE) STA. 0+428.3 TO 0+440.9, SOLID LT. (STA. 0+433.1, DOUBLE SOLID RT. (MANSION STREET WITH EDGELINE BREAKS FOR CROSSWALK))

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE) STA. 0+428.3 TO 0+440.9, SOLID LT. & RT. (WITH CENTERLINE BREAK FOR MANSION STREET) STA. 0+415.7, DOUBLE SOLID, LT. (MAPLE STREET) STA. 0+433.1, DOUBLE SOLID, RT. (MANSION STREET)

TEMPORARY AND DURABLE LETTER OR SYMBOL (TYPE I TAPE) STA. 0+415.7, LT. - "STOP" STA. 0+433.1, RT. - "STOP"

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE) STA. 0+415.7, LT. STA. 0+433.1, RT.

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE) (MOD.) STA. 0+400.0 TO 0+428.3, SOLID LT. & RT. (WITH EDGELINE BREAK FOR MAPLE STREET)

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE) (MOD.) STA. 0+400.0 TO 0+428.3, SOLID LT. & RT. (WITH CENTERLINE BREAK FOR MAPLE STREET)

TEMPORARY AND DURABLE 600 mm STOP BAR (TYPE I TAPE) STA. 0+415.7, LT. STA. 0+433.1, RT.

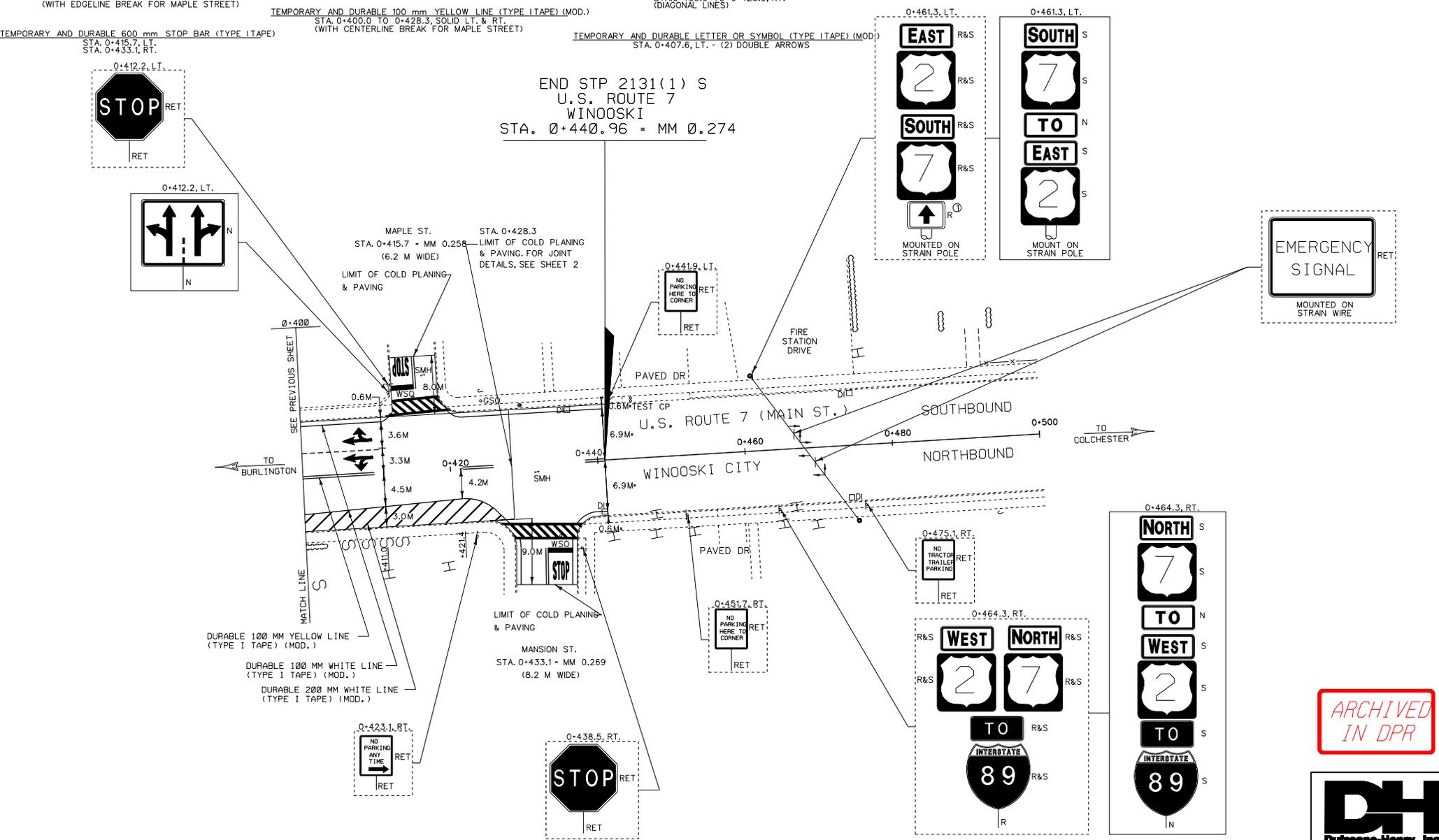
TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE) (MOD.) (DIAGONAL LINES) STA. 0+400.0 TO 0+428.0, RT.

TEMPORARY AND DURABLE LETTER OR SYMBOL (TYPE I TAPE) (MOD.) STA. 0+407.6, LT. - (2) DOUBLE ARROWS

RECTIFYING SALVAGED SIGNS AS SHOWN - 10

REMOVING SIGNS AS SHOWN - 11

END STP 2131(1) S
U.S. ROUTE 7
WINOOSKI
STA. 0+440.96 = MM 0.274



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NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.
△ LOOP NO. - FOR DETAILS SEE SHEET 19

BUILT AS DESIGNED

* MATCH EXISTING CONDITIONS

PROJECT LAYOUT # 4	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 11 OF 51

TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE)
 STA. 0+040.0 TO 0+140.0, SOLID LT. & RT.
 STA. 0+040.0 TO 0+081.8, SOLID LT. (2) (LANE LINES)
 (WITH BREAK FOR WEAVER LANE)
 STA. 0+040.0 TO 0+140.0, DASHED RT.
 STA. 0+040.0 TO 0+060.0, SOLID LT. (PARKING SPACES)
 STA. 0+072.8, DOUBLE SOLID LT. (WEAVER L.A.
 WITH EDGELINE BREAKS FOR CROSSWALK)
 STA. 0+081.8 TO 0+140.0, DASHED LT.
 STA. 0+080.0 TO 0+095.0, SOLID LT. (PARKING SPACE)
 STA. 0+086.0 TO 0+140.0, DOTTED RT.
 STA. 0+100.0 TO 0+117.0, SOLID LT. (PARKING SPACES)

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE)
 STA. 0+040.0 TO 0+140.0, SOLID LT. & RT.
 (WITH CENTERLINE BREAK FOR WEAVER LANE)
 STA. 0+072.8, DOUBLE SOLID LT. (WEAVER L.A.)

DURABLE LETTER OR SYMBOL (TYPE I TAPE)
 STA. 0+059.7, LT. - (2) ARROWS
 STA. 0+062.5, LT. - DOUBLE ARROW
 STA. 0+063.3, LT. - (2) "ONLY"
 STA. 0+072.8, LT. - "STOP"

TEMPORARY AND DURABLE 600 mm STOP BAR (TYPE I TAPE)
 STA. 0+072.8, LT.

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE)
 STA. 0+072.8, LT.

REMOVING SIGNS
 AS SHOWN - 3

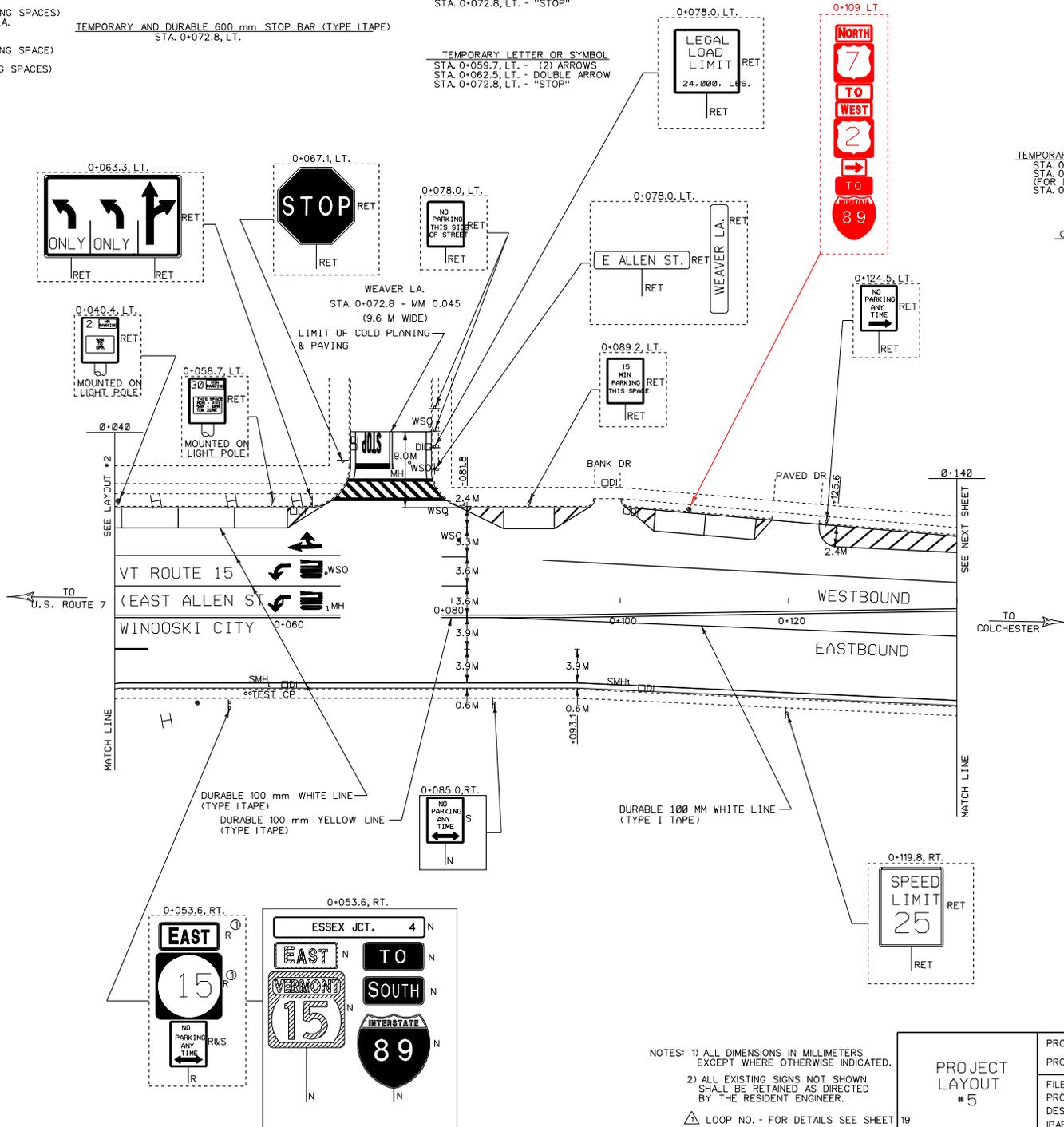
RECTIFYING SALVAGED SIGNS
 AS SHOWN - 1

PAINTED CURB
 STA. 0+060.0 TO 0+068.0, LT.
 STA. 0+077.8 TO 0+086.2, LT.
 STA. 0+092.1 TO 0+096.5, LT.
 STA. 0+100.0 TO 0+104.0, LT.
 STA. 0+116.0 TO 0+119.0, LT.

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE)
 STA. 0+040.0 TO 0+044.0, SOLID RT. (AT ISLAND)
 STA. 0+040.0 TO 0+140.0, SOLID LT. (NO PARKING ZONES)
 (FOR DETAILS SEE SHEET 5)
 STA. 0+124.0 TO 0+140.0, SOLID LT. (DIAGONAL LINES)

CHANGING ELEVATION OF D'S, CB'S OR MH'S

STA. 0+055.9, RT. - SMH
 STA. 0+060.2, RT. - DI
 STA. 0+061.2, LT. - DI
 STA. 0+065.0, LT. - MH
 STA. 0+068.1, LT. - DI
 STA. 0+077.1, LT. - DI
 STA. 0+080.8, LT. - SMH
 STA. 0+100.9, RT. - SMH
 STA. 0+102.4, RT. - DI



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NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
 2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.

PROJECT LAYOUT # 5	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 213(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	DESIGNED BY: D-H	SHEET 12 OF 51

ERECTING SALVAGED SIGNS
AS SHOWN - 5

PAINTED CURB
STA. 0+184.6, RT. (ISLAND)

TEMPORARY AND DURABLE 200 mm WHITE LINE (TYPE I TAPE)
STA. 0+140.0 TO 0+160.0, LT. (FOR DETAILS @ ISLAND SEE SHEET 3)
NO PARKING ZONES SEE SHEET 3
STA. 0+180.0, RT. (FOR DETAILS @ ISLAND SEE SHEET 3)

TEMPORARY LETTER OR SYMBOL
STA. 0+144.9, RT. - ARROW
STA. 0+148.2, RT. - (2) ARROWS
STA. 0+184.6, RT. - DOUBLE ARROW
STA. 0+184.6, RT. - ARROW
STA. 0+202.7, RT. - ARROW
STA. 0+203.5, LT. - ARROW
STA. 0+203.5, LT. - DOUBLE ARROW

CHANGING ELEVATION OF DI'S, CB'S OR MH'S
STA. 0+140.6, RT. - DI
STA. 0+140.6, LT. - DI
STA. 0+140.8, RT. - MH
STA. 0+146.8, LT. - MH
STA. 0+152.0, RT. - MH
STA. 0+180.7, LT. - DI
STA. 0+180.2, LT. - MH
STA. 0+180.2, RT. - MH
STA. 0+189.7, LT. - DI
STA. 0+189.4, RT. - DI
STA. 0+195.4, LT. - DI

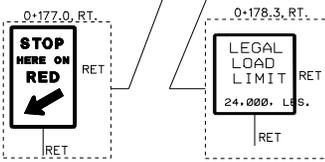
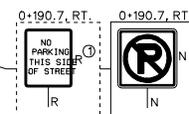
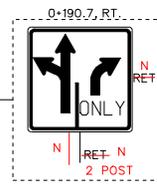
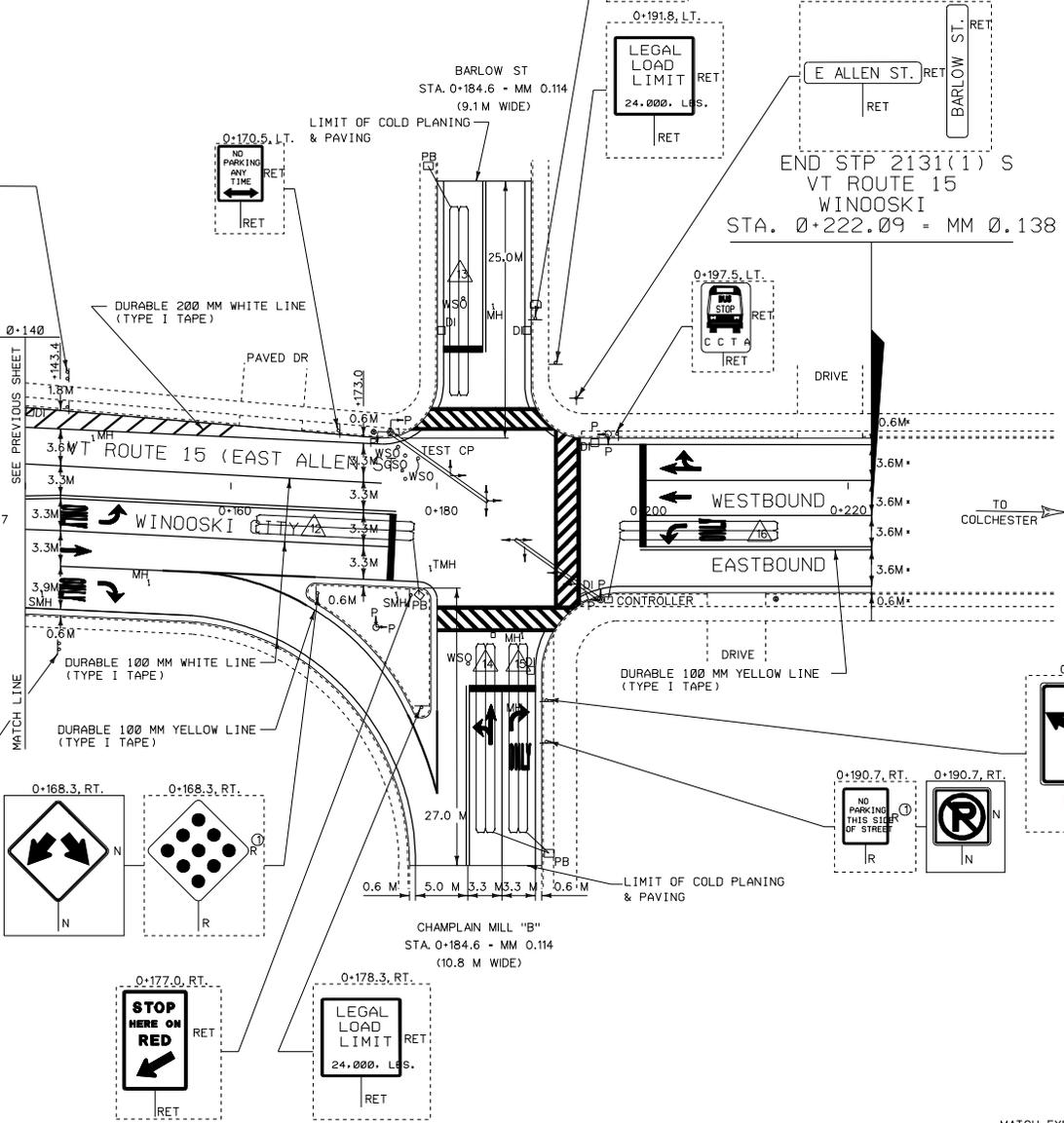
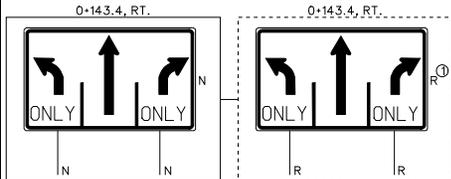
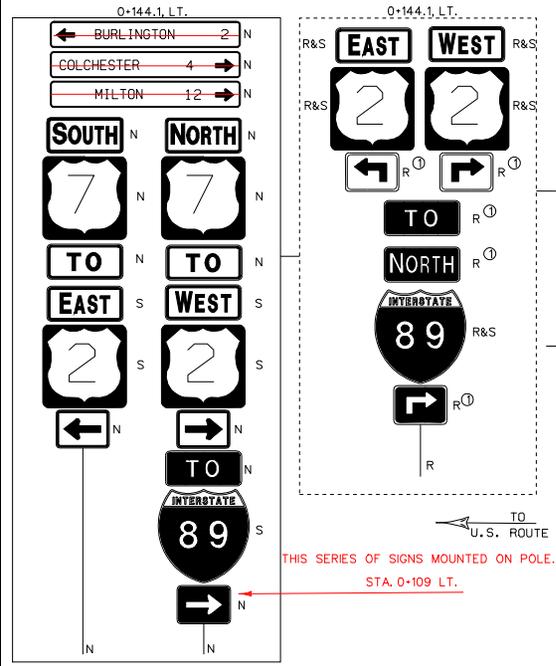
TEMPORARY AND DURABLE 100 mm WHITE LINE (TYPE I TAPE)
STA. 0+140.0 TO 0+222.1, SOLID LT. & RT.
(WITH EDGELINE BREAKS FOR SIDE STREETS & CROSSWALK)
STA. 0+140.0 TO 0+166.0, DASHED LT.
STA. 0+140.0 TO 0+143.4, DOTTED RT.
STA. 0+143.4 TO 0+175.0, SOLID RT. (LANE LINE)
STA. 0+143.4 TO 0+175.0, SOLID RT. (LANE LINE)
STA. 0+180.0, SOLID RT. (FOR DETAILS @ ISLAND SEE SHEET 3)
STA. 0+184.6, DOUBLE SOLID LT.
(WITH EDGELINE BREAKS FOR CROSSWALK)
STA. 0+184.6, DOUBLE SOLID RT. (CHAMPLAIN MILL "B"
WITH EDGELINE BREAKS FOR CROSSWALK)
STA. 0+200.0 TO 0+222.3, SOLID LT. (LANE LINE)
STA. 0+200.0 TO 0+222.3, SOLID LT. (LANE LINE)

TEMPORARY AND DURABLE 100 mm YELLOW LINE (TYPE I TAPE)
STA. 0+140.0 TO 0+222.3, SOLID LT. & RT.
(WITH CENTERLINE BREAK FOR BARLOW STREET)
STA. 0+184.6, DOUBLE SOLID LT. & RT. (BARLOW STREET &
CHAMPLAIN MILL "B")
STA. 0+180.0, SOLID RT. (FOR DETAILS @ ISLAND SEE SHEET 3)

DURABLE LETTER OR SYMBOL (TYPE I TAPE)
STA. 0+144.6, RT. - (2) "ONLY"
STA. 0+144.9, RT. - ARROW
STA. 0+148.2, RT. - (2) ARROWS
STA. 0+184.6, RT. - DOUBLE ARROW
STA. 0+184.6, RT. - "ONLY"
STA. 0+184.6, RT. - ARROW
STA. 0+202.7, RT. - ARROW
STA. 0+203.0, LT. - ARROW
STA. 0+203.5, LT. - DOUBLE ARROW
STA. 0+206.3, RT. - "ONLY"

TEMPORARY AND DURABLE CROSSWALK MARKINGS WITH DIAGONAL LINES (TYPE I TAPE)
STA. 0+184.6, LT. & RT.
STA. 0+193.0, LT. & RT.

TEMPORARY AND DURABLE 600 mm STOP BAR (TYPE I TAPE)
STA. 0+175.0, RT.
STA. 0+184.6, LT. & RT.
STA. 0+200.0, LT. & RT.
(STOP BARS SHALL BE PLACED A MINIMUM OF 12 M FROM NEAREST SIGNAL HEAD)



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NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.
2) ALL EXISTING SIGNS NOT SHOWN SHALL BE RETAINED AS DIRECTED BY THE RESIDENT ENGINEER.
△ LOOP NO. - FOR DETAILS SEE SHEET 19

PROJECT LAYOUT
6

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 2131(1)S
FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096i06.i
PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 13 OF 51

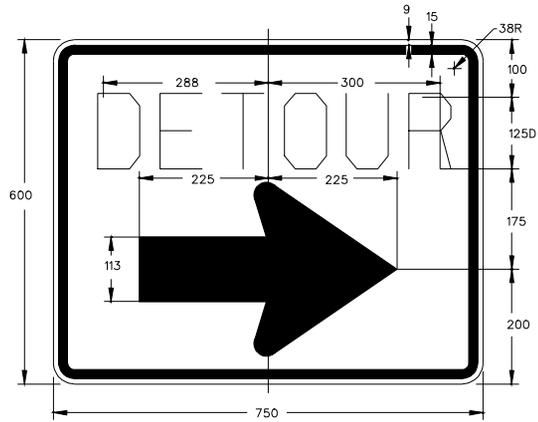
KILOMETER MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXISTING POSTS	NO. OF POSTS	FLANGED CHANNEL		SQUARE STEEL (mm)			NEW SIGN POSTS			TUBULAR STEEL Ø (mm)				W-SHAPE STEEL				REMARKS	SIGN DETAIL																
		EA	WIDTH (mm)	HEIGHT (mm)	"A"	"B"	SALV SIGN			SALV TIS	RETAIN	SALVAGE	1.7	3.0	4.5	44	50	63	ANCHORS	Ø	75	100	100 MOD	FOUND. ATION	75		89	100	125	FTG. SIZE		WEIGHT	POST SIZE	SIGN NUMBER	DPR NUMBER	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER						
																														kg/m								kg/m		kg/m		600 mm	750 mm
																														OPTION	ITEMS							3.4	3.9	5.0	1.9		
VT ROUTE 15 (CONT.)		1	1350	190	0.26																														E-123M E-123M E-123M								
0-144.1 LT		1	600	300	0.18																														E-136AM E-136AM E-136AM								
0-168.3 RT		1	600	600	0.36				1			2.7M 4.6																							INSTALL NEW POST USING SLEEVE, AS DETAILED ON STANDARD E-121M.	E-151M							
0-190.7 RT		1	300	300	0.09				+			3.5M 4.6																								E-143M							
0-190.7 RT.			900	750	0.68				2			8.0M																															
-DETOUR STAGING		25	300	450	3.30				25			115.0																								THESE SIGNS ARE TO BE INSTALLED AS NOTED ON THE TRAFFIC DETOUR PLAN #2, AND RETURNED TO THE CITY OF WINDOSKI WHEN NO LONGER NEEDED AS DIRECTED BY THE RESIDENT ENGINEER.	18						
SHEET 14 TOTALS					8.18	9.62	4					41.4	35.2					34.3	-2	97.2																							
SHEET 15 TOTALS					4.10	11.29	14					4.6	27.0					36.5	4	202.4																							
SHEET 16 TOTALS					2.60	5.12	7					22.3	28.6					12.0																									
SHEET 17 TOTALS					6.88	4.41	5					124.2	14.2					15.5	-2	138.9																							
SUBTOTALS					21.74		30					192.5						12.0		437.9																							
ROUNDING					0.29		-					2.5						1.0		12.1																							

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IN DPR

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S "SIGN POST DESIGN GUIDELINE."

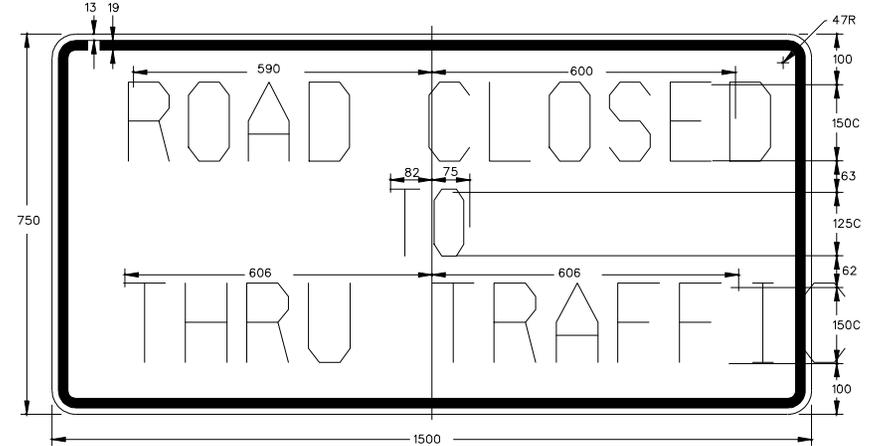
TOTALS	m ²	m ²	EA	m ²		m	m	m	m	m	m	EA	kg	kg	kg	kg	kg	kg	kg	kg	EA	EA	kg
	22	38.44	30			195							13	13	13	450				8			
						195							13	13	13	450				8			
						185.0 M							13	13	13	450				8			

PROJECT:	WINDOSKI	PROJECT NO.:	STP 2131(1)s
DESIGN FILE NAME:	pave/99d096/pd096.dgn	PLOT DATE:	08-JAN-2003
IPARM FILE NAME:	PD096SS4.1	SURVEY DATE:	06/02/99
SURVEYED BY:	D-H	DRAWN BY:	D-H
SQUAD LEADER:	JLL	SHEET:	17 OF 51



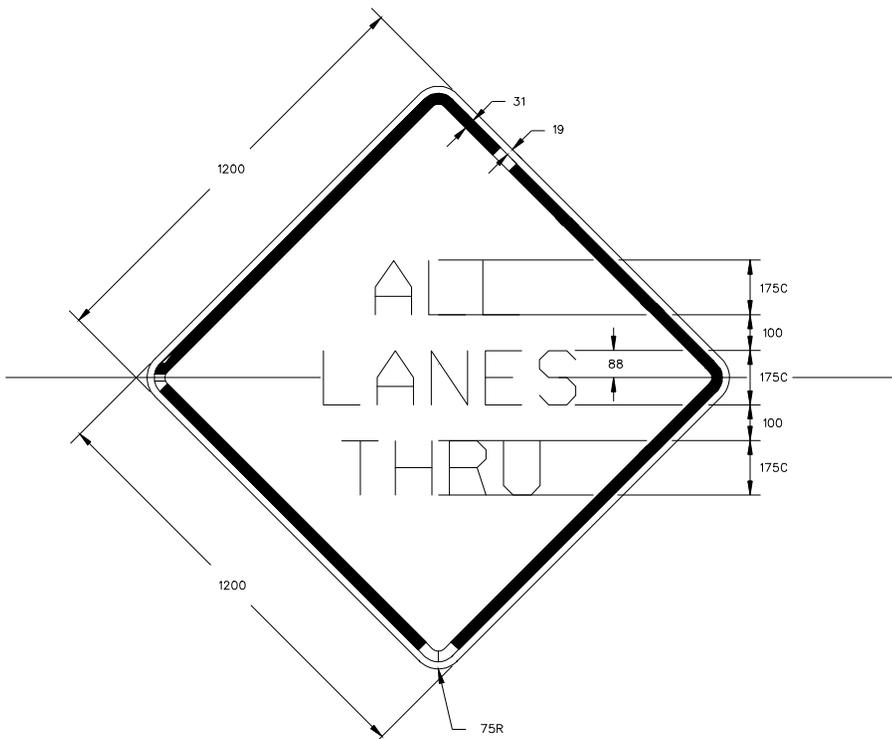
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ORANGE (REFL.) BACKGROUND
MATERIAL: PER VTrans STANDARD E-102AM

LOCATIONS
SEE SHEET 50 OF 51



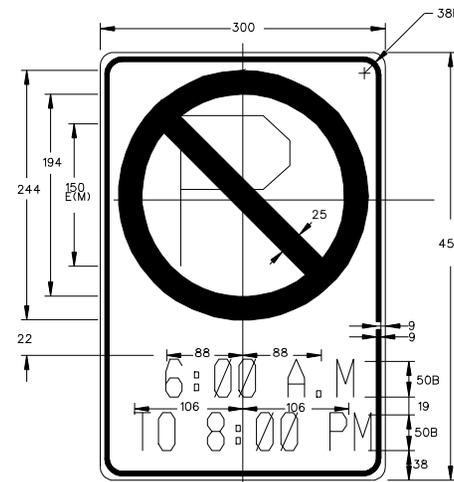
COLOR: BLACK BORDER & TEXT
ORANGE (REFL.) BACKGROUND
MATERIAL: PER VTrans STANDARD E-102AM

LOCATIONS
SEE SHEET 50 OF 51



COLOR: BLACK BORDER & TEXT
ORANGE (REFL.) BACKGROUND
MATERIAL: PER VTrans STANDARD E-102AM

LOCATIONS
SEE SHEET 47 OF 51



COLOR: RED BORDER, LEGEND, CIRCLE & DIAGONAL
1/4" BLACK
WHITE BACKGROUND
MATERIAL: PER VTrans STANDARD E-143M

LOCATIONS
SEE SHEET 50 OF 51

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IN DPR

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED.

TRAFFIC
SIGN
DETAILS

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 213(1)S

FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096sd.i

PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 18 OF 51

VEHICLE DETECTOR LOOPS												
LAYOUT SHEET	LOOP NO.	LANE	CALL β	SIZE (M)	TYPE & NO. TURNS	DELAY OR PRESENCE	INDUCTANCE		RESISTANCE		LEAKAGE TO GROUND	LOCKING MEMORY
							CALC.	ACT.	CALC.	ACT.		
1	1	NORTHBOUND LEFT ONLY		1.8 X 12.0	QUAD - 1	PRESENCE	118		0.449			YES
1	2	WEST CANAL ST.		1.8 X 12.0	QUAD - 1	PRESENCE	135		0.673			NO
1	3	CHAMPLAIN MILL ENTRANCE "A"		1.8 X 13.5	QUAD - 1	PRESENCE	132		0.493			NO
1	4	SOUTHBOUND LEFT ONLY		1.8 X 15.0	QUAD - 1	PRESENCE	162		0.760			YES
2	5	WEST ALLEN ST. RIGHT ONLY		1.8 X 12.0	QUAD - 1	PRESENCE	128		0.577			NO
2	6	WEST ALLEN ST. LEFT/THROUGH		1.8 X 12.0	QUAD - 1	PRESENCE	130		0.609			NO
2	7	VT ROUTE 15 LEFT ONLY		1.8 X 13.5	QUAD - 1	PRESENCE	126		0.416			YES
2	8	VT ROUTE 15 LEFT ONLY		1.8 X 13.5	QUAD - 1	PRESENCE	128		0.452			YES
2	9	VT ROUTE 15 RIGHT/THROUGH		1.8 X 15.0	QUAD - 1	PRESENCE	143		0.520			NO
2	10	SOUTHBOUND RIGHT/THROUGH		1.8 X 15.0	QUAD - 1	PRESENCE	171		0.868			NO
2	11	SOUTHBOUND LEFT/THROUGH		1.8 X 15.0	QUAD - 1	PRESENCE	174		0.908			NO
6	12	VT ROUTE 15 EB LEFT ONLY		1.8 X 15.0	QUAD - 1	PRESENCE	149		0.593			YES
6	13	BARLOW ST.		1.8 X 18.0	QUAD - 1	PRESENCE	216		1.198			NO
6	14	CHAMPLAIN MILL ENTRANCE "B" LEFT/THROUGH		1.8 X 18.0	QUAD - 1	PRESENCE	181		0.751			NO
6	15	CHAMPLAIN MILL ENTRANCE "B" RIGHT ONLY		1.8 X 18.0	QUAD - 1	PRESENCE	179		0.722			NO
6	16	VT ROUTE 15 WB LEFT ONLY		1.8 X 15.0	QUAD - 1	PRESENCE	134		0.401			YES
ALL CALCULATED VALUES ARE AT THE CONTROLLER. MEASURED VALUES MUST BE FILLED IN PRIOR TO TEST PERIOD.												

NOTES:

PRIOR TO COLD PLANING, ANY VEHICLE DETECTOR LOOPS SHALL BE DISCONNECTED IN THE CONTROLLER CABINET AND CUT AT THE CURB.

ONCE THE LOOP IS DISCONNECTED, THE SIGNAL PHASE THAT IT WAS CALLING SHALL BE PUT ON MAX RECALL OR THE SIGNAL PUT ON FLASH AND TRAFFIC CONTROLLED BY A UNIFORMED TRAFFIC OFFICER.

ALL BITUMINOUS AREAS TO RECEIVE NEW VEHICLE DETECTOR LOOPS SHALL BE LEVELED WITH TYPE IV BITUMINOUS CONCRETE PAVEMENT AS DIRECTED BY THE RESIDENT ENGINEER PRIOR TO THE INSTALLATION OF THE NEW DETECTOR LOOPS.

IN THE PORTLAND CEMENT CONCRETE PAVEMENT AREAS, ALL NEW LOOPS WILL BE INSTALLED USING THE SAME SAW CUTS USED FOR THE EXISTING LOOPS WITH THE EXCEPTION OF LOOP NO. 4 AT U.S. ROUTE 7 STA. 0+145 TO 0+160 AS SHOWN ON LAYOUT #1. FOR LOOP NO. 4, THE EASTERLY MOST EXISTING SAW CUT WILL BE USED FOR THE NEW LEAD-IN. THE REMAINING EXISTING SAW CUT BETWEEN STATIONS 0+140 AND 0+145 WILL BE SEALED IN THE SAME MANNER AS NEW SAW CUTS AS SHOWN ON VTrans STANDARD E-172M.

AFTER THE NEW LOOPS ARE INSTALLED, THE INDUCTANCE RESISTANCE AND LEAKAGE TO GROUND SHALL BE TESTED USING PROPERLY CALIBRATED EQUIPMENT. THE TEST RESULTS SHALL BE COMPARED WITH THE CALCULATED VALUES AND RECORDED ON THE PLANS. ALL LOAD TESTING SHALL BE PERFORMED AS PER VTrans STANDARD E-172M.

AFTER ACCEPTANCE OF THE LOOP INSTALLATION BY THE RESIDENT ENGINEER, RETURN THE SIGNAL TO NORMAL OPERATION. ALL WORK REQUIRED SHALL BE SUBSIDIARY TO ITEM 678.22, VEHICLE DETECTOR LOOP.

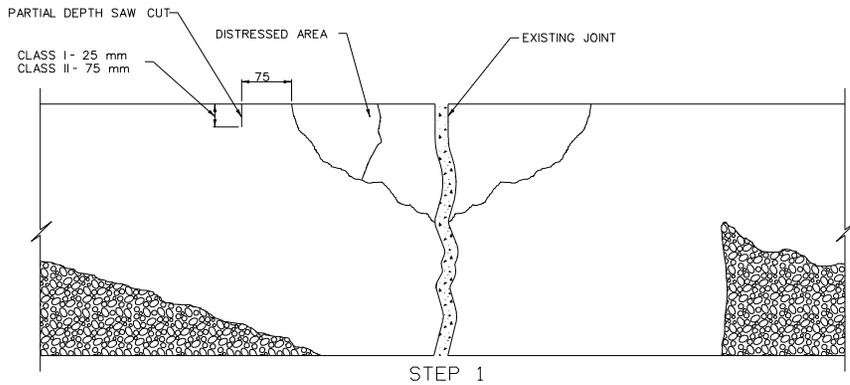
NOTE:
FOR VEHICLE DETECTOR LOOP DETAILS,
SEE VTrans STANDARD E-172M

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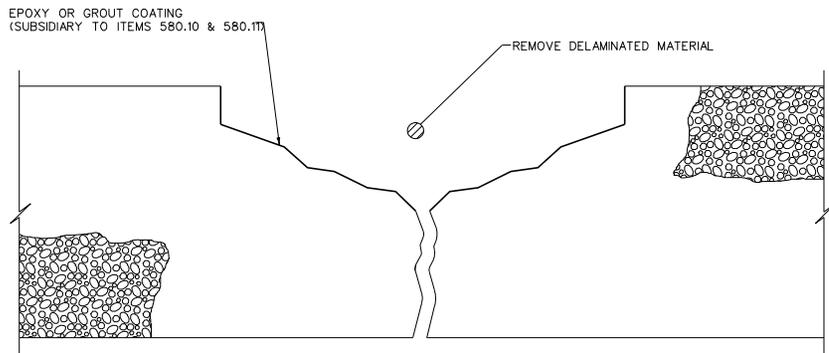


VEHICLE DETECTOR LOOPS	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 19 OF 51
	DESIGNED BY: D-H	
	IPARM FILE NAME: pd096vd1	

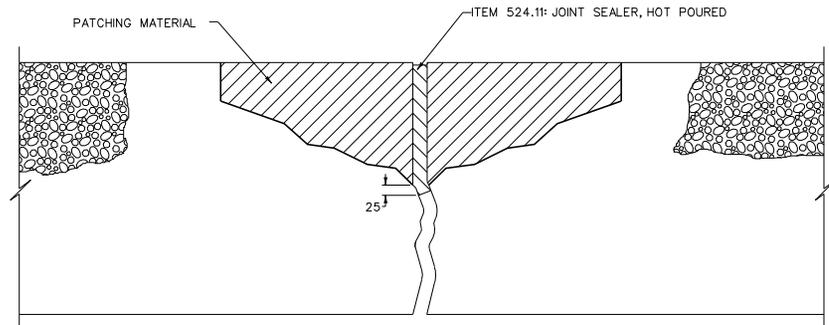
STEPS FOR PARTIAL DEPTH PATCHING - SECTION VIEW



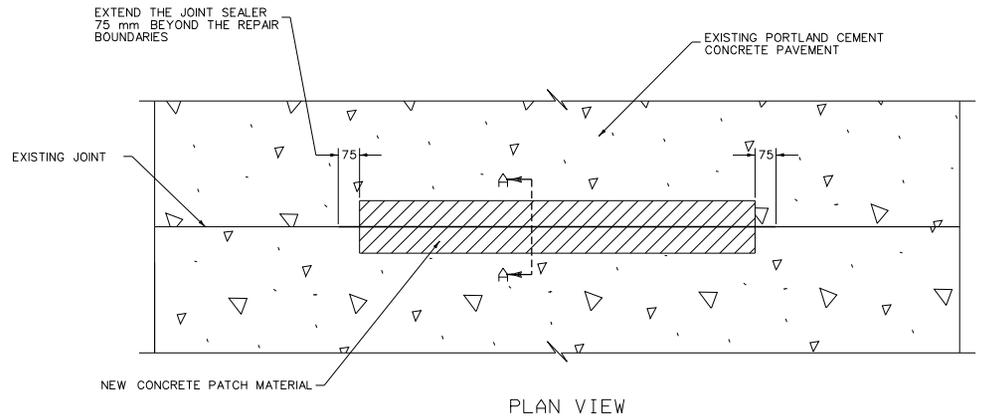
STEP 1



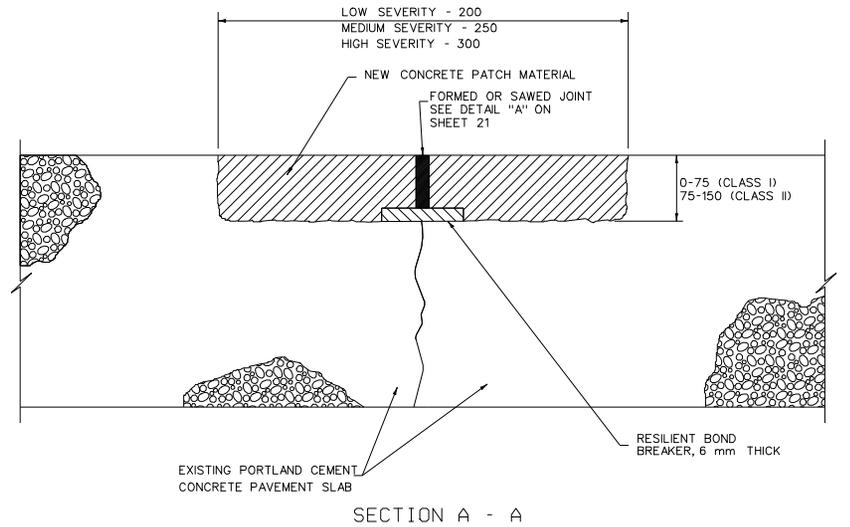
STEP 2



STEP 3



PLAN VIEW



SECTION A - A

PARTIAL DEPTH REPAIR

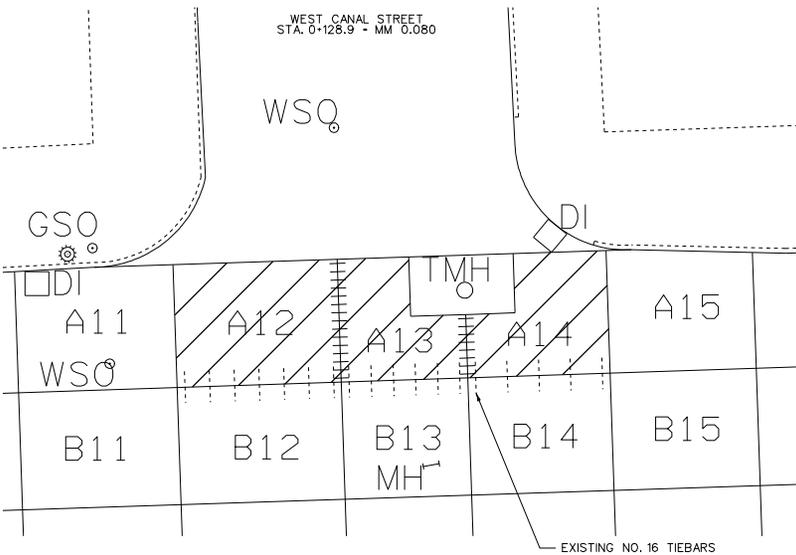
- NOTES:
- 1: ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED
 - 2: THE PARTIAL DEPTH REPAIR AS DETAILED ON THIS SHEET SHALL BE PAID FOR AS FOLLOWS:
 - ITEM 580.10, REPAIR OF CONCRETE SUPERSTRUCTURE SURFACE, CLASS I (MOD.)
 - ITEM 580.11, REPAIR OF CONCRETE SUPERSTRUCTURE SURFACE, CLASS II (MOD.)
 - 3: FOR ALL PARTIAL DEPTH REPAIR LOCATIONS SEE SHEETS 27-44
 - 4: THE QUANTITIES FOR THE PARTIAL DEPTH REPAIR AS DETAILED ON THIS SHEET AND SHEETS 27-44 HAVE BEEN CALCULATED USING THE FOLLOWING GUIDELINES:
 - ITEM 580.10: 70% OF TOTAL REPAIR AREA (0-75 mm DEPTH)
 - ITEM 580.11: 30% OF TOTAL REPAIR AREA (75-150 mm DEPTH)

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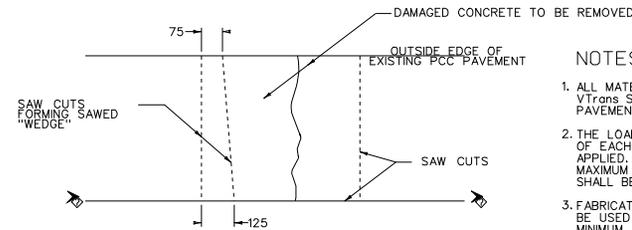


PORTLAND CEMENT CONCRETE, PARTIAL DEPTH REPAIR DETAILS	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 213(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 20 OF 51
DESIGNED BY: D-H		
IPARM FILE NAME: pd096pci.i		

SLAB REPAIR DETAILS



DETAIL B - SLAB REMOVAL DETAILS



NOTES:

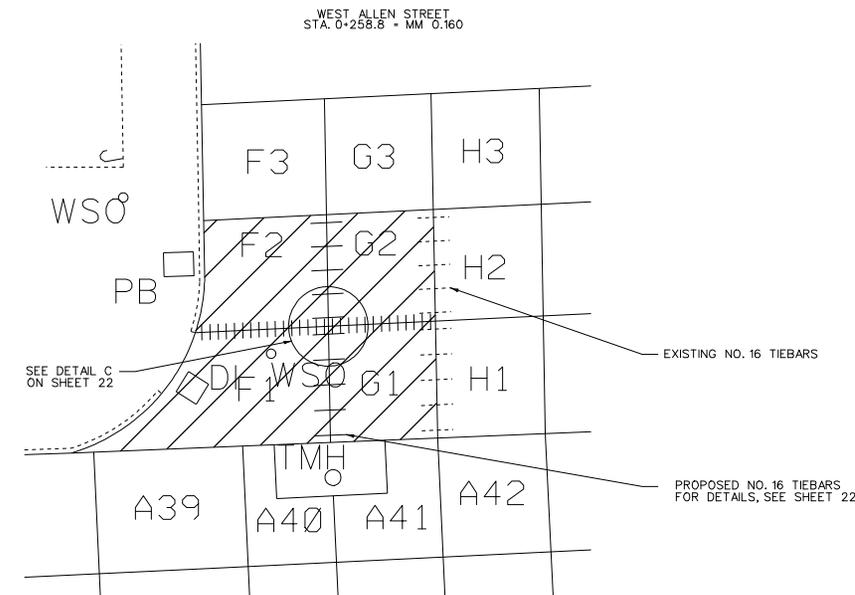
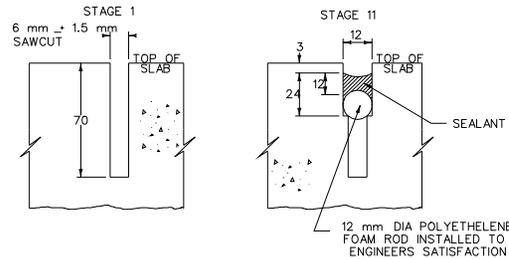
- SUFFICIENT TRANSVERSE AND/OR LONGITUDINAL VERTICAL SAWCUTS SHALL BE MADE IN EACH SLAB TO PERMIT LIFTING THE CONCRETE OFF OF THE SUBBASE WITH MINIMAL DAMAGE TO SUBBASE MATERIALS AND ADJACENT CONCRETE FACES. OVERCUTTING SHALL BE HELD TO A MINIMUM. IT MAY BE NECESSARY TO CUT THE SLAB INTO SMALLER PIECES BECAUSE OF LIFT-OUT EQUIPMENT LIMITATIONS.
- CARE SHALL BE TAKEN IN THE VICINITY OF THE TELEPHONE VAULT LOCATED WITHIN SLABS A13 AND A14, AS THE PORTLAND CEMENT CONCRETE PAVEMENT IN THIS AREA IS THINNER THAN THE REMAINDER OF THE SLABS.
- IF, DURING CONCRETE REMOVAL, THE CONTRACTOR SPALLS THE EXISTING CONCRETE OUTSIDE OF THE DESIGNED SAWCUTS, HE SHALL MAKE NEW SAWCUTS OUTSIDE OF THE ORIGINAL AREA, REMOVE AND REPLACE DAMAGED MATERIAL, AS PER PLANS, WITHOUT ADDITIONAL COMPENSATION.
- AS PER SPECIAL PROVISIONS, SECTION 408, ANY LOOSE, DISTURBED, OR UNSUITABLE SUBBASE MATERIAL SHALL BE REMOVED AND PAID AS SUBSIDIARY TO ONE COURSE CEMENT CONCRETE PAVEMENT, ITEM 408.30.



NOTES

- ALL MATERIALS AND WORKMANSHIP SHALL BE IN ACCORDANCE WITH THE CURRENT VTRANS SUPPLEMENTAL SPECIFICATION SECTION 408-ONE COURSE CEMENT CONCRETE PAVEMENT (NEW CONSTRUCTION).
- THE LOAD TRANSFER DEVICES SHALL BE EPOXY COATED, WITH AT LEAST ONE-HALF OF EACH DEVICE COVERED WITH AN APPROVED BOND BREAKER OR LUBRICANT, SHAP APPLIED. PERMISSIBLE VARIATION IN STRAIGHTNESS FOR THE DEVICES SHALL BE A MAXIMUM OF 2 mm IN THE LENGTH OF THE DEVICE. TOLERANCE ON THE DEVICE LENGTH SHALL BE ± 6 mm.
- FABRICATED-STEEL SUPPORTING UNITS OR BASKETS AS DETAILED ON SHEET 23 SHALL BE USED FOR PLACING THE DEVICES TO INSURE STABILITY. ALL WIRES SHOWN ARE MINIMUM ALLOWABLE SIZE AND SHALL CONFORM TO THE CURRENT SPECIFICATIONS FOR COLD-DRAWN STEEL WIRE FOR CONCRETE REINFORCEMENT, ASTM DESIGNATION A-82. THE SIZE OF THE WIRE SHOWN SHALL CONFORM TO THE UNITED STATES STEEL WIRE GAUGE STANDARD. THE MANUFACTURER SHALL SUPPLY SIX STAKES WITH EACH ASSEMBLY. ASSEMBLIES SHALL BE PACKED AND SHIPPED SO THAT THERE WILL BE NO DAMAGE DURING SHIPMENT BY COMMON CARRIER TO THE POINT OF DESTINATION. SPREADER WIRES MAY BE USED TO HOLD THE ASSEMBLY TOGETHER DURING SHIPMENT, HANDLING, AND PLACEMENT IN ITS FINAL POSITION ON SUBGRADE. FAILURE OF THE ASSEMBLIES TO MEET ANY ONE OF THE REQUIREMENTS OF THIS SPECIFICATION SHALL BE CAUSE FOR REJECTION.
- SINCE LANE WIDTHS VARY, THE LOAD TRANSFER DEVICE SPACING SHALL BE 300 mm, AS SHOWN WITH THE EXCESS SPACE EQUALIZED AT EACH SIDE OF THE PAVEMENT SLAB. HOWEVER, IN NO CASE SHALL A DEVICE BE CLOSER THAN 150 mm TO THE SECTION'S EDGE.
- THE LOAD TRANSFER DEVICES SHALL BE ALIGNED PARALLEL TO THE CENTERLINE AND TO THE SLAB'S SURFACE. TOLERANCE SHALL BE ± 6 mm PER DEVICE.
- CONCRETE PLACEMENT AROUND THE LOAD TRANSFER DEVICES IS CRITICAL TO JOINT PERFORMANCE. DEVICE ALIGNMENT MUST BE MAINTAINED, AND THE CONCRETE MUST BE WELL CONSOLIDATED UNDER AND AROUND EACH DEVICE. THE RESIDENT ENGINEER SHALL DETERMINE IF A HAND VIBRATOR IS NECESSARY TO OBTAIN CONSOLIDATION.
- TO ENSURE PROPER LOCATION OF SAW CUTS, A GUIDELINE SHALL BE ESTABLISHED. THIS MAY BE ACCOMPLISHED BY SNAPPING A STRINGLINE IN THE PLASTIC CONCRETE FOR THE SAW OPERATOR TO FOLLOW.
- SAWED GROOVES SHALL NOT BE MADE UNTIL THE CONCRETE IS STRONG ENOUGH TO ALLOW THE SAW TO CUT THROUGH THE CONCRETE WITH A MINIMUM AMOUNT OF RAVELING OR AGGREGATE PULLOUTS. (THE TIME WILL RANGE BETWEEN 4 AND 24 HOURS DEPENDING ON WEATHER AND OTHER CONDITIONS.)
- ALL JOINTS SHALL BE SAWS IN SUCCESSION. THEY SHALL BE SAWS WHILE THE PAVEMENT IS UNDER COMPRESSION, IF POSSIBLE, TO PREVENT THE SLAB FROM CRACKING AHEAD OF THE SAW.
- THE DEPTH OF THE SAW CUTS SHALL BE CHECKED FREQUENTLY WHILE SAWING IS UNDERWAY AND THE REQUIRED ADJUSTMENTS MADE TO ALLOW FOR BLADE WEAR, OR AN AUTOMATICALLY ADJUSTING SAW SHOULD BE USED.
- JOINTS SHALL BE SAWS BEFORE ANY VEHICLES OR HEAVY EQUIPMENT ARE ALLOWED ON THE PAVEMENT.
- THE JOINTS SHALL BE CLEANED IMMEDIATELY AFTER SAWING. IF SAWING RESIDUE IS ALLOWED TO SET IN THE JOINTS, THEY SHALL BE RESAWED AND CLEANED BEFORE SEALING.
- THE JOINT SEALANT RESERVOIR CAN BE FORMED BY MAKING A SECOND-STAGE CUT NO EARLIER THAN 72 HOURS AFTER PLACING THE CONCRETE OR BY PLACING DIFFERENT SIZED BLADES ON THE MANDREL AND SAWING IT SIMULTANEOUSLY WITH THE INITIAL CUT.
- THE SEALANT USED SHALL BE FIELD-MOLDED AND SHALL BE CAPABLE OF WITHSTANDING REPEATED EXTENSION AND COMPRESSION. SEE SUPPLEMENTAL SPECIAL PROVISIONS FOR MATERIAL.
- THE SEALANT RESERVOIR SHALL BE AS DETAILED. IF A ROPE OR CORD IS USED AS A BOND BREAKER, THE DEPTH OF THE RESERVOIR SHALL BE INCREASED ACCORDINGLY TO ACCOMMODATE IT. THIS SAME CORD CAN BE USED TO CURE THE JOINT FACES AND KEEP OUT FOREIGN MATTER BY PLACING IT AT THE PAVEMENT SURFACE WHEN THE JOINT IS FORMED, AND EMBEDDING IT DEEPER AT THE TIME OF SEALING.
- THE JOINT SURFACE MUST BE CLEAN AND FREE OF CURING COMPOUND, RESIDUE, AND ANY OTHER FOREIGN MATTER PRIOR TO SEALING. CLEANING MAY BE DONE BY SAND-BLASTING, HIGH PRESSURE WATER BLAST, WATER OR COMPRESSED AIR, DEPENDING ON THE JOINT SURFACE CONDITION AND THE SEALANT MANUFACTURERS RECOMMENDATIONS. THE SURFACES SHALL BE DRY WHEN THE SEALANT IS PLACED.
- THE SEALANT RESERVOIRS SHALL BE FILLED TO ABOUT 3 mm BELOW FLUSH WITH THE PAVEMENT SURFACE TO PREVENT TRACKING FROM VEHICLE TIRES.
- AFTER THE DIAMOND GRINDING PROCESS IS COMPLETE, ALL PCC JOINTS WILL BE CLEANED AND RE-SEALED. THIS PROCESS WILL BE PAID UNDER ITEM 414.11, JOINT SEALER. HOT POURED (MOD.) AND WILL INCLUDE THE CLEANING, SEALANT AND ANY REPLACEMENT OF BACKER ROD DEEMED NECESSARY PER THE RESIDENT ENGINEER.

DETAIL A
SAWCUT & SEALANT DETAILS FOR PORTLAND CEMENT CONCRETE



NOTE:
ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED

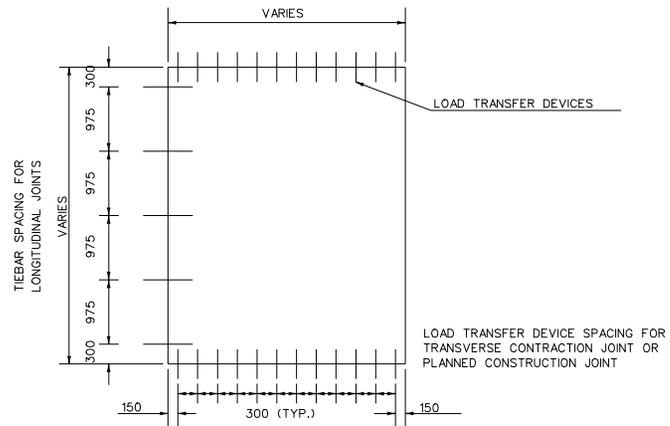
▨ AREAS OF FULL DEPTH REPAIR.

ARCHIVED
IN DPR

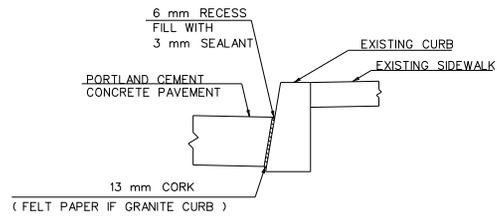


PORTLAND CEMENT CONCRETE FULL DEPTH REPAIR DETAILS	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 21 OF 51
DESIGNED BY: D-H		
IPARM FILE NAME: pd096pc2.i		

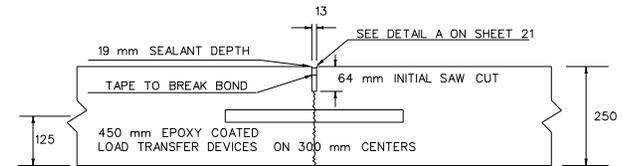
DETAIL C



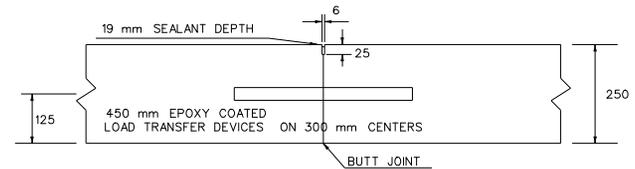
DETAIL G



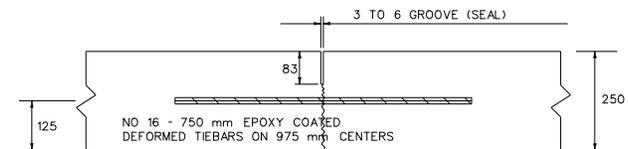
DETAIL D - TRANSVERSE CONTRACTION JOINT



DETAIL E - PLANNED TRANSVERSE CONSTRUCTION JOINT



DETAIL F - LONGITUDINAL JOINTS



NOTE:
ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED

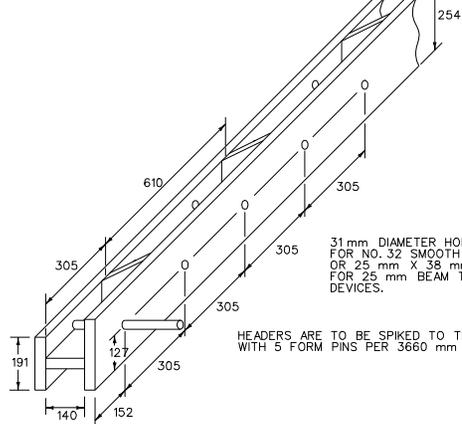
PORTLAND CEMENT
CONCRETE FULL
DEPTH REPAIR
DETAILS

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 2131(1)S
FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096pc3.i

PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 22 OF 51

ARCHIVED
IN DPR

WOODEN HEADER FOR PLANNED TRANSVERSE CONSTRUCTION JOINT.

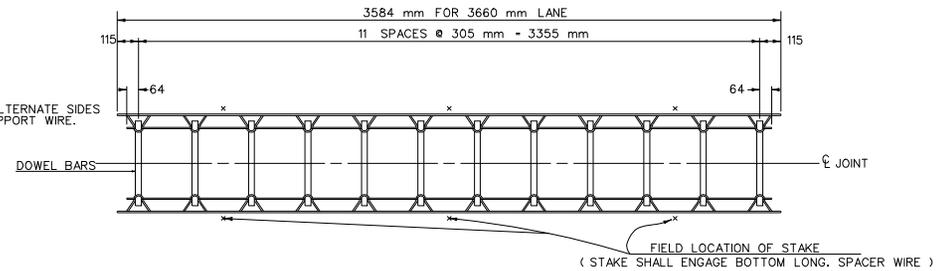


31 mm DIAMETER HOLES FOR NO. 32 SMOOTH DOWELS OR 25 mm X 38 mm RECTANGULAR HOLES FOR 25 mm BEAM TYPE LOAD TRANSFER DEVICES.

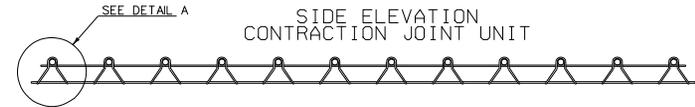
HEADERS ARE TO BE SPIKED TO THE SUBBASE WITH 5 FORM PINS PER 3660 mm LENGTH.

ARC OR RESISTANCE WELD DOWEL BARS ON ALTERNATE SIDES WITH TOP SPACER WIRE AND JOINT FILLER SUPPORT WIRE.

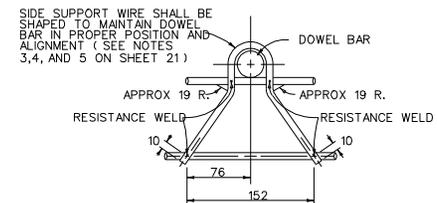
PLAN VIEW CONTRACTION JOINT UNIT



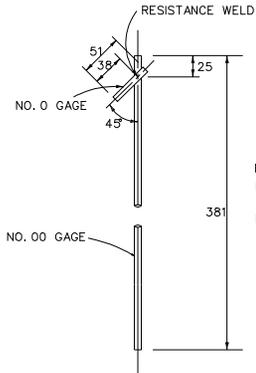
SIDE ELEVATION CONTRACTION JOINT UNIT



DETAIL A

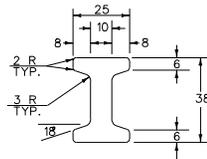


STAKE DETAIL

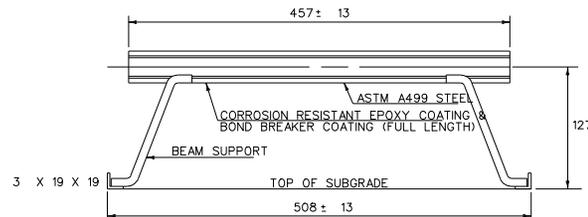


NOTE: A SINGLE WIRE MAY BE USED IN LIEU OF STAKE DETAIL SHOWN PROVIDED A 00 GAGE WIRE IS USED AND BENT INTO A HOOK AT THE TOP END TO CONFORM TO DETAIL.

END VIEW OF 25 MM BEAM TYPE DOWEL BAR



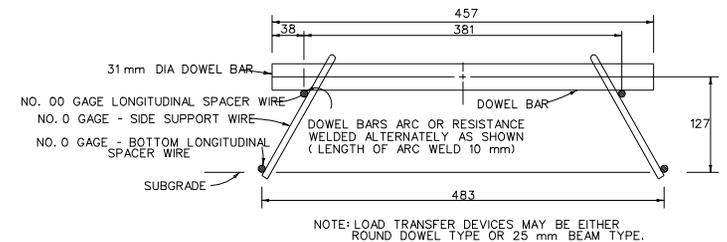
25 MM BEAM TYPE DOWEL ASSEMBLY



SPIKE TO SUBGRADE W/ 152 mm SPIKE (60D NAIL) 4 PER 1220 mm SECTION

BEAM SUPPORTS TO BE WELDED TO 25 mm BEAMS AND BASES

SECTION THROUGH CONTRACTION JOINT UNIT



NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED

PORTLAND CEMENT CONCRETE FULL DEPTH REPAIR DETAILS

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 213(1)S

FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096pc4.i

PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 23 OF 51

ARCHIVED IN DPR

WEST CANAL STREET
 STA. 0+128.9 = MM 0.080
 (9.6 M WIDE)

WEST CENTER STREET
 STA. 0+187.7 = MM 0.116
 (7.8 M WIDE)

BEGIN STP 2131(1) S
 U.S. ROUTE 7
 WINOOSKI
 STA. 0+053.11 = MM 0.033

BRIDGE NO. 150

0+040 TO BURLINGTON

0+060

A1	A2	A3	A4	A5	A6	A7	A8	A9	A10	A11	A12	A13	A14	A15	A16	A17	A18	A19	A20	A21	A22	A23	A24	A25	A26	A27	A28	A29	A30	A31
B1	B2	B3	B4	B5	B6	B7	B8	B9	B10	B11	B12	B13	B14	B15	B16	B17	B18	B19	B20	B21	B22	B23	B24	B25	B26	B27	B28	B29	B30	B31
C1	C2	C3	C4	C5	C6	C7	C8	C9	C10	C11	C12	C13	C14	C15	C16	C17	C18	C19	C20	C21	C22	C23	C24	C25	C26	C27	C28	C29	C30	C31
D1	D2	D3	D4	D5	D6	D7	D8	D9	D10	D11	D12	D13	D14	D15	D16	D17	D18	D19	D20	D21	D22	D23	D24	D25	D26	D27	D28	D29	D30	D31
E1	E2	E3	E4	E5	E6	E7	E8	E9	E10	E11	E12	E13	E14	E15	E16	E17	E18	E19	E20	E21	E22	E23	E24	E25	E26	E27	E28	E29	E30	E31

U.S. ROUTE 7 (MAIN ST.)

WINOOSKI CITY

GRAVEL LOT

2/5 BRICK FIDDLEHEAD #46 31/32

VACANT LOT

2 S.W.F. VT PAWN

3 STY. BRICK FOREST HILL RESTAURANT

3 SWF VT. PASTA CO.

COLD ISLAND BOOKS #28

CANAL ZONE STORE #28/2

3 SWF BRADSHAW'S TAVERN

1 SWF

3S BRICK SNEAKERS

KEIL & MAHONEY

SEE NEXT SHEET

TO COLCHESTER

MATCH LINE

CHAMPLAIN MILL ENTRANCE "A"
 STA. 0+128.9 = MM 0.080
 (13.8 M WIDE)

VNB BANK ENTRANCE

LOCATIONS OF FULL DEPTH CONCRETE REPAIR

ARCHIVED IN DPR



PORTLAND CEMENT CONCRETE PAVEMENT GRID LAYOUT	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 24 OF 51
	DESIGNED BY: D-H	

WEST ALLEN STREET
 STA. 0+258.8 - MM 0.160
 (13.5 M WIDE)

FOLLETT ST.

PAVED DR

PAVED DR
 (BANK ENT.)

F19	G19	H19	I19
F18	G18	H18	I18
F17	G17	H17	I17
F16	G16	H16	I16
F15	G15	H15	I15
F14	G14	H14	I14
F13	G13	H13	I13
F12	G12	H12	I12
F11	G11	H11	I11
F10	G10	H10	I10
F9	G9	H9	I9
F8	G8	H8	I8
F7	G7	H7	I7
F6	G6	H6	I6
F5	G5	H5	I5
F4	G4	H4	I4
F3	G3	H3	I3
F2	G2	H2	I2
F1	G1	H1	I1

BRIDGE 151
 STA 0+344.1 - MM 0.213
 (NO CONCRETE WORK ON BRIDGE NO. 151)

63/64

SEE NEXT SHEET

TO COLCHESTER

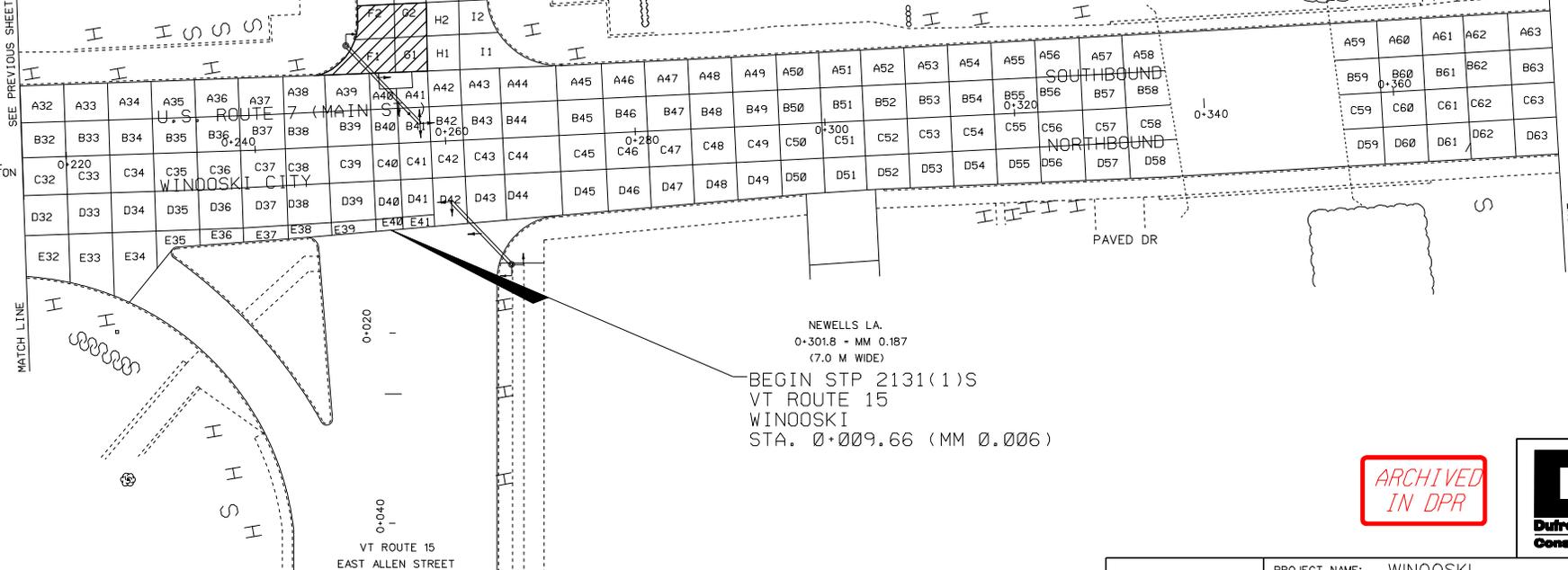
MATCH LINE

SEE PREVIOUS SHEET

31/32

TO BURLINGTON

MATCH LINE



NEWELLS LA.
 0+301.8 - MM 0.187
 (7.0 M WIDE)
 BEGIN STP 213(1)S
 VT ROUTE 15
 WINOOSKI
 STA. 0+009.66 (MM 0.006)

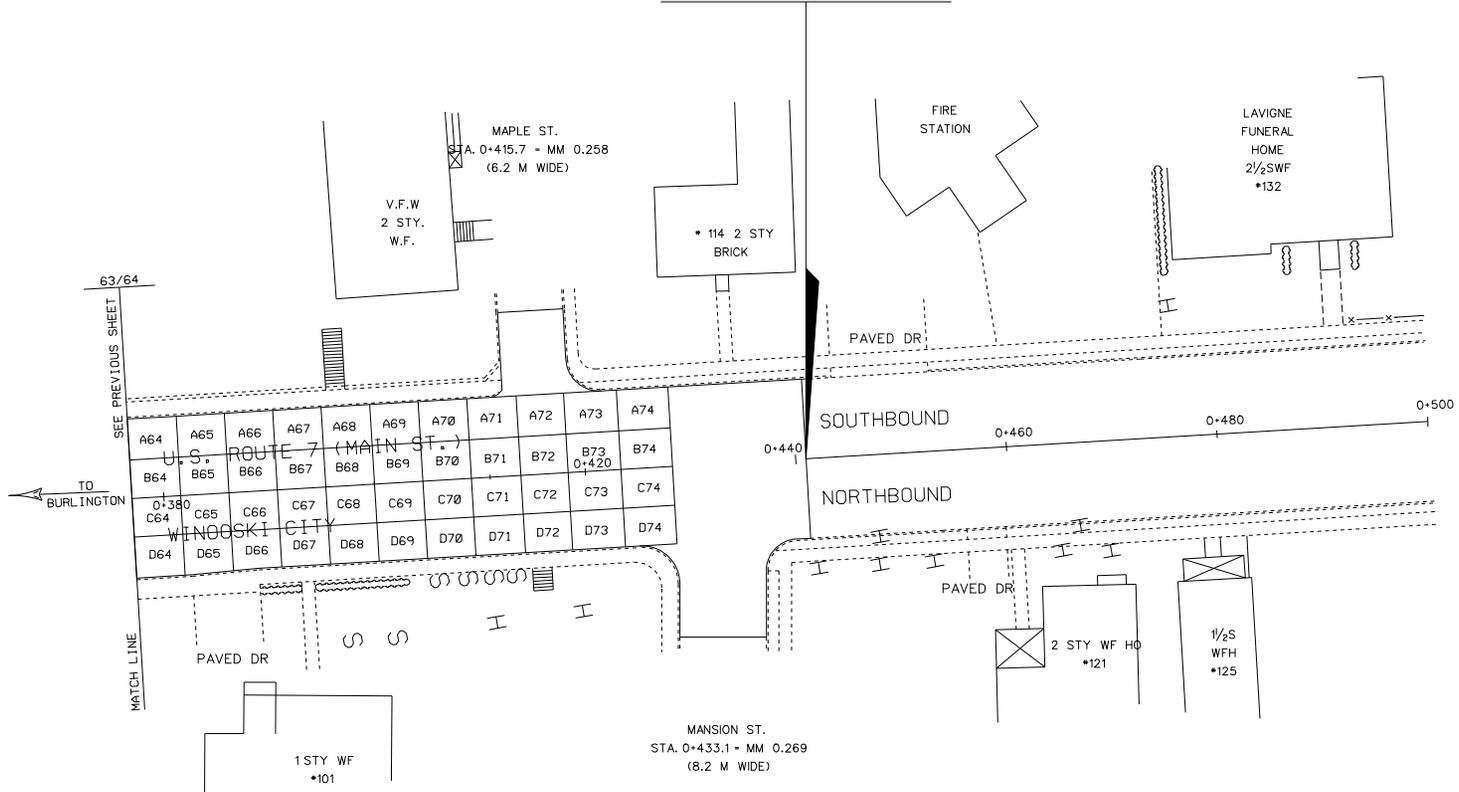
ARCHIVED
 IN DPR



LOCATIONS OF FULL DEPTH
 CONCRETE REPAIR

PORTLAND CEMENT CONCRETE PAVEMENT GRID LAYOUT #2	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 213(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 25 OF 51
	DESIGNED BY: D-H	

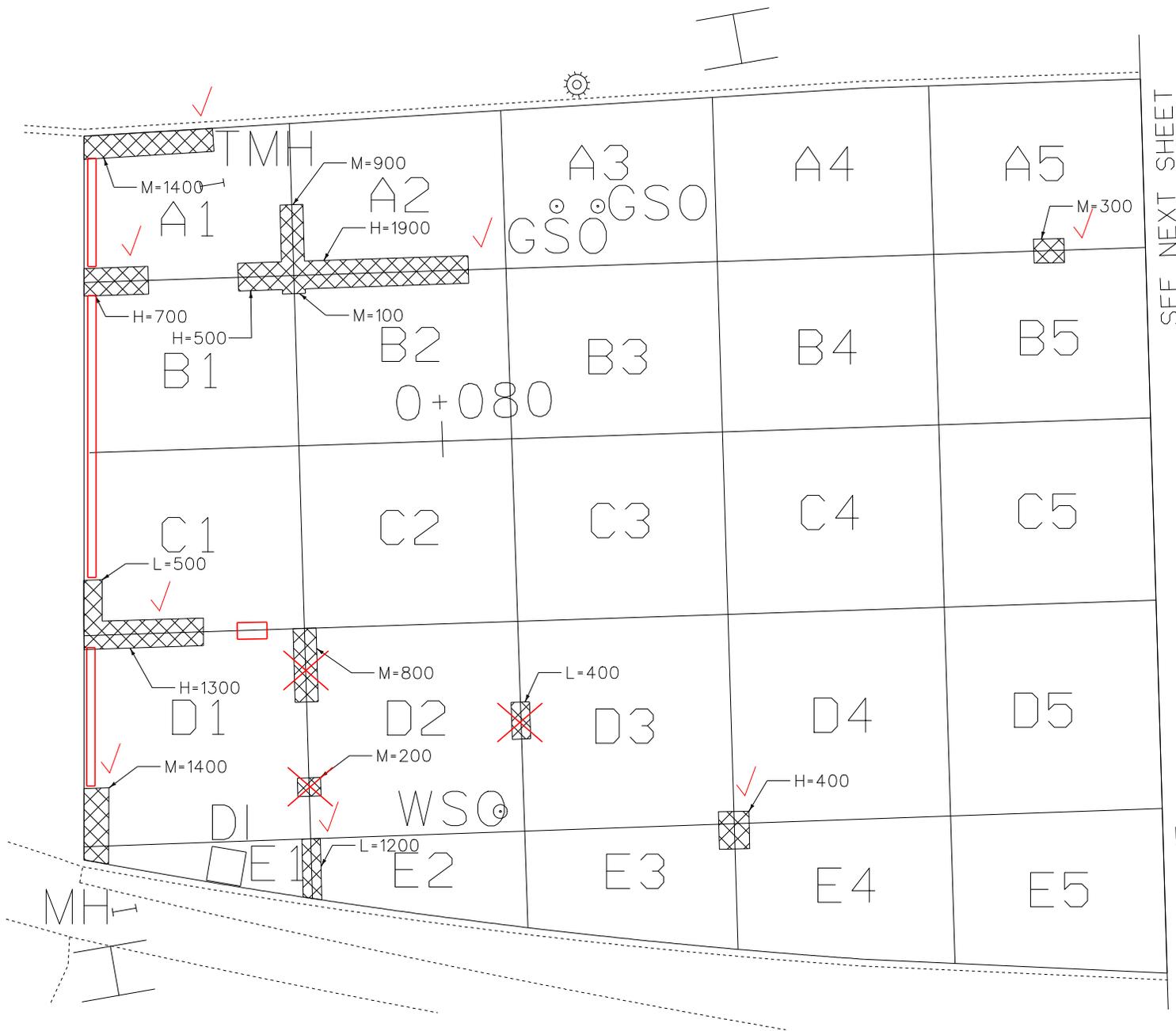
END STP 2131(1) S
 U.S. ROUTE 7
 WINOOSKI
 STA. 0+440.96 = MM 0.274



ARCHIVED
 IN DPR



PORTLAND CEMENT CONCRETE PAVEMENT GRID LAYOUT *3	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 26 OF 51
	DESIGNED BY: D-H	
IPARM FILE NAME: pd096g13.i		



SEE NEXT SHEET

REFER TO BOOK 7 FOR ACTUAL REPAIR DETAILS.

- ADDITIONAL REPAIR
- ✗ AREA NOT REPAIRED

LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
- ▨ AREAS OF PARTIAL DEPTH REPAIR

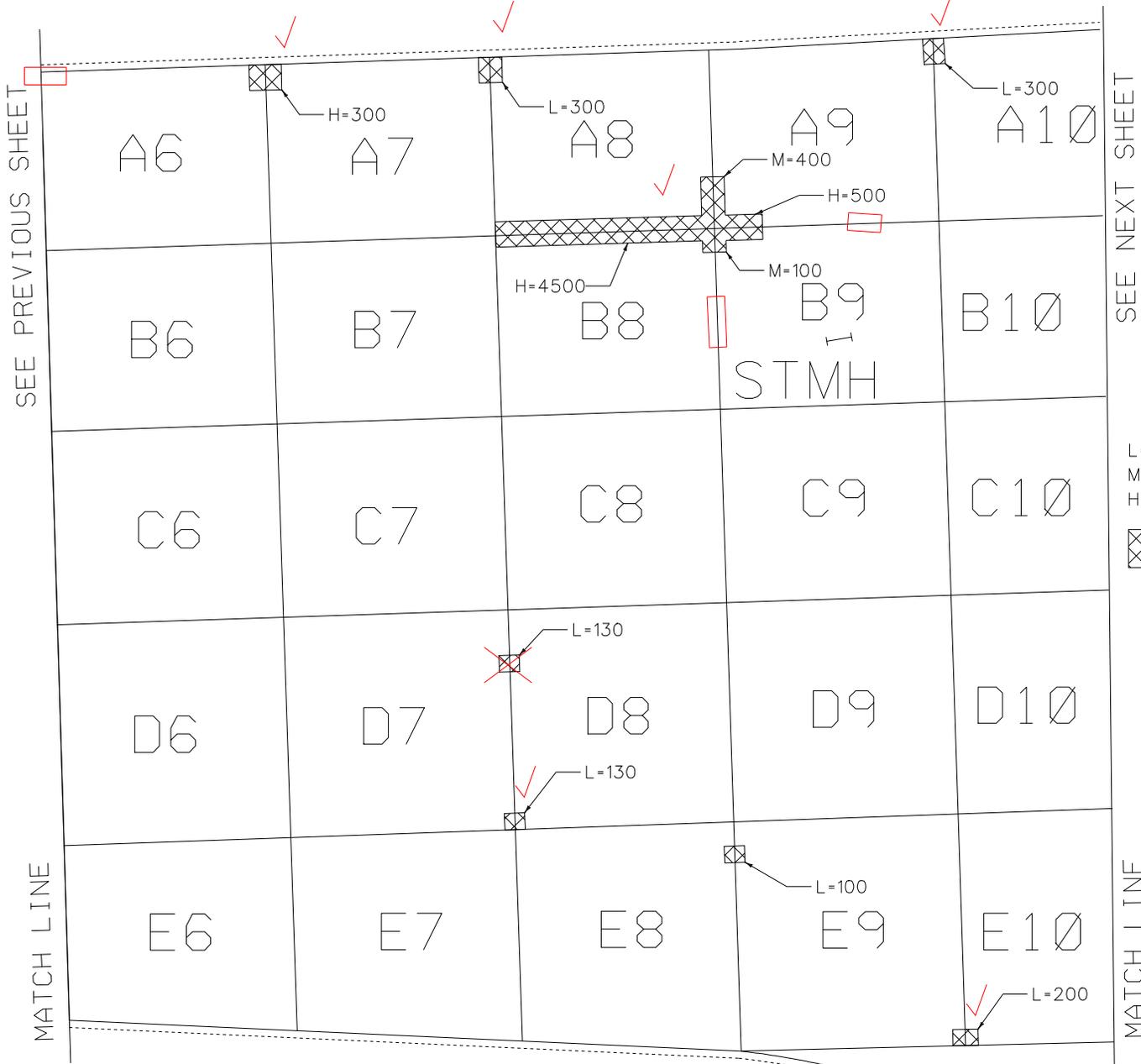
ARCHIVED IN DPR



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 1

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 27 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r01.i	



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
-  AREAS OF PARTIAL DEPTH REPAIR

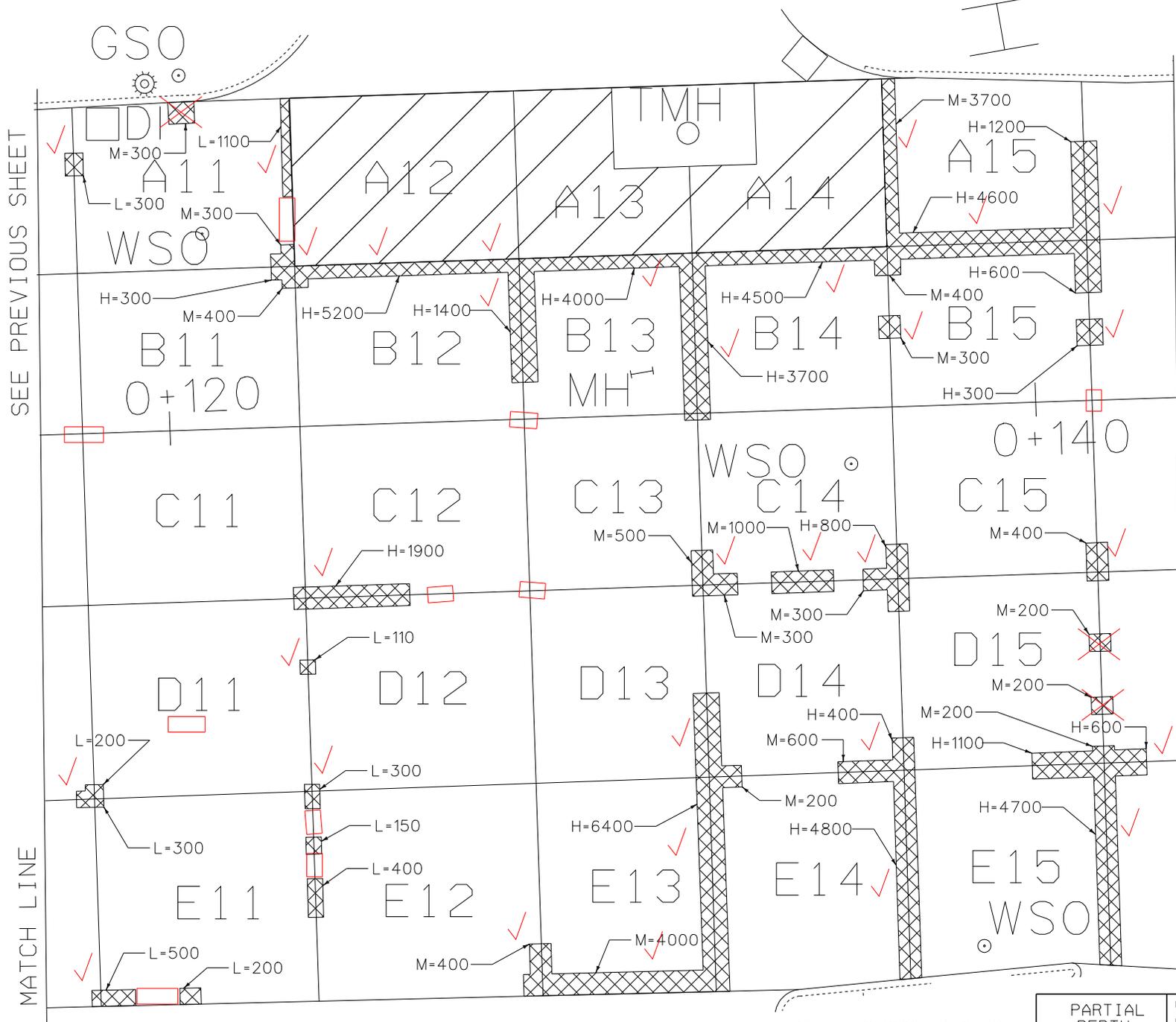
NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 2

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 28 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r02.i	

ARCHIVED
IN DPR





SEE PREVIOUS SHEET

SEE NEXT SHEET

MATCH LINE

MATCH LINE

LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
-  AREAS OF PARTIAL DEPTH REPAIR
-  AREAS OF FULL DEPTH REPAIR. FOR DETAILS, SEE SHEETS 21-23

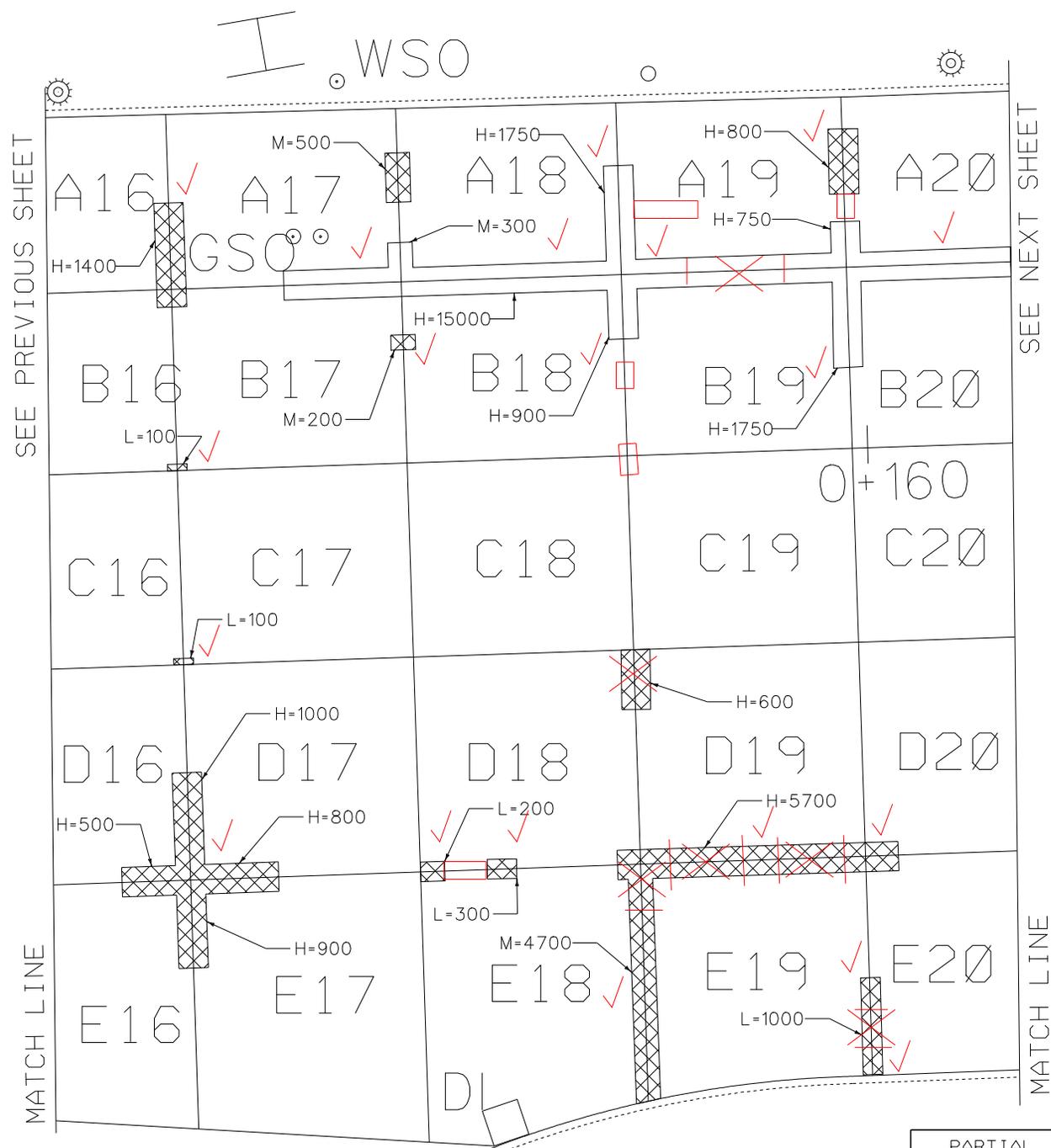
ARCHIVED
IN DPR



NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 3

PROJECT NAME: WINOOSKI
 PROJECT NUMBER: STP 2131(1)S
 FILE NAME: /pave/99d096/pd096.dgn
 PROJECT LEADER: JLL
 DESIGNED BY: D-H
 IPARM FILE NAME: pd096r03.i
 PLOT DATE: 08-JAN-2003 08
 DRAWN BY: D-H
 CHECKED BY:
 SHEET 29 OF 51



LEGEND

L= LOW SEVERITY (200 mm WIDTH)
M= MEDIUM SEVERITY (250 mm WIDTH)
H= HIGH SEVERITY (300 mm WIDTH)

 AREAS OF PARTIAL DEPTH REPAIR

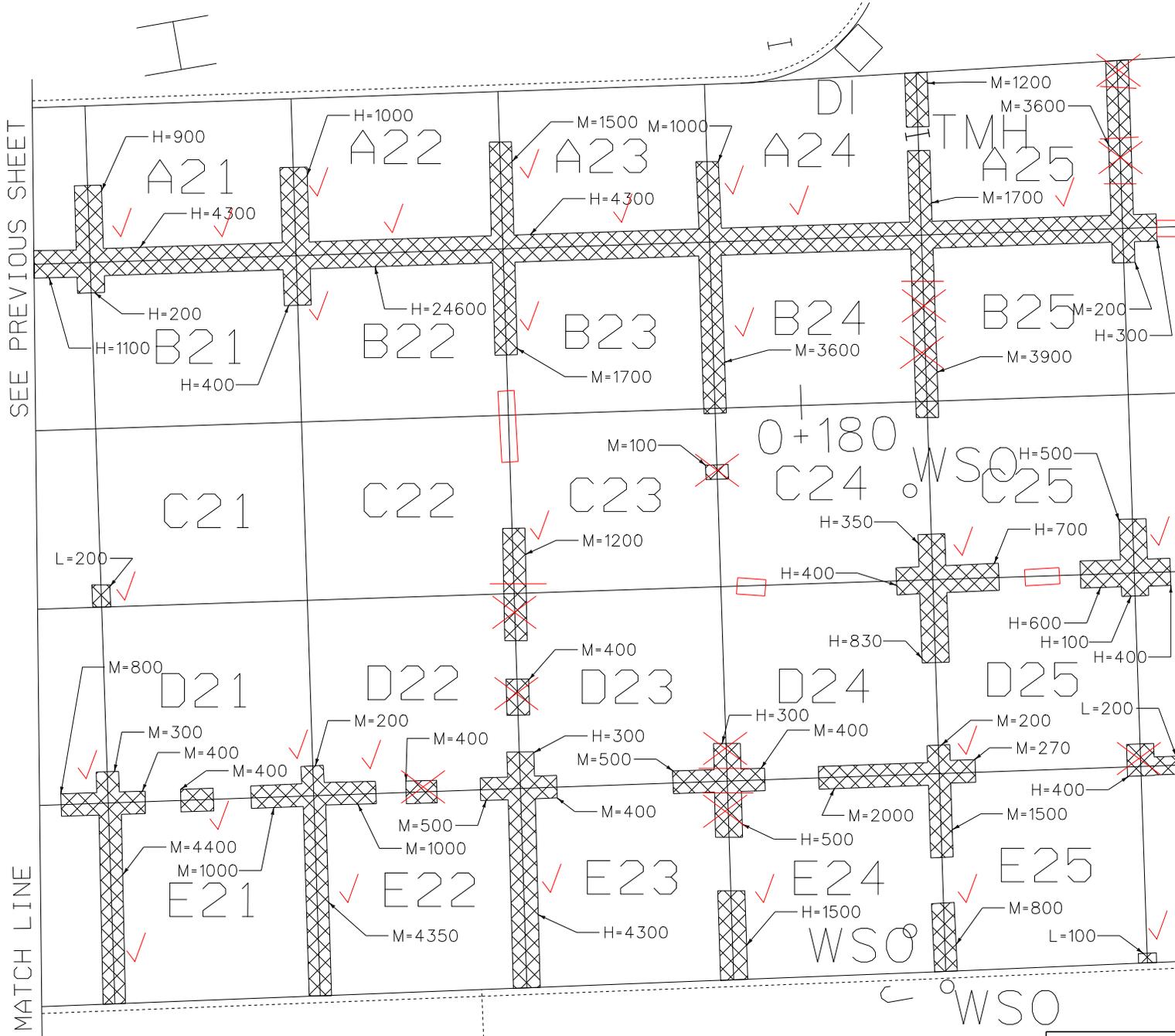
**ARCHIVED
IN DPR**



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 4

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 30 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r04.i	



SEE PREVIOUS SHEET

SEE NEXT SHEET

LEGEND
 L= LOW SEVERITY (200 mm WIDTH)
 M= MEDIUM SEVERITY (250 mm WIDTH)
 H= HIGH SEVERITY (300 mm WIDTH)
 AREAS OF PARTIAL DEPTH REPAIR

**ARCHIVED
 IN DPR**



SEE PREVIOUS SHEET

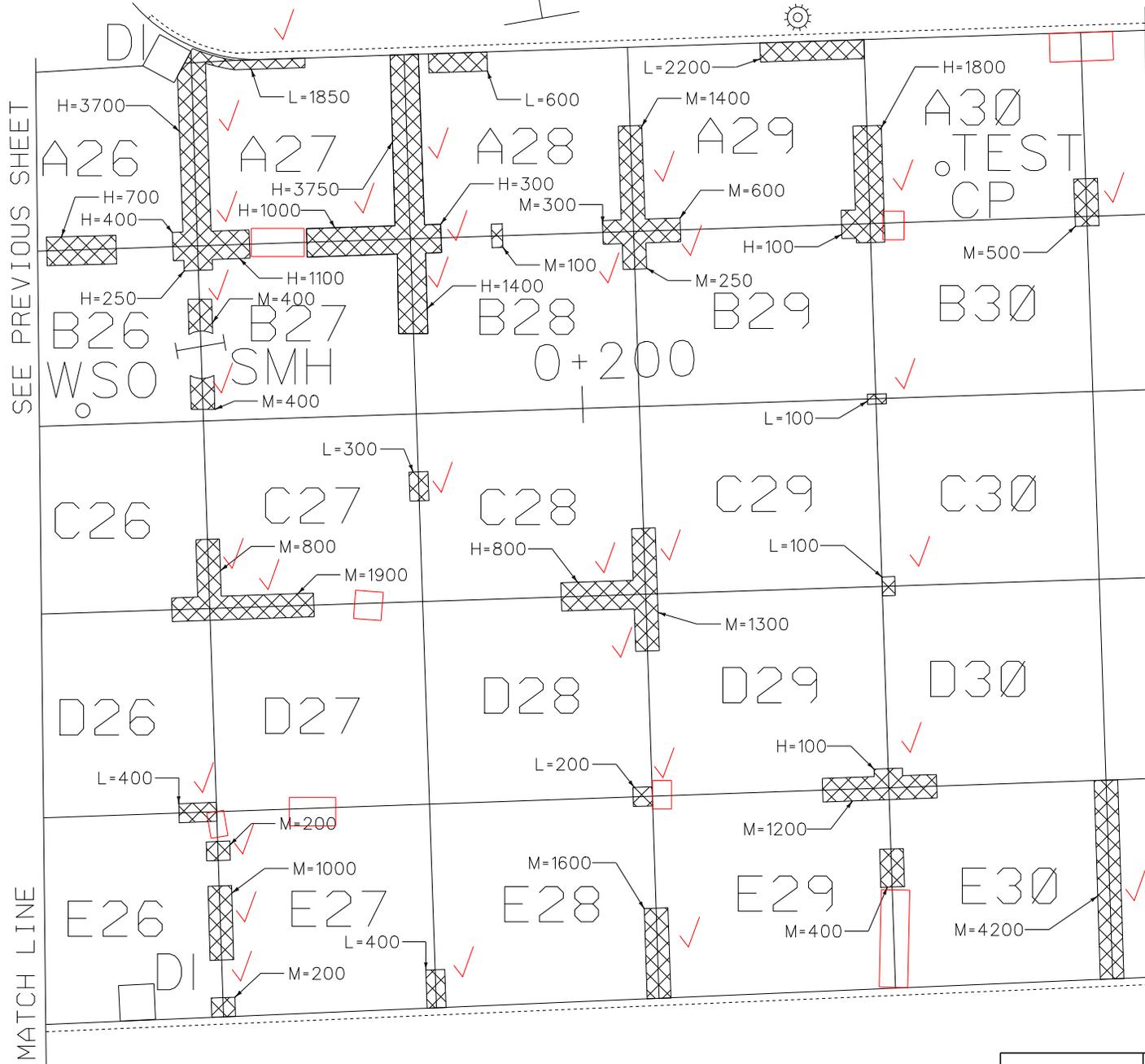
MATCH LINE

PARTIAL DEPTH REPAIR LOCATIONS # 5

PROJECT NAME: WINOOSKI
 PROJECT NUMBER: STP 2131(1)S
 FILE NAME: /pave/99d096/pd096.dgn
 PROJECT LEADER: JLL
 DESIGNED BY: D-H
 IPARM FILE NAME: pd096r05.i

PLOT DATE: 08-JAN-2003 08
 DRAWN BY: D-H
 CHECKED BY:
 SHEET 31 OF 51

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.



SEE PREVIOUS SHEET

SEE NEXT SHEET

MATCH LINE

MATCH LINE

LEGEND

L= LOW SEVERITY (200 mm WIDTH)
 M= MEDIUM SEVERITY (250 mm WIDTH)
 H= HIGH SEVERITY (300 mm WIDTH)

AREAS OF PARTIAL DEPTH REPAIR

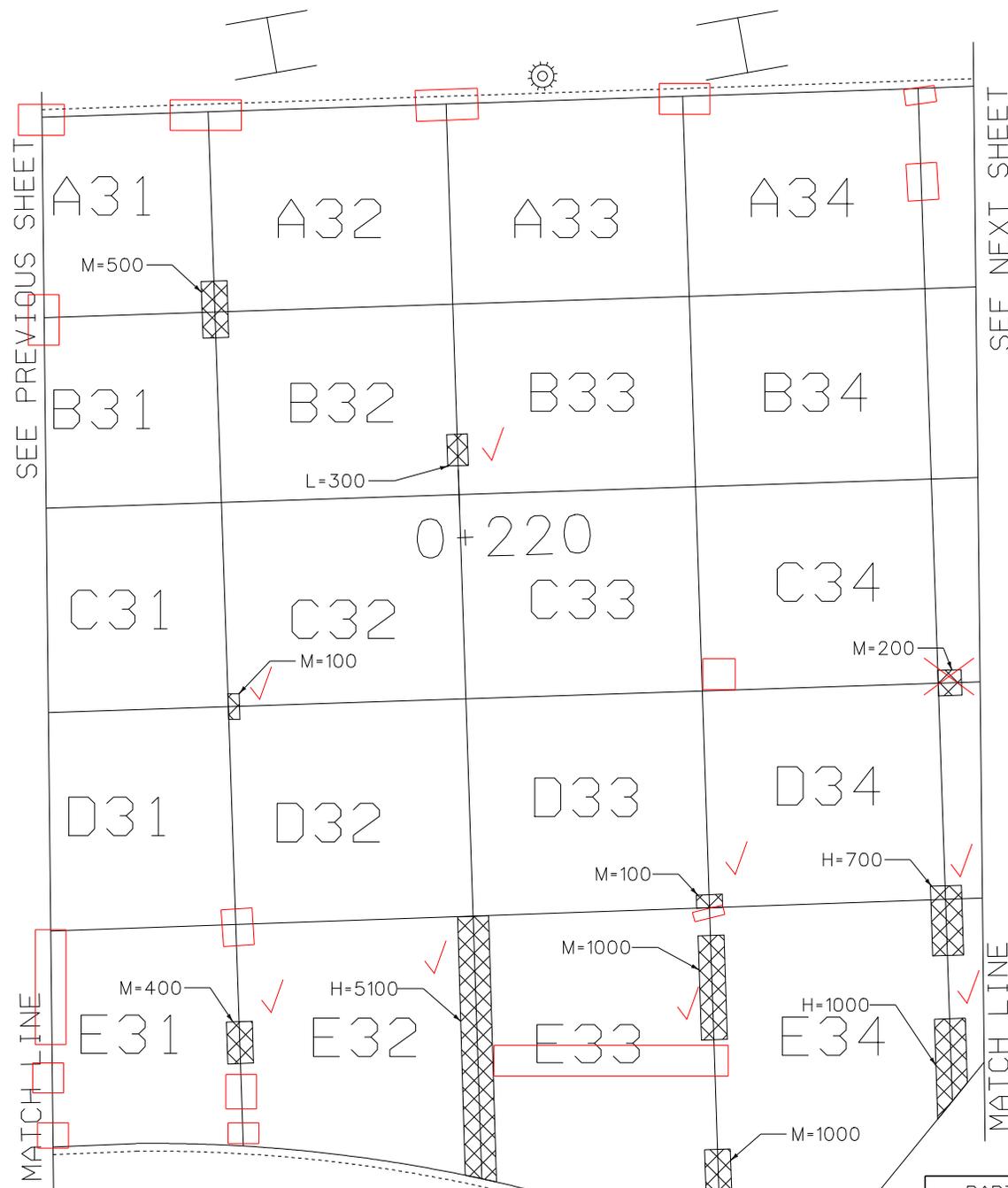
ARCHIVED
IN DPR



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 6

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 213(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 32 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r06.i	



SEE PREVIOUS SHEET

SEE NEXT SHEET

SEE PREVIOUS SHEET

MATCH LINE

MATCH LINE

LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
- AREAS OF PARTIAL DEPTH REPAIR

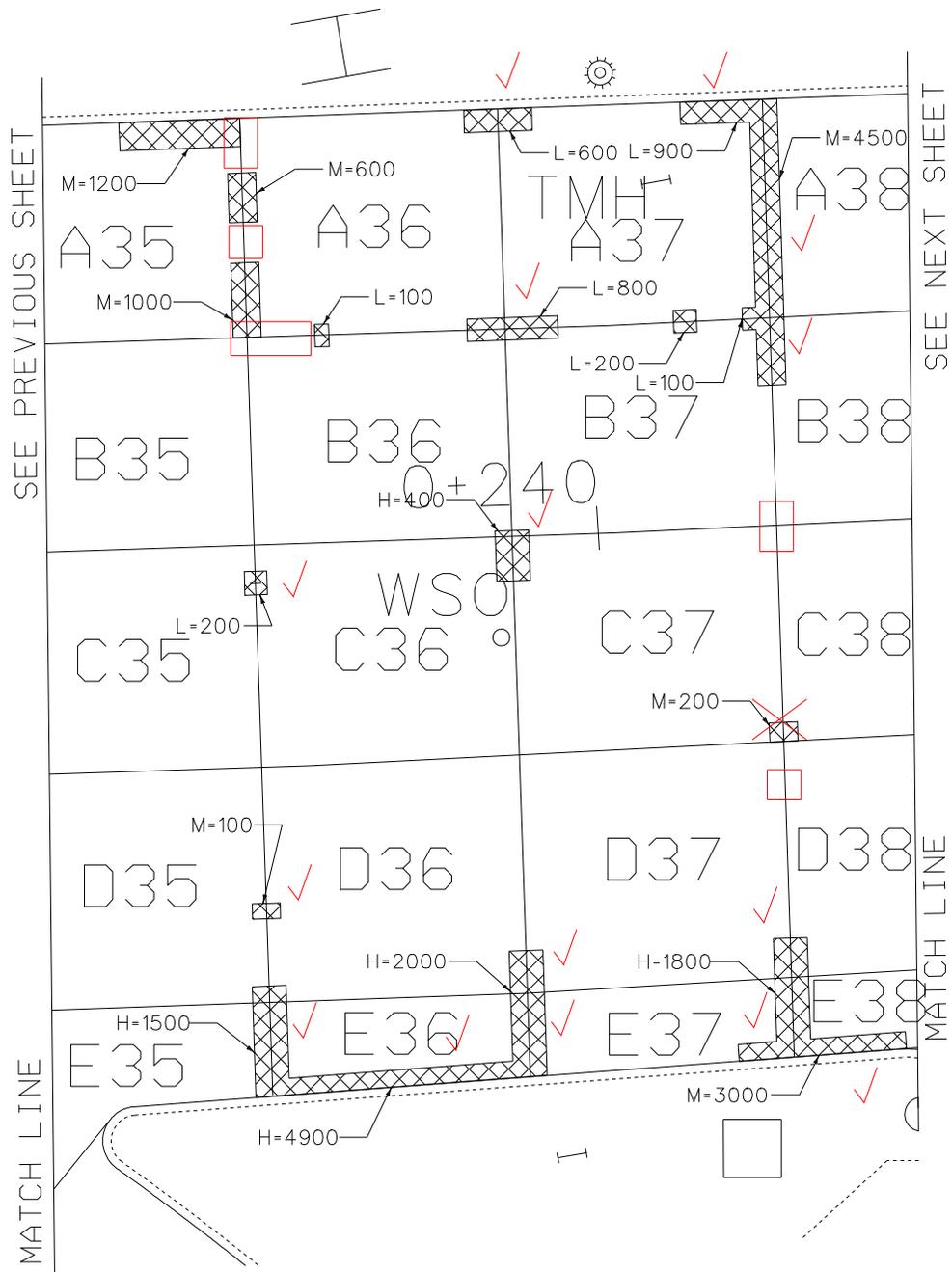
ARCHIVED
IN DPR



PARTIAL DEPTH REPAIR LOCATIONS # 7

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 33 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r07.i	

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)

AREAS OF PARTIAL DEPTH REPAIR

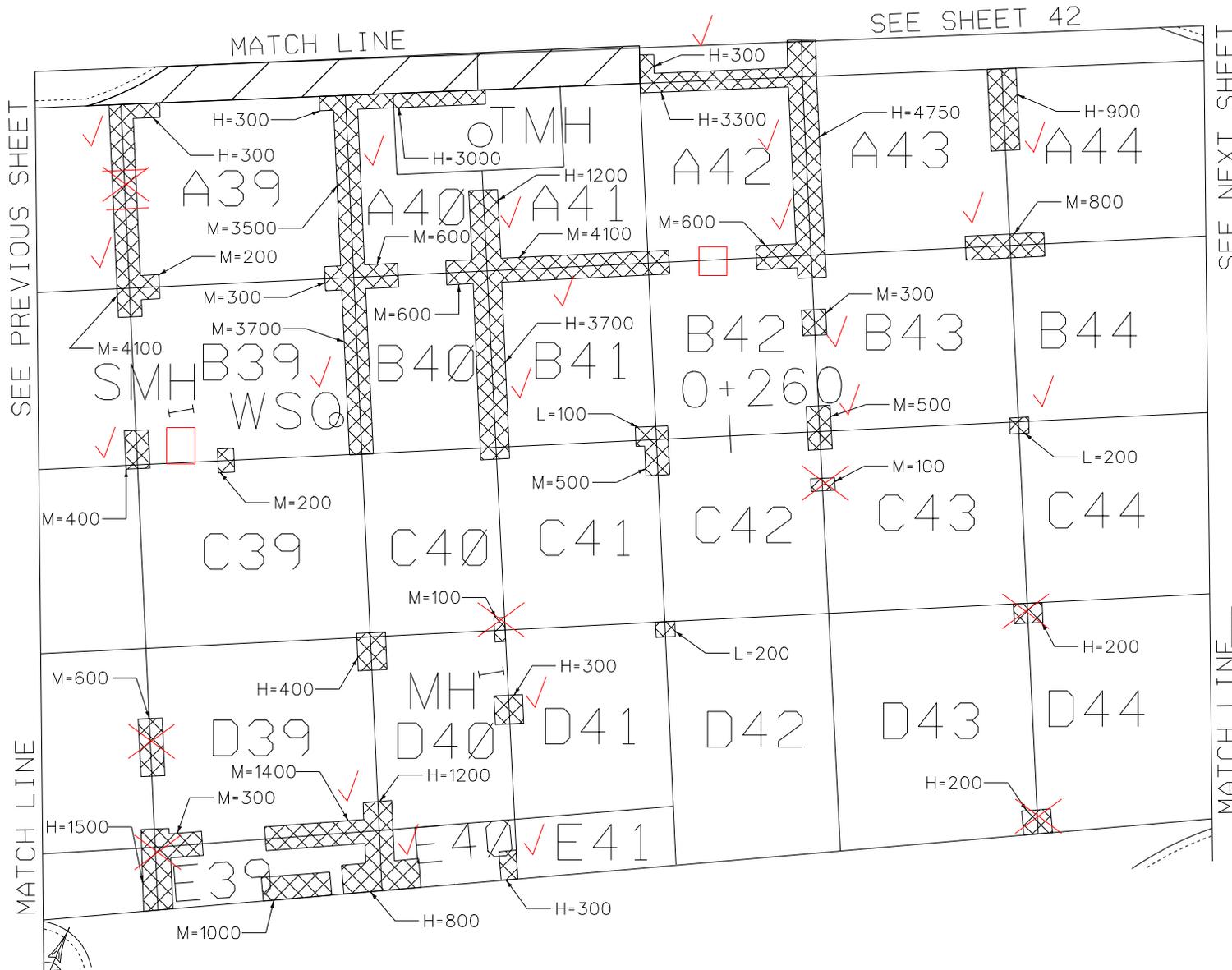
ARCHIVED
IN DPR



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED.

**PARTIAL
DEPTH
REPAIR
LOCATIONS**
8

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 34 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r08.i	



SEE PREVIOUS SHEET

SEE NEXT SHEET

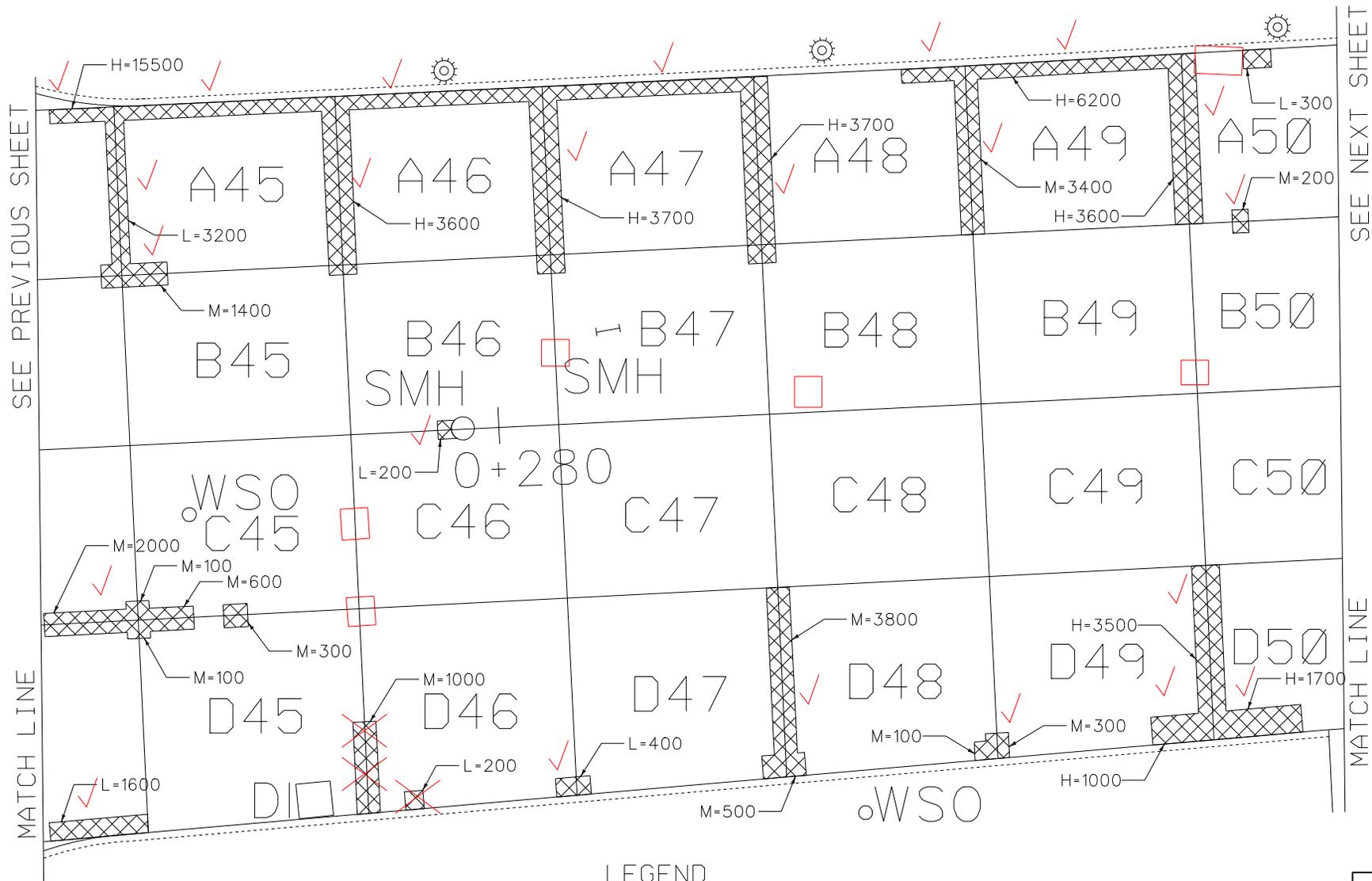
- LEGEND**
- L= LOW SEVERITY (200 mm WIDTH)
 - M= MEDIUM SEVERITY (250 mm WIDTH)
 - H= HIGH SEVERITY (300 mm WIDTH)
 - AREAS OF PARTIAL DEPTH REPAIR
 - AREAS OF FULL DEPTH REPAIR. FOR DETAILS, SEE SHEETS 21-23.

ARCHIVED
IN DPR



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 9	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 35 OF 51
	DESIGNED BY: D-H	
	IPARM FILE NAME: pd096r09.i	



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)



AREAS OF PARTIAL DEPTH REPAIR

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED

ARCHIVED
IN DPR

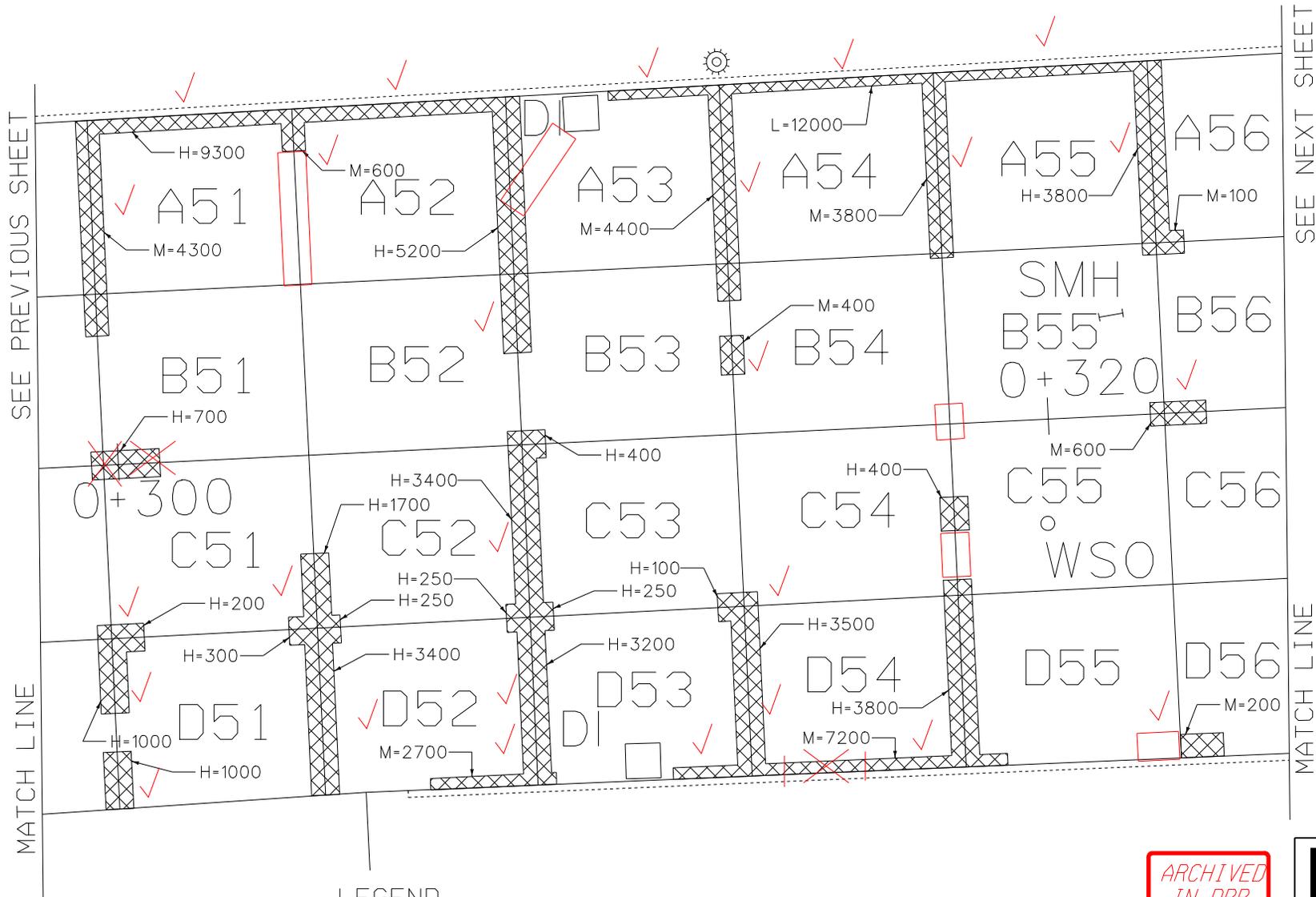


PARTIAL DEPTH REPAIR LOCATIONS
10

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 2131(1)S

FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096r10.i

PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 36 OF 51



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)

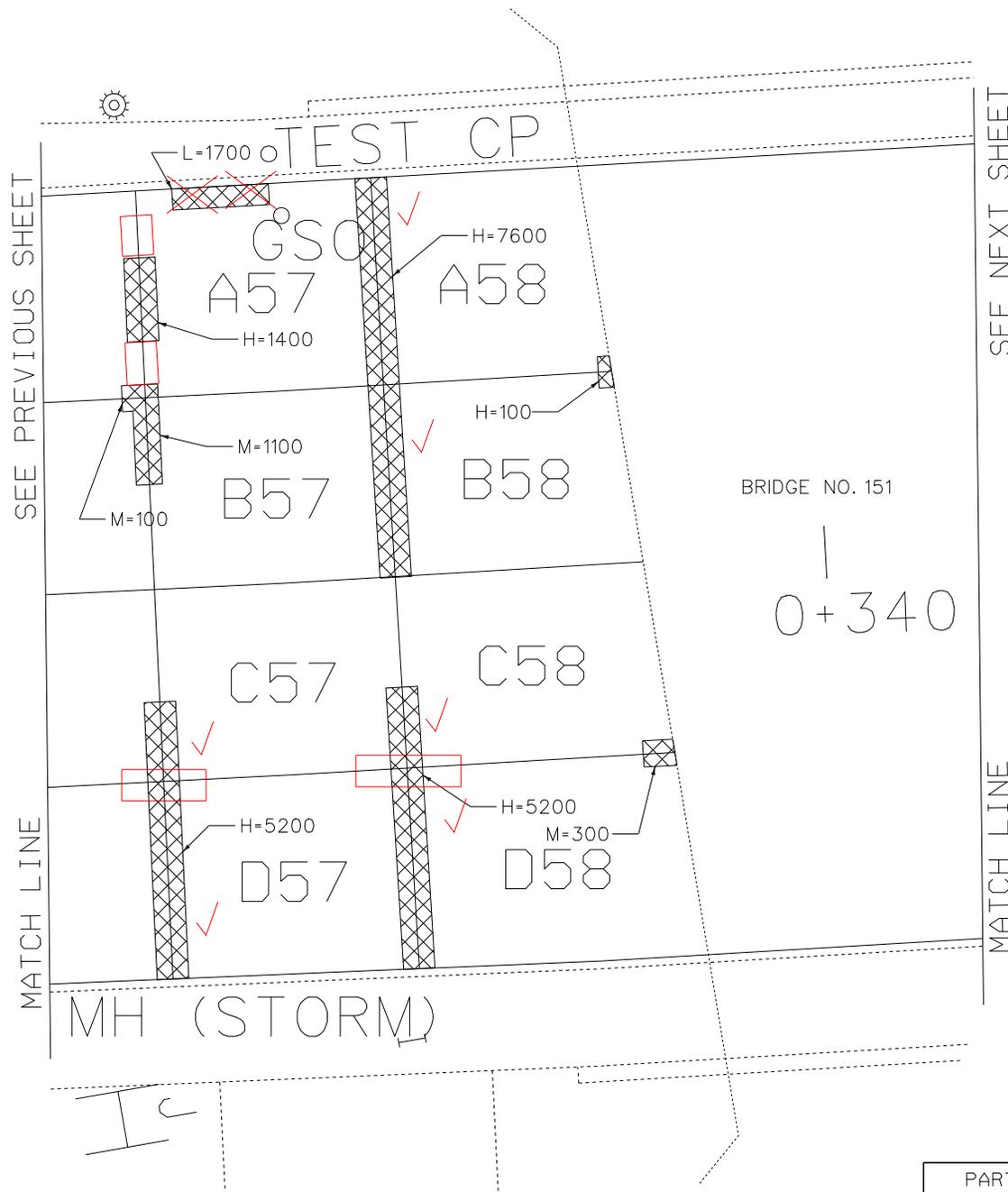
AREAS OF PARTIAL DEPTH REPAIR

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

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IN DPR



<p>PARTIAL DEPTH REPAIR LOCATIONS # 11</p>	<p>PROJECT NAME: WINOOSKI PROJECT NUMBER: STP 2131(1)S FILE NAME: /pave/99d096/pd096.dgn PROJECT LEADER: JLL DESIGNED BY: D-H IPARM FILE NAME: pd096r11.i</p> <p>PLOT DATE: 08-JAN-2003 08 DRAWN BY: D-H CHECKED BY: SHEET 37 OF 51</p>
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LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
-  AREAS OF PARTIAL DEPTH REPAIR

BRIDGE NO. 151

0+340

MH (STORM)

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IN DPR



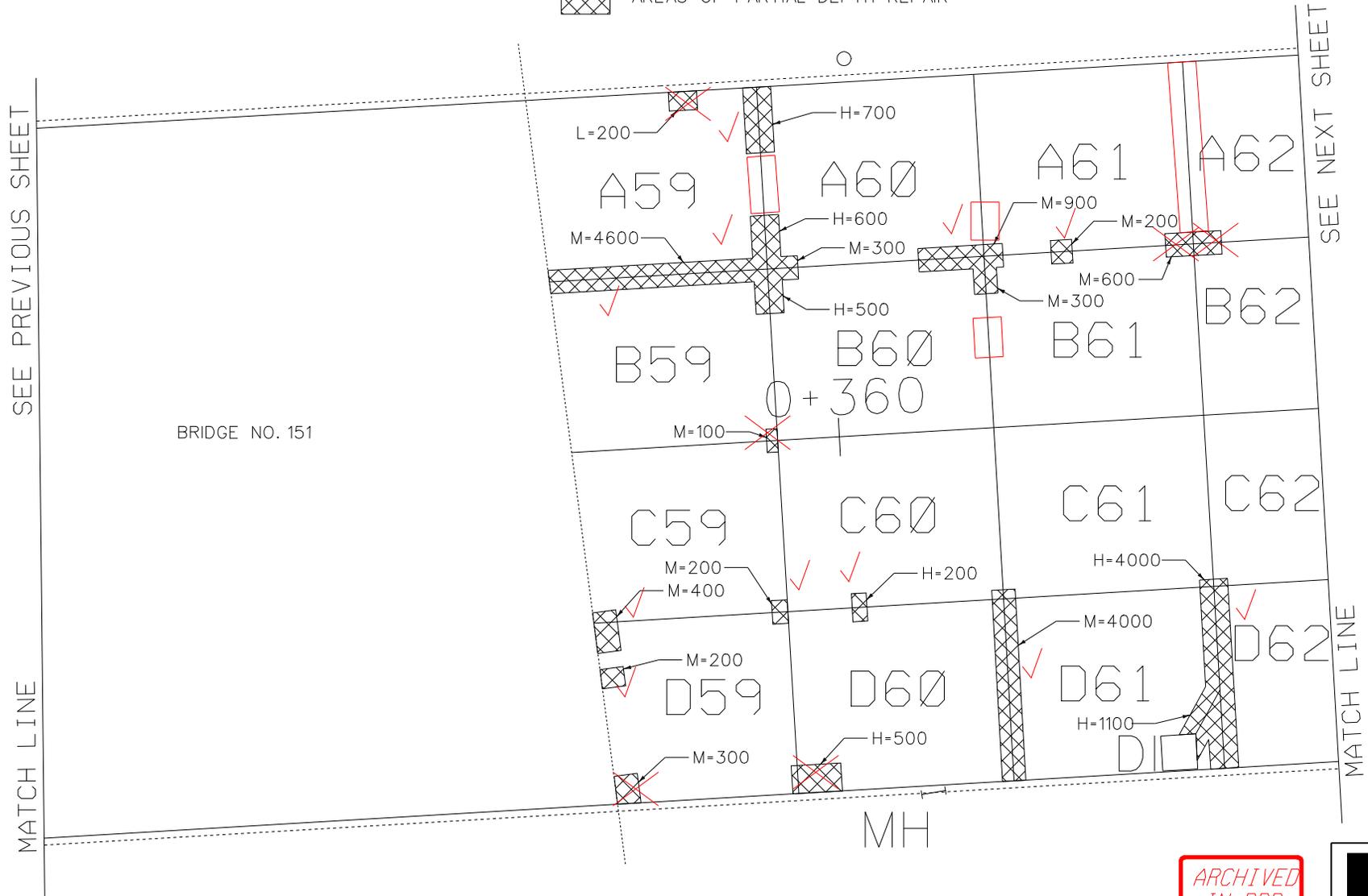
NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 12	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 38 OF 51
DESIGNED BY: D-H		
IPARM FILE NAME: pd096r12.i		

LEGEND

L= LOW SEVERITY (200 mm WIDTH)
M= MEDIUM SEVERITY (250 mm WIDTH)
H= HIGH SEVERITY (300 mm WIDTH)

AREAS OF PARTIAL DEPTH REPAIR



BRIDGE NO. 151

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IN DPR



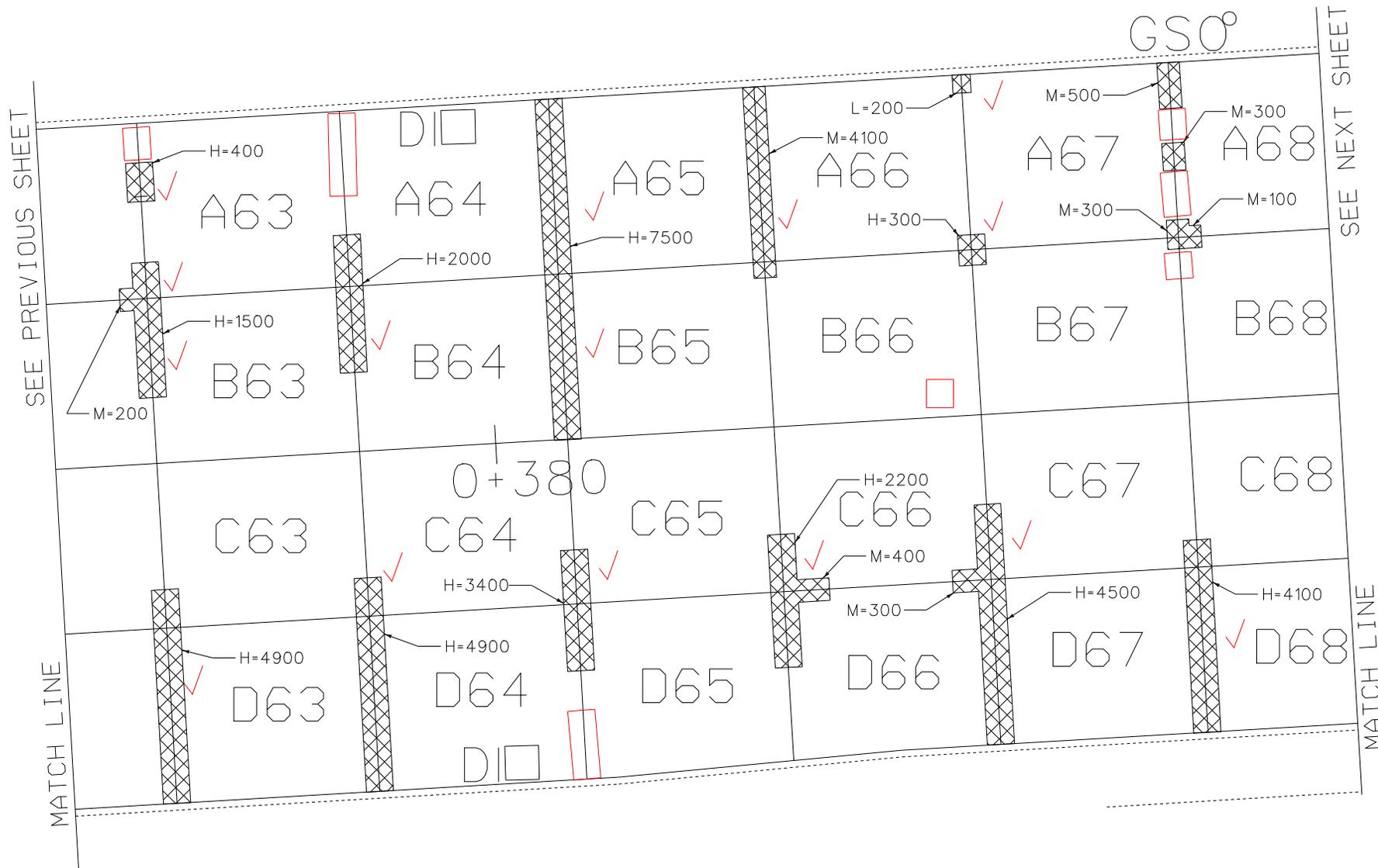
NOTES: 1) ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL
DEPTH
REPAIR
LOCATIONS
13

PROJECT NAME: WINOOSKI
PROJECT NUMBER: STP 2131(1)S

FILE NAME: /pave/99d096/pd096.dgn
PROJECT LEADER: JLL
DESIGNED BY: D-H
IPARM FILE NAME: pd096r13.i

PLOT DATE: 08-JAN-2003 08
DRAWN BY: D-H
CHECKED BY:
SHEET 39 OF 51



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)

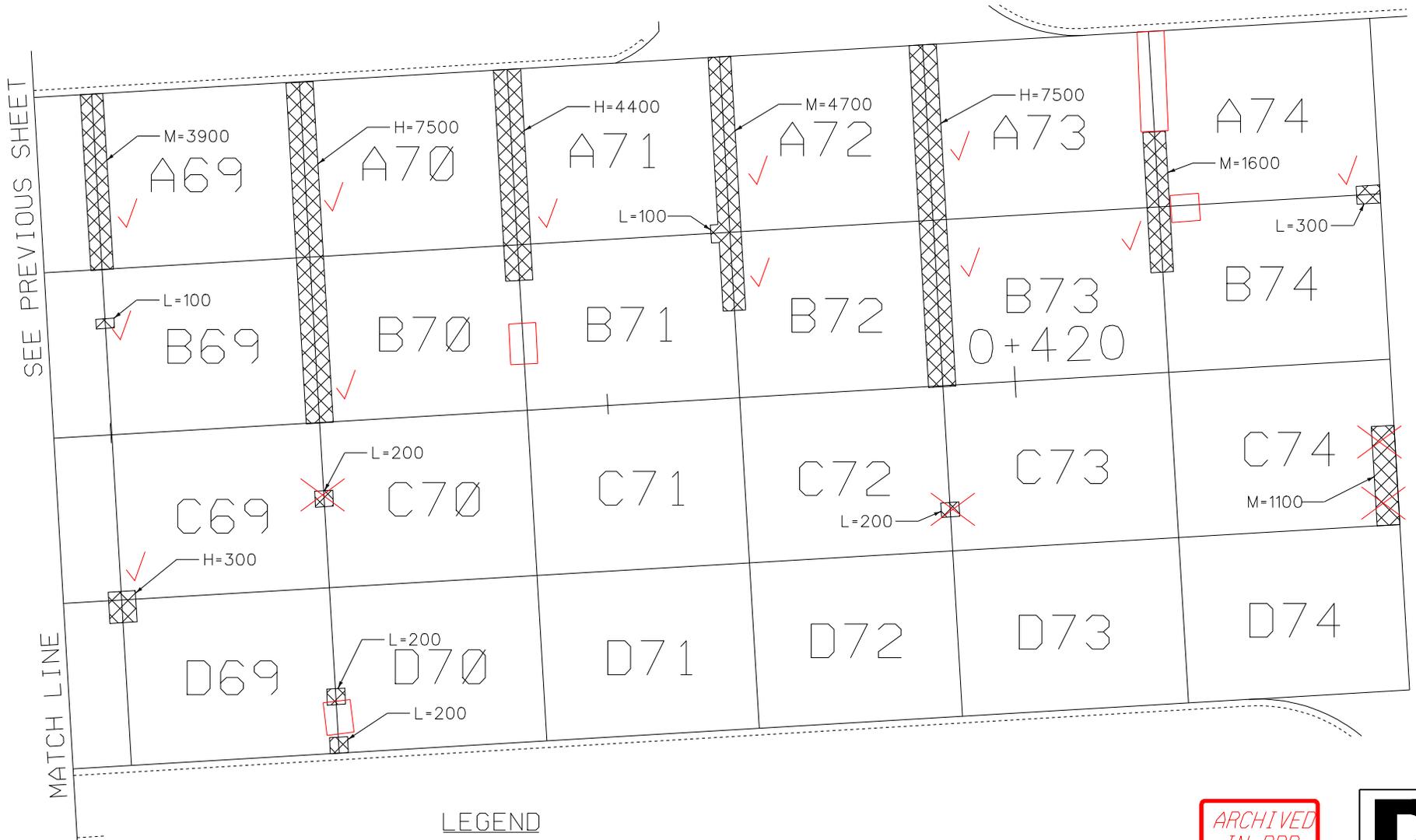
AREAS OF PARTIAL DEPTH REPAIR

NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

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IN DPR



<p style="text-align: center;">PARTIAL DEPTH REPAIR LOCATIONS # 14</p>	<p>PROJECT NAME: WINOOSKI PROJECT NUMBER: STP 2131(1)S</p> <p>FILE NAME: /pave/99d096/pd096.dgn PROJECT LEADER: JLL DESIGNED BY: D-H IPARM FILE NAME: pd096r14.i</p> <p>PLOT DATE: 08-JAN-2003 08 DRAWN BY: D-H CHECKED BY: SHEET 40 OF 51</p>
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LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)



AREAS OF PARTIAL DEPTH REPAIR

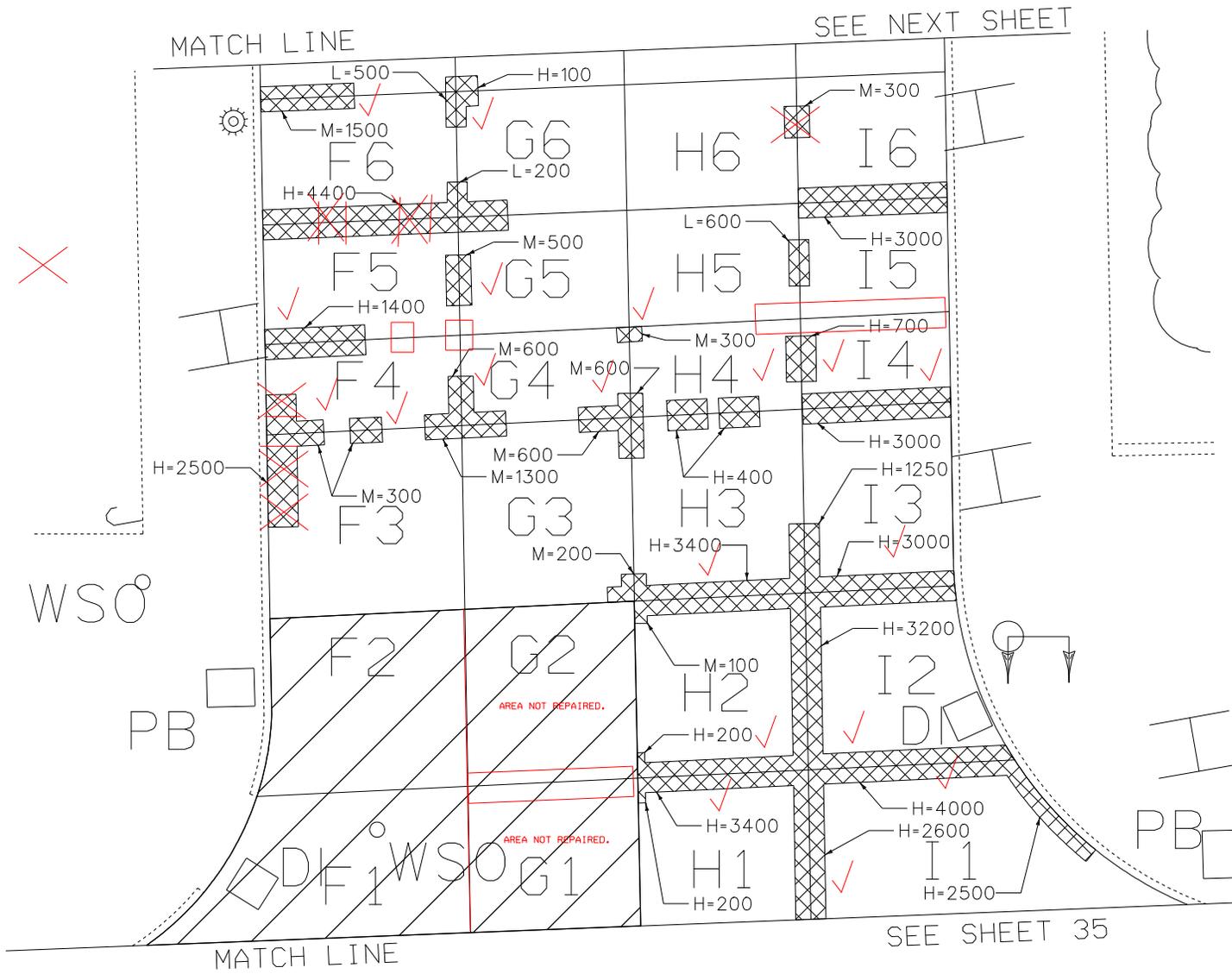
NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

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IN DPR



**PARTIAL
DEPTH
REPAIR
LOCATIONS
15**

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 41 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r15.i	



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)
- AREAS OF PARTIAL DEPTH REPAIR
- AREAS OF FULL DEPTH REPAIR. FOR DETAILS, SEE SHEETS 21-23

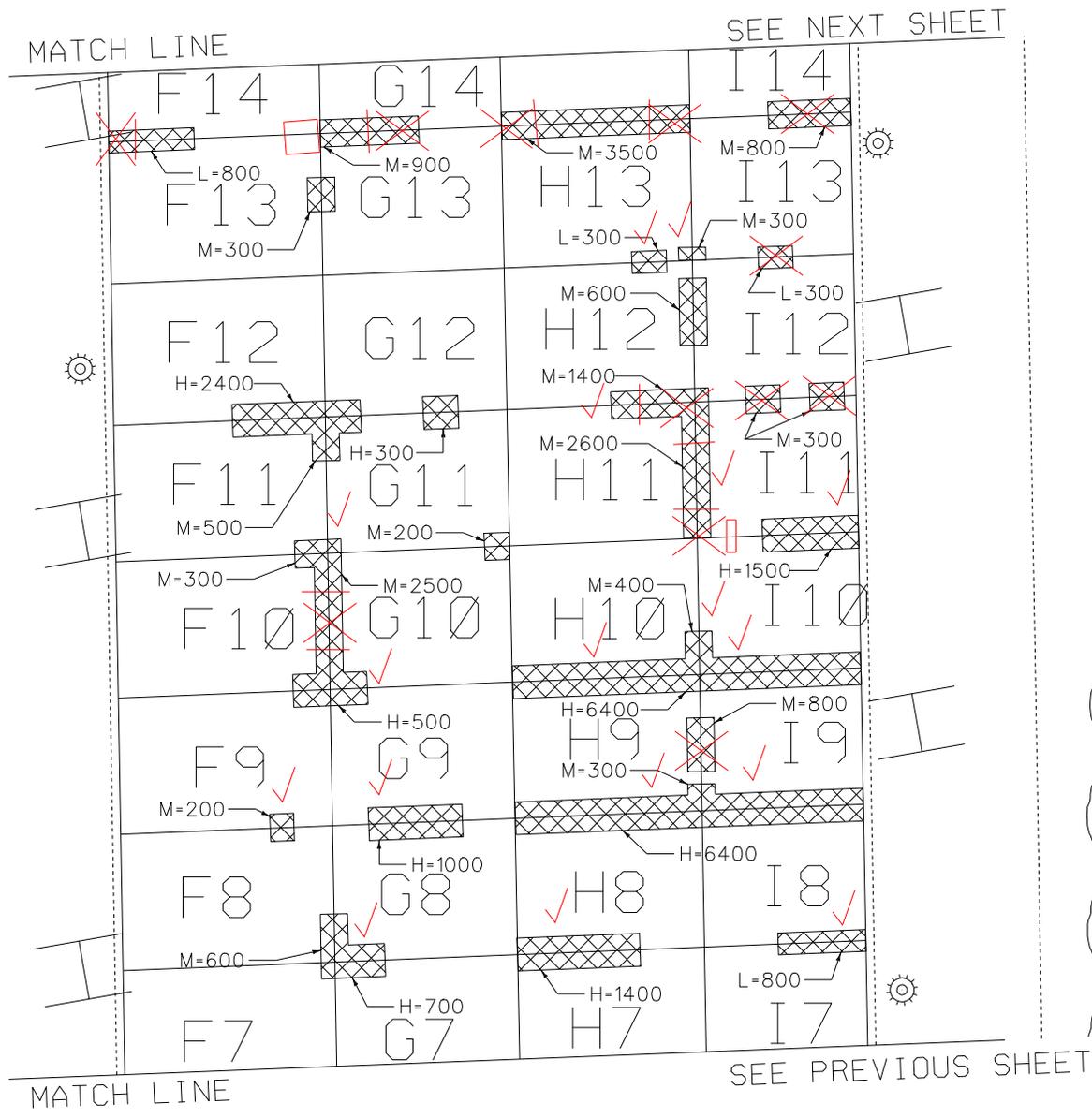
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NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 16

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 42 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r16.i	



LEGEND
 L- LOW SEVERITY (200 mm WIDTH)
 M- MEDIUM SEVERITY (250 mm WIDTH)
 H- HIGH SEVERITY (300 mm WIDTH)
 AREAS OF PARTIAL DEPTH REPAIR

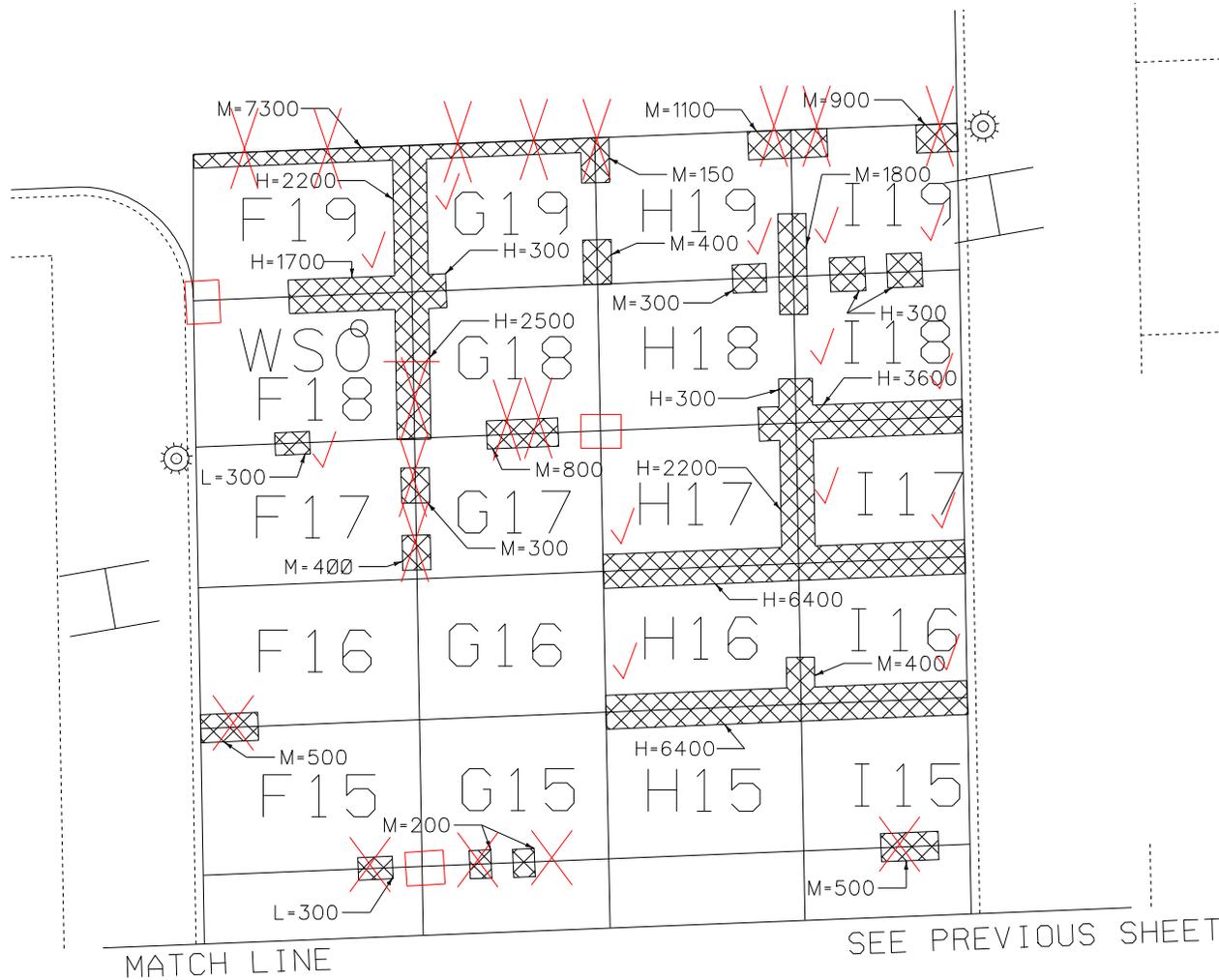
**ARCHIVED
IN DPR**



NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 17

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 43 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096r17.i	



LEGEND

- L= LOW SEVERITY (200 mm WIDTH)
- M= MEDIUM SEVERITY (250 mm WIDTH)
- H= HIGH SEVERITY (300 mm WIDTH)

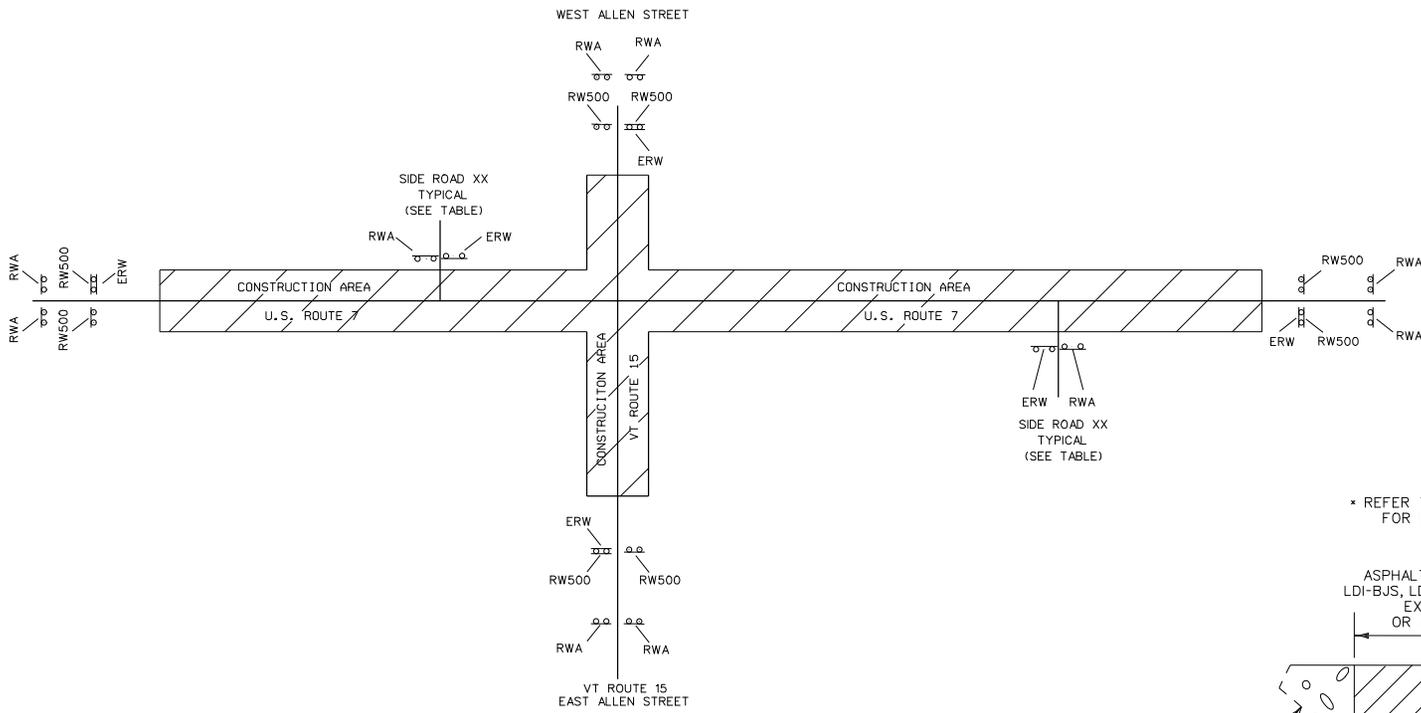
AREAS OF PARTIAL DEPTH REPAIR

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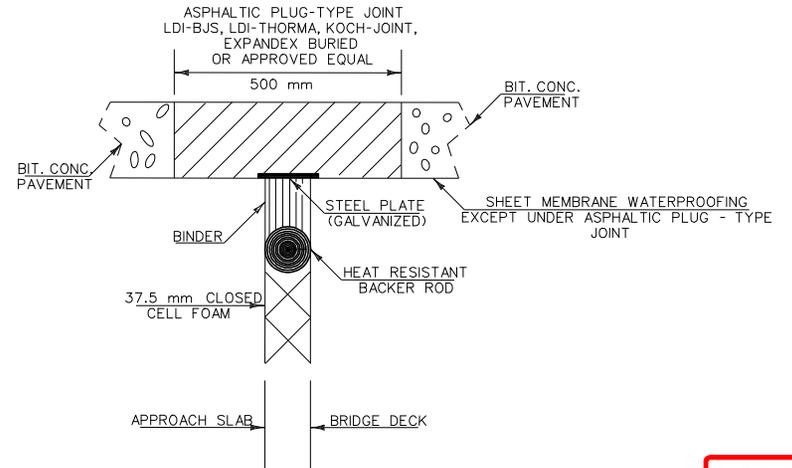


NOTES: 1) ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

PARTIAL DEPTH REPAIR LOCATIONS # 18	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 44 OF 51



* REFER TO SPECIAL PROVISIONS FOR BASIS OF PAYMENT



LEGEND

- ERW - END ROAD WORK
- RW500 - ROAD WORK 500 FT
- RWA - ROAD WORK AHEAD

LOCATION	ERW	RW500	RWA
U. S. ROUTE 7			
BEGIN PROJECT	1	2	2
WEST CANAL STREET	1		1
WEST CENTER STREET (ONE WAY)			1
WEST ALLEN STREET	1	2	2
NEWELLS LANE	1		1
MAPLE STREET	1		1
MANSION STREET	1		1
END PROJECT	1	2	2
VT ROUTE 15			
WEAVER LANE	1		1
BARLOW STREET	1		1
END PROJECT	1	2	2
TOTALS	10	8	15

ASPHALTIC PLUG TYPE JOINT DETAIL

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NOTE:
ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED

CONSTRUCTION APPROACH SIGNING	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 45 OF 51
	DESIGNED BY: D-H	
	IPARM FILE NAME: pd096cas.i	

INTRODUCTION:

UNLESS OTHERWISE NOTED, THE OBJECTIVE OF THIS SEQUENCE IS TO MINIMIZE IMPACT ON THE TRAFFIC FLOW WHILE PROVIDING A SAFE PASSAGE FOR VEHICLES DURING AND AFTER CONSTRUCTION WORK HOURS.

THE FOLLOWING TRAFFIC CONTROL INFORMATION IS INTENDED TO BE A GENERAL OUTLINE FOR HOW THE WORK SHOULD PROCEED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE SPECIFIC DETAILS TO ADDRESS SPECIFIC SITUATIONS. THIS RESPONSIBILITY INCLUDES PROVIDING A PLAN DETAILING THE USE AND PLACEMENT OF SIGNS, CHANNELIZING DEVICES, ARROW PANELS, FLAGGERS AND UNIFORMED TRAFFIC OFFICERS (UTO'S) DURING LANE CLOSURES. IF THE CONTRACTOR DOES NOT WISH TO FOLLOW THIS OUTLINE, HE MAY SUBMIT AN ALTERNATE PROPOSAL TO THE PAVEMENT MANAGEMENT DIVISION VIA THE ENGINEER. THE CONTRACTOR MUST ALLOW AT LEAST 4 WEEKS FOR REVIEW AND APPROVAL OF THE COMPREHENSIVE PLAN AND 2 WEEKS FOR REVIEW AND APPROVAL OF MINOR CHANGES/DETAILS. ALL TRAFFIC CONTROL DETAILS MUST BE DESIGNED AND IMPLEMENTED IN ACCORDANCE WITH THE MUTCD AND VTTRANS STANDARDS E-100M, E-101M, E-102M, E-102AM, E-106M, E-107M, E-107AM, E-108M AND E-110M.

GENERAL:

ALL WORK SHALL BE DONE BETWEEN THE HOURS OF 7:00 AM AND 7:00 PM.

THE CONTRACTOR SHALL BE OUT OF THE ROAD BY 3:00 PM EACH AND EVERY FRIDAY UNLESS GIVEN PERMISSION BY THE RESIDENT ENGINEER.

MAINTAIN AT LEAST THE NUMBER OF LANES OF TRAFFIC AS SHOWN IN THE NOTES SPECIFIC TO EACH TYPE OF WORK AT ALL TIMES.

PARKING SHALL BE ELIMINATED ON U.S. ROUTE 7 (MAIN STREET) AND VT ROUTE 15 (EAST ALLEN STREET) THROUGHOUT THE WORK AREAS, FOR THE DURATION OF THE PROJECT.

A MINIMUM LANE WIDTH OF 3.0 M SHALL BE MAINTAINED.

SIGNALIZED INTERSECTIONS MUST BE CONTROLLED BY UTO'S WHEN LANES ARE NOT IN NORMAL OPERATION.

THE BID PRICE FOR TRAFFIC CONTROL-ITEM 641.10 SHALL INCLUDE ALL APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY & WARNING SIGNS AND POSTS, ALL ADJUSTING, RELOCATING AND REMOVING OF THESE DEVICES SHALL ALSO BE INCLUDED. THE TEMPORARY "NO PARKING" SIGNS & POSTS PROVIDED FOR USE ON THE DETOUR SHALL BE PAID FOR AS ITEMS 675.20 & 675.301 AS DEPICTED ON SHEET 17. THE FOLLOWING ITEMS WILL BE PAID UNDER THEIR SPECIFIC BID PRICES:

1. 630.10 AND .15 UNIFORMED TRAFFIC OFFICERS AND FLAGGERS
2. 641.15 PORTABLE CHANGEABLE MESSAGE SIGN
3. 646.60, .61, AND .66 TEMPORARY PAVEMENT MARKINGS

WHEN COLD PLANED BITUMINOUS PAVEMENT IS OPEN TO TRAFFIC, A "MOTORCYCLES USE CAUTION" SIGN, AS PER VTTRANS STANDARD E-102AM, SHALL BE PROVIDED.

IT IS INTENDED TO HAVE NO LONGITUDINAL DROP-OFFS DURING THE OVERNIGHT HOURS. THEREFORE THE FULL ROADWAY WIDTH SHOULD BE COLD PLANED OR PAVED DURING THE DAILY WORK PERIOD. WHEN NECESSARY, DROP-OFF PROTECTION IN THESE AREAS SHALL CONFORM TO VTTRANS STANDARD E-108M.

MAINTAIN ACCESS TO ALL PROPERTIES AT ALL TIMES FOR EMERGENCY VEHICLES. MAINTAIN ACCESS TO ALL COMMERCIAL AND MUNICIPAL PROPERTIES DURING BUSINESS HOURS. ACCESS TO RESIDENTIAL PROPERTIES MAY BE RESTRICTED FOR A SHORT DURATION (A FEW HOURS) WITH PERMISSION OF THE OWNER. COORDINATE MAJOR WORK ON COMMERCIAL OR MUNICIPAL ACCESSES WITH THE OWNER AT LEAST 1 WEEK PRIOR TO STARTING THE WORK. ALL ACCESSES SHALL ALSO BE KEPT FREE OF WORK AND TRAFFIC CONTROLLED BY UTO'S WHEN REQUIRED.

TRAFFIC SHALL NOT BE CHANGED FROM ONE PHASE TO THE NEXT PHASE UNTIL ALL TEMPORARY MARKINGS, SIGNING AND SIGNAL WORK ARE COMPLETED. ANY CONFLICTING MARKINGS SHALL BE REMOVED.

SIGNALIZED INTERSECTIONS MUST BE CONTROLLED BY UTO'S AT ALL TIMES WHEN THEY ARE NOT IN NORMAL OPERATION EXCEPT DURING NIGHT TIME FLASHING OPERATION.

ALL NON-OPERATING SIGNAL HEADS SHALL BE REMOVED OR COMPLETELY COVERED AS DIRECTED BY THE RESIDENT ENGINEER.

THE CONTRACTOR SHALL MAINTAIN SAFE ACCESS FOR CCTA BUS PASSENGERS AT MARKED BUS STOPS.

SIX (6) PORTABLE MESSAGE BOARDS WILL BE PROVIDED FOR USE AT THE FOLLOWING LOCATIONS:

- I-89 NORTHBOUND - APPROXIMATELY 1/2 MILE SOUTH OF EXIT 14.
- I-89 SOUTHBOUND - APPROXIMATELY 1/2 MILE NORTH OF EXIT 16.
- U.S. ROUTE 7 SOUTHBOUND - NORTH OF THE PROJECT, JUST SOUTH OF THE I-89 EXIT 16 INTERCHANGE.
- VT ROUTE 15 WESTBOUND - WEST OF THE I-89 EXIT 15 INTERCHANGE, EAST OF MANSEAU STREET.
- IN BURLINGTON CITY ON RIVERSIDE AVE. - WEST OF THE NORTH PROSPECT STREET INTERSECTION.
- IN BURLINGTON CITY ON COLCHESTER AVE. - WEST OF THE EAST AVENUE INTERSECTION.

THIS QUANTITY IS PAID FOR AS ITEM 641.15, PORTABLE CHANGEABLE MESSAGE SIGN. THE PLACEMENT OF THESE MESSAGE BOARDS AS WELL AS THEIR MESSAGES WILL BE AS APPROVED BY THE RESIDENT ENGINEER.

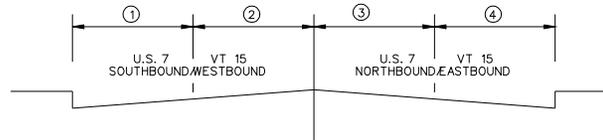
CONCRETE JOINT REPAIR & BITUMINOUS OVERLAY AREAS:

UNLESS OTHERWISE NOTED, MAINTAIN 3 LANES OF TRAFFIC AT ALL TIMES ON U.S. ROUTE 7 WITH 2 SOUTHBOUND LANES DURING THE A.M. HOURS AND 2 NORTHBOUND LANES DURING THE P.M. HOURS. MAINTAIN 3 LANES OF TRAFFIC AT ALL TIMES ON VT ROUTE 15 WITH 2 WESTBOUND LANES DURING THE A.M. HOURS AND 2 EASTBOUND LANES DURING THE P.M. HOURS. THE MINIMUM HOURS OF OPERATION ARE AS FOLLOWS:

A.M. - 7:00 TO 10:00 A.M.
P.M. - 3:00 TO 7:00 P.M.

THE FOLLOWING SEQUENCE FOR THE COLD PLANING, PAVING AND CONCRETE JOINT REPAIR AREAS IS SUGGESTED:

- LANE 1 - 3:00 P.M. TO 7:00 P.M.
- LANE 2 - 12:00 NOON TO 3:00 P.M.
- LANE 3 - 9:30 A.M. TO 12:00 NOON
- LANE 4 - 7:00 A.M. TO 9:30 A.M.



FOR LANE CLOSURE DETAILS, SEE SHEET 47

DIAMOND GRINDING AREAS:

THE DIAMOND GRINDING PROCESS SHALL TAKE PLACE WITHIN A TWO WEEK PERIOD OF TIME WITH MINIMAL INTERRUPTIONS IN THE WORK.

THE DIAMOND GRINDING PROCESS REQUIRES 2 LANES TO BE CLOSED AT A TIME TO ALLOW ROOM FOR ALL THE NECESSARY GRINDING EQUIPMENT AND SLURRY TRUCKS. MAINTAIN AT LEAST 2 LANES OF TRAFFIC AT ALL TIMES ON U.S. ROUTE 7 WITH 1 NORTHBOUND LANE AND 1 SOUTHBOUND LANE FOR USE BY THROUGH TRUCKS AND LOCAL TRAFFIC ONLY. FOR LANE CLOSURE DETAILS, SEE SHEET 48. THESE LANE CLOSURE DETAILS ARE PROVIDED FOR USE IN MAINTAINING THIS TRAFFIC AS DIRECTED BY THE RESIDENT ENGINEER. ALL OTHER VEHICLES WILL BE DIRECTED TO FOLLOW THE PROPOSED DETOUR AS SHOWN ON SHEETS 49 AND 50. SEE THE DETOUR NOTES ON THIS SHEET.

THE CONTRACTOR SHALL COORDINATE THE DIAMOND GRINDING TO ENSURE THAT AT LEAST ONE DRIVE TO THE KEY BANK IS KEPT FREE OF OBSTRUCTIONS AT ALL TIMES. COORDINATION OF DRIVE CLOSURES, DRIVE-UP WINDOW CLOSURES AND TRAFFIC FLOW THROUGH THE KEY BANK PARKING LOT SHALL BE ORGANIZED THROUGH THE PUBLIC RELATIONS REPRESENTATIVE AS DIRECTED BY THE RESIDENT ENGINEER.

TRAFFIC DETOUR



THE TRAFFIC DETOUR SHALL BE USED THROUGHOUT THE ENTIRE DIAMOND GRINDING PROCESS. THE INTENT OF THE DETOUR IS TO ELIMINATE AS MUCH PASSENGER VEHICLE TRAFFIC AS POSSIBLE FROM THE DIAMOND GRINDING AREAS. ALL LARGE (TRACTOR TRAILER) TRUCKS, DELIVERY VEHICLES AND LOCAL TRAFFIC (RESIDENTS) WILL BE ALLOWED TO USE THE LANES WITHIN THE DIAMOND GRINDING AREAS AS SHOWN ON SHEET 48. CHITTENDEN COUNTY TRANSPORTATION AUTHORITY (CCTA) BUSES SHALL BE RE-ROUTED AS NECESSARY TO REDUCE THE PEDESTRIAN TRAFFIC IN THE DIAMOND GRINDING AREAS AS MUCH AS POSSIBLE. THIS RE-ROUTING PROCESS SHALL BE COORDINATED WITH CCTA AND CITY OFFICIALS.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE HIS SCHEDULE IN SUCH A MANNER AS TO GIVE THE CITY AT LEAST TWO WEEKS NOTICE PRIOR TO COMMENCING THE DIAMOND GRINDING PROCESS. ALL COMMUNICATION SHALL INCLUDE EMERGENCY DISPATCH PERSONNEL AS DESCRIBED IN THE SPECIAL PROVISIONS.

WEST ALLEN STREET WILL BE CLOSED TO THROUGH TRAFFIC BETWEEN FOLLET STREET AND WEAVER STREET DURING THE DIAMOND GRINDING PROCESS TO ALLOW A CONTINUOUS LEFT TURN FROM FOLLET STREET ONTO WEST ALLEN STREET FOR NORTHBOUND DETOUR TRAFFIC.

PARKING SHALL BE ELIMINATED AT THE FOLLOWING SIDE STREET LOCATIONS WHILE THE TRAFFIC DETOUR IS IN USE:

- THE EAST SIDE OF FOLLET STREET FROM WEST CANAL STREET TO WEST ALLEN STREET.
- BOTH SIDES OF WEST ALLEN STREET FROM MAIN STREET TO WEAVER STREET.
- THE WEST SIDE OF WEAVER STREET FROM WEST CANAL STREET TO WEST ALLEN STREET, AND FROM MAPLE STREET TO WEST SPRING STREET.
- BOTH SIDES OF WEST SPRING STREET FROM WEAVER STREET TO MAIN STREET.

TEMPORARY "NO PARKING" SIGNS SHALL BE INSTALLED AS SHOWN ON SHEET 50. THE EXACT LOCATIONS OF THESE SIGNS SHALL BE DETERMINED BY THE CITY OF WINDOOSKI AND THE RESIDENT ENGINEER. THESE SIGNS SHALL BECOME THE PROPERTY OF THE CITY OF WINDOOSKI WHEN THEY ARE REMOVED AFTER CLOSING THE PROPOSED DETOUR.

ALL DETOUR SIGNING AS SHOWN ON SHEET 50 SHALL BE INSTALLED ON PERMANENT SIGN POSTS AND COVERED DURING NON-WORKING HOURS IN A MANNER WHICH COMPLETELY OBSCURES THE SIGNS TEXT AS DIRECTED BY THE RESIDENT ENGINEER.

THE TRAFFIC SIGNALS AT THE MAIN STREET/WEST CANAL STREET/LOWER CHAMPLAIN MILL ENTRANCE INTERSECTION AND THE MAIN STREET/EAST SPRING STREET INTERSECTION WILL BE SET ON "FLASH" MODE DURING WORKING HOURS. THE TRAFFIC WILL BE MAINTAINED AT THESE INTERSECTIONS WITH THE USE OF UNIFORMED TRAFFIC OFFICERS (UTO'S) AS DIRECTED BY THE RESIDENT ENGINEER.

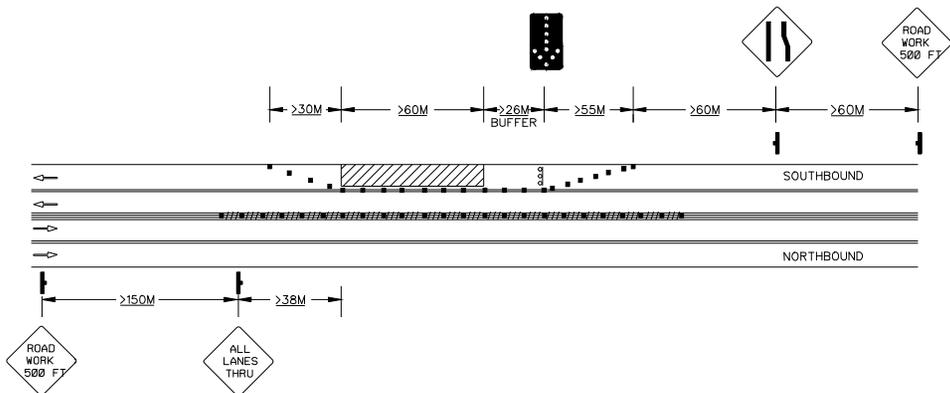
UTO'S OR FLAGGERS SHALL BE USED AT THE WEST ALLEN STREET/FOLLET STREET AND WEST ALLEN STREET/WEAVER STREET INTERSECTIONS AS DIRECTED BY THE RESIDENT ENGINEER. IF DEEMED NECESSARY BY THE RESIDENT ENGINEER, THE EAST SPRING STREET/HOOD STREET/EAST ALLEN STREET INTERSECTION SHALL BE MAINTAINED BY UTO'S OR FLAGGERS TO ENHANCE TRAFFIC FLOW AND TO MINIMIZE THE TRAFFIC IMPACT ON THE EAST ALLEN STREET/MAIN STREET INTERSECTION DURING THE DIAMOND GRINDING PROCESS.

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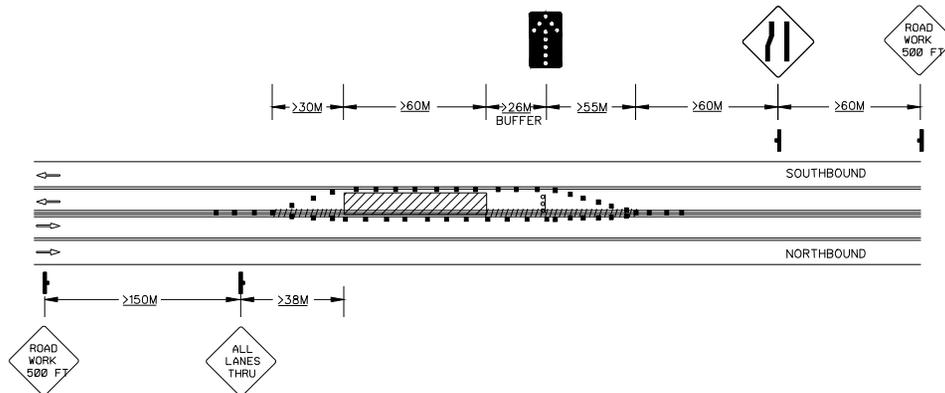


TRAFFIC CONTROL NOTES	PROJECT NAME: WINDOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	IPARM FILE NAME: pd096tcn.i

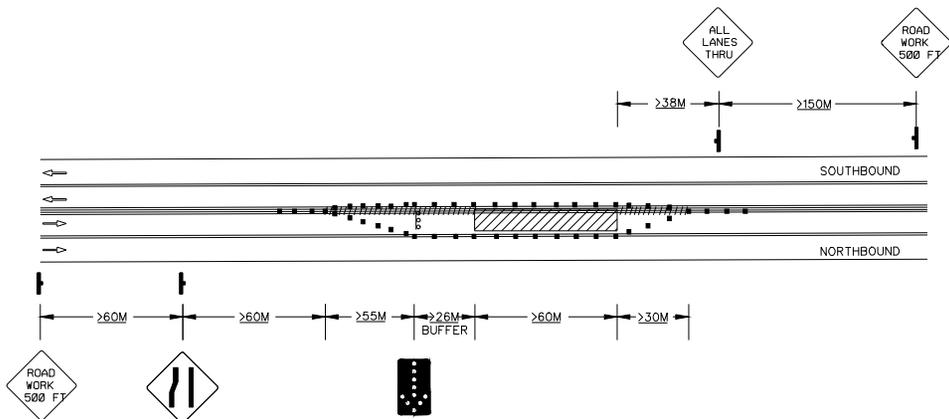
TRAFFIC CONTROL DETAILS FOR USE WITH
CONCRETE JOINT REPAIR AND COLD PLANE/BITUMINOUS OVERLAY AREAS



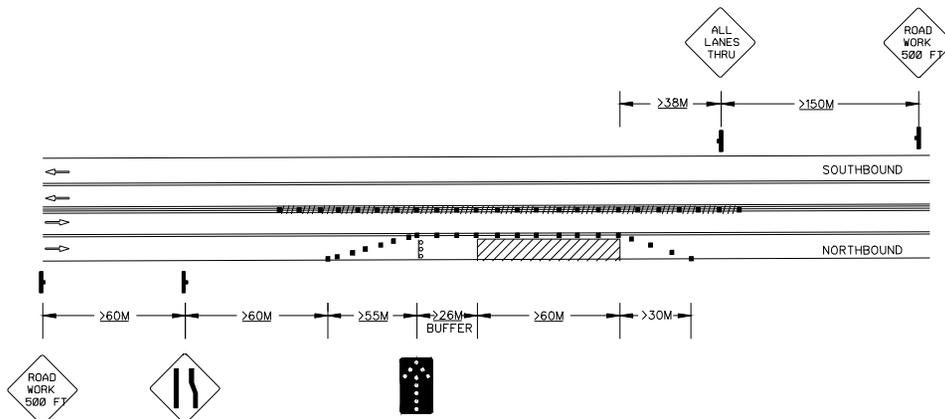
LANE 1 : P.M. LANE CLOSURE DETAIL



LANE 2 : P.M. LANE CLOSURE DETAIL



LANE 3 : A.M. LANE CLOSURE DETAIL



LANE 4 : A.M. LANE CLOSURE DETAIL

LEGEND

- TRAFFIC FLOW
- ALTERNATE TRAFFIC FLOW
- ARROW PANEL
- ARROW PANEL SUPPORT OR TRAILER
- CHANNELIZING DEVICE
- PAVEMENT MARKINGS THAT SHOULD BE REMOVED FOR A LONG TERM PROJECT
- SIGN (SHOWN FACING LEFT)
- WORK SPACE

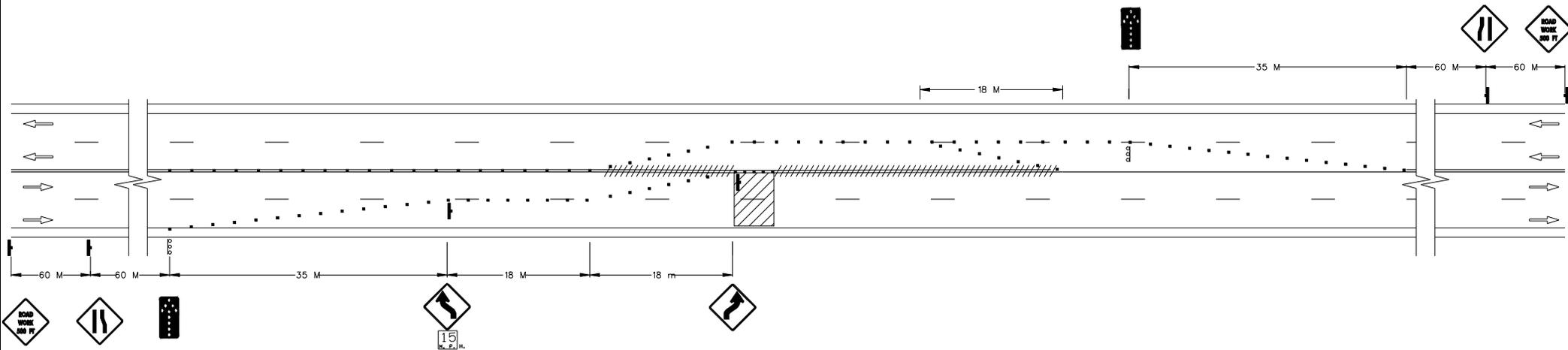
NOTES:

- 1) THE DETAILS SHOWN ON THIS SHEET REFLECT THE TRAVEL DIRECTIONS ON U.S. ROUTE 7. FOR VT ROUTE 15, SUBSTITUTE "EASTBOUND" FOR "NORTHBOUND" AND "WESTBOUND" FOR "SOUTHBOUND" AND THESE SAME DETAILS WILL APPLY TO THIS AREA.
- 2) IN THE AREAS WHERE THERE IS MORE WIDTH THAN THE 4 LANES SHOWN HERE, DUE TO AN ADDITIONAL LANE OR EXISTING PARKING SPACES, THE ADDITIONAL WIDTH MAY BE USED TO FURTHER ENHANCE TRAFFIC FLOW AS DIRECTED BY THE RESIDENT ENGINEER.
- 3) FOR THE DETAILS OF THE "ALL LANES THRU" SIGNS, SEE SHEET 18. FOR OTHER SIGNS SHOWN ON THIS SHEET AND SHEET 48, SEE THE VTTRANS STANDARDS FOR CONSTRUCTION SIGN DETAILS.

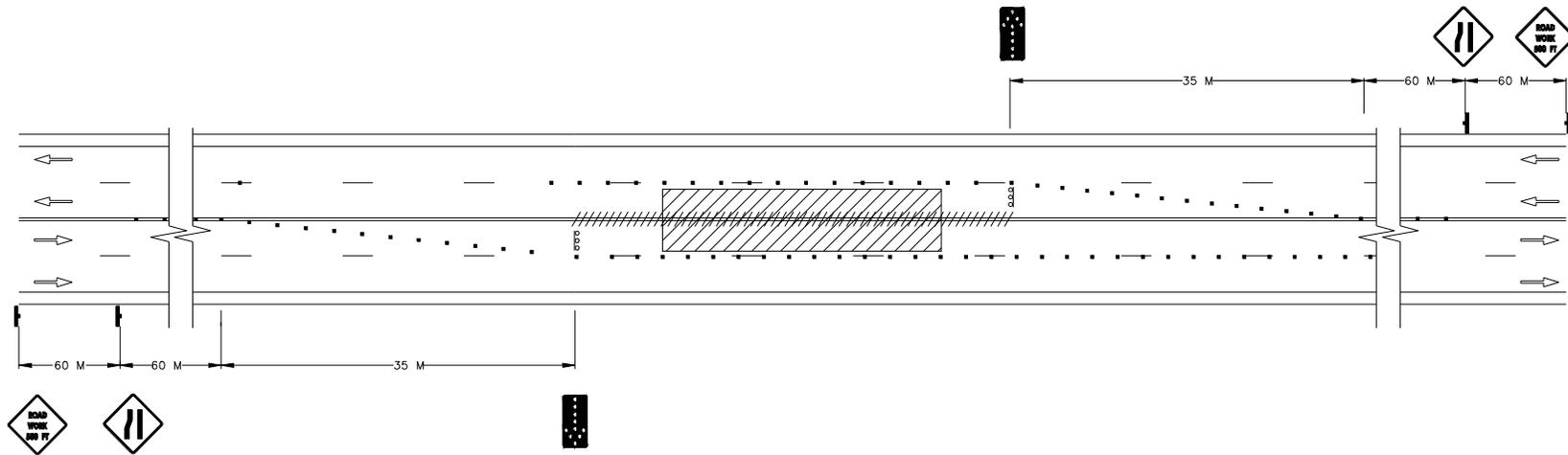
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TRAFFIC CONTROL DETAILS	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 47 OF 51
	DESIGNED BY: D-H	
	IPARM FILE NAME: pd096tcd.i	



DIAMOND GRINDING - HALF ROAD CLOSURE DETAIL



DIAMOND GRINDING - INTERIOR LANE CLOSURE DETAIL

LEGEND

- TRAFFIC FLOW
- ↔ ALTERNATE TRAFFIC FLOW
- ARROW PANEL
- ARROW PANEL SUPPORT OR TRAILER
- CHANNELIZING DEVICE
- ///// PAVEMENT MARKINGS THAT SHOULD BE REMOVED FOR A LONG TERM PROJECT
- ⌋ SIGN (SHOWN FACING LEFT)
- ▨ WORK SPACE

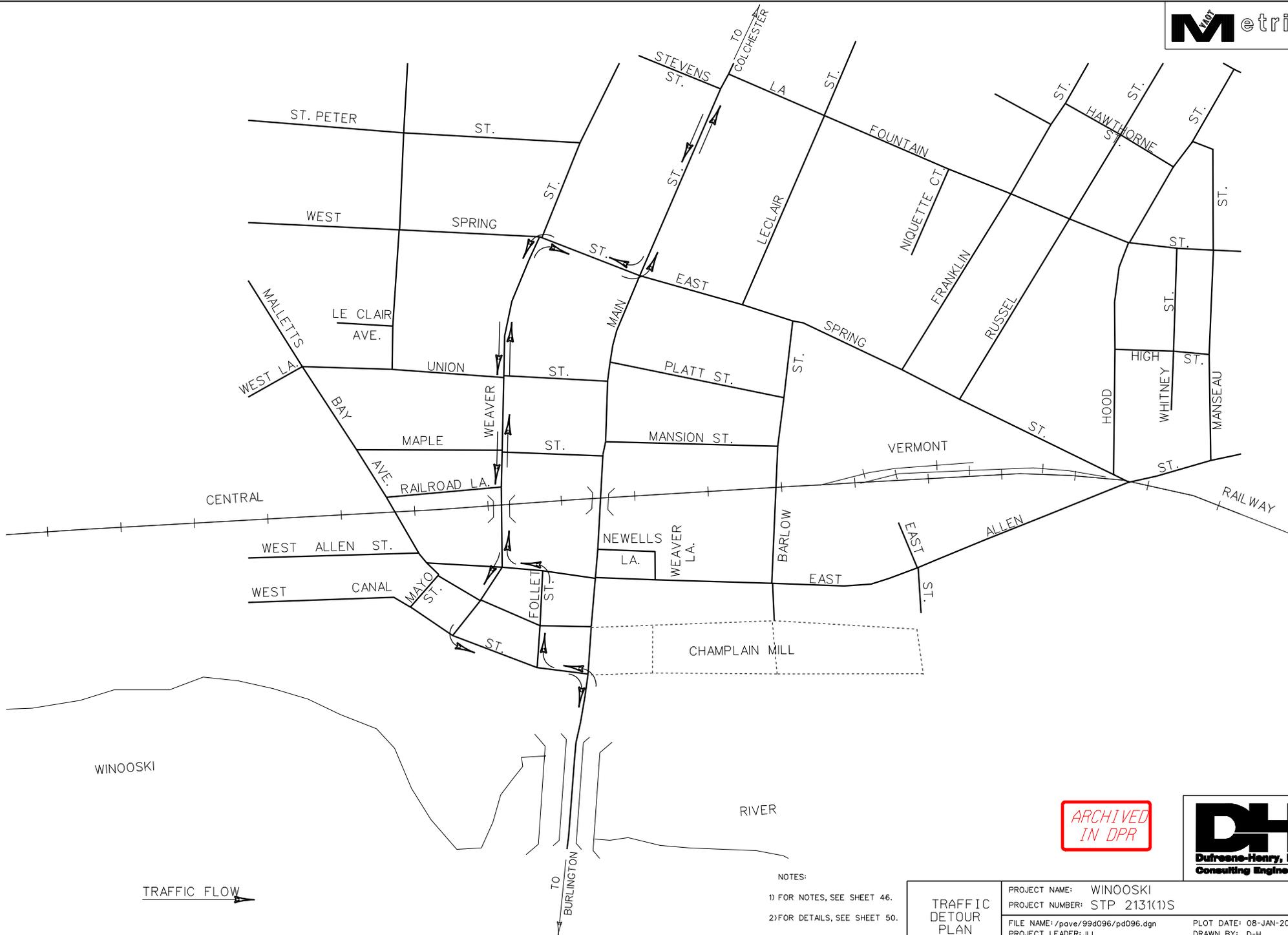
NOTE:

1) IN THE AREAS WHERE THERE IS MORE WIDTH THAN THE 4 LANES SHOWN HERE, DUE TO AN ADDITIONAL LANE OR EXISTING PARKING SPACES, THE ADDITIONAL WIDTH MAY BE USED TO FURTHER ENHANCE TRAFFIC FLOW AS DIRECTED BY THE RESIDENT ENGINEER.

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<p>TRAFFIC CONTROL DETAILS FOR DIAMOND GRINDING AREAS</p>	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL DESIGNED BY: D-H IPARM FILE NAME: pd096dg.i	SHEET 48 OF 51



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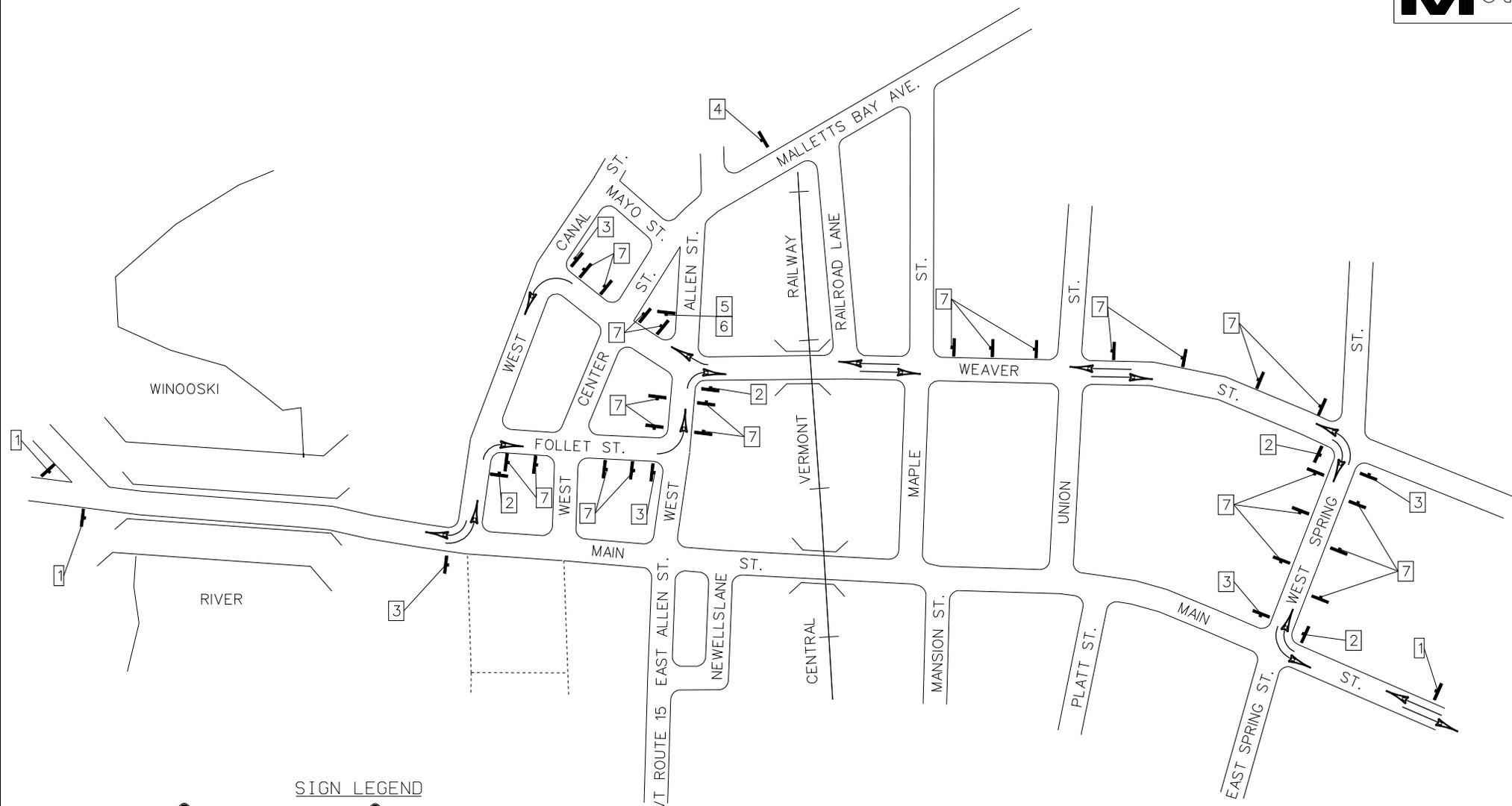


NOTES:
1) FOR NOTES, SEE SHEET 46.
2) FOR DETAILS, SEE SHEET 50.

**TRAFFIC
DETOUR
PLAN
1**

PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
PROJECT LEADER: JLL	SHEET 49 OF 51
DESIGNED BY: D-H	
IPARM FILE NAME: pd096t1.i	

TRAFFIC FLOW →



SIGN LEGEND

1		4		7	
2		5			
3		6			

TRAFFIC FLOW

NOTES:

- 1) FOR NOTES, SEE SHEET 46.
- 2) FOR SIGN DETAILS, SEE SHEET 18.
- 3) THE EXACT LOCATION OF THE TEMPORARY "NO PARKING" SIGNS, SIGN #7, SHALL BE DETERMINED BY THE CITY OF WINOOSKI AND THE RESIDENT ENGINEER.

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TRAFFIC DETOUR PLAN #2	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 50 OF 51
DESIGNED BY: D-H		
IPARM FILE NAME: pd096t02.i		

UTILITY LOCATIONS *
(FOR INFORMATIONAL PURPOSES ONLY)



STATION	POSITION	DESCRIPTION
U.S. ROUTE 7		
0-068.0	LT	TEST CP
0-075.0	LT	TMH
0-081.0	RT	WSO
0-082.9	LT	GSO
0-083.4	LT	GSO
0-097.2	LT	WSO (IN SIDEWALK)
0-105.0	LT	WSO (IN SIDEWALK)
0-111.3	LT	MH - STORM
0-120.5	LT	WSO
0-120.9	LT	GSO (IN SIDEWALK)
0-122.0	RT	GSO
0-122.7	LT	TMH (IN SIDEWALK)
0-126.5	RT	WSO (IN SIDEWALK)
0-127.0	RT	(2) WSO
0-128.2	LT	WSO
0-131.0	C	MH
0-131.8	LT	WSO
0-132.5	LT	TMH
0-135.0	RT	WSO
0-148.9	LT	(2) GSO
0-149.4	LT	WSO (IN SIDEWALK)
0-155.8	LT	WSO (IN SIDEWALK)
0-182.3	RT	WSO (IN SIDEWALK)
0-182.7	RT	WSO
0-183.0	LT	WSO
0-183.3	LT	TMH
0-185.9	LT	WSO
0-189.5	C	WSO
0-190.5	LT	WSO (IN SIDEWALK)
0-192.5	LT	SMH
0-209.0	LT	TEST CP
0-238.5	RT	WSO
0-241.2	LT	TMH
0-248.0	LT	WSO (IN SIDEWALK)
0-249.1	LT	SMH
0-251.8	C	WSO
0-253.0	LT	(2) WSO
0-254.8	LT	TMH
0-254.8	RT	MH
0-273.4	RT	WSO
0-279.0	RT	SMH
0-282.6	LT	SMH
0-287.6	RT	WSO (IN SIDEWALK)
0-299.3	RT	WSO
0-303.2	RT	GSO
0-320.0	RT	WSO
0-321.1	LT	SMH
0-329.6	LT	GSO
0-329.6	LT	TEST CP (IN SIDEWALK)
0-331.5	RT	MH - STORM (IN SIDEWALK)
0-360.6	LT	TEST CP (IN SIDEWALK)
0-361.6	RT	MH (IN SIDEWALK)

STATION	POSITION	DESCRIPTION
U.S. ROUTE 7 (CONT.)		
0-397.0	LT	GSO (IN SIDEWALK)
0-416.5	LT	WSO
0-417.0	LT	SMH
0-431.8	C	SMH
0-434.9	RT	WSO
0-444.9	LT	TEST CP (IN SIDEWALK)
VT ROUTE 15 (EAST ALLEN ST.)		
0-028.2	LT	WSO
0-033.2	LT	WSO (IN SIDEWALK)
0-055.9	RT	SMH
0-058.4	RT	WSO
0-065.0	LT	MH
0-072.5	LT	WSO
0-075.0	LT	WSO (IN SIDEWALK)
0-078.0	LT	WSO
0-079.4	LT	WSO
0-080.8	LT	SMH
0-100.9	RT	SMH
0-140.8	RT	MH
0-146.8	LT	MH
0-152.0	RT	MH
0-176.0	RT	SMH (IN ISLAND)
0-177.0	LT	WSO - (2)
0-177.0	LT	GSO
0-177.0	LT	TEST CP
0-179.2	RT	TMH
0-181.9	LT	WSO
0-183.1	RT	WSO
0-185.2	LT	MH
0-188.4	RT	MH

ADJUST ELEVATION OF VALVE BOX
(ITEM 629.20)

STATION	POSITION	DESCRIPTION
U.S. ROUTE 7		
0-127.0	RT	(2) WSO
0-128.2	LT	WSO
0-131.8	LT	WSO
0-183.0	LT	WSO
0-185.9	LT	WSO
0-299.3	RT	WSO
0-415.5	LT	WSO
0-434.9	RT	WSO
VT ROUTE 15 (EAST ALLEN ST.)		
0-028.2	LT	WSO
0-058.4	RT	WSO
0-072.5	LT	WSO
0-078.0	LT	WSO
0-079.4	LT	WSO
0-177.0	LT	(2) WSO
0-181.9	LT	WSO
0-183.1	RT	WSO

*NOTE: THE UTILITY LOCATIONS AND DESCRIPTIONS SHOWN ABOVE ARE APPROXIMATE AND ARE BASED ON INFORMATION OBTAINED FROM FIELD REVIEWS AND/OR RECORD UTILITY PLANS AS SUPPLIED BY THE MUNICIPALITY. REFER TO THE SPECIAL PROVISIONS FOR CONTACT PERSONS OF PRIVATE UTILITY COMPANIES.

CHANGING ELEVATION OF SEWER MANHOLES

(ITEM 604.42)

STATION	POSITION	DESCRIPTION
U.S. ROUTE 7		
0-417.0	LT	SMH
0-431.8	C	SMH
VT ROUTE 15 (EAST ALLEN ST.)		
0-055.9	RT	SMH
0-080.8	LT	SMH
0-100.9	RT	SMH

STRUCTURE CHANGING ELEVATION LOCATIONS

(ITEM 604.40)

STATION	POSITION	DESCRIPTION
VT ROUTE 15 (EAST ALLEN ST.)		
0-060.2	RT	DJ
0-061.2	LT	DJ
0-065.0	LT	MH
0-068.1	LT	DJ
0-077.1	LT	DJ
0-102.4	RT	DJ
0-140.6	RT	DJ
0-140.6	LT	DJ
0-140.8	RT	MH
0-146.8	LT	MH
0-152.0	RT	MH
0-180.7	LT	DJ
0-185.2	LT	MH
0-188.4	RT	MH
0-189.7	LT	DJ
0-195.4	RT	DJ
0-195.4	LT	DJ

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NOTE:
ALL DIMENSIONS IN MILLIMETERS
EXCEPT WHERE OTHERWISE INDICATED

UTILITY AND STRUCTURE LOCATIONS	PROJECT NAME: WINOOSKI	PLOT DATE: 08-JAN-2003 08
	PROJECT NUMBER: STP 2131(1)S	DRAWN BY: D-H
	FILE NAME: /pave/99d096/pd096.dgn	CHECKED BY:
	PROJECT LEADER: JLL	SHEET 51 OF 51