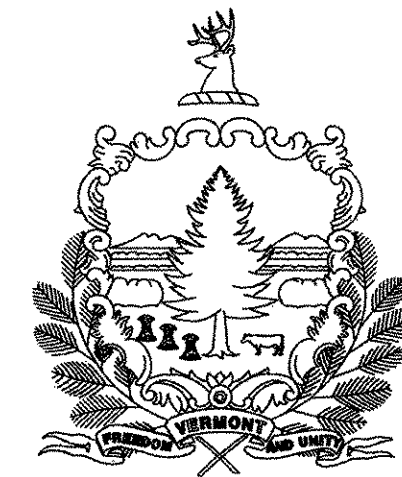


INDEX OF SHEETS  
SEE SHEET 2

# STATE OF VERMONT AGENCY OF TRANSPORTATION

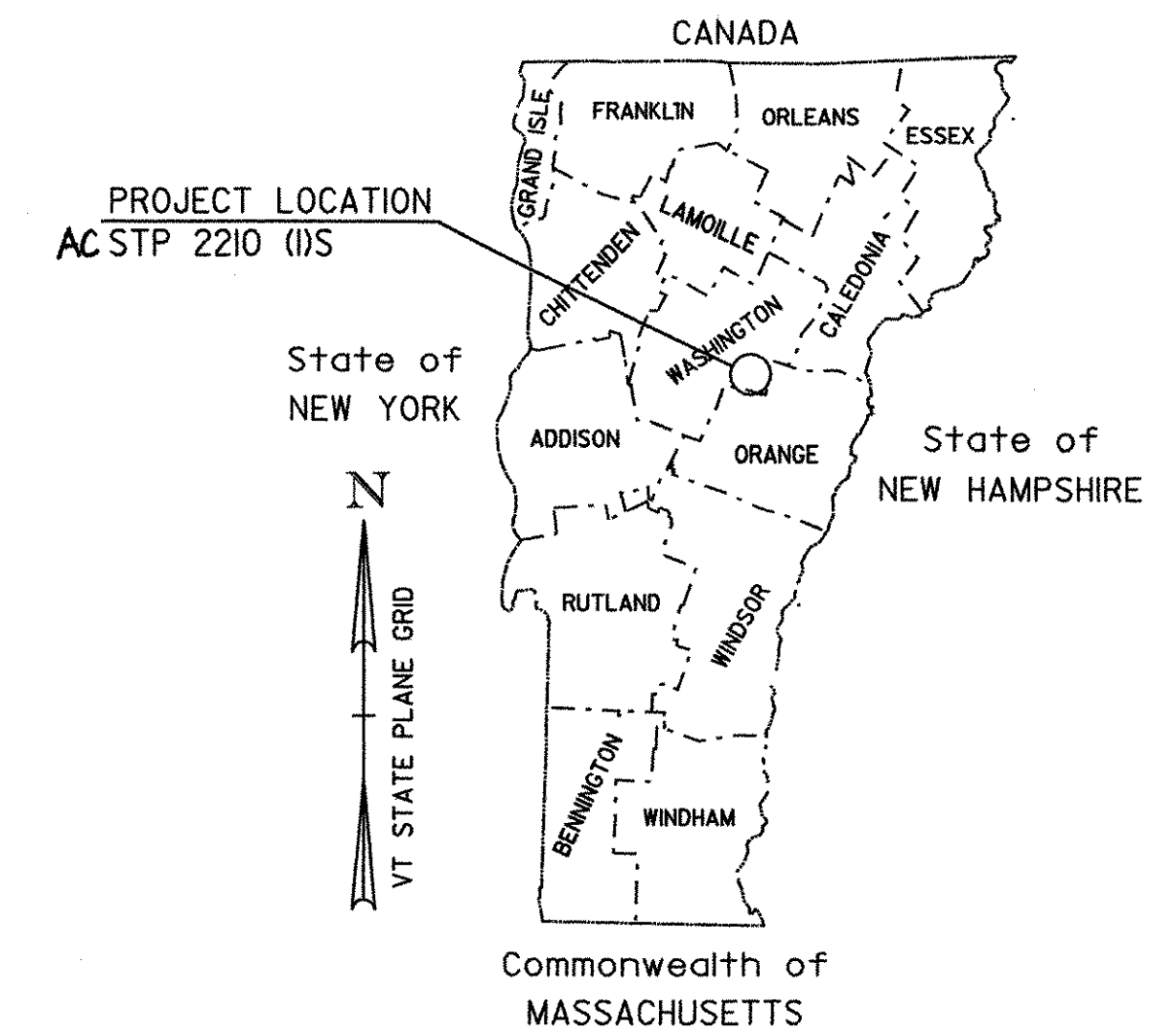


## PROPOSED IMPROVEMENT TOWNS OF WILLIAMSTOWN AND BARRE TOWN COUNTIES OF ORANGE AND WASHINGTON VT ROUTE 14

BEGINNING IN THE TOWN OF WILLIAMSTOWN AT MILE MARKER 3.800 = STA. 200+64.00 AND EXTENDING NORTHERLY ALONG VT ROUTE 14 FOR A DISTANCE OF 25,038.22 FT (4.742 MILES) TO AN ENDING POINT IN THE TOWN OF BARRE TOWN AT MILE MARKER 1.712 = STA. 90+40.00

STATION TO STATION DATA	LENGTH (FEET)	LENGTH (MILES)
TOWN OF WILLIAMSTOWN STA. 200+64.00 TO STA. 360+62.22 (MM 3.800 TO MM 6.830)	15,998.22	3.030
TOWN OF BARRE TOWN STA. 0+00.00 TO STA. 90+40.00 (MM 0.000 TO MM 1.712)	9,040.00	1.712
TOTAL ROADWAY LENGTH =	25,038.22	4.742
TOTAL PROJECT LENGTH =	25,038.22	4.742

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES COLD PLANING AND RESURFACING OF EXISTING HIGHWAY WITH A COMBINATION OF LEVELING AND WEARING COURSES, NEW PAVEMENT MARKINGS, GUARDRAIL INSTALLATION, DRAINAGE IMPROVEMENTS AND INCIDENTAL ITEMS.



### TRAFFIC DATA

	ADT			DHV		CUM. ESALS (2008-2018)	CUM. ESALS (2008-2028)
	2008	2018	2028	2008	2018		
BEGINNING OF PROJECT TO CONSTRUCTION HILL RD. (MM 3.800 WILLIAMSTOWN - MM 4.577 WILLIAMSTOWN)	4900	5500	6100	550	620	788,000	2,055,000
CONSTRUCTION HILL RD. TO HOLDEN RD. (MM 4.577 WILLIAMSTOWN - MM 0.881 BARRE TOWN)	4300	4800	5300	490	540	938,000	2,210,000
HOLDEN RD. TO END OF PROJECT (MM 0.881 BARRE TOWN - MM 1.712 BARRE TOWN)	8000	8300	8700	830	860	1,320,000	3,308,000

### RECORD PLANS

CONTRACTOR: PIKE INDUSTRIES, INC. - BERLIN, VT  
RESIDENT ENGINEER: CARL FIELDER  
CONSTRUCTION BEGAN: JUNE 19, 2008  
CONSTRUCTION COMPLETE: OCTOBER 17, 2008  
RECORD PLANS BY: CARL FIELDER & C. PIERCE

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.

BY *Carl Fielder* RESIDENT ENGINEER  
DATE *4/21/09*

NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.



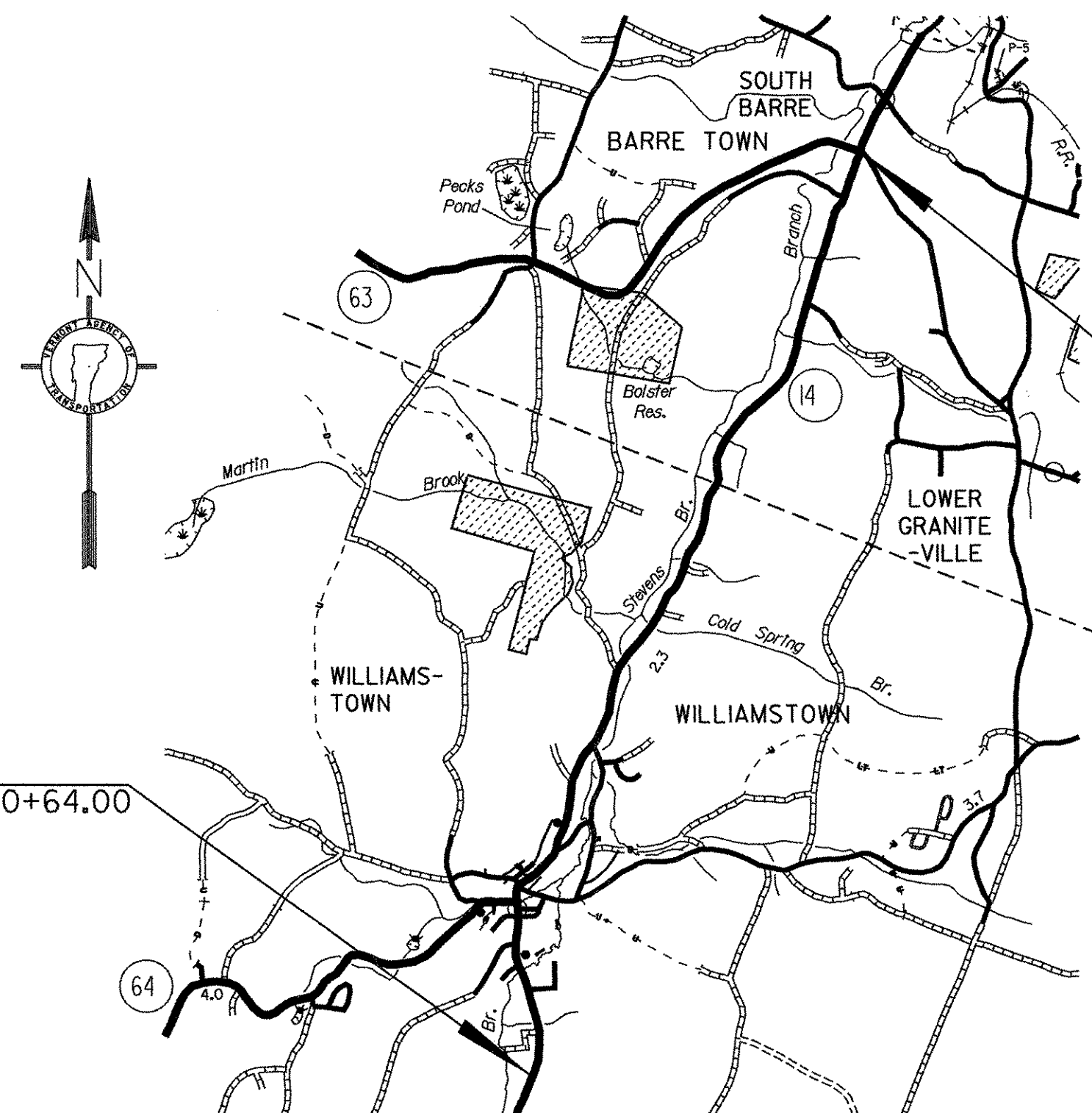
engineering planning management development

### DATUM

VERTICAL N/A  
HORIZONTAL N/A

### CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	



END STP 2210 (1)S  
MM 1.712 = STA. 90+40.00

BEGIN STP 2210 (1)S  
MM 3.800 = STA. 200+64.00

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

BITUMINOUS CONCRETE PAVEMENT SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE / DESIGN LIFE ESAL	1,654,000
DESIGN NUMBER OF GYRATIONS	50
PERFORMANCE GRADE ASPHALT BINDER	PG 58-34

DIRECTOR OF PROGRAM DEVELOPMENT

APPROVED *Ted Domey* DATE *10-11-07*

PROJECT MANAGER : TED DOMEY

PROJECT NAME : WILLIAMSTOWN-BARRE TOWN

PROJECT NUMBER : AC STP 2210 (1)S

SHEET 1 OF 32 SHEETS

PLOTTED: 11-OCT-2007

INDEX OF SHEETS

1	TITLE SHEET
2	INDEX OF SHEETS
3-4	PROJECT TYPICAL SHEETS
5	GUARDRAIL DETAIL SHEET
6	BRIDGE DETAIL SHEET
7	BRIDGE LOCATION AND DETAIL SHEET
8-9	QUANTITY SHEETS
10-11	ITEM DETAIL SUMMARY SHEETS
12	DITCH CLEANING DETAIL SHEET
13-23	PROJECT LAYOUT SHEETS
24	INTERSECTION DETAIL SHEET
25	LOOP DETECTOR LAYOUT SHEET
26-31	TRAFFIC SIGN SUMMARY SHEETS
32	CONSTRUCTION APPROACH SIGNING SHEET

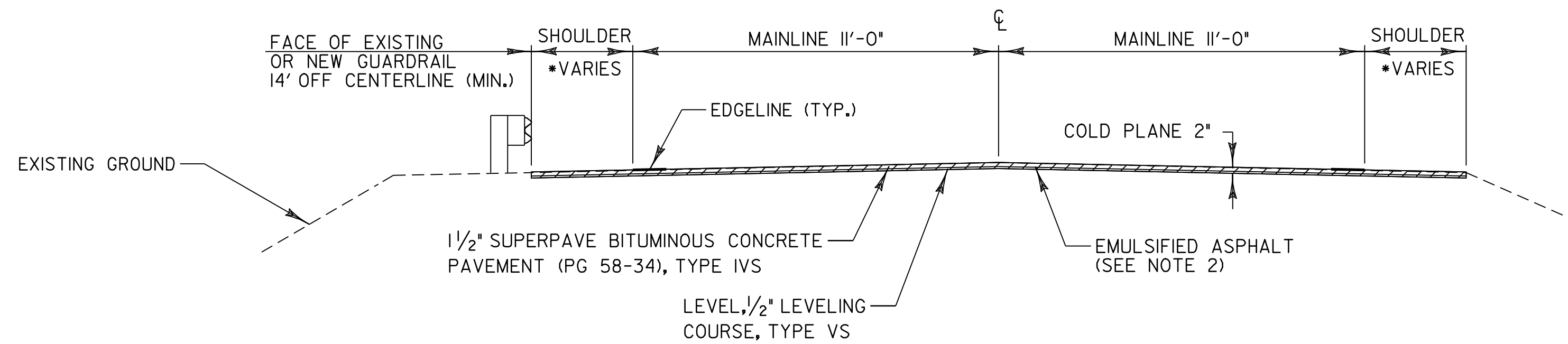
VAOT STANDARDS

D-3	TREATED GUTTERS	06/01/94
D-6	REINFORCED CONCRETE DROP INLET W/ GRATE FOR USE IN DITCHES	06/01/94
D-8	REINFORCED CONCRETE DROP INLET WITH PRECAST COVER REINFORCED CONCRETE DROP INLET WITH GRATE (BOTTOM SECTION)	01/03/00
D-9	REINFORCED CONCRETE DROP INLET WITH VERTICAL CURB REINFORCED CONCRETE DROP INLET THROAT ADAPTER	06/01/94
D-10	TOP FOR A DROP INLET WITH BITUMINOUS CONCRETE CURB TOP FOR A DROP INLET GRANITE SLOPE EDGING	06/01/94
D-15	PRECAST REINFORCED CONCRETE CATCH BASIN W/ CAST IRON GRATE PRECAST REINFORCED MAN HOLE W/ CAST IRON GRATE CAST IRON GRATE WITH FRAME, TYPE D CAST IRON GRATE WITH FRAME, TYPE E	06/01/94
D-30	UNDERDRAIN CONSTRUCTION DETAILS	08/13/07
D-33	REINFORCED CONCRETE STRAIGHT HEADWALL	03/12/07
D-34	REINFORCED CONCRETE CRADLE HEADWALL	03/12/07
E-100	CONSTRUCTION APPROACH SIGNS	01/02/04
E-100A	SIDE ROAD CONSTRUCTION APPROACH SIGNS	01/02/04
E-101	CONSTRUCTION SIGN DETAILS	05/30/03
E-102	CONSTRUCTION SIGN DETAILS	06/30/03
E-102A	CONSTRUCTION SIGN DETAILS	05/01/04
E-103	MAINLINE TRAFFIC CONTROL DIVIDED HIGHWAY ONE LANE CLOSED	03/01/04
E-106	TRAFFIC CONTROL - MISCELLANEOUS DETAILS	03/01/04
E-107	DELINEATION, BARRICADES AND DETOURS FOR CONSTRUCTION AREAS	06/30/03
E-110	MAJOR MAINTENANCE OPERATION LANE CLOSURE	08/08/95
E-111	MINOR MAINTENANCE OPERATION	03/11/97
E-121	STANDARD SIGN PLACEMENT - CONVENTIONAL ROAD	08/08/95
E-138	MILEMARKER DETAILS - STATE & TOWN HIGHWAYS	05/30/03
E-141	REGULATORY SIGN DETAILS	09/20/95
E-143	REGULATORY SIGN DETAILS	06/15/04
E-145A	REGULATORY SIGN DETAILS LANE USE CONTROL SIGNS TWO AND FOUR LANE APPROACHES	12/23/94
E-152	WARNING SIGN DETAILS	05/01/04
E-162	TUBULAR ALUMINUM SIGN POST	05/20/99
E-164	SQUARE STEEL SIGN POST	05/20/99
E-172	VEHICLE DETECTOR LOOP DETAILS	08/09/95
E-191	PAVEMENT MARKING DETAILS	02/01/99
E-192	PAVEMENT MARKING DETAILS	10/12/00
E-193	PAVEMENT MARKING DETAILS	08/18/95
G-1	STEEL BEAM GUARDRAIL WITH STEEL POSTS	01/03/00
G-1D	STEEL BEAM GUARDRAIL APPROACH END TERMINAL STEEL BEAM GUARDRAIL TRAILING END TERMINAL ANCHOR FOR STEEL BEAM GUARDRAIL STEEL BEAM MEDIAN BARRIER	01/03/00
G-4	YIELDING MARKER POSTS	06/01/94
G-19	GENERIC PLANS FOR GUARDRAIL END TERMINALS	11/15/02

**INDEX  
OF  
SHEETS**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S

FILE NAME: p99cl72.dgn	PLOT DATE: 29-APR-2009 13:3
PROJECT LEADER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD
PLOT FILE: p99cl721s.I	SHEET 2 OF 32



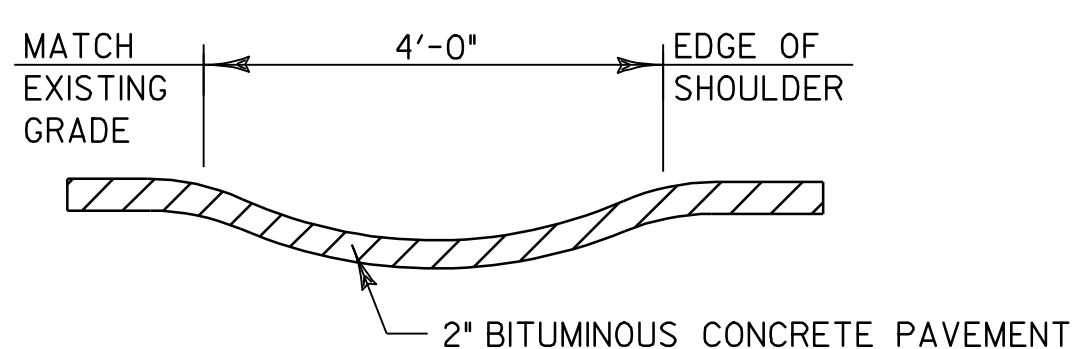
**COLD PLANE AND OVERLAY TYPICAL SECTION**

WILLIAMSTOWN  
 STA. 200+64.00 TO 360+62.22  
 BARRE  
 STA. 0+00.00 TO 90+40.00

\* SEE PROJECT PAVING LIMITS  
 TABLE, SHEET 4 OF 32

**NOTES:**

1. THE WEARING COURSE SHALL BE TYPE IVS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE VS SUPERPAVE BITUMINOUS CONCRETE PAVEMENT. LEVELING HAS BEEN INCLUDED TO RESHAPE THE ROADWAY PRIOR TO PAVING THE TOP COURSE OF PAVEMENT. ALL ASPHALT CEMENT USED IN THE SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 58-34.
2. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, ON ALL COLD PLANED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT AT A RATE OF 0.025 GAL / SY OR AS DIRECTED BY THE RESIDENT ENGINEER.
3. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = +/- 1/4 INCH (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
4. COLD PLANING SHALL BE COMPLETED ACCORDING TO THE TYPICAL OR AS OTHERWISE NOTED ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE BEGINNING AND END OF THE PROJECT, ALL SIDE ROAD APPROACHES AS DENOTED ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
5. ALL DRIVES SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER. ANY AND ALL REQUIRED EXCAVATION IN DRIVE AREAS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER THE APPLICABLE RENTAL ITEMS). IF REQUIRED, A NEW DRIVEWAY SUBBASE SHALL BE CONSTRUCTED AND WILL BE PAID FOR UNDER ITEM 900.680, SPECIAL PROVISION (SUBBASE, RAP). THE NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 490.30, \*SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34)\*.
6. GRAVEL PULL-OFFS SHALL RECEIVE A 5 FOOT PAVED APRON AS DIRECTED BY THE ENGINEER. ITEM 402.I3, \*AGGREGATE SHOULDERS, RAP\* SHALL BE ADDED TO SHIM THE REMAINING GRAVEL SURFACE. ANY AND ALL REQUIRED EXCAVATION IN PULL-OFF AREAS SHALL BE AS SHOWN ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER THE APPLICABLE RENTAL ITEMS), THE NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 490.30, \*SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34)\*.
7. THREE FEET OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 6 FOOT POSTS. IF THIS CANNOT BE OBTAINED THEN 8 FOOT POSTS SHALL BE USED. THESE ITEMS SHALL BE PAID UNDER ITEM 62I.20 \*STEEL BEAM GUARDRAIL, GALVANIZED\*, ITEM 62I.205 \*STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS\*, AND ITEM 62I.2I \*HD STEEL BEAM GUARDRAIL, GALVANIZED\*.
8. IN AREAS WHERE A UTILITY POLE OR SIMILAR OBJECT IS LOCATED WITHIN 4 FEET BEHIND GUARDRAIL, A REDUCED POST SPACING SHALL BE USED. THE REDUCED SPACING IS DETAILED ON PROJECT TYPICAL SHEET 2.
9. A QUANTITY FOR ITEM 604.4I2, \*REHABING DI, CB OR MH CLASS I\* HAS BEEN INCLUDED IN THE QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER. ALL D.I.'S SHALL BE RAISED OR REHABILITATED SUCH THAT THE NEW GRATE ELEVATION IS LEVEL WITH THE SURROUNDING TERRAIN.
10. ALL EDGES OF PAVEMENT SHALL BE BACKED UP FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.I3, \*AGGREGATE SHOULDERS, RAP\*.
11. ITEM 616.47, \*BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS\* WILL BE PAID ONLY WHERE SPECIFIED IN THE PLANS OR IN OTHER LOCATIONS AS DIRECTED BY THE RESIDENT ENGINEER. ALL OTHER BITUMINOUS CONCRETE PAVEMENT WORK, WHICH COULD INVOLVE SOME HAND-WORK (SUCH AS DRIVE AND SIDEROAD APPROACHES AND AROUND DRAINAGE AND UTILITY STRUCTURES) SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR ITEM 490.30, \*SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34)\*.
12. ESTIMATED QUANTITIES OF ITEM 608.25, \*ALL PURPOSE EXCAVATOR RENTAL, TYPE I\*, 608.37, \*TRUCK RENTAL\*, AND 608.40, \*LOADER RENTAL, TYPE I\* HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL END SECTIONS WITH EXCAVATED DITCHING MATERIAL. AN ESTIMATED QUANTITY OF 203.30, \*EARTH BORROW\* HAS BEEN INCLUDED IN THE CASE THAT THE DITCHING MATERIAL IS NOT SUITABLE TO USE IN THE GUARDRAIL END SECTIONS. 25 CY OF ITEM 203.30, \*EARTH BORROW\* HAS BEEN ESTIMATED FOR EACH NEW GUARDRAIL END SECTION. GUARDRAIL END SECTIONS SHALL BE CAPPED WITH AN ESTIMATED 3 INCH DEPTH OF ITEM 402.I3, \*AGGREGATE SHOULDERS, RAP\* UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF ITEM 402.I3, \*AGGREGATE SHOULDERS, RAP\* FOR EACH NEW GUARDRAIL END SECTION. ITEM 653.20, \*TEMPORARY EROSION MATTING\* SHALL BE PLACED ON SLOPES GREATER THAN I:6 CREATED BY THE GUARDRAIL END SECTIONS. THE QUANTITIES INCLUDED REFLECT 25 SY OF ITEM 653.20, \*TEMPORARY EROSION MATTING\* FOR EACH NEW GUARDRAIL END SECTION. PRIOR TO THE PLACEMENT OF TEMPORARY EROSION MATTING, THE AREA SHALL BE TOPSOILED, SEED, AND MULCHED USING ITEM 65I.35, \*TOPSOIL\*, ITEM 65I.I5, \*SEED\*, AND ITEM 65I.25, \*HAY MULCH\*.
13. YIELDING MARKER POSTS ARE TO BE USED TO REPLACE NON-YIELDING MARKER POSTS AT PIPE INLETS AND OUTLETS ONLY.
14. STEEL BEAM GUARDRAIL WITH STEEL POSTS SHALL BE USED ON THIS PROJECT.
15. SEE SHEET 7 OF 32 FOR THINNING AND TRIMMING FOR SIGNS DETAIL AND LOCATIONS WHERE IT IS NEEDED.

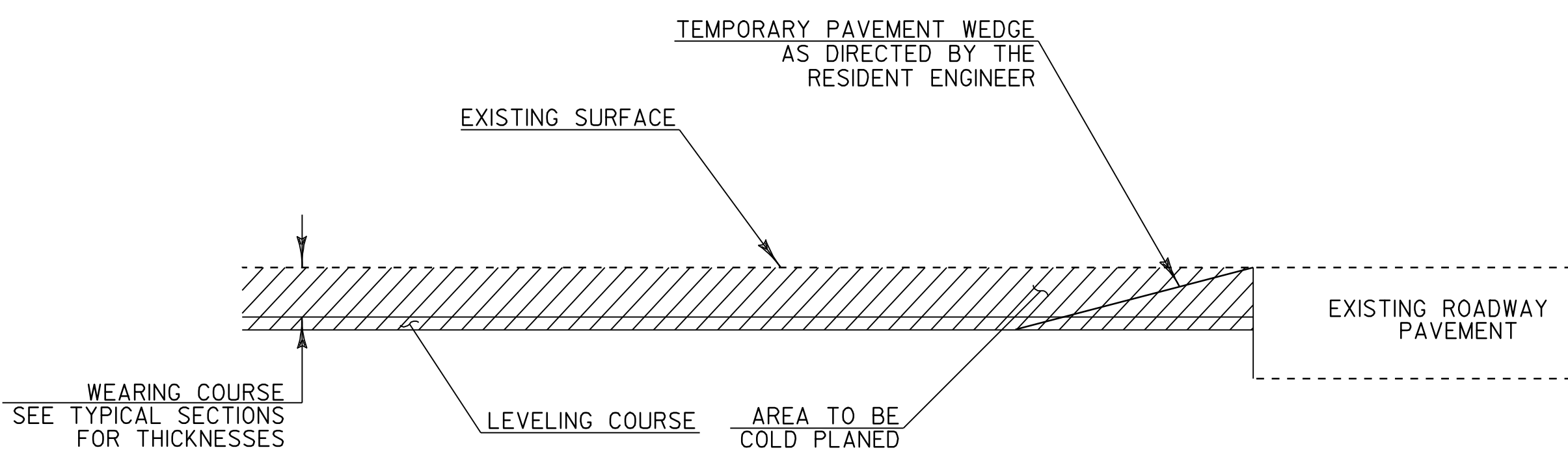


**BITUMINOUS CONCRETE GUTTER DETAIL**

LOCATIONS  
 AS INDICATED ON PLANS

**NOTES:**

1. EXISTING BITUMINOUS CONCRETE GUTTERS SHALL BE REMOVED WHERE INDICATED ON THE PLANS.
2. THE BED UPON WHICH THE BITUMINOUS CONCRETE GUTTER WILL BE PLACED SHALL BE COMPACTED TO THE LINE, GRADE AND SLOPE AS DIRECTED BY THE RESIDENT ENGINEER.
3. THE BITUMINOUS CONCRETE PAVEMENT SHALL BE LAID IN TWO COURSES, EACH ROLLED WITH A 150 POUND ROLLER.
4. THIS WORK IS TO BE PAID FOR UNDER ITEM 616.47, \*BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS\*. NOT INCLUDED IN THIS WORK IS ANY ADDITIONAL SUBBASE MATERIAL.



**APPROACH AREA DETAIL (BEGIN AND END PROJECT)**

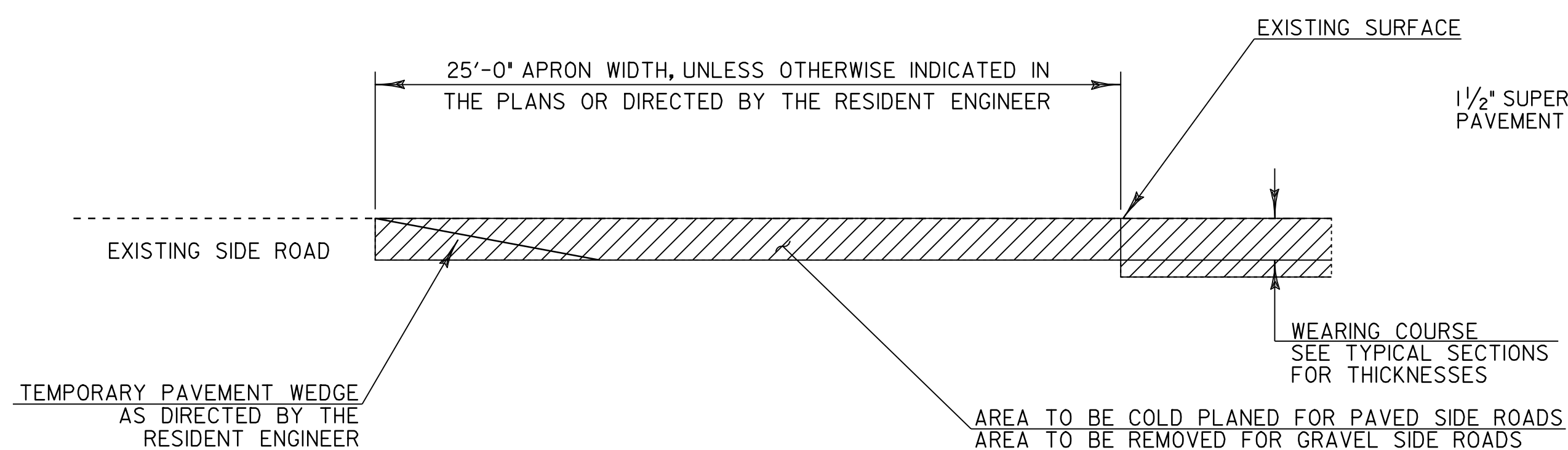
WILLIAMSTOWN  
 STA. 200+64.00  
 BARRE  
 STA. 90+40.00

**SEEDING FORMULA  
 RURAL AREAS**

% WT.	LBS./A.	NAME	PUR %	GERM %
37.5	22.5	CREeping RED FESCUE	98	85
37.5	22.5	TALL FESCUE	95	90
5.0	3.0	RED TOP	95	90
15.0	9.0	BIRDSFOOT TREFOLI	98	85
5.0	3.0	ANNUAL RYEGRASS	95	85
100.0	60.0			

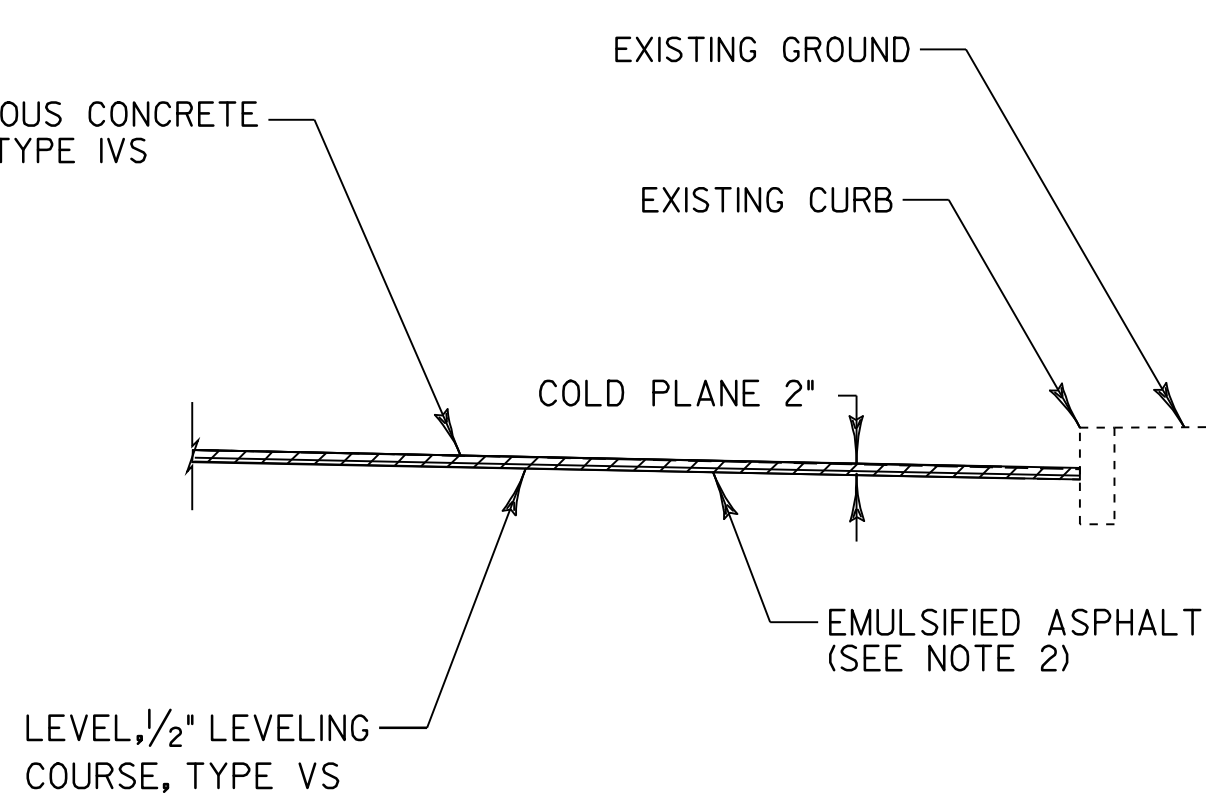
**GENERAL NOTES**

- SEED MIXTURE: SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS SEED.
- SEED: TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE ENGINEER.
- FERTILIZER: FORMULA 10-20-10, TO BE USED WITH SEED, APPLIED AT THE RATE OF 500 LBS./ACRE. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA).
- AGRICULTURAL LIMESTONE: TO BE APPLIED AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.
- HAY MULCH: TO BE PLACED ON EARTH SLOPES AT THE RATE OF 2 TONS/ACRE, OR AS DIRECTED BY THE ENGINEER.
- TOPSOIL: TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE ENGINEER.



**TRANSITION AREA FOR SIDE ROADS**

SEE LAYOUT SHEETS FOR LOCATIONS  
 OF ALL SIDE ROADS



**CURB TYPICAL SECTION**

**PROJECT  
 TYPICAL  
 SHEET 1**

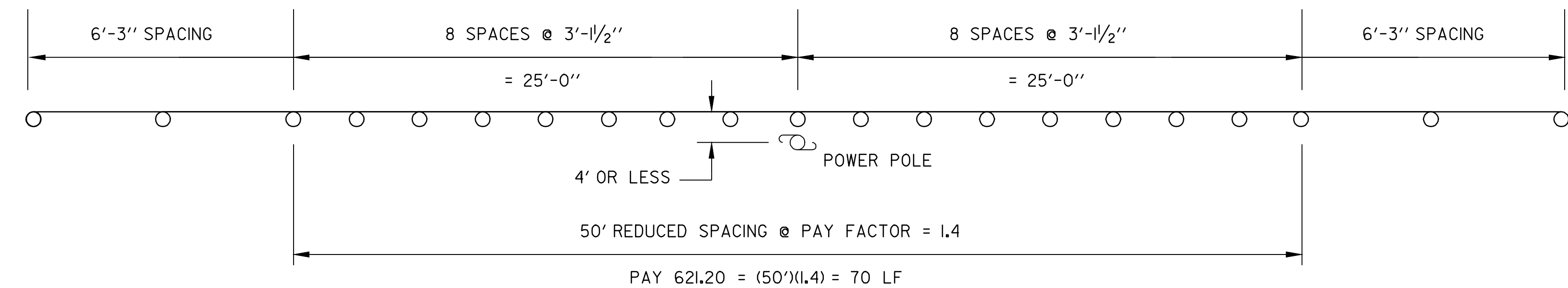
PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
 PROJECT NUMBER: STP 2210 (I)S  
 FILE NAME: p99ci72.dgn  
 PROJECT LEADER: EPD  
 DESIGNED BY: CDL  
 PLOT FILE: p99ci72+pl.i  
 PLOT DATE: 29-APR-2009 13:3  
 DRAWN BY: RHB  
 CHECKED BY: EPD  
 SHEET 3 OF 32

**DETAILS ARE NOT TO SCALE**

**PROJECT PAVING LIMITS**

TOWN AND ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING TON	NOTES
WILLIAMSTOWN	200+64.00	222+00.00	3FT - 11FT - 11FT - 3FT	1 1/2"	186.90	COLD PLANE 2", LEVEL, 1/2" TYPE IVS
WILLIAMSTOWN	222+00.00	241+00.00	6FT - 11FT - 11FT - 10FT	1 1/2"	237.50	COLD PLANE 2", LEVEL, 1/2" TYPE IVS
WILLIAMSTOWN	241+00.00	360+62.22	3FT - 11FT - 11FT - 3FT	1 1/2"	1046.69	COLD PLANE 2", LEVEL, 1/2" TYPE IVS
BARRE TOWN	0+00.00	84+20	3FT - 11FT - 11FT - 3FT	1 1/2"	736.76	COLD PLANE 2", LEVEL, 1/2" TYPE IVS
BARRE TOWN	84+20	90+40	VARIES SEE INTERSECTION DETAIL SHEET	1 1/2"	118.46	COLD PLANE 2", LEVEL, 1/2" TYPE IVS

<b>BRIDGE QUANTITY SHEET</b>					
STATION	POS.	BRIDGE NUMBER	OFFSET BLOCK	525.10 REMOVAL OF EXISTING RAILING LF	900.640 BRIDGE RAILING REPAIR, TYPE I LF
WILLIAMSTOWN					
206+78.50 - 207+22.25	LT	57	8'	18.75	18.75
206+95.50 - 207+64.25	RT	57	8'	18.75	18.75
219+21.00 - 220+08.50	RT	58	8'	31.25	31.25
219+21.50 - 220+09.00	LT	58	8'	31.25	31.25
306+21.25 - 307+15.00	RT	60	8'	37.5	37.5
306+47.00 - 307+40.75	LT	60	8'	37.5	37.5
BARRE TOWN					
57+72.50 - 58+10.00	RT	-	8'	37.5	37.5
SUBTOTAL				212.5	212.5
ROUNDING				-	-
TOTAL				212.5	212.5



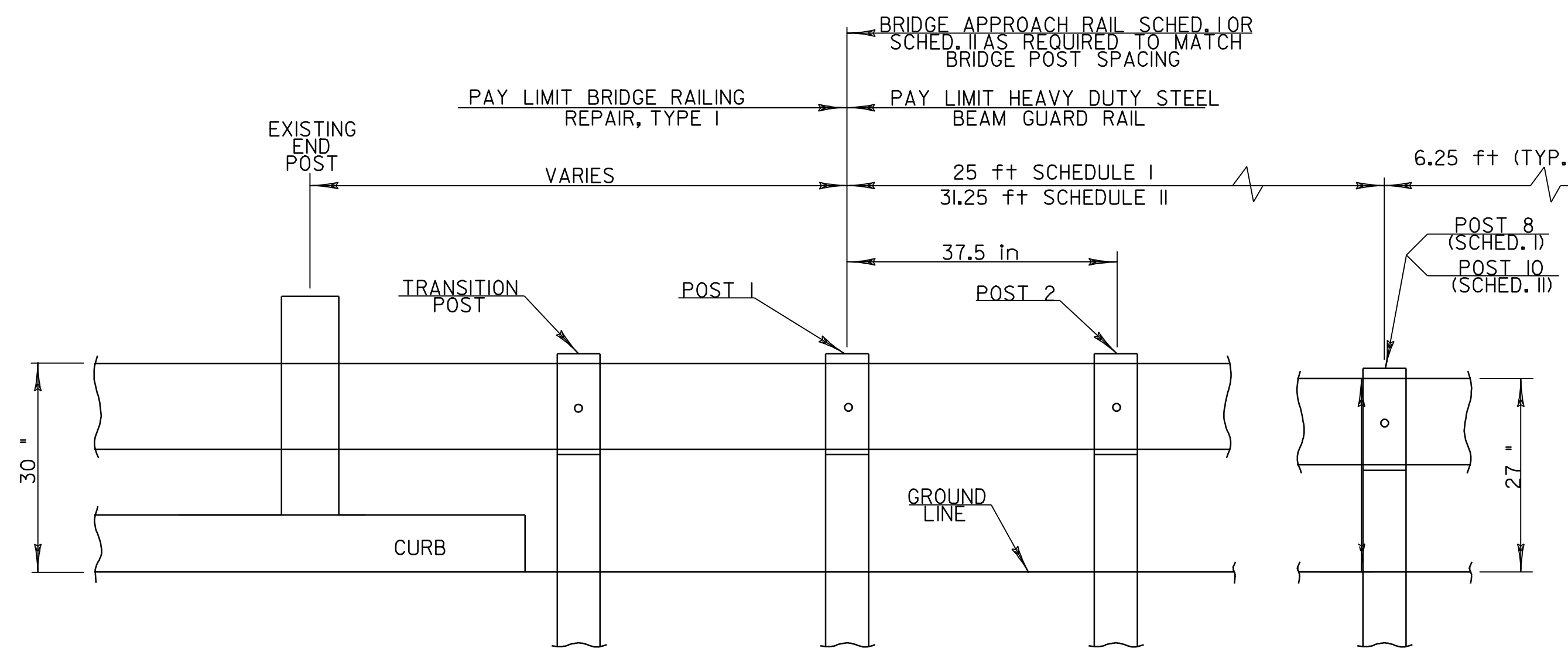
**NON-YIELDING OBJECT APPROACH DETAIL**

OBJECTS WITHIN 4' OF GUARDRAIL

WILLIAMSTOWN	BARRE
STA. 246+89.50 RT	STA. 324+35.00 LT
STA. 248+52.00 RT	STA. 326+60.00 LT
STA. 295+40.00 RT	STA. 328+72.50 LT
STA. 300+15.00 RT	

**BRIDGE APPROACH RAILING**

WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1  
USE SCHEDULE I FOR APPROACH RAILING WHEN A RAIL  
PANEL SPLICE OCCURS AT BRIDGE END POST USE  
SCHEDULE II FOR APPROACH RAILING.



**BRIDGE APPROACH RAILING**

**NOTES:**

- BRIDGE RAIL SHALL BE HEAVY DUTY STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 25 FEET.
- APPROACH RAILING SHALL BE HEAVY DUTY STEEL BEAM FOR 25 FEET FROM THE END OF THE BRIDGE.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICAL TO THE MID-POINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST 1.
- SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.

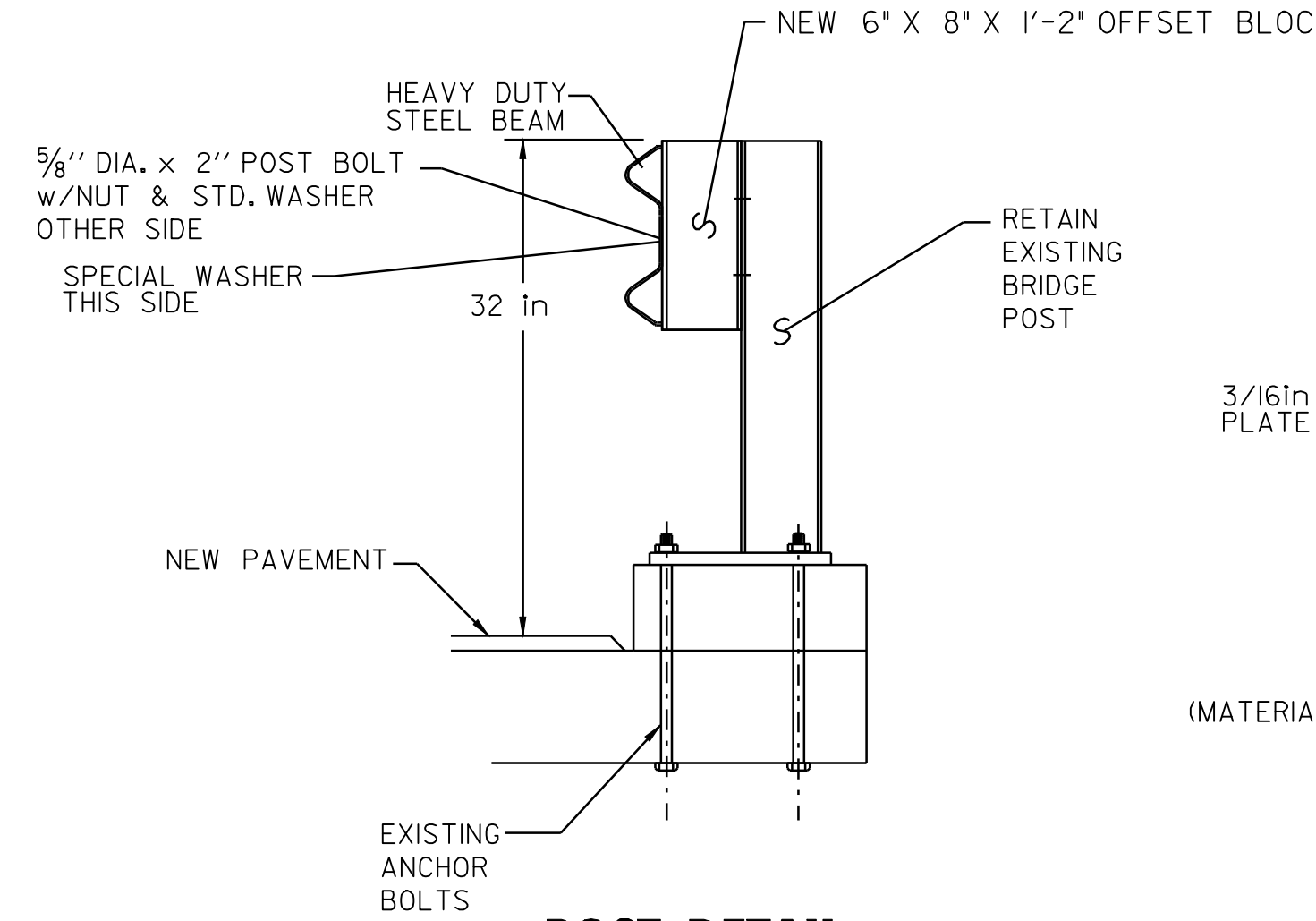
SCHEDULE I		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 12.5 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	4'-2"	1.2 x 12.5 ft
7	4'-2"	
8	4'-2"	
9	6'-3" (TYP.)	1.0 (TYP.)
PAYMENT LENGTH = 32'-6"		

SCHEDULE II		
POST NO.	SPACING	PAYMENT FACTOR
1	3'-1 1/2"	1.4 x 18.75 ft
2	3'-1 1/2"	
3	3'-1 1/2"	
4	3'-1 1/2"	
5	3'-1 1/2"	
6	3'-1 1/2"	1.2 x 12.5 ft
7	3'-1 1/2"	
8	4'-2"	
9	4'-2"	
10	4'-2"	1.0 (TYP.)
11	6'-3" (TYP.)	
PAYMENT LENGTH = 41'-3"		

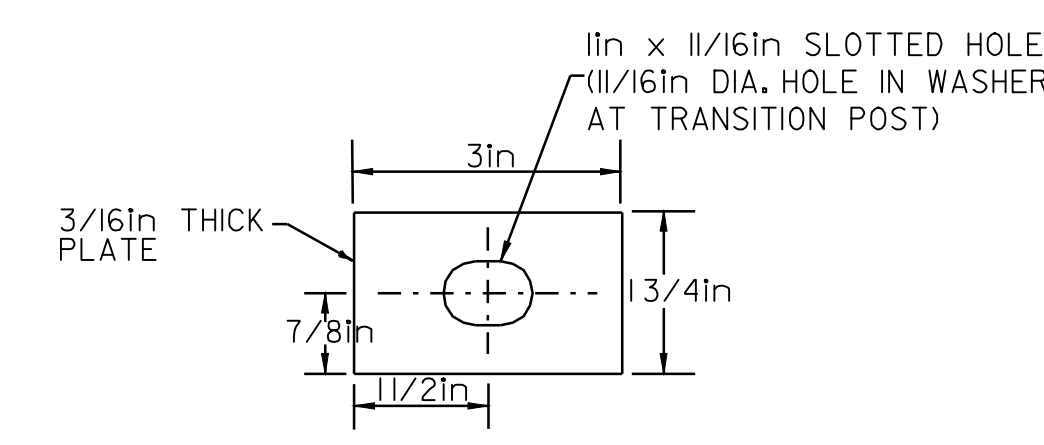
**PROJECT  
TYPICAL  
SHEET 2**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S  
FILE NAME: P99CI72.DGN  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99ci72+p2.1  
PLOT DATE: 29-APR-2009 13:3  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 4 OF 32

**DETAILS ARE NOT TO SCALE**

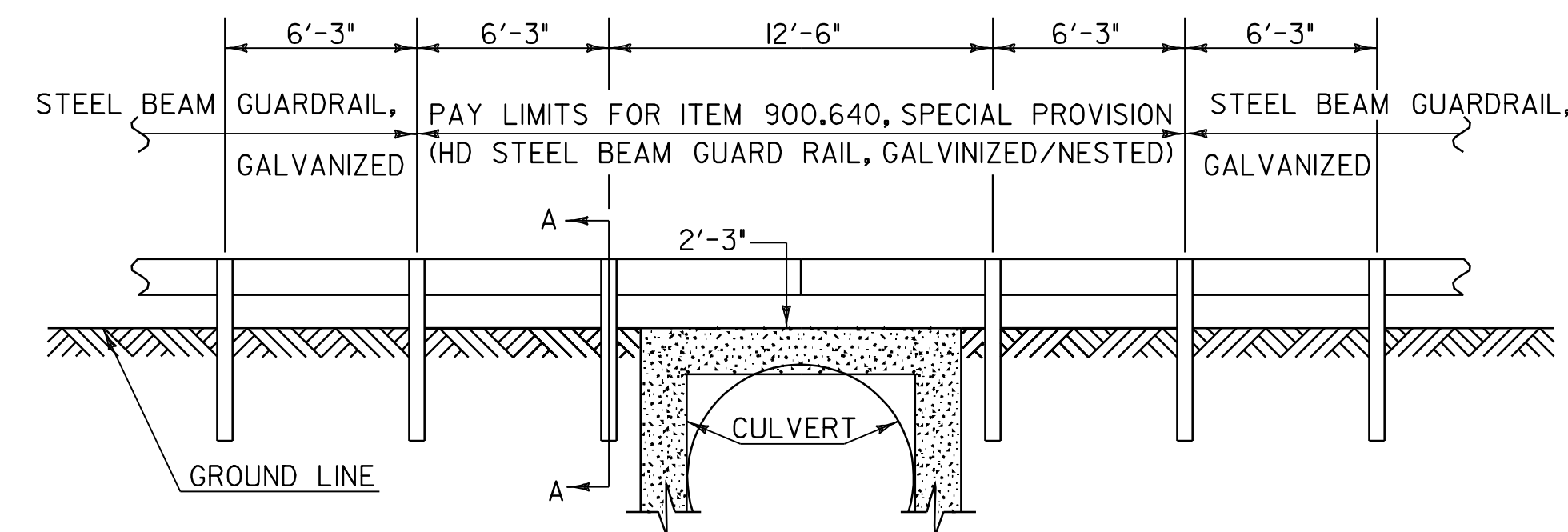


**POST DETAIL**



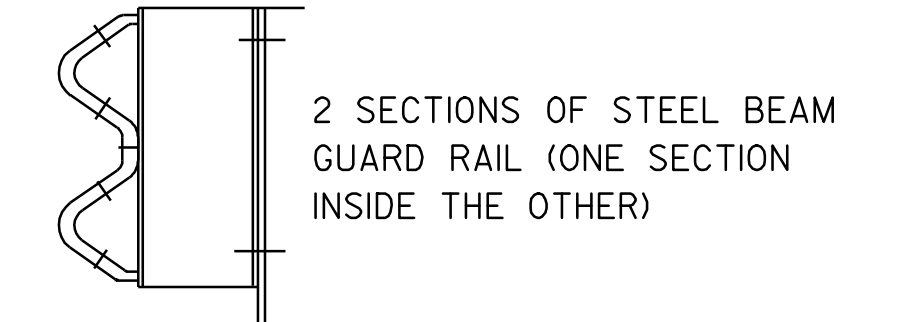
**SPECIAL WASHER DETAIL**

(MATERIAL SHALL MEET AASHTO M270M/M270 GRADE 50)

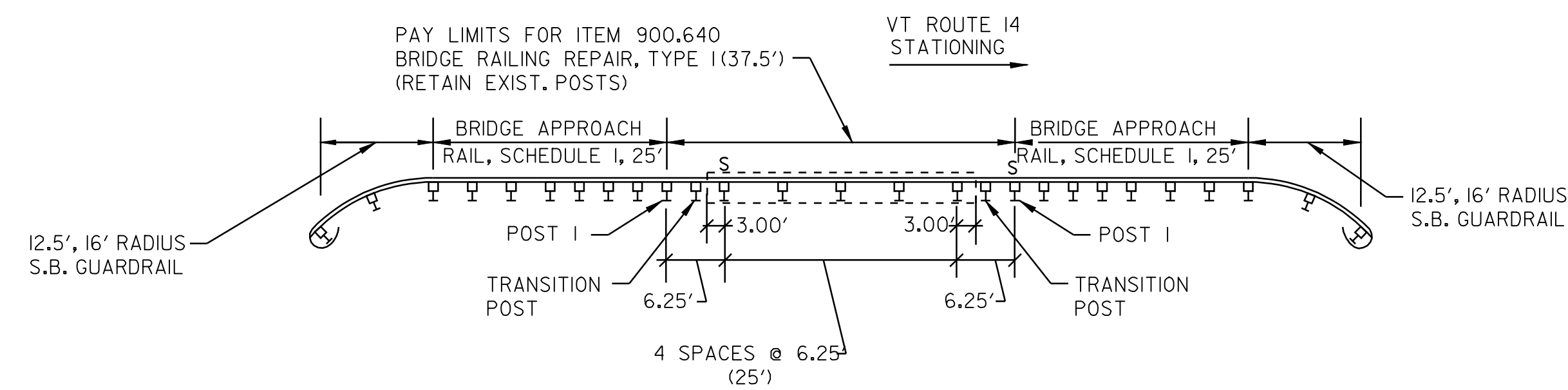


**DETAIL OF STEEL BEAM GUARD RAIL AT SMALL CULVERTS**

WILLIAMSTOWN BARRE TOWN  
 STA. 340+IIRT STA. 37+6ILT  
 STA. 340+IILT

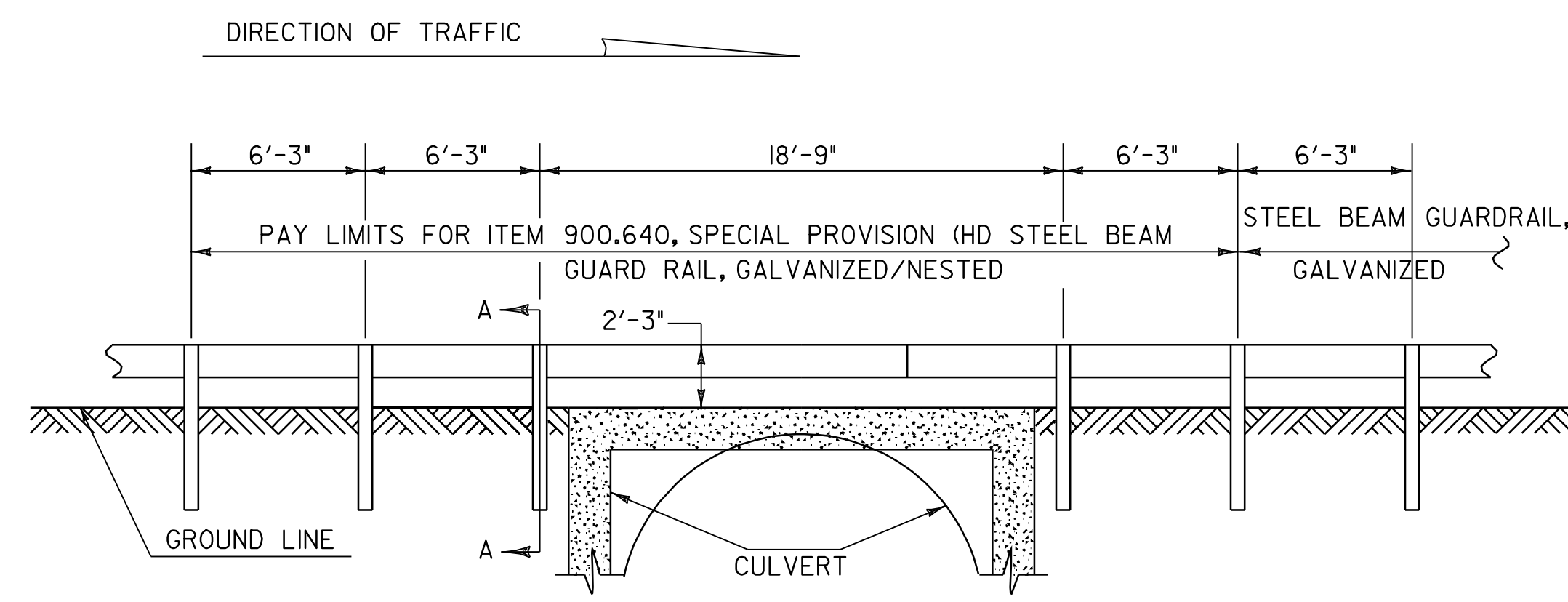


**DETAIL A**



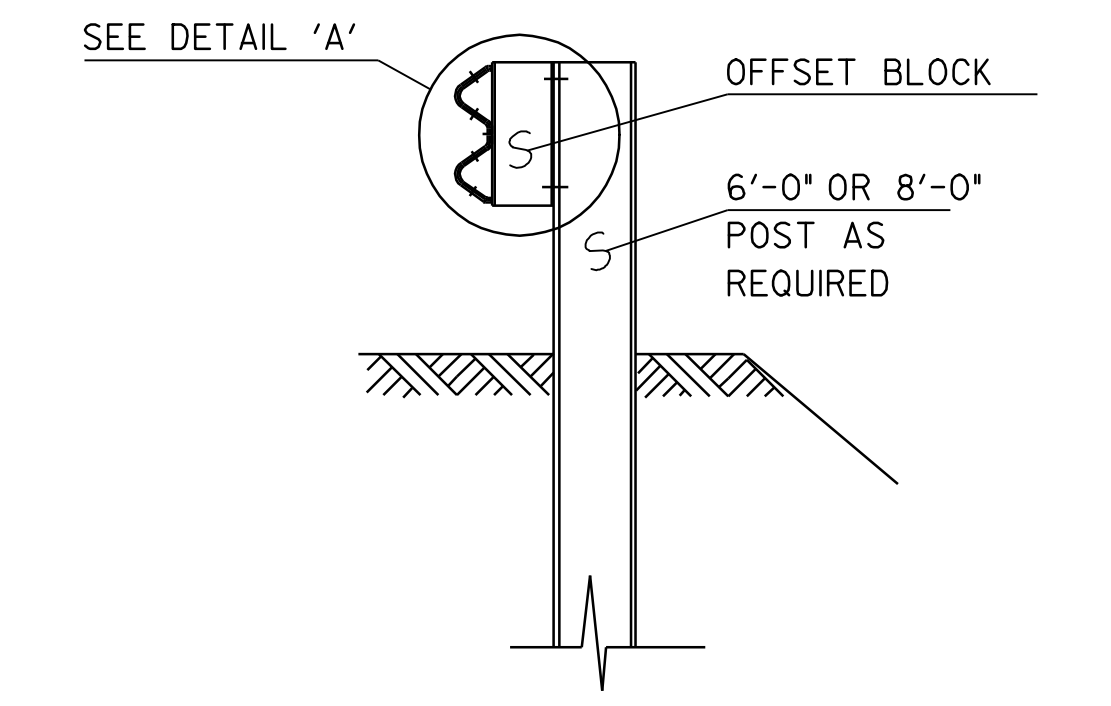
**BARRE TOWN - CULVERT**

FIELD STA. 57+91 - MM 1.097

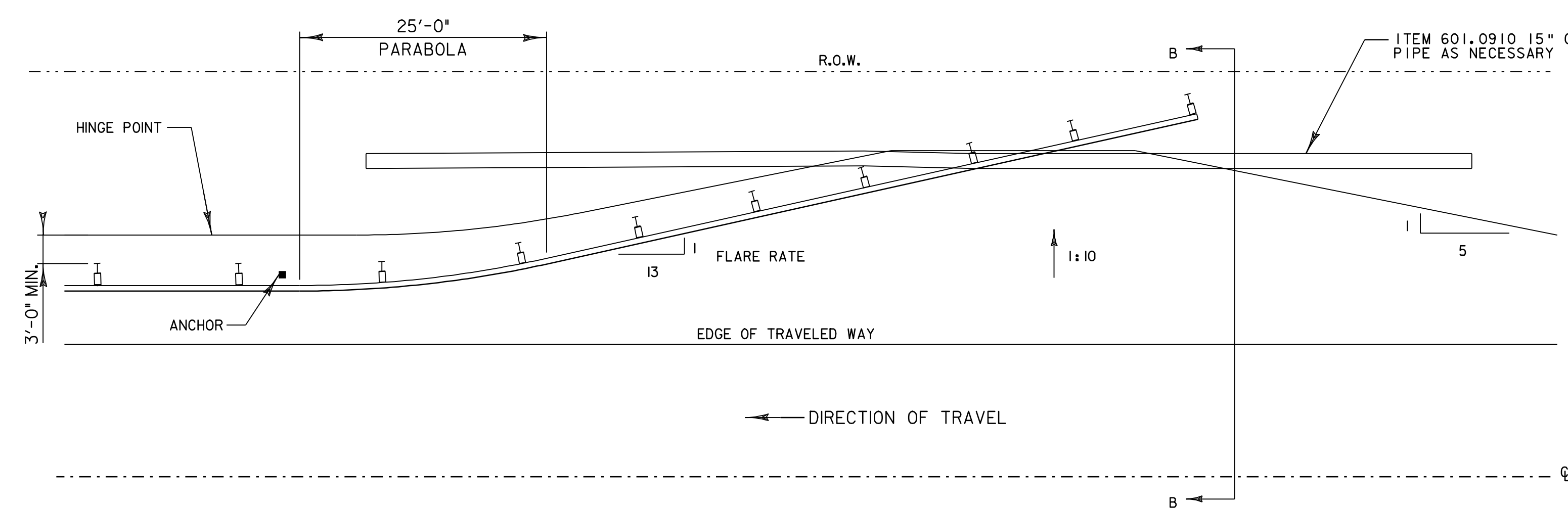


**DETAIL OF STEEL BEAM GUARD RAIL AT LARGE CULVERTS**

BARRE TOWN  
 STA. 37+67 RT



**SECTION A-A**



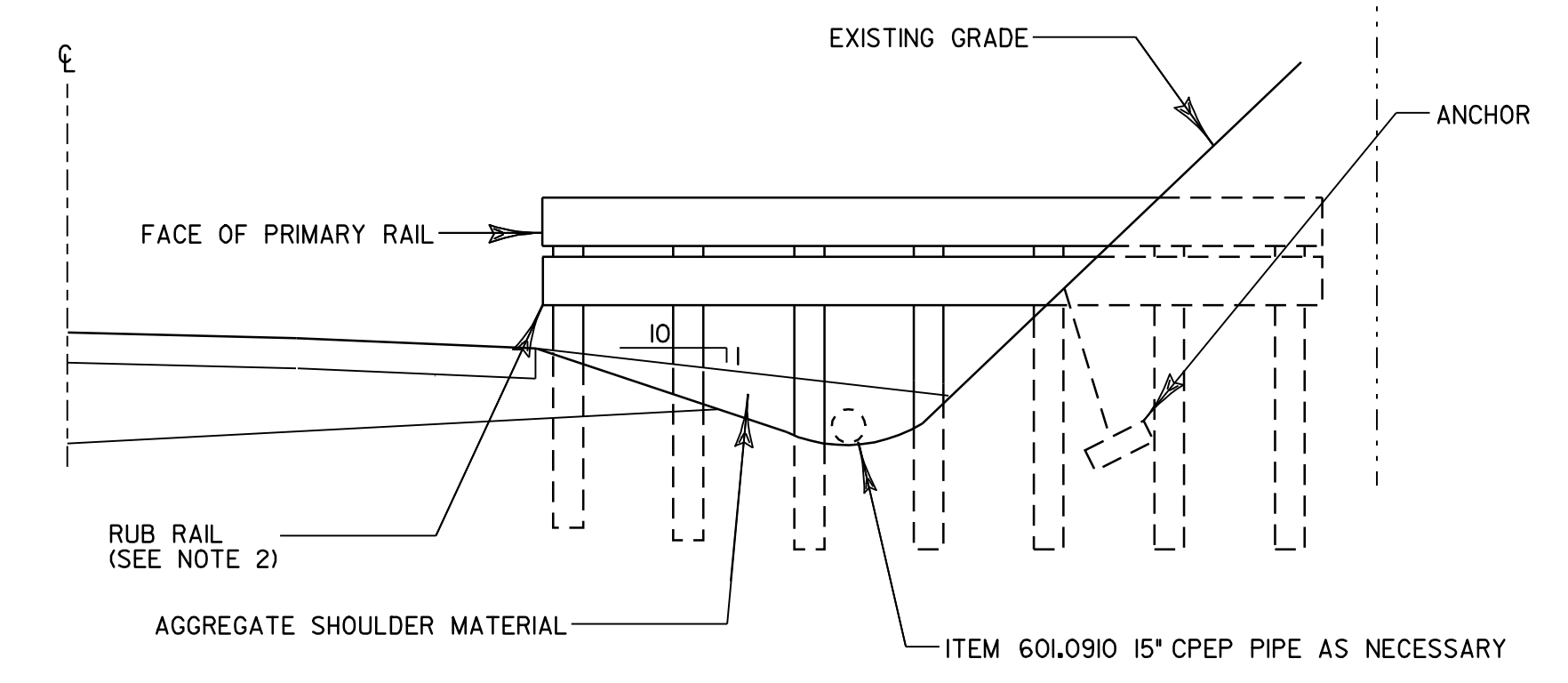
**DETAIL FOR BURIED GUARDRAIL ENDS INTO BACKSLOPE WITH RUBRAIL**

NOT TO SCALE

WILLIAMSTOWN  
 STA. 281+IIRT  
 STA. 283+20 LT

**NOTES:**

1. PRIMARY RAIL SHALL REMAIN AT A CONSTANT HEIGHT (LEVEL) RELATIVE TO THE HEIGHT OF RAIL AT THE EDGE OF SHOULDER.
2. ADDITION OF RUB RAIL IS REQUIRED WHEN OPENING BENEATH PRIMARY RAIL EXCEEDS 18 INCHES. RUB RAIL EXTENDS FROM THE EDGE OF SHOULDER TO THE BACK SLOPE.
3. TRENCHING AND BACKFILLING ASSOCIATED WITH INSTALLING 15" CPEP PIPE SHALL BE PAID UNDER ITEM 608.25, "ALL PURPOSE EXCAVATOR RENTAL, TYPE I" AND ITEM 608.37, "TRUCK RENTAL".



**SECTION B-B**

NOT TO SCALE

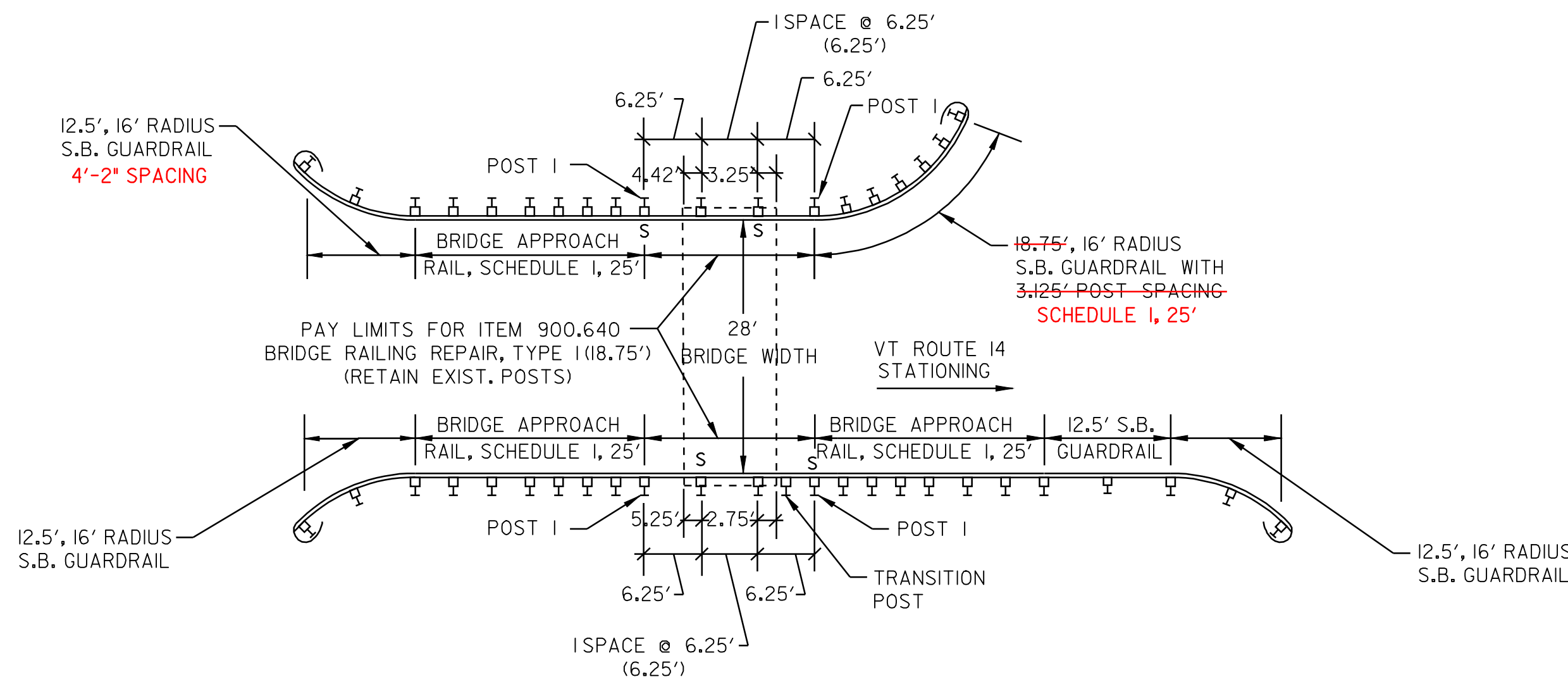
**GUARDRAIL  
 DETAIL SHEET**

**DETAILS ARE NOT TO SCALE**

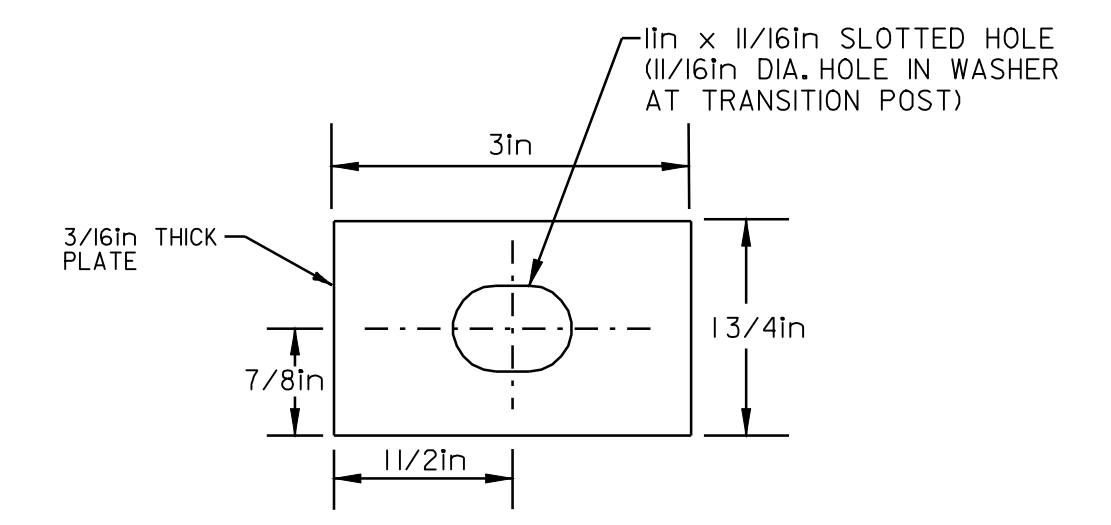
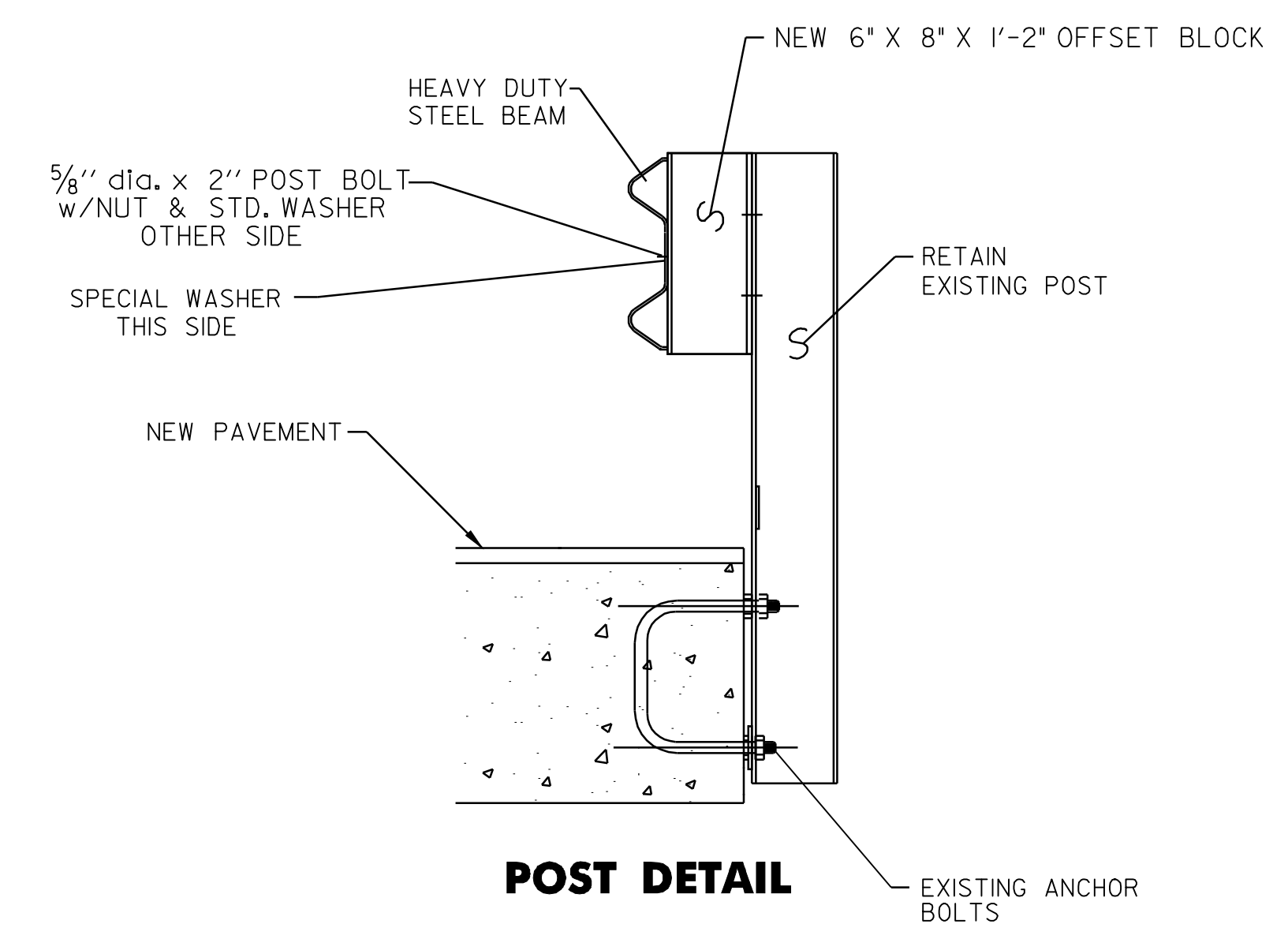
PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
 PROJECT NUMBER: STP 2210 (I)S

FILE NAME: p99cl72.dgn  
 PROJECT LEADER: EPD  
 DESIGNED BY: CDL  
 PLOT FILE: p99cl72gds.1

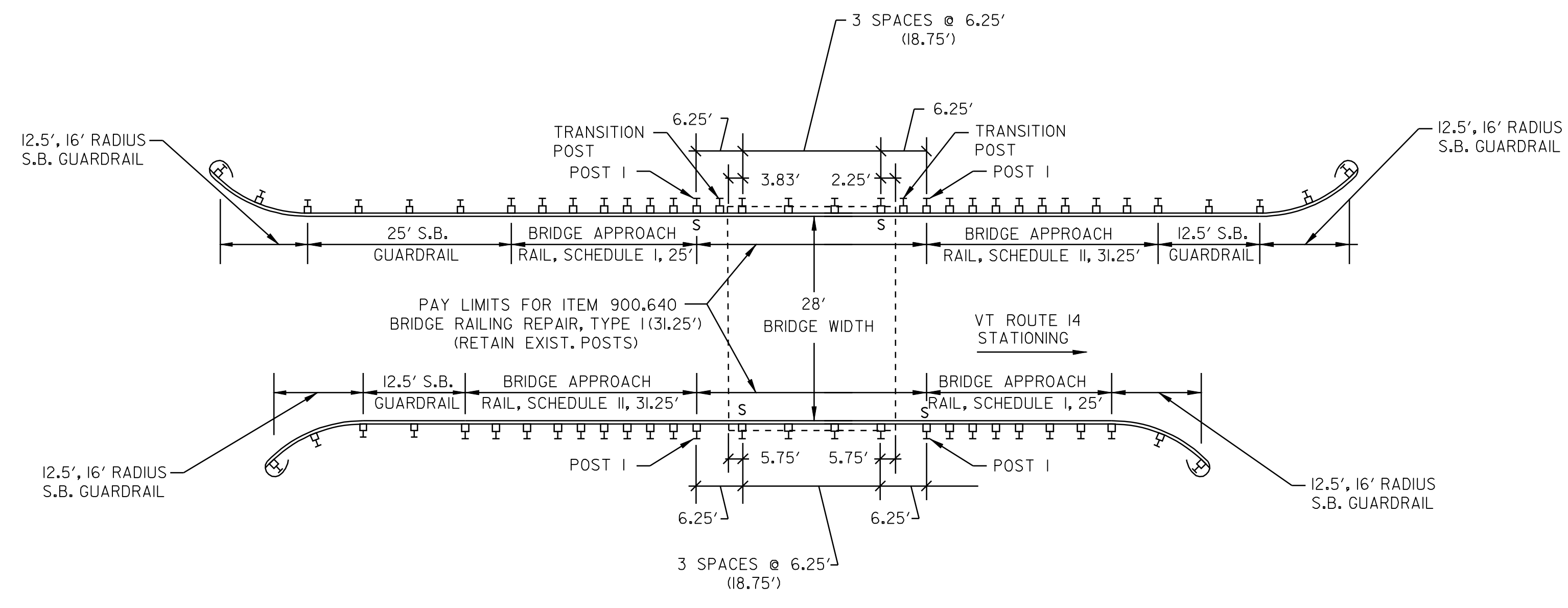
PLOT DATE: 29-APR-2009 13:3  
 DRAWN BY: RHB  
 CHECKED BY: EPD  
 SHEET 5 OF 32



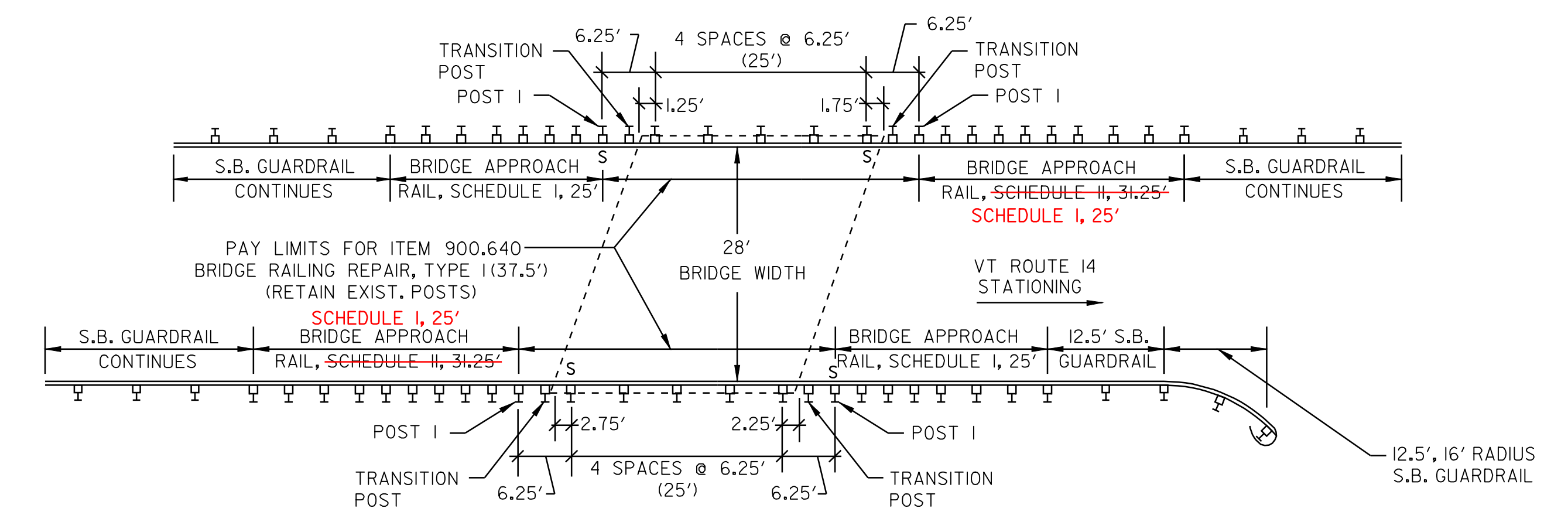
**WILLIAMSTOWN BRIDGE #57**  
**FIELD STA. 207+04 - MM 3.921**



**SPECIAL WASHER DETAIL**  
 (MATERIAL SHALL MEET AASHTO M270M/M270 GRADE 50)



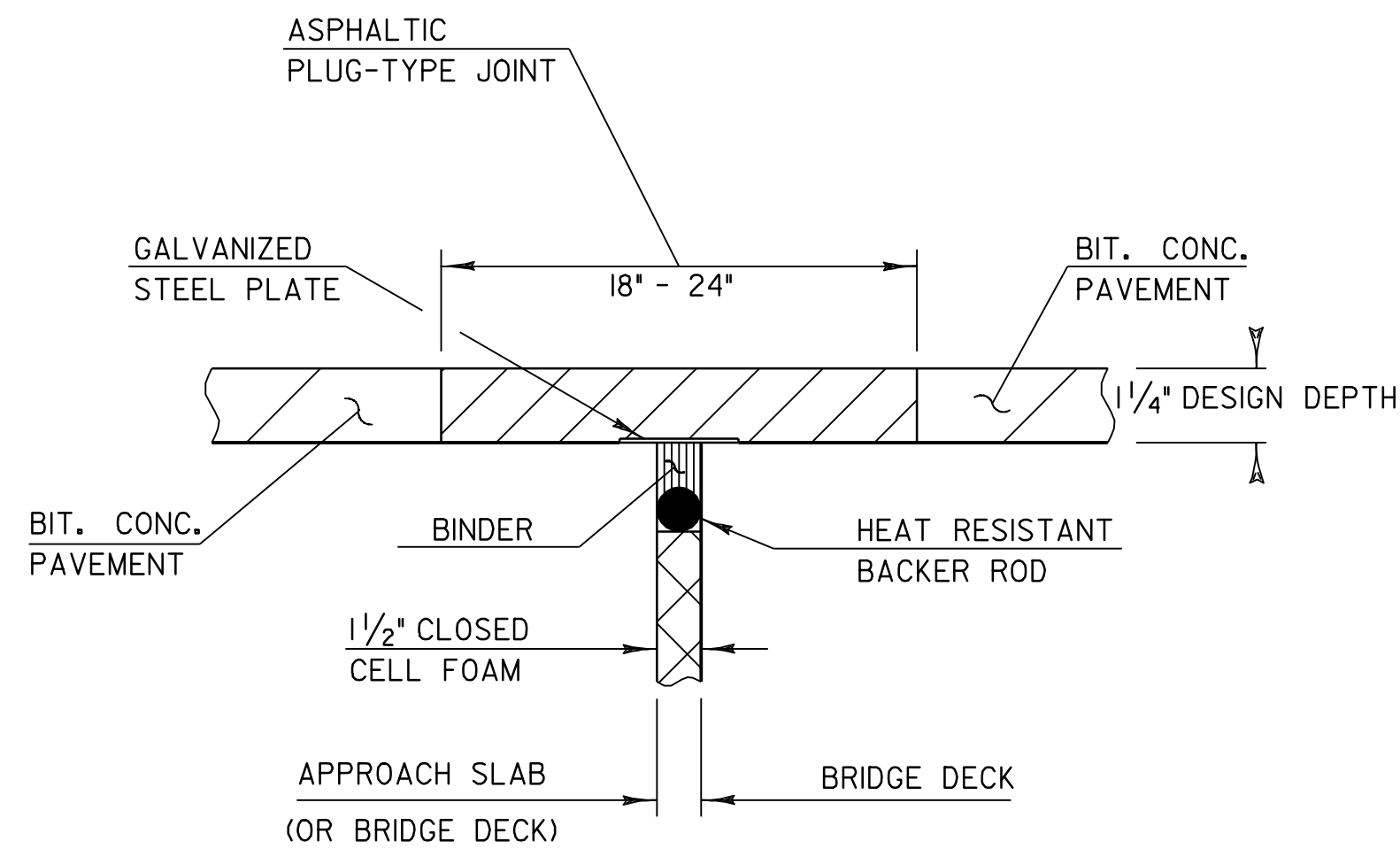
**WILLIAMSTOWN BRIDGE #58**  
**FIELD STA. 219+30 - MM 4.153**



**WILLIAMSTOWN BRIDGE #60**  
**FIELD STA. 306+85 - MM 5.812**

**DETAILS ARE NOT TO SCALE**

<b>BRIDGE DETAIL SHEET</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	
	PROJECT NUMBER: STP 2210 (1)S	
	FILE NAME: p99cl72.dgn	PLOT DATE: 29-APR-2009 13:3
	PROJECT LEADER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD	
PLOT FILE: p99cl72bds.1		SHEET 6 OF 32



**ASPHALTIC PLUG TYPE JOINT DETAIL**

WILLIAMSTOWN  
STA. 306+68  
~~STA. 306+96~~

**ASPHALTIC PLUG BRIDGE JOINT  
GENERAL NOTES**

**INSTALLATION:**

THE JOINT SHALL BE LOCATED CENTRALLY OVER THE DECK EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.

THE JOINT SHALL BE EXCAVATED AS SHOWN ON THE PLANS BY USE OF SAWS AND PNEUMATIC HAMMER OR A HAMMER AND CHISEL.

THE JOINT AREA SHALL BE BLAST CLEANED OF DEBRIS AND ASPHALT. THE JOINT AREA SHALL BE THOROUGHLY DRIED USING HOT COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.

SPALLED AND DEFECTIVE CONCRETE SHALL BE REPAIRED WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.

PROPERLY SIZED HEAT RESISTANT BACKER ROD SHALL BE PLACED IN THE MOVEMENT GAP ALLOWING FOR 25MM (1 INCH) +/- OF BINDER ABOVE THE ROD.

THE BINDER MATERIAL SHALL BE HEATED AND PLACED AS RECOMMENDED BY THE MANUFACTURER.

PLACE 6 MM (1/4 INCH) THICK BY 200 MM (8 INCH) WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER.

A. THE STEEL PLATES MAY BE OMITTED WHERE THE APPROACH SLAB IS COVERED WITH A STONE BASE OR BITUMINOUS PAVEMENT, AND VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.

THE BINDER MATERIAL AND AGGREGATE SHALL BE HEATED AND MIXED AS RECOMMENDED BY THE MANUFACTURER.

THE INSTALLATION OF MATERIAL, COMPACTION, AND TOPCOATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.

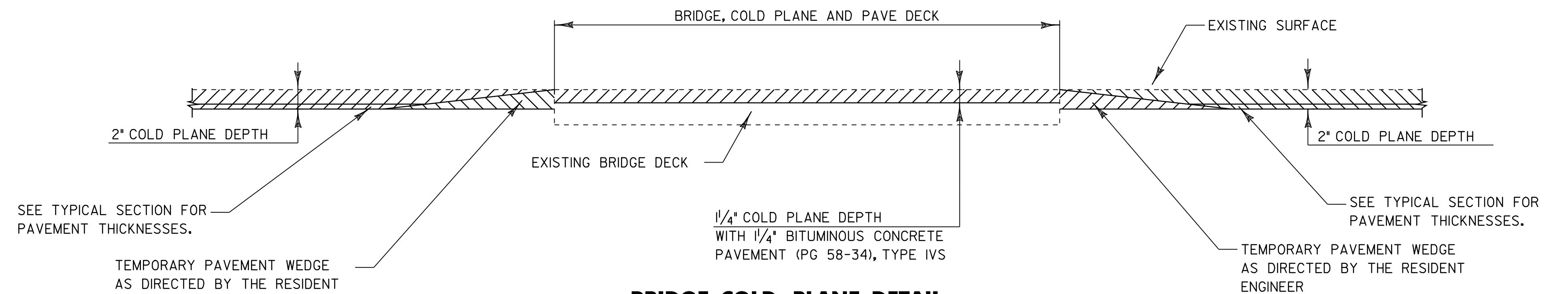
IMMEDIATELY AFTER TOPCOATING, AN ANTI-SKID MATERIAL SHALL BE CAST OVER THE JOINT TO REDUCE THE RISK OF TRACKING.

JOINT SHALL BE PROTECTED FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 52 C +/- (125 F).

**WEATHER LIMITATIONS:**

BINDER MATERIAL SHALL BE APPLIED ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL:

- (A) THE AMBIENT AIR TEMPERATURE IS AT LEAST 10° C (50° F) AND RISING.
- (B) THE ROAD SURFACE IS SUFFICIENTLY DRY.
- (C) WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF THE SATISFACTORY WORK.



**BRIDGE COLD PLANE DETAIL**

BRIDGE #59, #60 AND #64

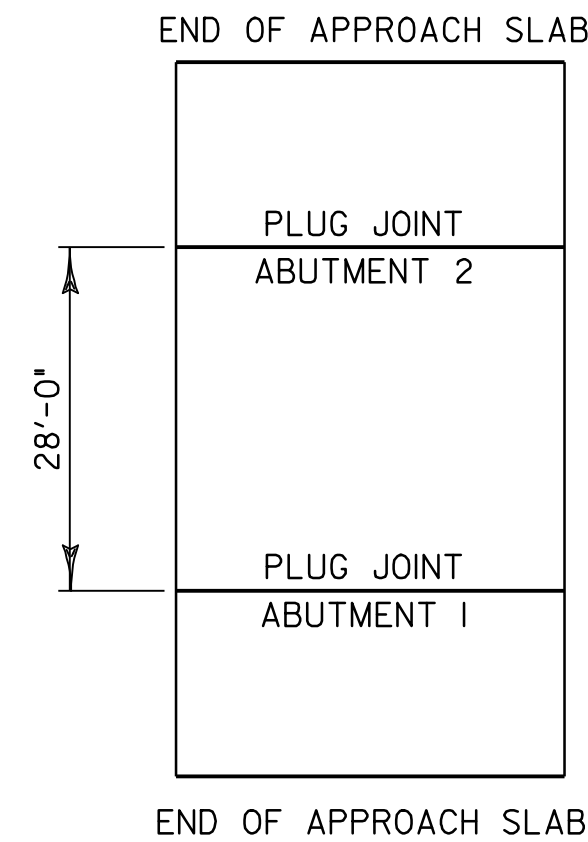
SEE TYPICAL SECTION FOR PAVEMENT THICKNESSES.

TEMPORARY PAVEMENT WEDGE AS DIRECTED BY THE RESIDENT ENGINEER

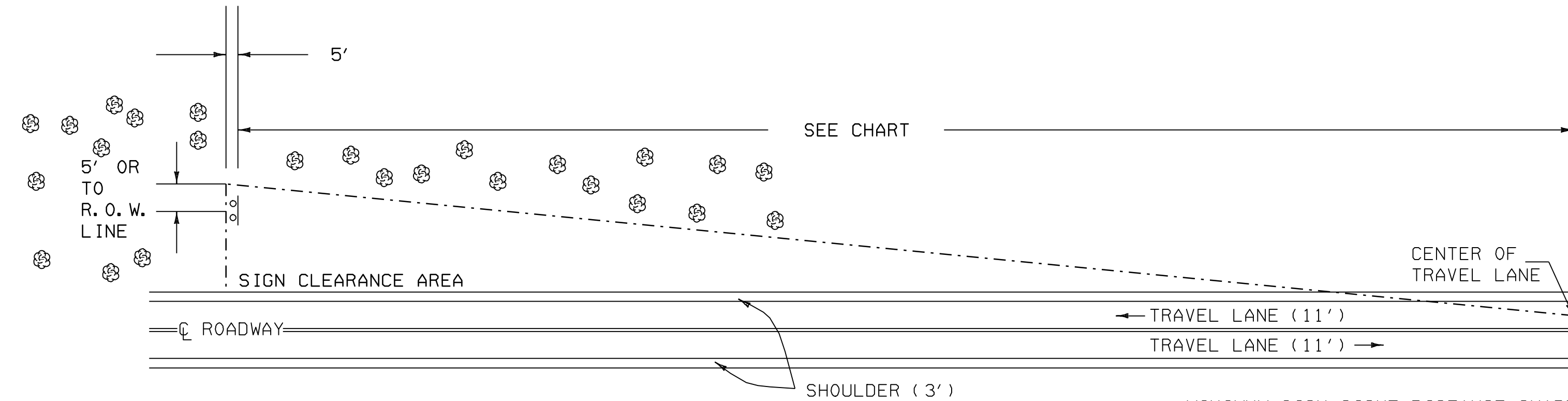
JOINT	STATION	LENGTH
1	306+68	36' - 0"
2	306+96	36' - 0"
BRIDGE #60		72' - 0"
TOTAL LENGTH OF JOINT		

**NOTES:**

1. REFER TO THIS SHEET FOR ASPHALTIC PLUG JOINT AND DETAILS. ALL NEW JOINTS TO BE PAID FOR UNDER ITEM 516.10, \*BRIDGE EXPANSION JOINT, ASPHALTIC PLUG\*.
2. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID DAMAGING DRAINAGE STRUCTURES AND EXPANSION JOINTS. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE EXPENSE OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID THE ACCUMULATION OF DEBRIS IN THE DRAINAGE STRUCTURES LOCATED AT CURB LINE AND IN THE EXPANSION JOINTS. THE CONTRACTOR SHALL EXAMINE THESE BRIDGE FEATURES ON A REGULAR BASIS TO ENSURE THAT DEBRIS HAS NOT ACCUMULATED. ANY DEBRIS WHICH IS PRESENT SHALL BE REMOVED BY THE CONTRACTOR AT NO COST TO THE STATE.
4. THE CONTRACTOR SHALL PERFORM COLD PLANING WITH CAUTION. ANY DAMAGE WHICH OCCURS TO MEMBRANES AS A RESULT OF COLD PLANE OPERATIONS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND THE MEMBRANE WILL BE REPAIRED OR REPLACED AT NO COST TO THE STATE.



**BRIDGE #60  
FIELD STA. 306+85 (MM 5.812)  
ROUTE LOG STA. 306+99 (MM 5.814)  
COLD PLANE AND PAVE**



THE CONTRACTOR SHALL REMOVE ALL WOODY STEMMED GROWTH INCLUDING BRUSH, SAPLINGS, TREE LIMBS GROWING WITHIN OR PROJECTING INTO THE CLEARANCE AREA AND DOWN TO GROUND LEVEL. PAYMENT WILL BE FOR THINNING AND TRIMMING FOR SIGNS, ITEM 201.31, AND PAID FOR PER EACH. NO CHEMICALS (POISONS OR DEFOLIANTS) ALLOWED.

**MINIMUM SIGN SIGHT DISTANCE CHART**

APPROACH SPEED (mph)	SIGHT DISTANCE (feet)
30 OR LESS	300
35	350
40	400
45	450
50	500
55	550

**THINNING AND TRIMMING FOR SIGNS DETAIL**

WILLIAMSTOWN	BARRE
STA. 212+96 LT	STA. 53+05 LT
STA. 220+82 LT	STA. 86+65 LT
STA. 221+03 RT	STA. 86+75 RT
STA. 259+55 RT	STA. 88+79 LT
STA. 347+05 RT	

**BRIDGE LOCATION AND DETAIL SHEET**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (1)S

FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl72blds.1  
PLOT DATE: 29-APR-2009 13:3  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 7 OF 32

DETAILS ARE NOT TO SCALE

# QUANTITY SHEET 1

SUMMARY OF ESTIMATED QUANTITIES										TOTALS		DESCRIPTIONS				DETAILED SUMMARY OF QUANTITIES			
								ROADWAY	BRIDGE	FULL C.E. ITEMS	GRAND TOTAL	FINAL	UNIT	ITEMS	ITEM NUMBER	ROUND	QUANTITIES	UNIT	ITEMS
								9			9		EACH	THINNING AND TRIMMING FOR SIGNS	201.31	-			
								100			100		CY	COMMON EXCAVATION	203.15	EST			COLD PLANING, BITUMINOUS CONCRETE
								2			2		CY	SOLID ROCK EXCAVATION	203.16	0.5	85157.57	SY	VT ROUTE 14
								425			425		CY	EARTH BORROW	203.30	-	42.43	SY	ROUNDING
								3			3		CY	TRENCH EXCAVATION OF EARTH	204.20	0.1	85200	SY	TOTAL
								1			1		CY	TRENCH EXCAVATION OF EARTH, EXPLORATORY (N.A.B.I.)	204.22	EST			SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34)
								85200			85200		SY	COLD PLANING, BITUMINOUS PAVEMENT	210.10	42.43			WEARING COURSE
								525			525		TON	AGGREGATE SHOULDERS, RAP	402.13	24	6978.92	TON	TYPE IVS
								365			365		CWT	EMULSIFIED ASPHALT	404.65	4.87			SIDERoads, DRIVES, AND PULL-OFFS
								1			1		LU	PRICE ADJUSTMENT, ASPHALT CEMENT (N.A.B.I.)	406.50	-	336.68	TON	TYPE IVS
								9800			9800		TON	SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (PG 58-34)	490.30	158.09			LEVELING COURSE
								1			1		LU	AIR VOIDS PAY ADJUSTMENT (N.A.B.I.)	490.31	-			TYPE VS
									75		75		LF	BRIDGE EXPANSION JOINT, ASPHALTIC PLUG	516.10	3	2326.31	TON	
									213		213		LF	REMOVAL OF EXISTING RAILING	525.10	-			
								105			105		LF	15" CPEP	601.0910	5			
								4			4		LF	RE-LAYING PIPE CULVERTS (15")	601.99	-			
								2			2		CY	CEMENT MASONRY	602.15	0.7			AGGREGATE SHOULDERS, RAP
								3			3		EACH	CHANGING ELEVATION OF DROP INLETS, CATCH BASINS, OR MANHOLES	604.40	-	187.50	TON	USED ALONG SHOULDERS
								89			89		EACH	REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I	604.412	-	85.00	TON	USED AT MANUFACTURED TERMINAL SECTIONS
								3			3		EACH	CHANGING ELEVATION OF SEWER MANHOLES	604.42	-	228.50	TON	USED AT GRAVEL TURNOUTS
								125			125		HR	POWER GRADER RENTAL	608.15	EST	24.00	TON	ROUNDING
								345			345		HR	ALL PURPOSE EXCAVATOR RENTAL, TYPE I	608.25	EST	525	TON	TOTAL
								80			80		HR	POWER BROOM RENTAL, TYPE I	608.30	EST			
								40			40		HR	POWER BROOM RENTAL, TYPE II	608.31	EST			
								700			700		HR	TRUCK RENTAL	608.37	EST			
								125			125		HR	LOADER RENTAL, TYPE I	608.40	EST			
								410			410		CY	STONE FILL, TYPE I	613.10	10			
								90			90		TON	BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS	616.47	1.2			
								4			4		EACH	RELOCATE MAILBOX, SINGLE SUPPORT	617.10	-			
								9			9		SY	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	618.10	-			
								95			95		EACH	YIELDING MARKER POSTS	619.17	2			
								8811			8811		LF	STEEL BEAM GUARDRAIL, GALVANIZED	621.20	-			
								400			400		LF	STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS	621.205	-			
								450			450		LF	HD STEEL BEAM GUARDRAIL, GALVANIZED	621.21	-			
								17			17		EACH	MANUFACTURED TERMINAL SECTION, FLARED	621.50	-			
								32			32		EACH	ANCHOR FOR STEEL BEAM RAIL	621.60	-			
								10022			10022		LF	REMOVAL AND DISPOSAL OF GUARDRAIL	621.80	-			
								500			500		HR	UNIFORMED TRAFFIC OFFICERS	630.10	EST			
								3000			3000		HR	FLAGGERS	630.15	EST			
										1	1		LS	FIELD OFFICE, ENGINEERS	631.10	-			

PROJECT NAME:	WILLIAMSTOWN-BARRE TOWN
PROJECT NUMBER:	STP 2210 (1)S
FILE NAME: p99c172.dgn	PLOT DATE: 10/26/2007
PROJECT MANAGER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD
QUANTITY SHEET #1	SHEET 8 OF 32

# QUANTITY SHEET 2

SUMMARY OF ESTIMATED QUANTITIES										TOTALS		DESCRIPTIONS				DETAILED SUMMARY OF QUANTITIES			
								ROADWAY	BRIDGE	FULL C.E. ITEMS	GRAND TOTAL	FINAL	UNIT	ITEMS	ITEM NUMBER	ROUND	QUANTITIES	UNIT	ITEMS
										1	1		LS	TESTING EQUIPMENT, BITUMINOUS	631.17	-			
										1	1		LU	FIELD OFFICE TELEPHONE (N.A.B.I.)	631.25	-			
							1				1		LS	MOBILIZATION/DEMOBILIZATION	635.11	-			
							1				1		LS	TRAFFIC CONTROL	641.10	-			
							2				2		EACH	PORTABLE CHANGEABLE MESSAGE SIGN	641.15	-			
							50900				50900		LF	DURABLE 4 INCH WHITE LINE, EPOXY PAINT	646.403				
							50300				50300		LF	DURABLE 4 INCH YELLOW LINE, EPOXY PAINT	646.413				
							60				60		LF	DURABLE 8 INCH YELLOW LINE, THERMOPLASTIC	646.452	6			
							520				520		LF	DURABLE 24 INCH STOP BAR, THERMOPLASTIC	646.482	12			
							131				131		EACH	DURABLE LETTER OR SYMBOL, THERMOPLASTIC	646.492	-			
							85				85		LF	DURABLE CROSSWALK MARKING, THERMOPLASTIC	646.502	4			
							101800				101800		LF	TEMPORARY 4 INCH WHITE LINE, PAINT	646.602	1008			
							100600				100600		LF	TEMPORARY 4 INCH YELLOW LINE, PAINT	646.612	1007			
							115				115		LF	TEMPORARY 8 INCH YELLOW LINE, PAINT	646.652	7			
							1040				1040		LF	TEMPORARY 24 INCH STOP BAR, PAINT	646.682	24			
							262				262		EACH	TEMPORARY LETTER OR SYMBOL, PAINT	646.692	-			
							170				170		LF	TEMPORARY CROSSWALK MARKING, PAINT	646.702	8			
							1950				1950		EACH	LINE STRIPING TARGETS	646.76	72			
							1215				1215		SY	GEOTEXTILE UNDER STONE FILL	649.31	13			
							50				50		LB	SEED	651.15	5			
							400				400		LB	FERTILIZER	651.18	25			
							2				2		TON	AGRICULTURAL LIMESTONE	651.20	0.5			
							2				2		TON	HAYMULCH	651.25	0.5			
							415				415		CY	TOPSOIL	651.35	12			
							3500				3500		SY	TEMPORARY EROSION MATTING	653.20	38			
							295				295		SF	TRAFFIC SIGNS, TYPE A	675.20	2.59			
														BEGIN OPTION AA					
							765				765		LF	FLANGED CHANNEL SIGN POST	675.301	-			
							765				765		LF	SQUARE TUBE SIGN POST AND ANCHOR	675.341	-			
														END OPTION AA					
							89				89		EACH	REMOVING SIGNS	675.50	-			
							29				29		EACH	ERECTING SALVAGED SIGNS	675.60	-			
							48				48		EACH	DELINEATOR WITH STEEL POST	676.10	-			
							1225				1225		LF	VEHICLE LOOP DETECTOR	678.22	24			
							1				1		LU	PRICE ADJUSTMENT, FUEL (N.A.B.I.)	690.50	-			
								213			213		LF	SPECIAL PROVISION (BRIDGE RAILING REPAIR, TYPE I)	900.640	-			
							113				113		LF	SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL, GALVANIZED NESTED)	900.640	-			
							300				300		TON	SPECIAL PROVISION (SUBBASE, RAP)	900.680	EST			

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (1)S

FILE NAME: p99c172.dgn  
PROJECT MANAGER: EPD  
DESIGNED BY: CDL  
QUANTITY SHEET #2

PLOT DATE: 10/26/2007  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 9 OF 32

STATE OF VERMONT  
AGENCY OF TRANSPORTATION

ITEM DETAIL SUMMARY SHEET 1

BEGIN STATION	END STATION	POS.	201.31 THIN TRIM FOR SIGNS	203.15 COMMON EXCAV.	203.16 SOLID ROCK EXCAV.	WASTE DITCH. MAT'L	203.30 EARTH BORROW	204.20 TRENCH EXCAV. OF EARTH	204.22 TRENCH EXCAV. OF EARTH, EXP. (N.A.B.I.)	402.13 AGG. SHOULD. RAP	601.99 RE-LAYING PIPE CULVERTS	602.15 CEMENT MASONRY	604.40 CHANGE ELEV. DI	604.41 REHAB. D.I. CLASS I	604.42 CHANGE ELEV. SEWER MANHOLE	616.47 BIT. CONC. GUTTERS AND TRAF. ISLANDS	618.10 P.C.C. SIDEWALK 5 INCH	619.17 YIELDING MARKER POSTS	621.20 STEEL BEAM G.R., GALV	621.205 STEEL BEAM G.R., GALV 8FT. POST	621.21 HD STEEL BEAM G.R. GALV.	621.50 MANUF. TERMINAL SECTION, FLARED	621.60 ANCHOR FOR STEEL BEAM RAIL	621.80 REMOVE & DISP. OF G.R.	653.20 TEMP. EROSION MATTING	676.10 DELINE. WITH STEEL POSTS	900.640 HD STEEL BEAM G.R., NESTED	900.680 SUBBASE, RAP	REMARKS			
			EACH	CY	CY	CY	CY		CY	TON	LF	CY	EACH	EACH	EACH	TON	SY	EACH	LF	LF	LF	EACH	EACH	LF	SY	EACH	LF	TON				
WILLIAMSTOWN																																
200+64	360+62.22	LT&RT	5	50		426			1	240			2	52	1	17.35		75			25									150	ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER	
206+66	207+28.5	LT																	35.25		50		3	61.25			2			INSTALL 12'-6", 16' RAD. W/ANCHOR AT STA. 206+66 AND 18'-9", 16' RAD. WITH ANCHOR AT STA. 207+22.25		
206+83	207+89.25	RT																	41.5		50		2	70.75			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 206+83 AND STA. 207+76.75		
218+84	220+34	LT																	66.5		56.25		2	101.25			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 218+84 AND STA. 220+21.5		
218+96	220+21	RT																	41.5		56.25		2	91.25			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 218+96 AND STA. 220+08.5		
241+57	243+94.5	RT																	241.5				2	241.5			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 241+57 AND STA. 243+82		
244+27	252+27	RT																	604	200			2	804			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 244+27 AND STA. 252+14.5		
252+46	254+08.5	RT																	166.5				2	166.5			2			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 252+46 AND STA. 253+96		
274+30	280+30	LT					50			10									525			2		600	50	2				INSTALL FLARED MTS AT STA. 274+30 AND STA. 279+92.5		
280+70	283+20	LT					25			5									212.5			1	1	250	25	2				INSTALL FLARED MTS AT STA. 280+70 AND BURIED END IN BACKSLOPE AT STA. 283+20		
281+11	284+93	RT																	387.5				1	387.5			1			BURIED END IN BACKSLOPE AT STA. 281+11. TIE NORTH END INTO EXISTING GUARDRAIL AT STA. 284+93		
285+37	290+30	RT					25			5									462.5			1		462.5	25	1				INSTALL FLARED MTS AT STA. 289+92.5. TIE SOUTH END INTO EXISTING GUARDRAIL AT STA. 285+37		
290+65	307+40	RT					25			5									1581.25		56.25	1	1	1637.5	25	2				INSTALL FLARED MTS AT STA. 290+65 AND 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 307+27.5		
305+71	316+83.5	LT					50			10									818.75	200	56.25	2		1075	50	2				INSTALL FLARED MTS AT STA. 305+71 AND STA. 316+46		
318+75	319+87.5	RT					50			10									37.5			2		112.5	50	2				INSTALL FLARED MTS AT STA. 318+75 AND STA. 319+50		
323+35	334+60	LT					50			10									1050			2		1125	50	2				INSTALL FLARED MTS AT STA. 323+35 AND STA. 334+22.5		
339+60	340+91.25	RT					25			5									77			1	2	112.5	25	2	25			INSTALL 25', 16' RAD. W/ANCHORS AT STA. 339+65 AND STA. 339+81 AND INSTALL FLARED MTS AT STA. 340+53.75		
339+65	340+77.5	LT					50			10									12.5			2		112.5	50	2	25			INSTALL FLARED MTS AT STA. 339+65 AND STA. 340+40		
346+25	351+25	LT					50			10									425			2		500	50	2				INSTALL FLARED MTS AT STA. 346+25 AND STA. 350+87.5		
354+86	2+75.5	LT					25			5									800			1		854	25	2				INSTALL FLARED MTS AT STA. 354+86 AND 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 2+63		
<b>SUBTOTAL</b>			5	50	0	426	425	0	1	325	0	0	2	52	1	17.35	0	75	7586.25	400	350	17	20	8765.5	425	36	50	150				

<b>ITEM DETAIL SUMMARY SHEET 1</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN
	PROJECT NUMBER: STP 2210 (I)S
	FILE NAME: p99cl72.dgn
	PLOT DATE: 29-APR-2009 13:3
PROJECT LEADER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD
PLOT FILE: p99cl721dssl.i	SHEET 10 OF 32

STATE OF VERMONT  
AGENCY OF TRANSPORTATION

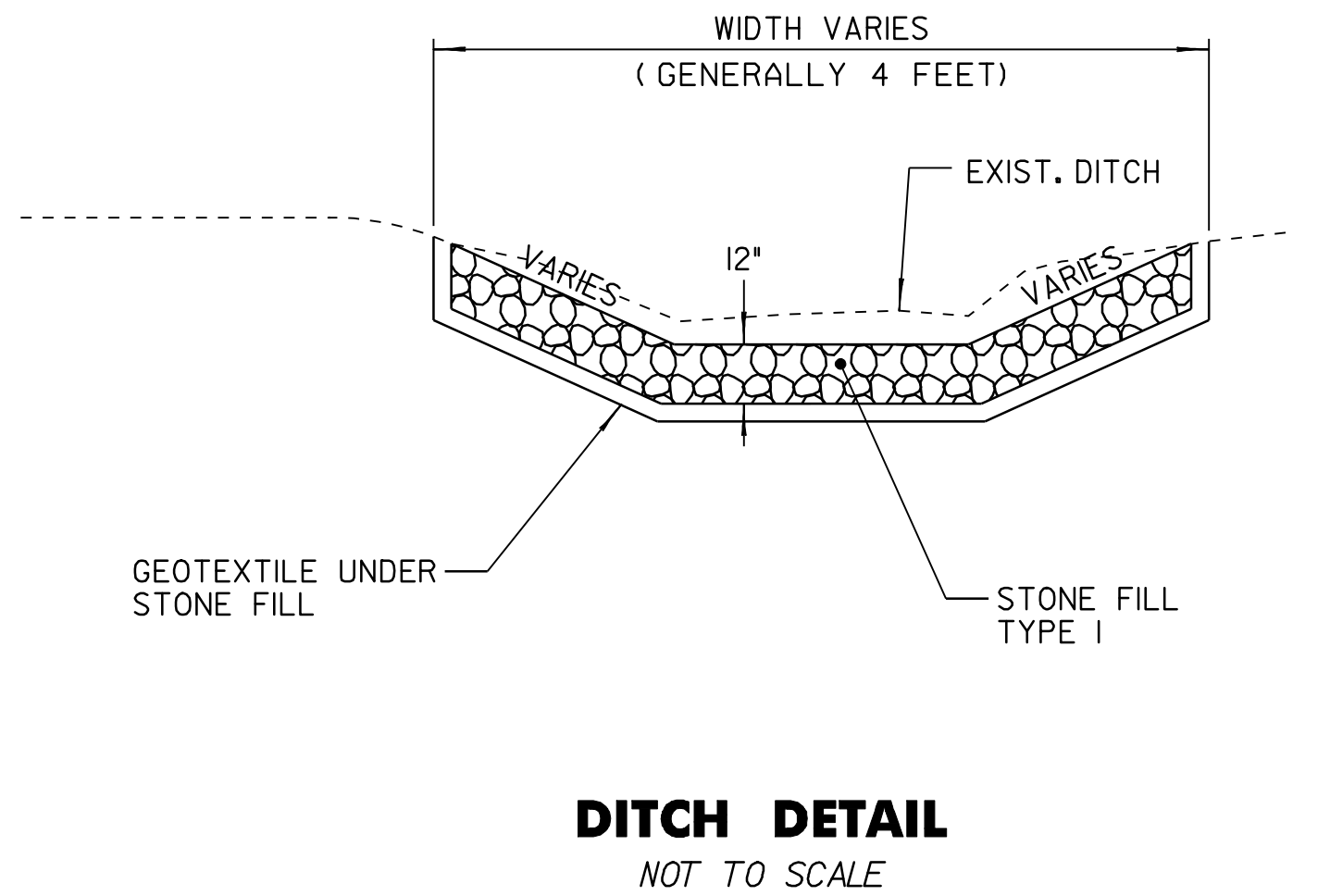
ITEM DETAIL SUMMARY SHEET 2

BEGIN STATION	END STATION	POS.	201.31 THIN TRIM FOR SIGNS	203.15 COMMON EXCAV.	203.16 SOLID ROCK EXCAV.	WASTE DITCH. MAT'L	203.30 EARTH BORROW	204.20 TRENCH EXCAV. OF EARTH	204.22 TRENCH EXCAV. OF EARTH, EXP. (N.A.B.I.)	402.13 AGG. SHOULD. RAP	601.99 RE-LAYING PIPE CULVERTS	602.15 CEMENT MASONRY	604.40 CHANGE ELEV. DI	604.41 REHAB. D. I. CLASS 1	604.42 CHANGE ELEV. SEWER MANHOLE	616.47 BIT. CONC. GUTTERS AND TRAF. ISLANDS	618.10 P. C. C. SIDEWALK, 5 INCH	619.17 YIELDING MARKER POSTS	621.20 STEEL BEAM G. R., GALV	621.205 STEEL BEAM G. R., GALV 8FT. POST	621.21 HD STEEL BEAM G. R. GALV.	621.50 MANUF. TERMINAL SECTION, FLARED	621.60 ANCHOR FOR STEEL BEAM RAIL	621.80 REMOVE & DISP. OF G. R.	653.20 TEMP. EROSION MATTING	676.10 DELINE. WITH STEEL POSTS	900.640 HD STEEL BEAM G. R., NESTED	900.680 SUBBASE, RAP	REMARKS	
			EACH	CY	CY	CY	CY	CY	CY	TON	LF	CY	EACH	EACH	EACH	TON	SY	EACH	LF	LF	LF	EACH	EACH	LF	SY	EACH	LF	TON		
BARRE																														
0+00	90+40.00	LT&RT	4	50		17				176			1	37	2	71.45													150	ESTIMATED QUANTITIES TO BE USED AS DIRECTED BY THE RESIDENT ENGINEER
20+56	28+68.5	LT																	816.5				2	812.5		2				INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 20+56 AND STA. 28+56
28+97	30+22	LT																	129				2	129		2				INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 28+97 AND STA. 30+09.5
37+19	37+94	LT																	54				2	70		2	25			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 37+19 AND STA. 37+81.5
37+35	38+10	RT																	41.5				2	70		2	37.5			INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 37+35 AND STA. 37+97.5
	44+00	RT						2.9			4	1.3																		
56+85	58+35	LT																	154				2	150		2				INSTALL 12'-6" LONG, 16' RADIUS WITH ANCHOR AT STA. 56+85 AND STA. 58+22.5
57+35	58+47.5	RT																	29		50		2	25		2				
77+31		RT			1.5												9													REMOVE AND REPLACE PORTLAND CEMENT CONCRETE SIDEWALK TO REHAB. EXISTING DROP INLET
<b>SUBTOTAL, THIS SHEET</b>			4	50	1.5	17	0	2.9	0	176	4	1.3	1	37	2	71.45	9	18	1224	0	100	0	12	1256.5	0	12	62.5	150		
<b>SUBTOTAL, SHEET 10</b>			5	50	0	426	425	0	1	325	0	0	2	52	1	17.35	0	75	7586.25	400	350	17	20	8765.5	425	36	50	150		
<b>ROUNDING</b>			-	-	0.5	-	-	0.1	-	24	-	0.7	-	-	-	1.2	-	2	-	-	-	-	-	-	-	-	-	-		
<b>GRAND TOTAL</b>			9	100	2.0	443	425	3.0	1	525	4	2.0	3	89	3	90	9	95	8810.25	400	450	17	32	10022	425	48	112.5	300		

**ITEM  
DETAIL  
SUMMARY  
SHEET 2**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S  
FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl721dss2.i  
PLOT DATE: 29-APR-2009 13:30  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET II OF 32

LOCATION				FEET OF DITCHING			MISC. ITEMS			REMARKS	LOCATION				FEET OF DITCHING			MISC. ITEMS			REMARKS						
SITE	STATION	STATION	POS.	PERCENT GRADE			653.20	613.10	649.31		SITE	STATION	STATION	POS.	PERCENT GRADE			653.20	613.10	649.31							
				0-1	1-2.5	2.5-10									TEMP. EROS. MATT.	STONE FILL TYP. I	GEOT. UNDER STONE FILL					0-1	1-2.5	2.5-10	TEMP. EROS. MATT.	STONE FILL TYP. I	GEOT. UNDER STONE FILL
VT I4 WILLIAMSTOWN																											
1	200+64	204+50	RT		386		172																				
2	206+97		RT	10																	CLEAN OUTLET						
3	241+82	244+46	LT			264		39	117																		
4	245+52	248+16	LT			264		39	117																		
5	257+14	270+25	RT			1311		194	583																		
6	264+27		LT	10																	CLEAN INLET						
7	264+47		LT	10																	CLEAN OUTLET						
8	267+23		LT	10																	CLEAN OUTLET						
9	270+75	274+00	RT		375		166																				
10	273+50	273+70	LT	20																							
11	273+70		LT	20																							
12	274+40	276+40	LT		200		89																				
13	274+70	280+36	RT			566		84	252																		
14	279+61		LT	10																	CLEAN OUTLET						
15	280+60	297+00	LT		1640		728																				
16	288+64		RT	10																	CLEAN OUTLET						
17	297+60	300+00	LT		240		107																				
18	300+00	303+00	LT			300		44	133																		
19	304+00	305+18	LT		118		52																				
20	308+75	311+50	RT		275		122																				
21	311+96		RT	10																	CLEAN INLET						
22	312+30	316+25	RT		395		176														CLEAN OUTLETS AT STA. 312+30 & 316+25						
23	316+27		LT	10																	CLEAN OUTLET						
24	324+72	331+75	RT		703		312																				
25	333+50	337+10	RT		360		160																				
26	337+27	337+57	LT		30		13																				
27	337+60	339+00	RT		140		63																				
28	340+20	350+85	RT		1065		473																				
29	345+24		LT	10																	CLEAN OUTLET						
30	352+00	353+25	RT		125		56																				
31	354+25	0+15	RT		652		290																				
32	360+19		LT	10																	CLEAN OUTLET						
VT I4 BARRE																											
33	19+11		LT	10																	CLEAN OUTLET						
34	20+00	21+00	RT		100		45																				
35	25+09	25+39	RT		30		13																				
36	87+00	87+77	RT	77																							
SUBTOTAL					227	6834	2705	3037	400	1202																	
GUARDRAIL END SECTIONS								425	-	-																	
ROUNDING								38	10	13																	
TOTAL								3500	410	1215																	



**DITCH DETAIL**  
NOT TO SCALE

NOTES:

- PIPE INLET AND OUTLET AREAS AND DITCH CLEANING THROUGHOUT PROJECT SHALL BE PERFORMED AT LOCATIONS IDENTIFIED ON THIS SHEET AND AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER THE APPLICABLE EQUIPMENT RENTAL ITEMS).
- ESTIMATED QUANTITIES OF TEMPORARY EROSION MATTING, SEED, AND STONE FILL TYPE I HAVE BEEN INCLUDED. DITCHES WITH A GRADE LESS THAN 1 PERCENT SHALL BE SEEDED. EROSION MATTING SHALL BE USED IN ALL DITCHES WITH A GRADE BETWEEN 1 AND 2.5 PERCENT. STONE FILL TYPE I SHALL BE USED IN ALL DITCHES WITH A GRADE BETWEEN 2.5 AND 10 PERCENT.

<b>DITCH CLEANING DETAIL SHEET</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	
	PROJECT NUMBER: STP 2210 (I)	
	FILE NAME: p99cl72.dgn	PLOT DATE: 29-APR-2009 13:30
	PROJECT LEADER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD	
PLOT FILE: p99cl72dcds.i	SHEET 12 OF 32	

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 200+64 - STA. 209+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 206+40 LT (EDGELINES, TH# 100)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 200+64 - STA. 209+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 206+40 LT (CENTERLINE, TH# 100)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 206+15 LT - STA. 206+40 LT (TH# 100)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 206+30 LT "STOP" (TH# 100)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 200+64 - STA. 209+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 206+40 LT (EDGELINES, TH# 100)

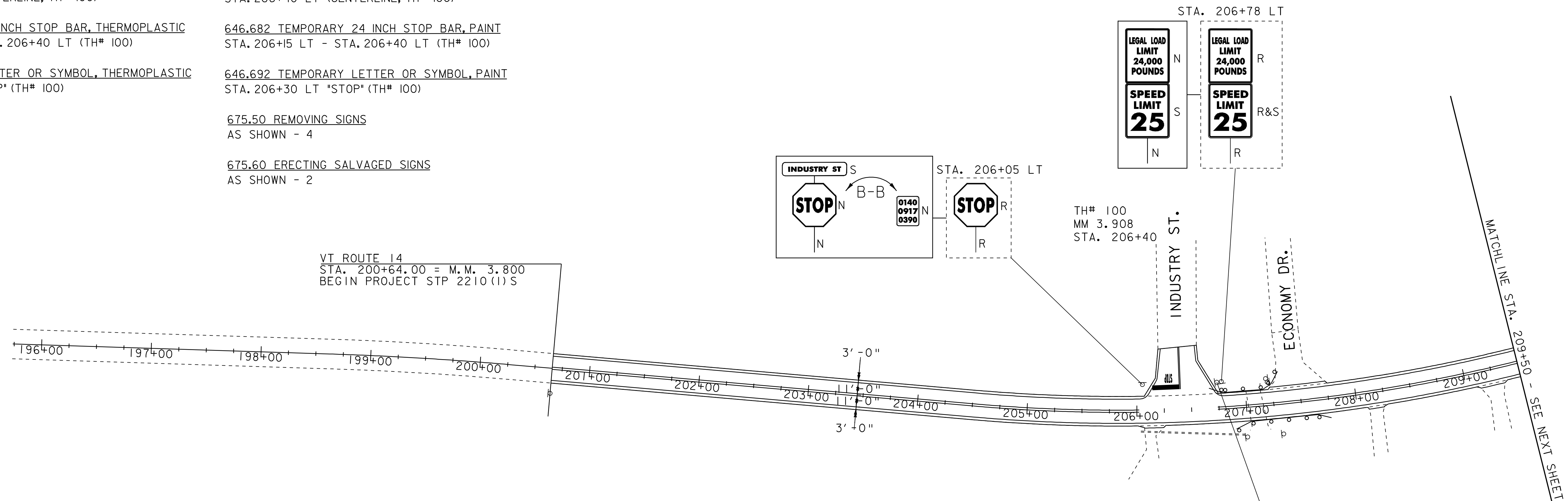
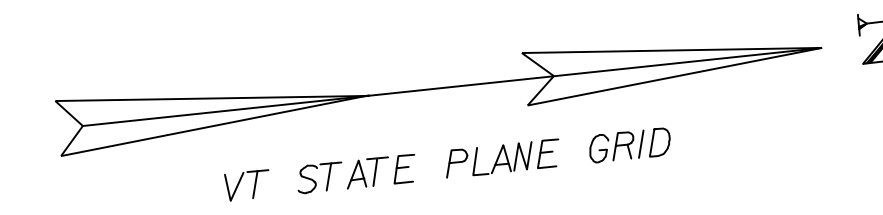
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 200+64 - STA. 209+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 206+40 LT (CENTERLINE, TH# 100)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 206+15 LT - STA. 206+40 LT (TH# 100)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 206+30 LT "STOP" (TH# 100)

675.50 REMOVING SIGNS  
 AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 2



VT ROUTE 14  
 STA. 200+64.00 = M.M. 3.800  
 BEGIN PROJECT STP 2210(I)S

TH# 100  
 MM 3.908  
 STA. 206+40

MATCHLINE STA. 209+50 - SEE NEXT SHEET

525.10 REMOVAL OF EXISTING RAILING  
 STA. 207+03.50 LT - STA. 207+22.25 LT  
 STA. 207+20.50 RT - STA. 207+39.25 RT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 206+66.00 LT - STA. 206+78.50 LT  
 STA. 206+83.00 RT - STA. 206+95.50 RT  
 STA. 207+22.25 LT - STA. 207+28.50 LT (48.75 LF)  
 STA. 207+64.25 RT - STA. 207+89.25 RT

206+66.00 LT ~ 206+78.5 LT  
 207+22.25 LT ~ 207+47.25 LT

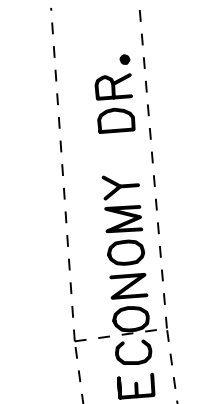
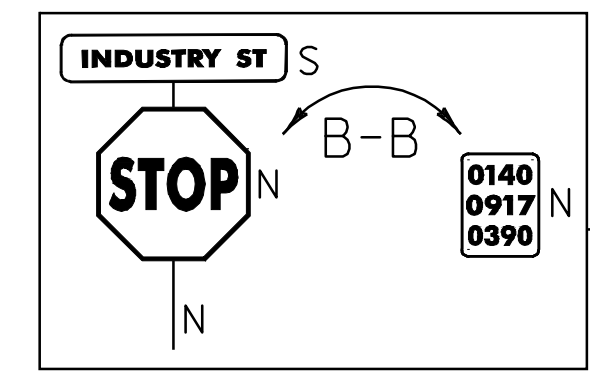
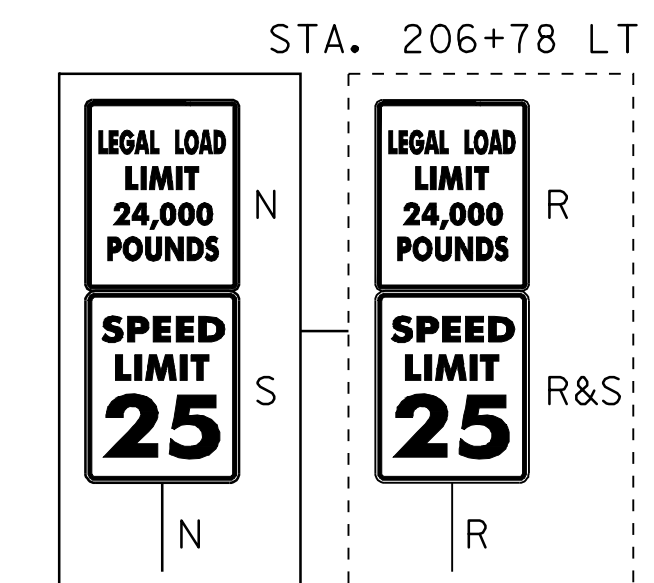
676.10 DELINEATOR WITH STEEL POST  
 STA. 206+66.00 LT  
 STA. 206+83.00 RT  
 STA. 207+28.50 LT  
 STA. 207+76.75 RT

621.21HD STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 206+78.50 LT - STA. 207+03.50 LT  
 STA. 206+95.50 RT - STA. 207+20.50 RT  
 STA. 207+39.25 RT - STA. 207+64.25 RT

900.640 SPECIAL PROVISION (BRIDGE RAILING REPAIR, TYPE I)  
 STA. 207+03.50 LT - STA. 207+22.25 LT  
 STA. 207+20.50 RT - STA. 207+39.25 RT

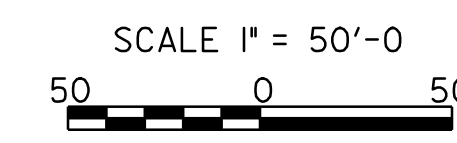
621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 206+66.00 LT  
 STA. 206+83.00 RT  
~~STA. 207+22.25 LT ELIMINATE~~  
 STA. 207+28.50 LT  
 STA. 207+76.75 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 206+66.00 LT - STA. 207+03.50 LT  
 STA. 206+83.00 RT - STA. 207+20.50 RT  
 STA. 207+22.25 LT - STA. 207+28.50 LT (23.75') 37.5'  
 STA. 207+39.25 RT - STA. 207+72.50 RT



**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



<b>PROJECT LAYOUT SHEET 1</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	PLOT DATE: 29-APR-2009 13:3
	PROJECT NUMBER: STP 2210(I)S	DRAWN BY: RHB
	FILE NAME: p99c172.dgn	CHECKED BY: EPD
	DESIGNED BY: CDL	SHEET 13 OF 32

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 209+50 - STA. 223+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 215+64 RT (EDGELINES, TH# 99)  
 STA. 221+04 LT (EDGELINES, TH# 40)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 209+50 - STA. 223+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 215+64 RT (CENTERLINE, TH# 99)  
 STA. 221+04 LT (CENTERLINE, TH# 40)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 215+64 RT - STA. 215+85 RT (TH# 99)  
 STA. 220+88 LT - STA. 221+04 LT (TH# 40)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 215+80 RT "STOP" (TH# 99)  
 STA. 221+00 LT "STOP" (TH# 40)

675.50 REMOVING SIGNS  
 AS SHOWN - 6

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 1

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 209+50 - STA. 223+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 215+64 RT (EDGELINES, TH# 99)  
 STA. 221+04 LT (EDGELINES, TH# 40)

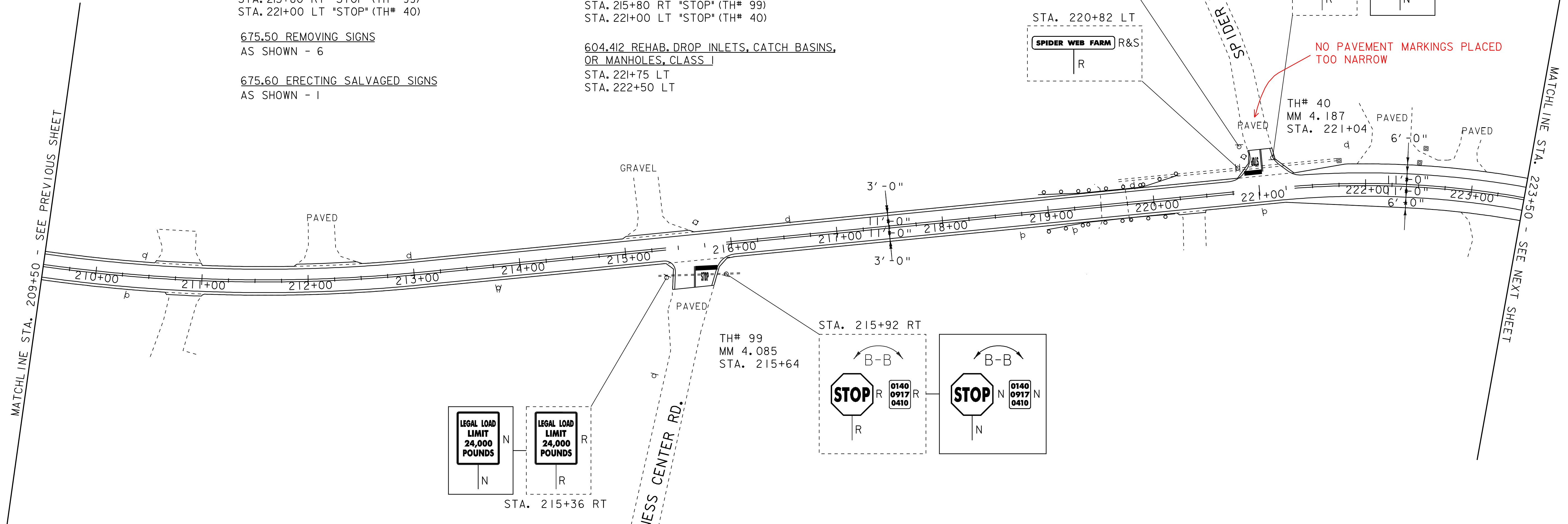
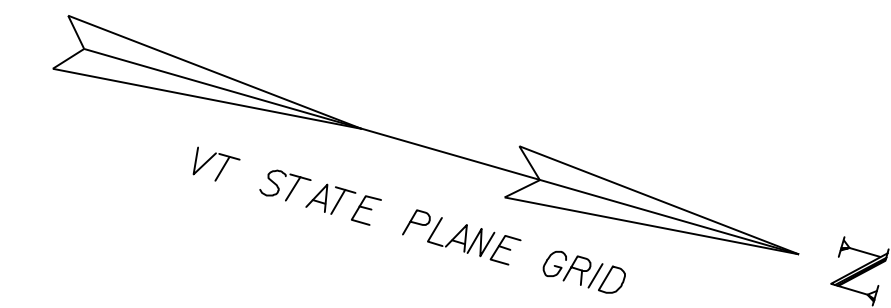
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 209+50 - STA. 223+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 215+64 RT (CENTERLINE, TH# 99)  
 STA. 221+04 LT (CENTERLINE, TH# 40)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 215+64 RT - STA. 215+85 RT (TH# 99)  
 STA. 220+88 LT - STA. 221+04 LT (TH# 40)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 215+80 RT "STOP" (TH# 99)  
 STA. 221+00 LT "STOP" (TH# 40)

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I  
 STA. 221+75 LT  
 STA. 222+50 LT

MATCHLINE STA. 209+50 - SEE PREVIOUS SHEET



525.10 REMOVAL OF EXISTING RAILING  
 STA. 219+46.50 LT - STA. 219+77.75 LT  
 STA. 219+52.25 RT - STA. 219+83.50 RT

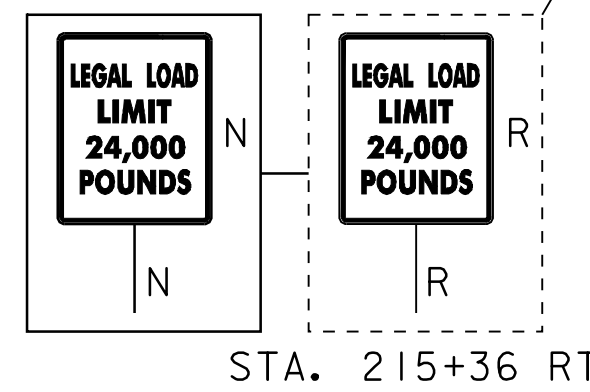
621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 218+84.00 LT - STA. 219+21.50 LT  
 STA. 218+96.00 RT - STA. 219+21.00 RT  
 STA. 220+08.50 RT - STA. 220+21.00 RT  
 STA. 220+09.00 LT - STA. 220+34.00 LT

621.21HD STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 219+21.00 RT - STA. 219+52.25 RT  
 STA. 219+21.50 LT - STA. 219+46.50 LT  
 STA. 219+77.75 LT - STA. 220+09.00 LT  
 STA. 219+83.50 RT - STA. 220+08.50 RT

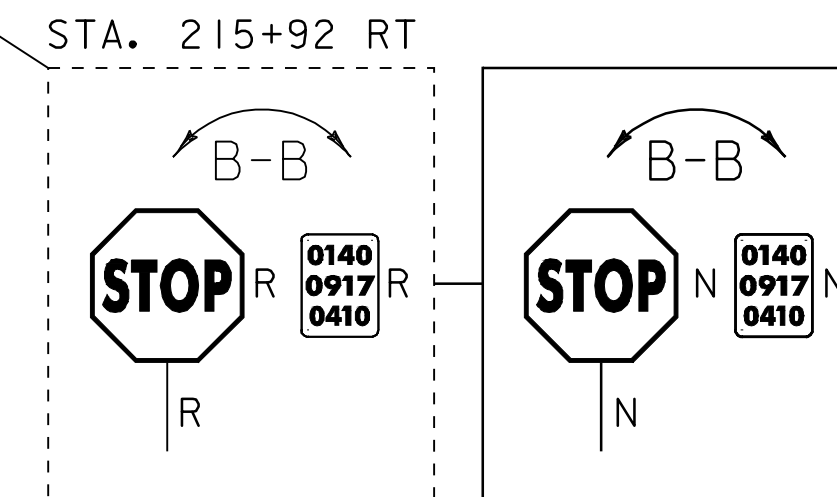
621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 218+84.00 LT  
 STA. 218+96.00 RT  
 STA. 220+08.50 RT  
 STA. 220+21.50 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 218+92.75 LT - STA. 219+46.50 LT  
 STA. 218+97.25 RT - STA. 219+52.25 RT  
 STA. 219+77.75 LT - STA. 220+25.25 LT  
 STA. 219+83.50 RT - STA. 220+19.75 RT

TH# 99  
 MM 4.085  
 STA. 215+64



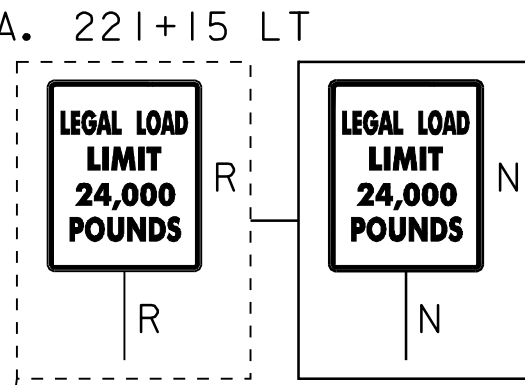
STA. 215+36 RT



STA. 215+92 RT

676.10 DELINEATOR WITH STEEL POST  
 STA. 218+84.00 LT  
 STA. 218+96.00 RT  
 STA. 220+21.00 RT  
 STA. 220+34.00 LT

900.640 SPECIAL PROVISION (BRIDGE RAILING REPAIR, TYPE I)  
 STA. 219+46.50 LT - STA. 219+77.75 LT  
 STA. 219+52.25 RT - STA. 219+83.50 RT

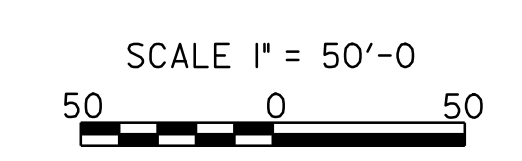


NO PAVEMENT MARKINGS PLACED TOO NARROW

TH# 40  
 MM 4.187  
 STA. 221+04

**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



<b>PROJECT LAYOUT SHEET 2</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	PLOT DATE: 29-APR-2009 13:3
	PROJECT NUMBER: STP 2210(I)S	DRAWN BY: RHB
	FILE NAME: p99cl72.dgn	CHECKED BY: EPD
	PROJECT LEADER: EPD	DESIGNED BY: CDL
	PLOT FILE: p99cl72102.1	SHEET 14 OF 32

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 223+50 - STA. 238+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 226+80 LT (EDGELINES, TH# 39)  
 STA. 233+54 LT (EDGELINES, TH# 73)  
 STA. 233+74 RT (EDGELINES, TH# 72)  
 STA. 235+55 RT (EDGELINES, TH# 71)  
 STA. 237+13 LT (EDGELINES, VT 64)

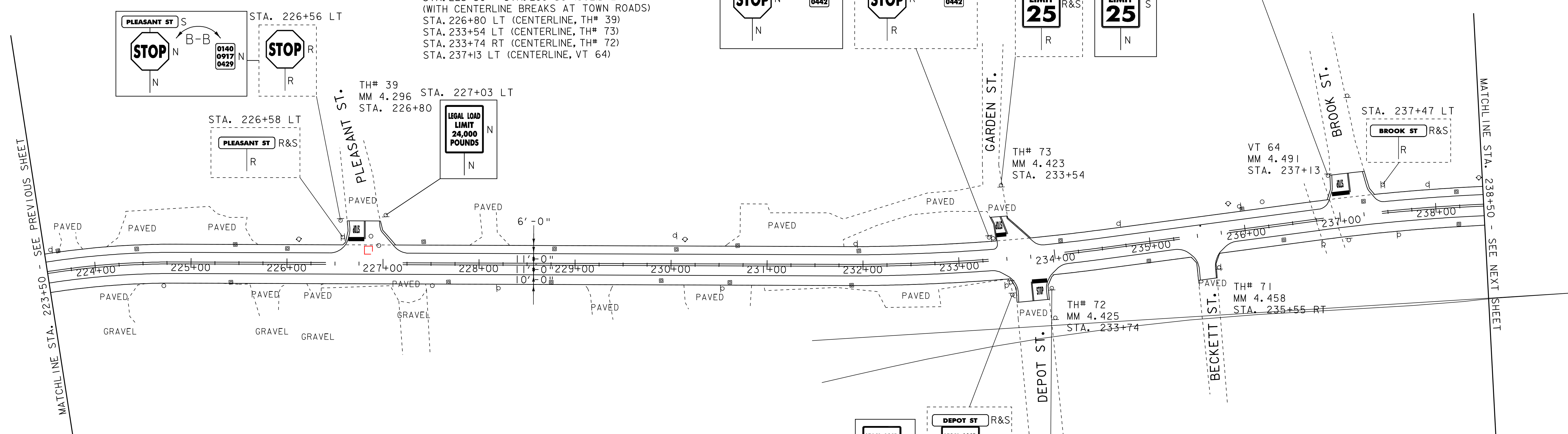
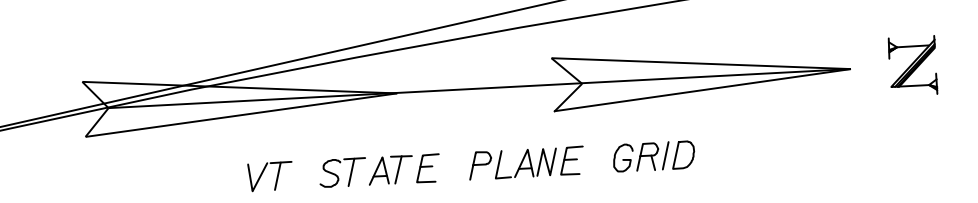
646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 223+50 - STA. 238+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 226+80 LT (CENTERLINE, TH# 39)  
 STA. 233+54 LT (CENTERLINE, TH# 73)  
 STA. 233+74 RT (CENTERLINE, TH# 72)  
 STA. 237+13 LT (CENTERLINE, VT 64)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 226+63 LT - STA. 226+80 LT (TH# 39)  
 STA. 233+42 LT - STA. 233+54 LT (TH# 73)  
 STA. 233+74 RT - STA. 233+94 RT (TH# 72)  
 STA. 236+91LT - STA. 237+13 LT (VT 64)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 223+50 - STA. 238+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 226+80 LT (EDGELINES, TH# 39)  
 STA. 233+54 LT (EDGELINES, TH# 73)  
 STA. 233+74 RT (EDGELINES, TH# 72)  
 STA. 235+55 RT (EDGELINES, TH# 71)  
 STA. 237+13 LT (EDGELINES, VT 64)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 223+50 - STA. 238+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 226+80 LT (CENTERLINE, TH# 39)  
 STA. 233+54 LT (CENTERLINE, TH# 73)  
 STA. 233+74 RT (CENTERLINE, TH# 72)  
 STA. 237+13 LT (CENTERLINE, VT 64)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 226+63 LT - STA. 227+80 LT (TH# 39)  
 STA. 233+42 LT - STA. 233+54 LT (TH# 73)  
 STA. 233+74 RT - STA. 233+94 RT (TH# 72)  
 STA. 236+91LT - STA. 237+13 LT (VT 64)



646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 226+75 LT "STOP" (TH# 39)  
 STA. 233+50 LT "STOP" (TH# 73)  
 STA. 233+85 RT "STOP" (TH# 72)  
 STA. 237+00 LT "STOP" (VT 64)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 226+75 LT "STOP" (TH# 39)  
 STA. 233+50 LT "STOP" (TH# 73)  
 STA. 233+85 RT "STOP" (TH# 72)  
 STA. 237+00 LT "STOP" (VT 64)

604.40 CHANGING ELEVATION OF DROP INLETS,  
 CATCH BASINS, OR MANHOLES  
 STA. 226+96 LT  
 STA. 238+23 RT

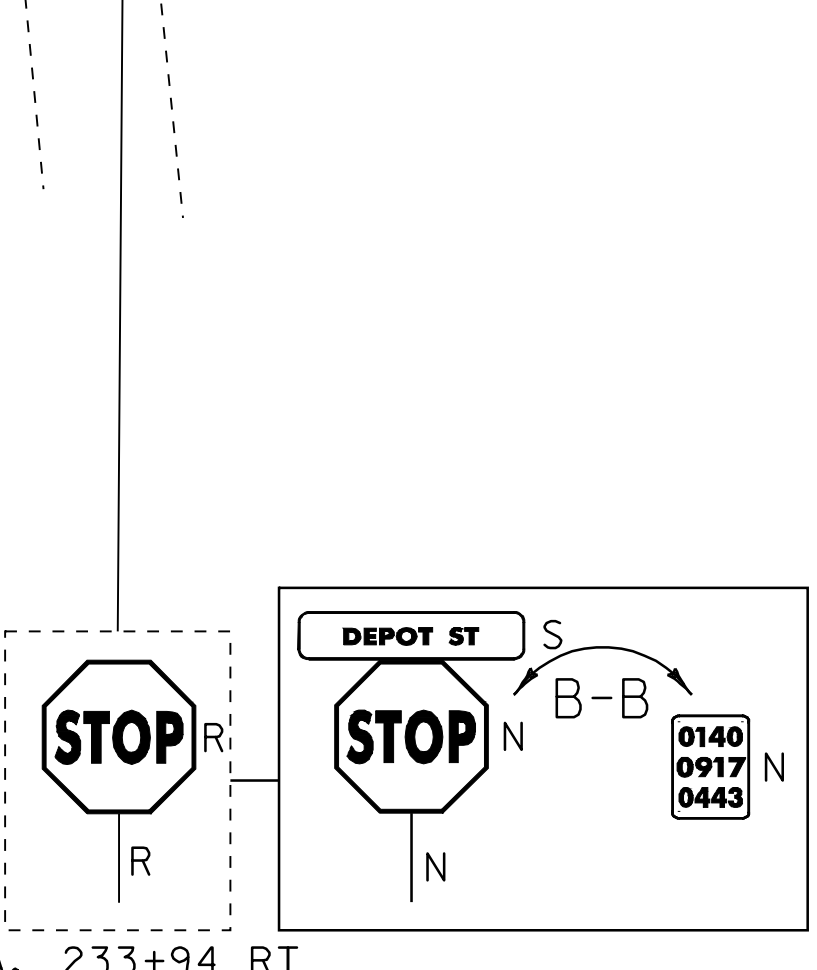
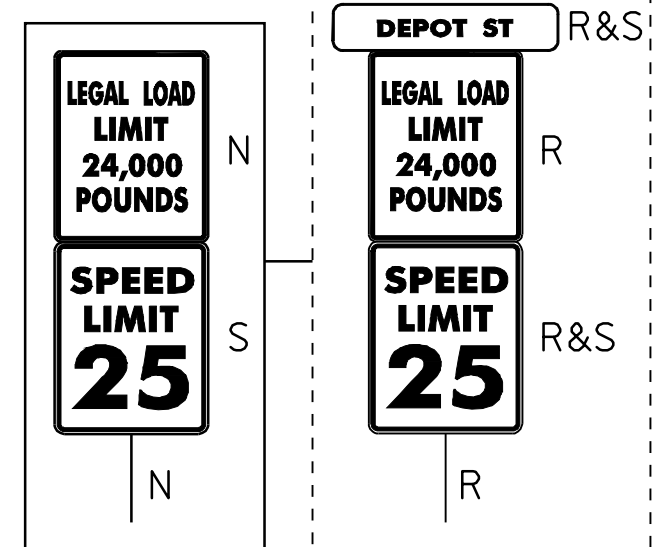
604.42 CHANGING ELEVATION OF SEWER MANHOLES  
 STA. 226+88 LT

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I

STA. 223+63 LT	STA. 228+78 LT	STA. 234+96 RT
STA. 224+45 LT	STA. 229+90 RT	STA. 236+00 LT
STA. 225+41RT	STA. 230+05 LT	STA. 236+39 RT
STA. 225+46 LT	STA. 230+61RT	STA. 237+26 LT
STA. 226+00 LT	STA. 230+69 LT	STA. 238+22 LT
STA. 227+42 LT	STA. 231+88 RT	<b>STA. 226+ LT</b>
STA. 227+68 RT	STA. 231+96 LT	
STA. 228+75 RT	STA. 234+07 LT	

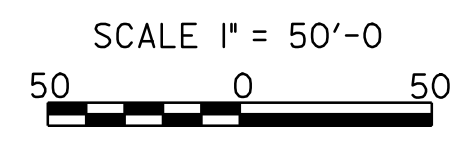
675.50 REMOVING SIGNS  
 AS SHOWN - 14

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 6



**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



**PROJECT LAYOUT SHEET 3**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	PLOT DATE: 29-APR-2009 13:3
PROJECT NUMBER: STP 2210(I)S	DRAWN BY: RHB
FILE NAME: p99ci72.dgn	CHECKED BY: EPD
DESIGNED BY: CDL	SHEET 15 OF 32
PLOT FILE: p99ci72103.1	

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 238+50 - STA. 253+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 241+06 RT (EDGELINES, TH# 1)  
 STA. 241+61LT (EDGELINES, TH# 24)  
 STA. 248+54 LT (EDGELINES, TH# 78)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 238+50 - STA. 253+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 241+06 RT (CENTERLINE, TH# 1)  
 STA. 241+61LT (CENTERLINE, TH# 24)  
 STA. 248+54 LT (CENTERLINE, TH# 78)

646.452 DURABLE 8 INCH YELLOW LINE, THERMOPLASTIC  
 STA. 241+03 RT (HATCHED, TH# 1)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 241+06 RT - STA. 241+26 RT (TH# 1)  
 STA. 241+46 LT - STA. 241+61LT (TH# 24)  
 STA. 248+42 LT - STA. 248+54 LT (TH# 78)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 241+20 RT "STOP" (TH# 1)  
 STA. 241+55 LT "STOP" (TH# 24)  
 STA. 248+40 LT "STOP" (TH# 78)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 238+50 - STA. 253+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 241+06 RT (EDGELINES, TH# 1)  
 STA. 241+61LT (EDGELINES, TH# 24)  
 STA. 248+54 LT (EDGELINES, TH# 78)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 238+50 - STA. 253+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 241+06 RT (CENTERLINE, TH# 1)  
 STA. 241+61LT (CENTERLINE, TH# 24)  
 STA. 248+54 LT (CENTERLINE, TH# 78)

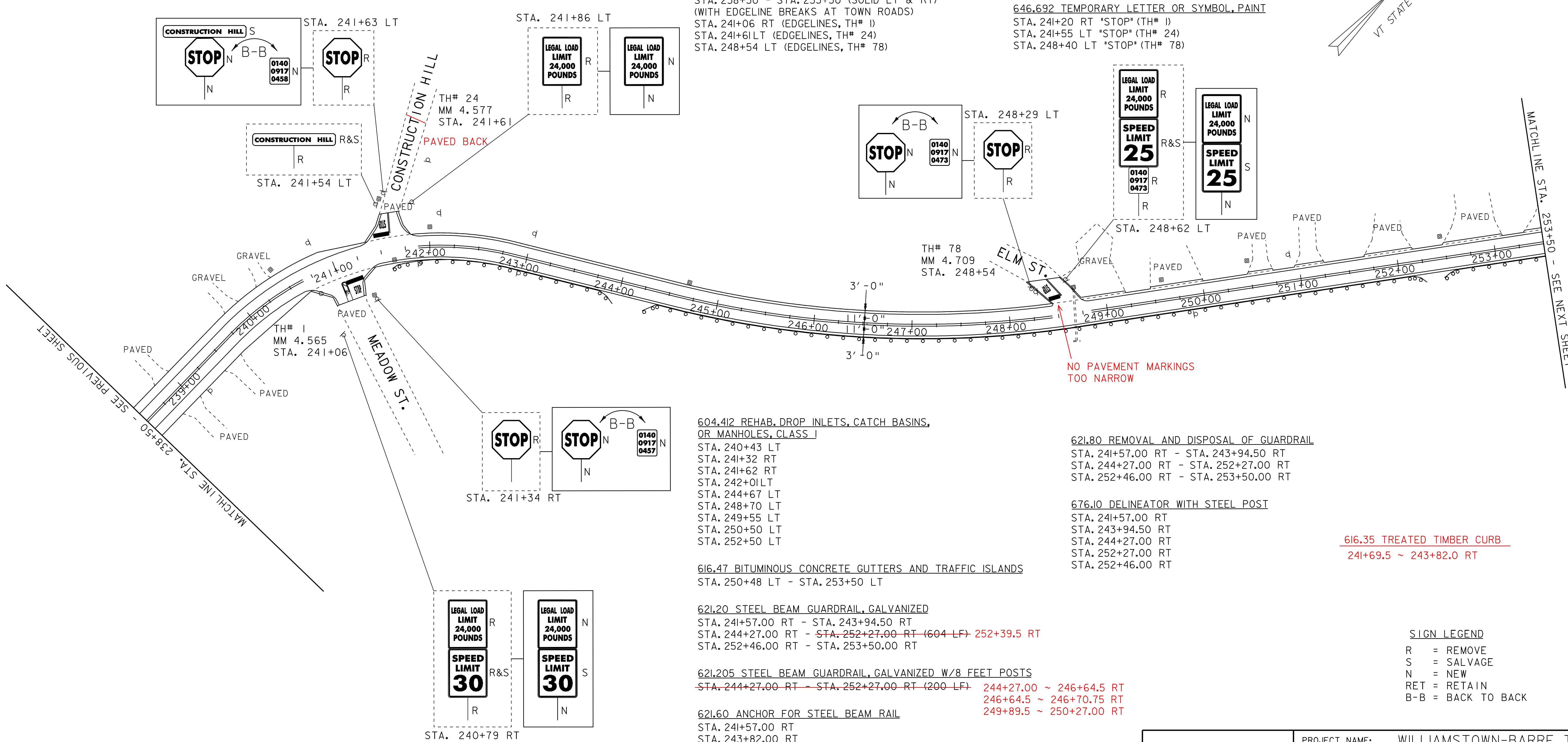
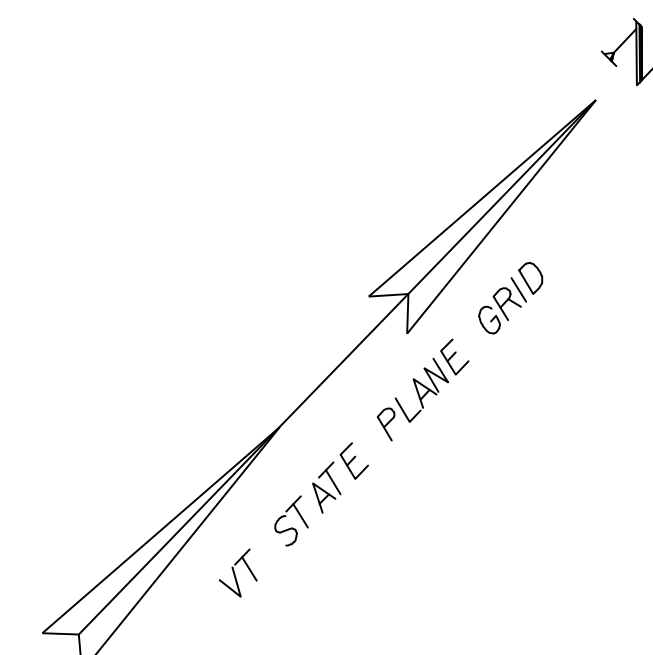
646.652 TEMPORARY 8 INCH YELLOW LINE, PAINT  
 STA. 241+03 RT (HATCHED, TH# 1)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 241+06 RT - STA. 241+26 RT (TH# 1)  
 STA. 241+46 LT - STA. 241+61LT (TH# 24)  
 STA. 248+42 LT - STA. 248+54 LT (TH# 78)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 241+20 RT "STOP" (TH# 1)  
 STA. 241+55 LT "STOP" (TH# 24)  
 STA. 248+40 LT "STOP" (TH# 78)

675.50 REMOVING SIGNS  
 AS SHOWN - 10

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 3



604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I

STA. 240+43 LT  
 STA. 241+32 RT  
 STA. 241+62 RT  
 STA. 242+01LT  
 STA. 244+67 LT  
 STA. 248+70 LT  
 STA. 249+55 LT  
 STA. 250+50 LT  
 STA. 252+50 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 241+57.00 RT - STA. 243+94.50 RT  
 STA. 244+27.00 RT - STA. 252+27.00 RT  
 STA. 252+46.00 RT - STA. 253+50.00 RT

676.10 DELINEATOR WITH STEEL POST  
 STA. 241+57.00 RT  
 STA. 243+94.50 RT  
 STA. 244+27.00 RT  
 STA. 252+27.00 RT  
 STA. 252+46.00 RT

616.35 TREATED TIMBER CURB  
 241+69.5 ~ 243+82.0 RT

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS  
 STA. 250+48 LT - STA. 253+50 LT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 241+57.00 RT - STA. 243+94.50 RT  
 STA. 244+27.00 RT - ~~STA. 252+27.00 RT (604 LF)~~ 252+39.5 RT  
 STA. 252+46.00 RT - STA. 253+50.00 RT

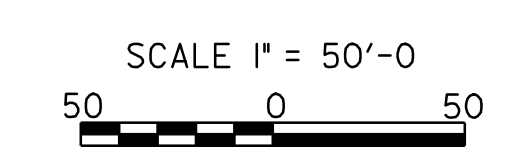
621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS  
~~STA. 244+27.00 RT - STA. 252+27.00 RT (200 LF)~~ 244+27.00 ~ 246+64.5 RT  
 246+64.5 ~ 246+70.75 RT  
 249+89.5 ~ 250+27.00 RT

621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 241+57.00 RT  
 STA. 243+82.00 RT  
 STA. 244+27.00 RT  
~~STA. 252+14.50 RT - 252+39.5 RT~~  
 STA. 252+46.00 RT

SIGN LEGEND

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



<b>PROJECT LAYOUT SHEET 4</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN
	PROJECT NUMBER: STP 2210(I)S
	FILE NAME: p99c172.dgn
	PLOT DATE: 29-APR-2009 13:3
PROJECT LEADER: EPD	DRAWN BY: RHB
DESIGNED BY: CDL	CHECKED BY: EPD
PLOT FILE: p99c172104.1	SHEET 16 OF 32

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 253+50 - STA. 268+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 254+55 RT (EDGELINES, TH# 25)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 253+50 - STA. 268+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 254+55 RT (CENTERLINE, TH# 25)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 254+55 RT - STA. 254+70 RT (TH# 25)

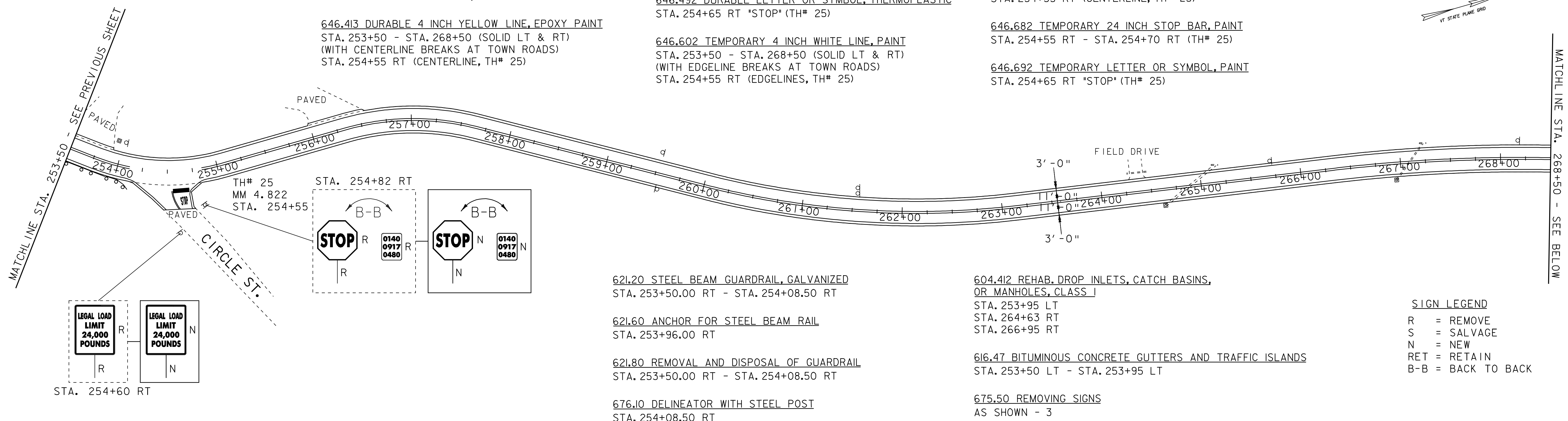
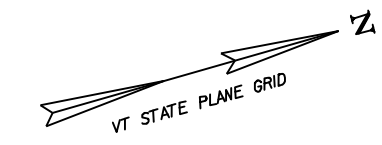
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 254+65 RT "STOP" (TH# 25)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 253+50 - STA. 268+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 254+55 RT (EDGELINES, TH# 25)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 253+50 - STA. 268+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 254+55 RT (CENTERLINE, TH# 25)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 254+55 RT - STA. 254+70 RT (TH# 25)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 254+65 RT "STOP" (TH# 25)



621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 253+50.00 RT - STA. 254+08.50 RT

621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 253+96.00 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 253+50.00 RT - STA. 254+08.50 RT

676.10 DELINEATOR WITH STEEL POST  
 STA. 254+08.50 RT

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I  
 STA. 253+95 LT  
 STA. 264+63 RT  
 STA. 266+95 RT

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS  
 STA. 253+50 LT - STA. 253+95 LT

675.50 REMOVING SIGNS  
 AS SHOWN - 3

**SIGN LEGEND**

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK

**621.205 STEEL BEAM GUARDRAIL W/ 8' POSTS**

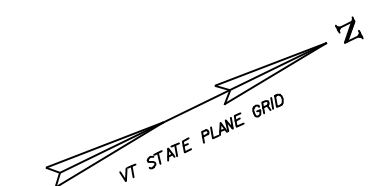
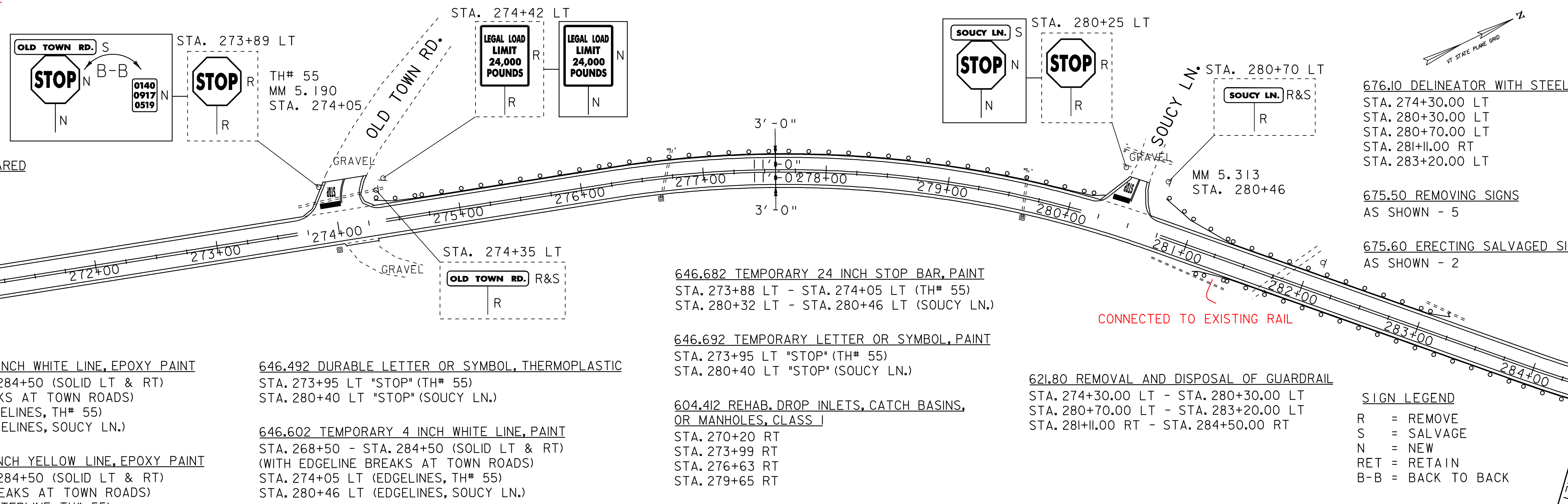
- 281+72.5 ~ 282+22.5 RT
- 284+22.5 ~ 284+72.5 RT
- 281+82.5 ~ 282+20.0 LT

**621.20 STEEL BEAM GUARDRAIL, GALVANIZED**

- 274+30.0 STA. 274+67.50 LT - STA. 279+92.50 LT
- 281+20.0 STA. 281+07.50 LT - STA. 283+20.00 LT
- 281+35.0 STA. 281+11.00 RT - STA. 284+50.00 RT

**621.50 MANUFACTURED TERMINAL SECTION, FLARED**

- STA. 274+30.00 LT - STA. 274+67.50 LT
- STA. 279+92.50 LT - STA. 280+30.00 LT
- STA. 280+70.00 LT - STA. 281+07.50 LT
- 280+82.5 LT ~ 281+20.0 LT



646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 273+88 LT - STA. 274+05 LT (TH# 55)  
 STA. 280+32 LT - STA. 280+46 LT (SOUCY LN.)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 273+95 LT "STOP" (TH# 55)  
 STA. 280+40 LT "STOP" (SOUCY LN.)

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I  
 STA. 270+20 RT  
 STA. 273+99 RT  
 STA. 276+63 RT  
 STA. 279+65 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 274+30.00 LT - STA. 280+30.00 LT  
 STA. 280+70.00 LT - STA. 283+20.00 LT  
 STA. 281+11.00 RT - STA. 284+50.00 RT

**SIGN LEGEND**

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 268+50 - STA. 284+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 274+05 LT (EDGELINES, TH# 55)  
 STA. 280+46 LT (EDGELINES, SOUCY LN.)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 268+50 - STA. 284+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 274+05 LT (CENTERLINE, TH# 55)  
 STA. 280+46 LT (CENTERLINE, SOUCY LN.)

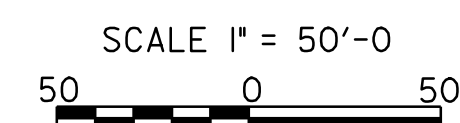
646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 273+88 LT - STA. 274+05 LT (TH# 55)  
 STA. 280+32 LT - STA. 280+46 LT (SOUCY LN.)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 273+95 LT "STOP" (TH# 55)  
 STA. 280+40 LT "STOP" (SOUCY LN.)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 268+50 - STA. 284+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 274+05 LT (EDGELINES, TH# 55)  
 STA. 280+46 LT (EDGELINES, SOUCY LN.)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 268+50 - STA. 284+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 274+05 LT (CENTERLINE, TH# 55)  
 STA. 280+46 LT (CENTERLINE, SOUCY LN.)

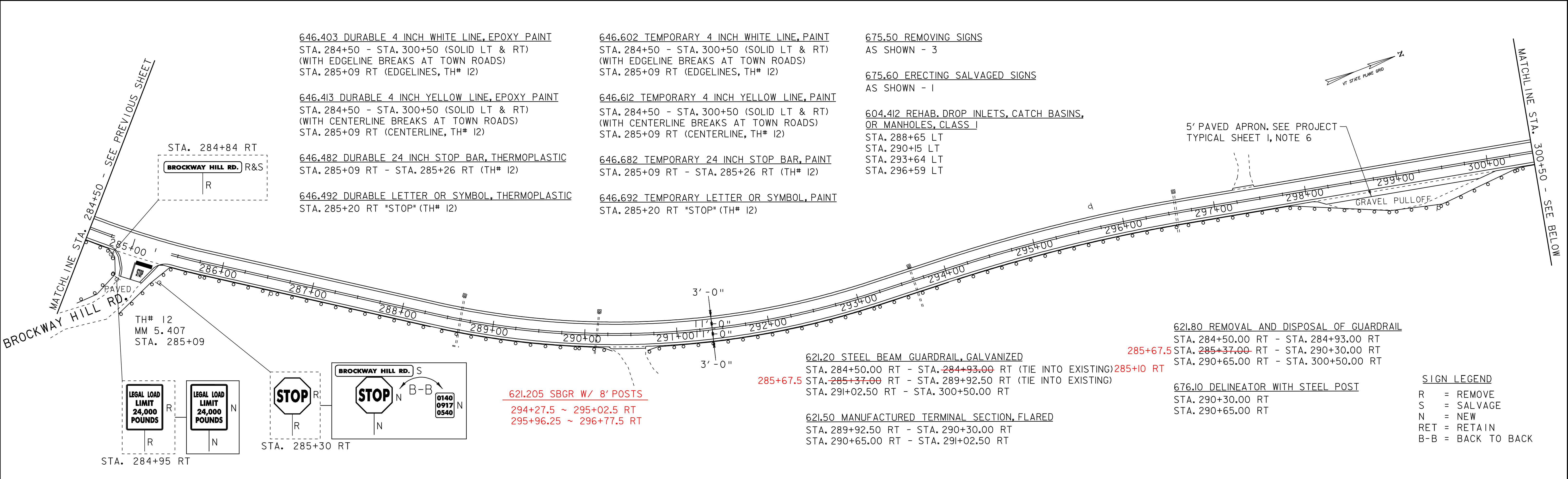
621.60 ANCHOR FOR STEEL BEAM RAIL  
 STA. 281+11.00 RT (BURIED END SECTION) 281+35 RT  
 STA. 283+07.50 LT (BURIED END SECTION)  
 274+30.0 LT



**PROJECT LAYOUT SHEET 5**

PROJECT NAME:	WILLIAMSTOWN-BARRE TOWN
PROJECT NUMBER:	STP 2210(I)S
FILE NAME:	p99c172.dgn
PROJECT LEADER:	EPD
DESIGNED BY:	CDL
PLOT FILE:	p99c172105.1
PLOT DATE:	29-APR-2009 13:3
DRAWN BY:	RHB
CHECKED BY:	EPD
SHEET	17 OF 32

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 284+50 - STA. 300+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 285+09 RT (EDGELINES, TH# 12)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 284+50 - STA. 300+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 285+09 RT (CENTERLINE, TH# 12)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 285+09 RT - STA. 285+26 RT (TH# 12)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 285+20 RT "STOP" (TH# 12)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 284+50 - STA. 300+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 285+09 RT (EDGELINES, TH# 12)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 284+50 - STA. 300+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 285+09 RT (CENTERLINE, TH# 12)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 285+09 RT - STA. 285+26 RT (TH# 12)

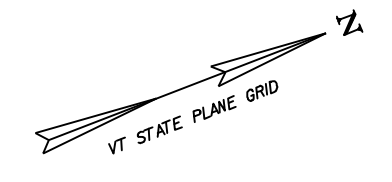
646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 285+20 RT "STOP" (TH# 12)

675.50 REMOVING SIGNS  
 AS SHOWN - 3

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 1

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS 1  
 STA. 288+65 LT  
 STA. 290+15 LT  
 STA. 293+64 LT  
 STA. 296+59 LT

5' PAVED APRON. SEE PROJECT  
 TYPICAL SHEET 1, NOTE 6



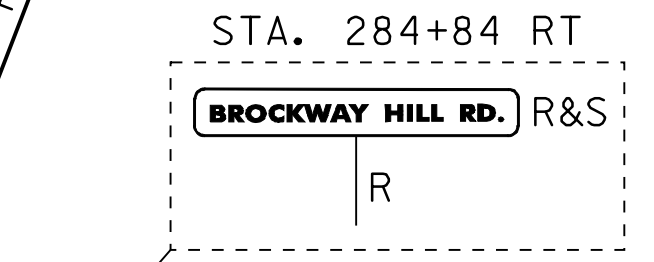
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 284+50.00 RT - STA. 284+93.00 RT  
 STA. 285+37.00 RT - STA. 290+30.00 RT  
 STA. 290+65.00 RT - STA. 300+50.00 RT

**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

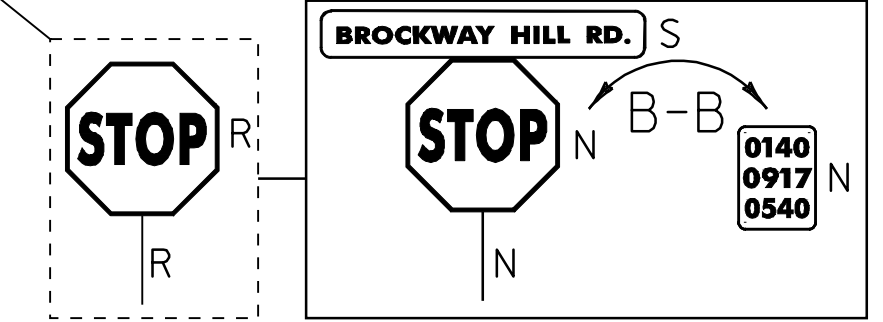
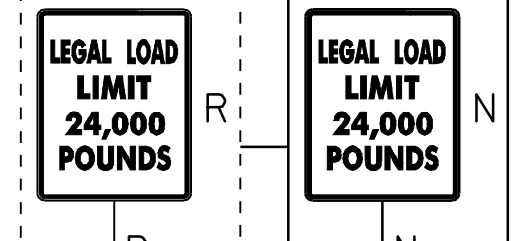
621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 284+50.00 RT - STA. 284+93.00 RT (TIE INTO EXISTING)  
 STA. 285+37.00 RT - STA. 289+92.50 RT (TIE INTO EXISTING)  
 STA. 291+02.50 RT - STA. 300+50.00 RT

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 289+92.50 RT - STA. 290+30.00 RT  
 STA. 290+65.00 RT - STA. 291+02.50 RT

621.205 SBGR W/ 8' POSTS  
 294+27.5 ~ 295+02.5 RT  
 295+96.25 ~ 296+77.5 RT



TH# 12  
 MM 5.407  
 STA. 285+09

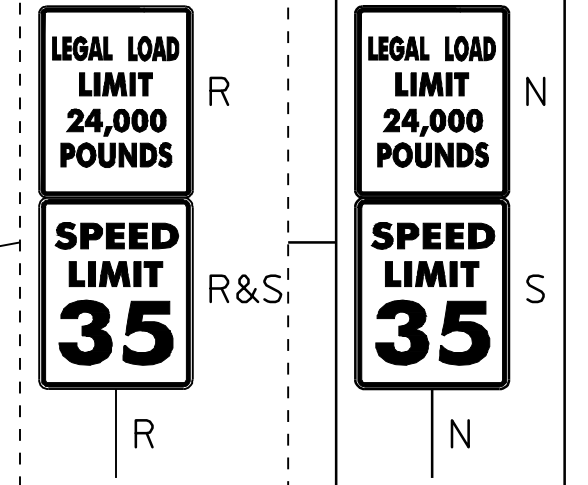


STA. 284+95 RT

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 300+50 - STA. 315+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 305+43 LT (EDGELINES, TH# 8)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 300+50 - STA. 315+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 305+43 LT (CENTERLINE, TH# 8)

TH# 8  
 MM 5.788  
 STA. 305+43



646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 305+20 LT - STA. 305+43 LT (TH# 8)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 305+30 LT "STOP" (TH# 8)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 300+50 - STA. 315+50 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 305+43 LT (EDGELINES, TH# 8)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 300+50 - STA. 315+50 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 305+43 LT (CENTERLINE, TH# 8)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 305+20 LT - STA. 305+43 LT (TH# 8)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 305+30 LT "STOP" (TH# 8)

675.50 REMOVING SIGNS  
 AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 2

**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

676.10 DELINEATOR WITH STEEL POST  
 STA. 305+71.00 LT  
 STA. 307+40.00 RT

900.640 SPECIAL PROVISION (BRIDGE RAILING  
 REPAIR, TYPE 1)  
 STA. 306+52.50 RT - STA. 306+90.00 RT  
 STA. 306+72.00 LT - STA. 307+09.50 LT  
 STA. 306+71.0 LT STA. 307+08.50 LT

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
 STA. 305+71.00 LT - STA. 306+08.50 LT  
 290+65 ~ 291+02.5 RT

621.60 ANCHOR FOR STEEL BEAM GUARDRAIL  
 STA. 307+27.50 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
 STA. 300+50.00 RT - STA. 306+52.50 RT  
 STA. 305+71.00 LT - STA. 306+72.00 LT  
 STA. 306+90.00 RT - STA. 307+40.00 RT  
 STA. 307+09.50 LT - STA. 315+50.00 LT

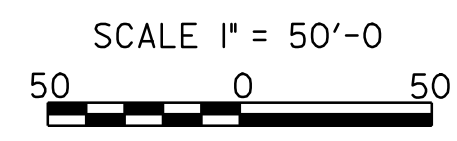
525.10 REMOVAL OF EXISTING RAILING  
 STA. 306+52.50 RT - STA. 306+90.00 RT  
 STA. 306+72.00 LT - STA. 307+09.50 LT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 300+50.00 RT - STA. 306+21.25 RT 306+27.5  
 STA. 306+08.50 LT - STA. 306+47.00 LT  
 STA. 307+15.00 RT - STA. 307+40.00 RT  
 STA. 307+40.75 LT - STA. 315+50.00 LT (609.25 LF)

621.205 STEEL BEAM GUARDRAIL, GALVANIZED W/8 FEET POSTS  
 STA. 307+40.75 LT - STA. 315+50.00 LT (200 LF)  
 314+96.0 LT 315+58.5 LT

621.21HD STEEL BEAM GUARDRAIL, GALVANIZED  
 STA. 306+21.25 RT - STA. 306+52.50 RT  
 STA. 306+47.00 LT - STA. 306+72.00 LT  
 STA. 306+90.00 RT - STA. 307+15.00 RT  
 STA. 307+09.50 LT - STA. 307+40.75 LT  
 307+08.50 LT 307+33.5 LT

DATUM  
 VERTICAL N/A  
 HORIZONTAL N/A



**PROJECT LAYOUT SHEET 6**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
 PROJECT NUMBER: STP 2210(I)S

FILE NAME: p99ci72.dgn  
 PROJECT LEADER: EPD  
 DESIGNED BY: CDL  
 PLOT FILE: p99ci72106.1  
 PLOT DATE: 29-APR-2009 13:3  
 DRAWN BY: RHB  
 CHECKED BY: EPD  
 SHEET 18 OF 32

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 315+50 - STA. 329+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 321+86 RT (EDGELINES, TH# 89)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 315+50 - STA. 329+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 321+86 RT (CENTERLINE, TH# 89)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 321+86 RT - STA. 321+96 RT (TH# 89)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 321+90 RT "STOP" (TH# 89)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 315+50 - STA. 329+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 321+86 RT (EDGELINES, TH# 89)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 315+50 - STA. 329+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 321+86 RT (CENTERLINE, TH# 89)

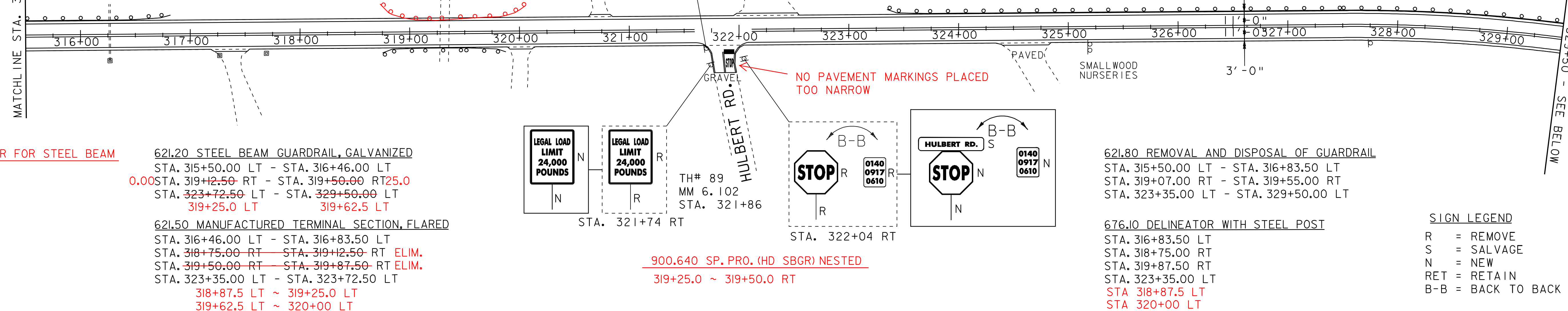
646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
STA. 321+86 RT - STA. 321+96 RT (TH# 89)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 321+90 RT "STOP" (TH# 89)

675.50 REMOVING SIGNS  
AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 1

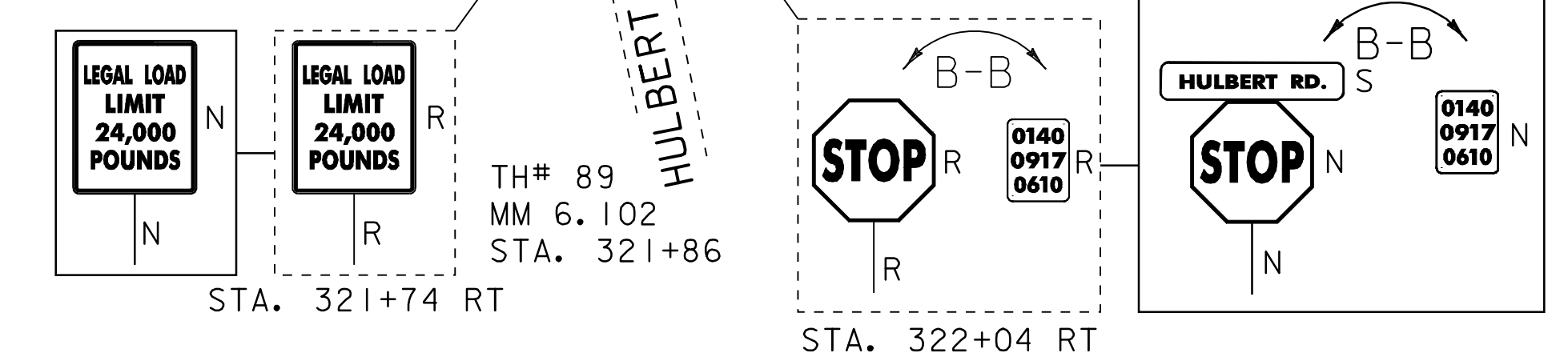
604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I  
STA. 316+26 RT  
STA. 317+26 RT  
STA. 317+70 RT



621.60 ANCHOR FOR STEEL BEAM GUARDRAIL  
316+83.5 LT  
319+00 RT  
319+87.5 RT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
STA. 315+50.00 LT - STA. 316+46.00 LT  
0.00 STA. 319+25.0 RT - STA. 319+50.00 RT 25.0  
STA. 323+72.50 LT - STA. 329+50.00 LT  
319+25.0 LT 319+62.5 LT

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
STA. 316+46.00 LT - STA. 316+83.50 LT  
STA. 318+75.00 RT - STA. 319+12.50 RT ELIM.  
STA. 319+50.00 RT - STA. 319+87.50 RT ELIM.  
STA. 323+35.00 LT - STA. 323+72.50 LT  
318+87.5 LT ~ 319+25.0 LT  
319+62.5 LT ~ 320+00 LT



621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 315+50.00 LT - STA. 316+83.50 LT  
STA. 319+07.00 RT - STA. 319+55.00 RT  
STA. 323+35.00 LT - STA. 329+50.00 LT

676.10 DELINEATOR WITH STEEL POST  
STA. 316+83.50 LT  
STA. 318+75.00 RT  
STA. 319+87.50 RT  
STA. 323+35.00 LT  
STA 318+87.5 LT  
STA 320+00 LT

SIGN LEGEND  
R = REMOVE  
S = SALVAGE  
N = NEW  
RET = RETAIN  
B-B = BACK TO BACK

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 329+50 - STA. 344+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 339+36 RT (EDGELINES, TH# 75)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 329+50 - STA. 344+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 339+36 RT (CENTERLINE, TH# 75)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 339+36 RT - STA. 339+49 RT (TH# 75)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 339+40 RT "STOP" (TH# 75)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 329+50 - STA. 344+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 339+36 RT (EDGELINES, TH# 75)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 329+50 - STA. 344+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 339+36 RT (CENTERLINE, TH# 75)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
STA. 339+36 RT - STA. 339+49 RT (TH# 75)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 339+40 RT "STOP" (TH# 75)

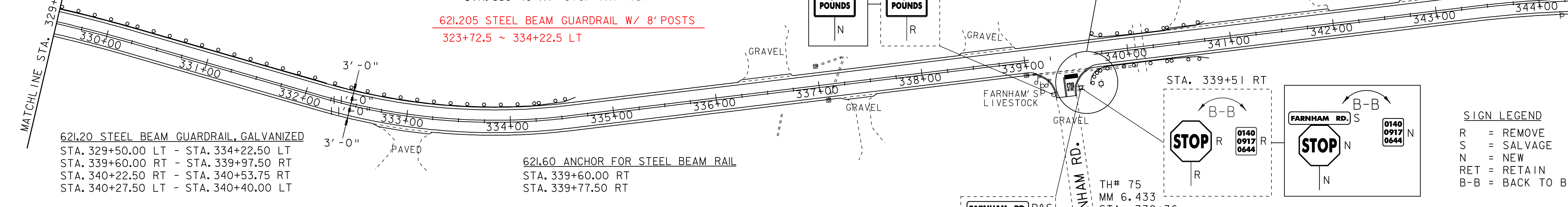
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 329+50.00 LT - STA. 334+60.00 LT  
STA. 339+65.00 RT - STA. 340+77.50 RT  
STA. 339+65.00 LT - STA. 340+77.50 LT

676.10 DELINEATOR WITH STEEL POST  
STA. 334+60.00 LT  
STA. 339+65.00 LT  
STA. 339+65.00 RT  
STA. 340+77.50 LT  
STA. 340+91.25 RT

604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I  
STA. 336+99 LT  
STA. 337+07 RT  
STA. 339+00 RT

675.50 REMOVING SIGNS  
AS SHOWN - 4

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 1

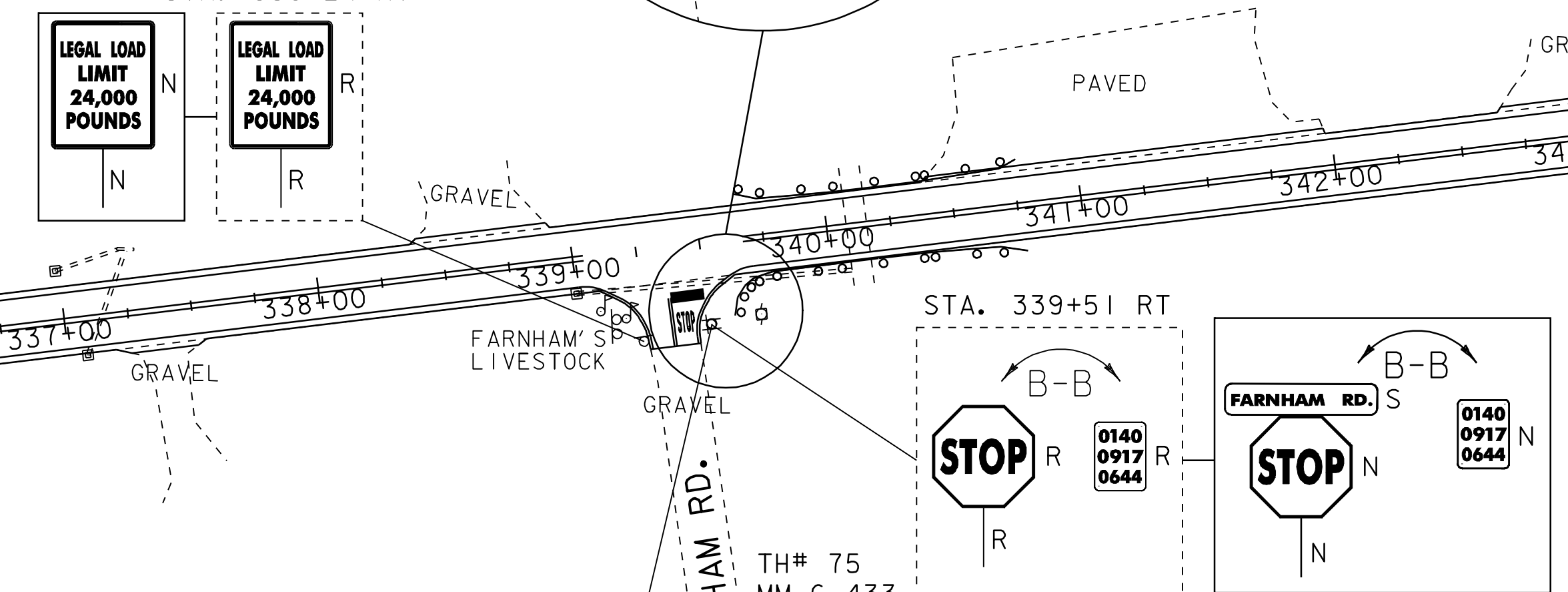


621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
STA. 329+50.00 LT - STA. 334+22.50 LT  
STA. 339+60.00 RT - STA. 339+97.50 RT  
STA. 340+22.50 RT - STA. 340+53.75 RT  
STA. 340+27.50 LT - STA. 340+40.00 LT

621.50 MANUFACTURED TERMINAL SECTION, FLARED  
STA. 334+22.50 LT - STA. 334+60.00 LT  
STA. 339+65.00 LT - STA. 340+02.50 LT  
STA. 340+40.00 LT - STA. 340+77.50 LT  
STA. 340+53.75 RT - STA. 340+91.25 RT

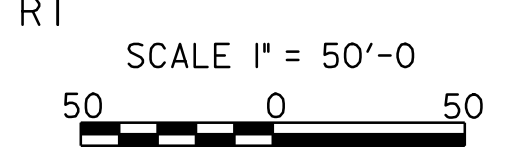
621.60 ANCHOR FOR STEEL BEAM RAIL  
STA. 339+60.00 RT  
STA. 339+77.50 RT

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL,  
GALVANIZED NESTED)  
STA. 339+97.50 RT - STA. 340+22.50 RT  
STA. 340+02.50 LT - STA. 340+27.50 LT



SIGN LEGEND  
R = REMOVE  
S = SALVAGE  
N = NEW  
RET = RETAIN  
B-B = BACK TO BACK

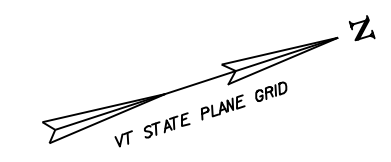
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



<b>PROJECT LAYOUT SHEET 7</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN
	PROJECT NUMBER: STP 2210(I)S
	FILE NAME: p99c172.dgn
	PROJECT LEADER: EPD
	DESIGNED BY: CDL
PLOT FILE: p99c172107.1	PLOT DATE: 29-APR-2009 13:3
	DRAWN BY: RHB
	CHECKED BY: EPD
	SHEET 19 OF 32

MATCHLINE STA. 344+50 - SEE PREVIOUS SHEET

MATCHLINE STA. 359+50 - SEE BELOW



646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 344+50 - STA. 359+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 353+62 RT (EDGELINES, PIKE DRIVE)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 344+50 - STA. 359+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 353+62 RT (EDGELINES, PIKE DRIVE)

604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I  
STA. 345+25 RT  
STA. 350+62 RT

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 344+50 - STA. 359+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 353+62 RT (CENTERLINE, PIKE DRIVE)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 344+50 - STA. 359+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 353+60 RT (CENTERLINE, PIKE DRIVE)

675.50 REMOVING SIGNS  
AS SHOWN - I

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 353+62 RT - STA. 353+85 RT (PIKE DRIVE)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
STA. 353+62 RT - STA. 353+85 RT (PIKE DRIVE)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 353+75 RT "STOP" (PIKE DRIVE)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 353+75 RT "STOP" (PIKE DRIVE)

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
STA. 346+62.50 LT - STA. 350+87.50 LT  
STA. 355+23.50 LT - STA. 359+50.00 LT  
354+67.72 LT

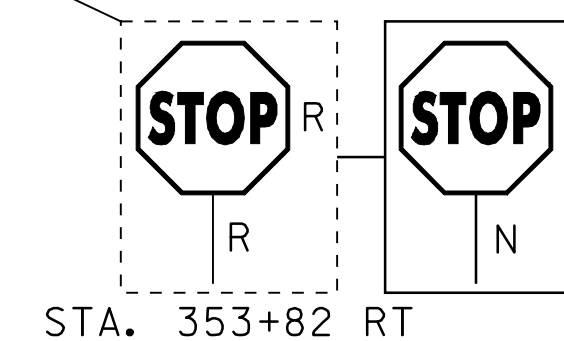
621.50 MANUFACTURED TERMINAL SECTION, FLARED  
~~STA. 346+25.00 LT - STA. 346+62.50 LT ELIM.~~  
STA. 350+87.50 LT - STA. 351+25.00 LT  
STA. 354+86.00 LT - STA. 355+23.50 LT  
354+50.22 LT 354+87.72 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 346+25.00 LT - STA. 351+25.00 LT  
STA. 354+86.00 LT - STA. 359+50.00 LT

676.10 DELINEATOR WITH STEEL POST  
STA. 346+25.00 LT  
STA. 351+25.00 LT  
STA. 354+86.00 LT

621.60 ANCHOR FOR STEEL BEAM RAIL  
346+25.0 LT

PIKE INDUSTRIES



SIGN LEGEND

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 359+50 - STA. 360+62.22 (SOLID LT & RT)  
STA. 0+00 - STA. 14+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 2+08 RT (EDGELINES, TH# 124)  
STA. 6+26 RT (EDGELINES, TH# 130)  
STA. 8+81 RT (EDGELINES, TH# 167)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 2+08 RT - STA. 2+17 RT (TH# 124)  
STA. 6+26 RT - STA. 6+37 RT (TH# 130)  
STA. 8+81 RT - STA. 8+97 RT (TH# 167)

604.40 CHANGING ELEVATION OF DROP INLETS,  
CATCH BASINS, OR MANHOLES  
STA. 10+27 LT

604.42 CHANGING ELEVATION OF SEWER MANHOLES  
STA. 6+35 RT

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 359+50 - STA. 360+62.22 (SOLID LT & RT)  
STA. 0+00 - STA. 14+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 2+08 RT (EDGELINES, TH# 124)  
STA. 6+26 RT (EDGELINES, TH# 130)  
STA. 8+81 RT (EDGELINES, TH# 167)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 359+50 - STA. 360+62.22 (SOLID LT & RT)  
STA. 0+00 - STA. 14+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 2+08 RT (CENTERLINE, TH# 124)  
STA. 6+26 RT (CENTERLINE TH# 130)  
STA. 8+81 RT (CENTERLINE TH# 167)

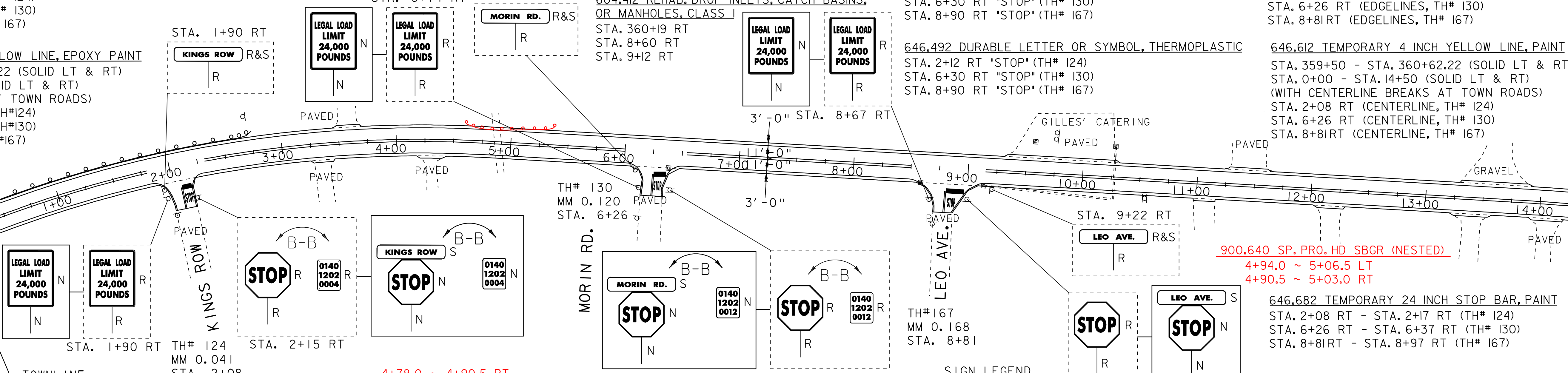
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 359+50 - STA. 360+62.22 (SOLID LT & RT)  
STA. 0+00 - STA. 14+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 2+08 RT (CENTERLINE, TH# 124)  
STA. 6+26 RT (CENTERLINE, TH# 130)  
STA. 8+81 RT (CENTERLINE, TH# 167)

604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I  
STA. 360+19 RT  
STA. 8+60 RT  
STA. 9+12 RT

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 2+12 RT "STOP" (TH# 124)  
STA. 6+30 RT "STOP" (TH# 130)  
STA. 8+90 RT "STOP" (TH# 167)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 2+12 RT "STOP" (TH# 124)  
STA. 6+30 RT "STOP" (TH# 130)  
STA. 8+90 RT "STOP" (TH# 167)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 359+50 - STA. 360+62.22 (SOLID LT & RT)  
STA. 0+00 - STA. 14+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 2+08 RT (CENTERLINE, TH# 124)  
STA. 6+26 RT (CENTERLINE, TH# 130)  
STA. 8+81 RT (CENTERLINE, TH# 167)



TOWNSHIP  
WILLIAMSTOWN STA. 360+62.22 = M.M. 6.833  
BARRE TOWN STA. 0+00.00 = M.M. 0.000

675.50 REMOVING SIGNS  
AS SHOWN - II

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 3

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
STA. 359+50.00 LT - STA. 360+62.22 LT  
STA. 0+00.00 LT - STA. 2+75.50 LT  
4+69.0 LT ~ 4+94.0 LT  
5+06.5 LT ~ 5+44.0 LT  
621.60 ANCHOR FOR STEEL BEAM RAIL  
STA. 2+63.00 LT, 2+75.5 LT, 4+69 LT, 5+44 LT  
4+78 RT, 5+28 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 359+50.00 LT - STA. 360+62.22 LT  
STA. 0+00.00 LT - STA. 2+75.50 LT  
676.10 DELINEATOR WITH STEEL POST  
STA. 2+75.50 LT, 4+78 RT, 5+28 RT  
4+69 LT, 5+44 LT

SIGN LEGEND  
R = REMOVE  
S = SALVAGE  
N = NEW  
RET = RETAIN  
B-B = BACK TO BACK  
SCALE 1" = 50'-0"  
50 0 50

PROJECT LAYOUT SHEET 8

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	PLOT DATE: 29-APR-2009 13:3
PROJECT NUMBER: STP 2210(I)S	DRAWN BY: RHB
FILE NAME: p99c172.dgn	CHECKED BY: EPD
DESIGNED BY: CDL	SHEET 20 OF 32
PLOT FILE: p99c172108.1	

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

MATCHLINE STA. 359+50 - SEE ABOVE

MATCHLINE STA. 14+50 - SEE NEXT SHEET

604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I

STA. 18+72 RT  
STA. 19+11 RT  
STA. 20+98 RT  
STA. 25+39 RT

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 14+50 - STA. 28+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 14+50 - STA. 28+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 14+50 - STA. 28+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)

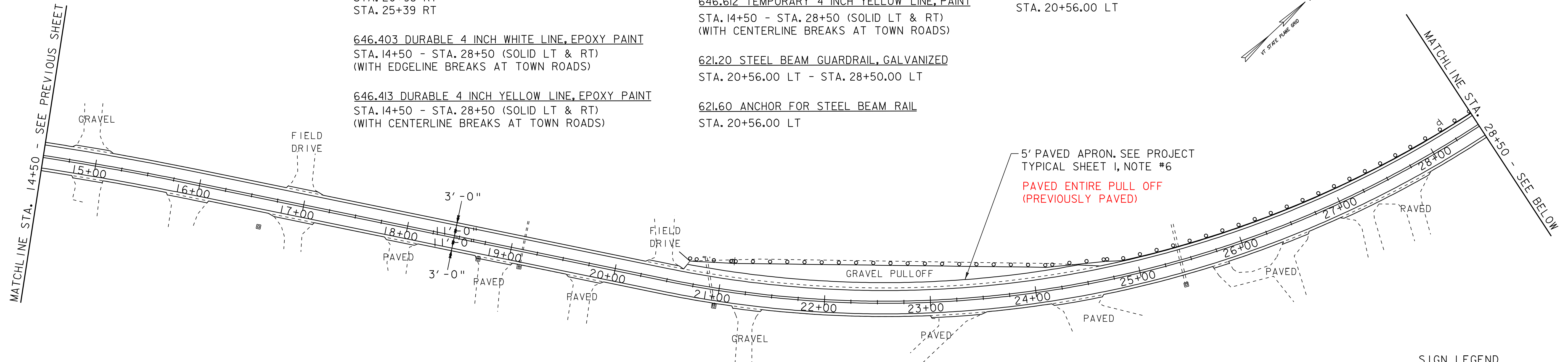
646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 14+50 - STA. 28+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)

621.20 STEEL BEAM GUARDRAIL, GALVANIZED  
STA. 20+56.00 LT - STA. 28+50.00 LT

621.60 ANCHOR FOR STEEL BEAM RAIL  
STA. 20+56.00 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL  
STA. 20+56.00 LT - STA. 28+50.00 LT

676.10 DELINEATOR WITH STEEL POST  
STA. 20+56.00 LT



**SIGN LEGEND**

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK

621.20 STEEL BEAM GUARDRAIL, GALVANIZED

STA. 28+50.00 LT - STA. 28+68.50 LT  
STA. 28+97.00 LT - STA. 30+22.00 LT  
~~37+12.75~~ STA. 37+19.00 LT - STA. 37+50.25 LT  
STA. 37+35.00 RT - STA. 37+53.75 RT  
STA. 37+75.25 LT - STA. ~~37+94.00~~ LT ~~38+37.75~~ LT  
STA. 37+91.25 RT - STA. ~~38+10.00~~ RT ~~38+28.75~~ RT

621.60 ANCHOR FOR STEEL BEAM RAIL

STA. 28+56.00 LT  
STA. 28+97.00 LT  
STA. 30+09.50 LT  
STA. ~~37+19.00~~ LT ~~37+12.75~~ LT  
STA. ~~37+35.00~~ RT ~~37+28.75~~ RT  
STA. ~~37+81.50~~ LT ~~38+37.75~~ LT  
STA. ~~37+97.50~~ RT ~~38+28.75~~ RT

900.640 SPECIAL PROVISION (HD STEEL BEAM GUARDRAIL,  
GALVANIZED NESTED)

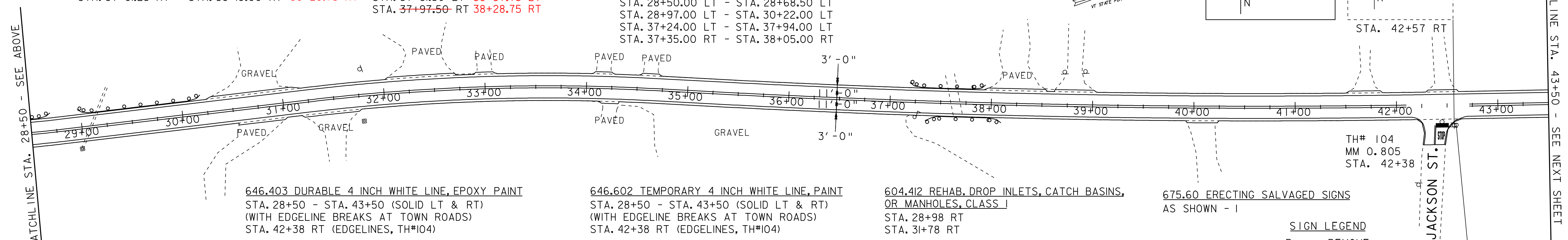
STA. 37+50.25 LT - STA. 37+75.25 LT  
STA. 37+53.75 RT - STA. 37+91.25 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 28+50.00 LT - STA. 28+68.50 LT  
STA. 28+97.00 LT - STA. 30+22.00 LT  
STA. 37+24.00 LT - STA. 37+94.00 LT  
STA. 37+35.00 RT - STA. 38+05.00 RT

676.10 DELINEATOR WITH STEEL POST

STA. 28+68.50 LT STA. 37+35.00 RT  
STA. 28+97.00 LT STA. 37+94.00 LT  
STA. 30+22.00 LT STA. 38+10.00 RT  
STA. 37+19.00 LT



646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 28+50 - STA. 43+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 42+38 RT (EDGELINES, TH#104)

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 28+50 - STA. 43+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 42+38 RT (CENTERLINE, TH#104)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 42+38 RT - STA. 42+51 RT (TH# 104)

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 42+44 RT "STOP" (TH# 104)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 28+50 - STA. 43+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 42+38 RT (EDGELINES, TH#104)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 28+50 - STA. 43+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 42+38 RT (CENTERLINE, TH#104)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
STA. 42+38 RT - STA. 42+51 RT (TH# 104)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 42+44 RT "STOP" (TH# 104)

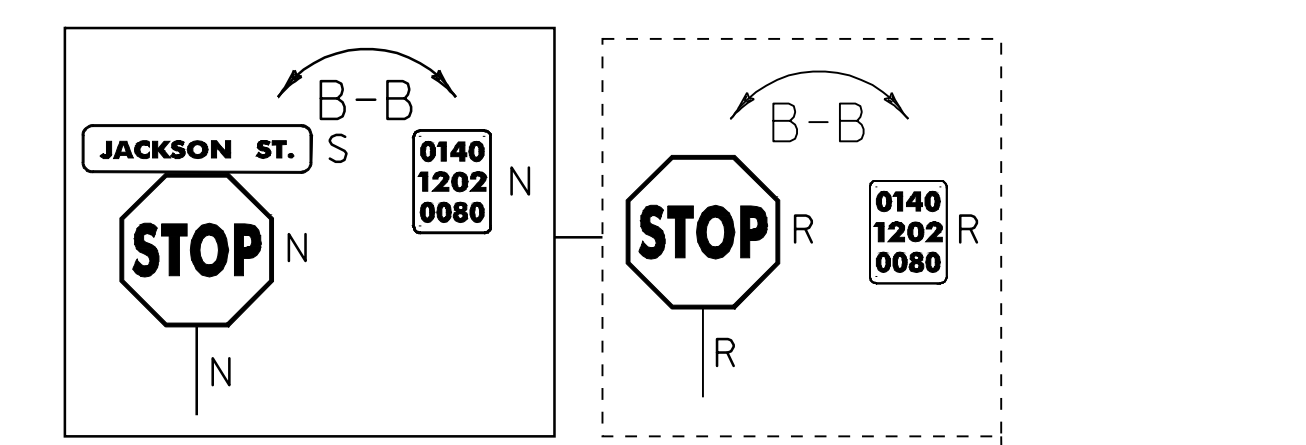
604.412 REHAB. DROP INLETS, CATCH BASINS,  
OR MANHOLES, CLASS I  
STA. 28+98 RT  
STA. 31+78 RT

604.42 CHANGING ELEVATION OF SEWER MANHOLES  
STA. 42+69 RT

617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
STA. ~~37+25~~ RT  
~~37+41~~ RT

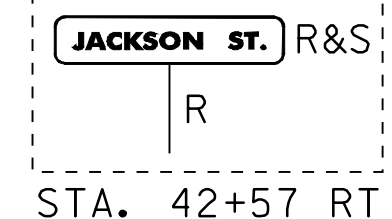
675.50 REMOVING SIGNS  
AS SHOWN - 3

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 1

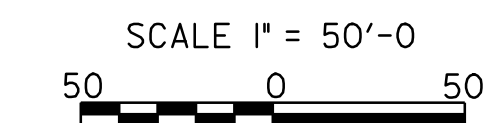


**SIGN LEGEND**

- R = REMOVE
- S = SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK



DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



**PROJECT LAYOUT SHEET 9**

PROJECT NAME:	WILLIAMSTOWN-BARRE TOWN	PLOT DATE:	29-APR-2009 13:3
PROJECT NUMBER:	STP 2210(1)S	DRAWN BY:	RHB
FILE NAME:	p99c172.dgn	CHECKED BY:	EPD
DESIGNED BY:	CDL	PLOT FILE:	p99c172109.1
		SHEET 21 OF 32	

MATCHLINE STA. 43+50 - SEE PREVIOUS SHEET

MATCHLINE STA. 59+00 - SEE BELOW

604.412 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I

STA. 46+80 RT  
STA. 48+37 RT  
STA. 49+08 RT  
STA. 50+20 RT

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS  
STA. 44+00 RT - STA. 46+00 RT

625.10 REMOVAL OF EXISTING RAILING  
STA. 57+72.50 RT - STA. 58+10.00 RT

621.20 STEEL BEAM GUARDRAIL, GALVANIZED

STA. 56+85.00 LT - STA. 58+35.00 LT  
STA. 57+35.00 RT - STA. 57+47.50 RT  
STA. 58+35.00 RT - STA. 58+47.50 RT  
58+10.0 58+22.5

621.21 HD STEEL BEAM GUARDRAIL, GALVANIZED

STA. 57+47.50 RT - STA. 57+72.50 RT  
STA. 58+10.00 RT - STA. 58+35.00 RT

621.60 ANCHOR FOR STEEL BEAM RAIL

STA. 56+85.00 LT  
STA. 57+35.00 RT  
STA. 58+22.50 LT 58+35.0  
STA. 58+47.50 RT 58+22.5

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

STA. 56+85.00 LT - STA. 58+35.00 LT  
STA. 57+53.75 RT - STA. 57+72.50 RT  
STA. 58+10.00 RT - STA. 58+28.75 RT

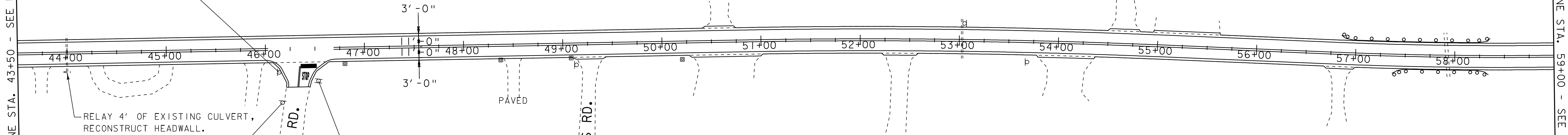
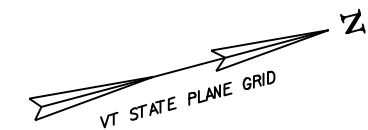
676.10 DELINEATOR WITH STEEL POST

STA. 56+85.00 LT  
STA. 57+35.00 RT  
STA. 58+35.00 LT  
STA. 58+47.50 RT

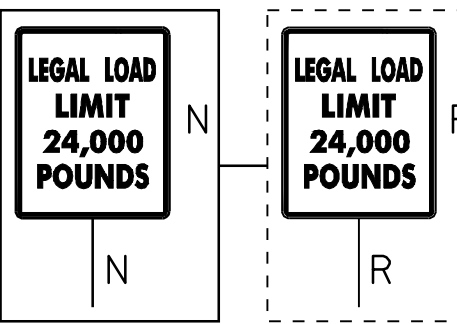
900.640 SPECIAL PROVISION (BRIDGE RAILING REPAIR, TYPE I)

STA. 57+72.50 RT - STA. 58+10.00 RT

621.205 STEEL BEAM GUARDRAIL W/ 8' POSTS  
57+72.5 ~ 57+97.5 LT

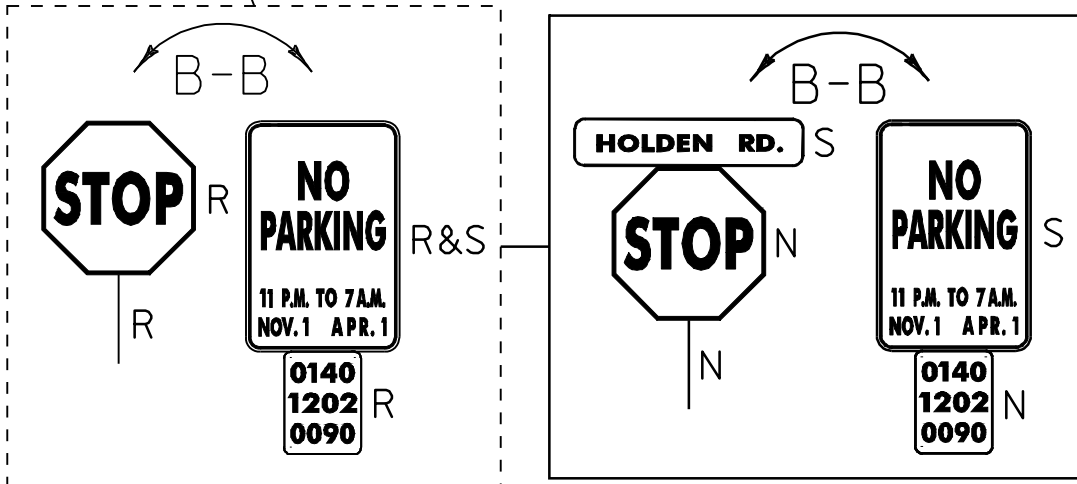


RELAY 4' OF EXISTING CULVERT, RECONSTRUCT HEADWALL.



STA. 46+16 RT

TH# 60  
MM 0.881  
STA. 46+35



646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 43+50 - STA. 59+00 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 46+35 RT (EDGELINES, TH#60)

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 43+50 - STA. 59+00 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)  
STA. 46+35 RT (EDGELINES, TH#60)

675.50 REMOVING SIGNS  
AS SHOWN - 5

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 43+50 - STA. 59+00 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 46+35 RT (CENTERLINE, TH#60)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 43+50 - STA. 59+00 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)  
STA. 46+35 RT (CENTERLINE, TH#60)

675.60 ERECTING SALVAGED SIGNS  
AS SHOWN - 2

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
STA. 46+35 RT - STA. 46+52 RT (TH# 60)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
STA. 46+35 RT - STA. 46+52 RT (TH# 60)

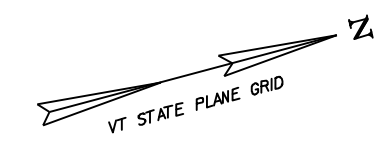
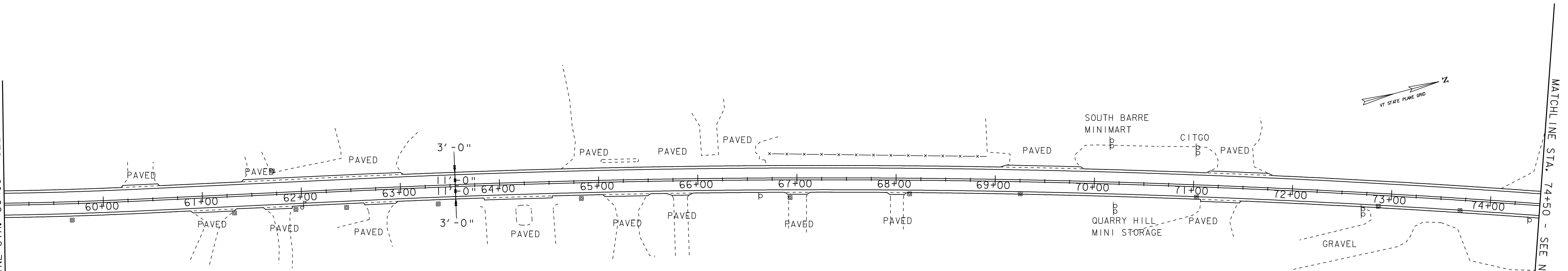
646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
STA. 46+40 RT "STOP" (TH# 60)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
STA. 46+40 RT "STOP" (TH# 60)

**SIGN LEGEND**  
R = REMOVE  
S = SALVAGE  
N = NEW  
RET = RETAIN  
B-B = BACK TO BACK

MATCHLINE STA. 59+00 - SEE ABOVE

MATCHLINE STA. 74+50 - SEE NEXT SHEET



646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
STA. 59+00 - STA. 74+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
STA. 59+00 - STA. 74+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS  
STA. 61+32 RT - STA. 61+65 RT  
STA. 62+95 RT - STA. 63+77 RT  
STA. 64+60 RT - STA. 65+05 RT  
STA. 65+95 RT - STA. 66+89 RT  
STA. 68+14 RT - STA. 71+04 RT  
STA. 71+51 RT - STA. 72+78 RT

646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
STA. 59+00 - STA. 74+50 (SOLID LT & RT)  
(WITH CENTERLINE BREAKS AT TOWN ROADS)

604.412 REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I

STA. 59+70 RT STA. 66+92 RT  
STA. 61+30 RT STA. 68+12 RT  
STA. 61+70 LT STA. 69+26 RT  
STA. 61+90 RT STA. 71+02 RT  
STA. 62+45 RT STA. 72+88 RT  
STA. 63+38 RT STA. 73+70 RT  
STA. 64+80 RT

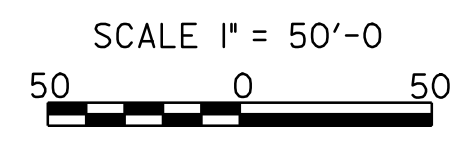
617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
STA. 62+00 RT

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
STA. 59+00 - STA. 74+50 (SOLID LT & RT)  
(WITH EDGELINE BREAKS AT TOWN ROADS)

**SIGN LEGEND**  
R = REMOVE  
S = SALVAGE  
N = NEW  
RET = RETAIN  
B-B = BACK TO BACK

DATUM

VERTICAL	N/A
HORIZONTAL	N/A



**PROJECT LAYOUT SHEET 10**

PROJECT NAME:	WILLIAMSTOWN-BARRE TOWN	PLOT DATE:	29-APR-2009 13:3
PROJECT NUMBER:	STP 2210(I)S	DRAWN BY:	RHB
FILE NAME:	p99cl72.dgn	DESIGNED BY:	CDL
DESIGNED BY:	CDL	CHECKED BY:	EPD
PLOT FILE:	p99cl7210.1		SHEET 22 OF 32

646.403 DURABLE 4 INCH WHITE LINE, EPOXY PAINT  
 STA. 74+50 - STA. 90+40 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 76+55 LT (EDGELINES, TH#59)  
 STA. 79+55 RT (EDGELINES, TH#214)  
 STA. 82+65 LT (EDGELINES, SMOKEHOUSE LN.)  
 STA. 84+75 - STA. 87+10 (DOTTED LT)  
 STA. 87+10 - STA. 88+60 (SOLID LT)  
 STA. 89+24 LT (EDGELINES, VT 63)  
 STA. 89+24 RT (EDGELINES, TH#4)  
 STA. 89+81 - STA. 90+40 (SOLID LT)

646.482 DURABLE 24 INCH STOP BAR, THERMOPLASTIC  
 STA. 76+41LT - STA. 76+55 LT (TH# 59)  
 STA. 79+55 RT - STA. 79+67 RT (TH# 214)  
 STA. 82+46 LT - STA. 82+65 LT (SMOKEHOUSE LN.)  
 STA. 88+61LT & RT  
 STA. 88+83 LT - STA. 89+15 LT (VT 63)  
 STA. 89+24 RT - STA. 89+50 RT (TH# 4)  
 STA. 89+80 LT  
 STA. 90+06 LT

646.602 TEMPORARY 4 INCH WHITE LINE, PAINT  
 STA. 74+50 - STA. 90+40 (SOLID LT & RT)  
 (WITH EDGELINE BREAKS AT TOWN ROADS)  
 STA. 76+55 LT (EDGELINES, TH#59)  
 STA. 79+55 RT (EDGELINES, TH#214)  
 STA. 82+65 LT (EDGELINES, SMOKEHOUSE LN.)  
 STA. 84+75 - STA. 87+10 (DOTTED LT)  
 STA. 87+10 - STA. 88+60 (SOLID LT)  
 STA. 89+24 LT (EDGELINES, VT 63)  
 STA. 89+24 RT (EDGELINES, TH#4)  
 STA. 89+81 - STA. 90+40 (SOLID LT)

646.692 TEMPORARY LETTER OR SYMBOL, PAINT  
 STA. 76+48 LT "STOP" (TH# 59) STA. 88+46 RT "ARROW"  
 STA. 79+61RT "STOP" (TH# 214) STA. 88+49 RT "ARROW"  
 STA. 81+46 RT "SIGNAL" STA. 88+52 LT "ARROW"  
 STA. 81+86 RT "AHEAD" STA. 88+91LT "ARROW" (VT 63)  
 STA. 82+58 LT "STOP" (SMOKEHOUSE LN.) STA. 88+93 LT "ARROW" (VT 63)  
 STA. 89+08 LT "ARROW" (VT 63)  
 STA. 87+18 LT "ARROW" STA. 89+93 LT "ARROW"  
 STA. 87+18 RT "ARROW" STA. 89+95 LT "ARROW"  
 STA. 87+21RT "ARROW" STA. 90+15 LT "ARROW"  
 STA. 87+85 LT "ONLY"

646.492 DURABLE LETTER OR SYMBOL, THERMOPLASTIC  
 STA. 76+48 LT "STOP" (TH# 59) STA. 88+46 RT "ARROW"  
 STA. 79+61RT "STOP" (TH# 214) STA. 88+49 RT "ARROW"  
 STA. 81+46 RT "SIGNAL" STA. 88+52 LT "ARROW"  
 STA. 81+86 RT "AHEAD" STA. 88+91LT "ARROW" (VT 63)  
 STA. 82+58 LT "STOP" (SMOKEHOUSE LN.) STA. 88+93 LT "ARROW" (VT 63)  
 STA. 89+08 LT "ARROW" (VT 63)  
 STA. 87+18 LT "ARROW" STA. 89+93 LT "ARROW"  
 STA. 87+18 RT "ARROW" STA. 89+95 LT "ARROW"  
 STA. 87+21RT "ARROW" STA. 90+15 LT "ARROW"  
 STA. 87+85 LT "ONLY"

646.612 TEMPORARY 4 INCH YELLOW LINE, PAINT  
 STA. 74+50 - STA. 88+65 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 76+55 LT (CENTERLINE, TH#59)  
 STA. 79+55 RT (CENTERLINE, TH#214)  
 STA. 82+65 LT (CENTERLINE, SMOKEHOUSE LN.)  
 STA. 89+24 RT (CENTERLINE, TH#4)  
 STA. 89+24 LT (CENTERLINE, VT 63)  
 STA. 90+05 - STA. 90+40 (SOLID LT, CENTERLINE)

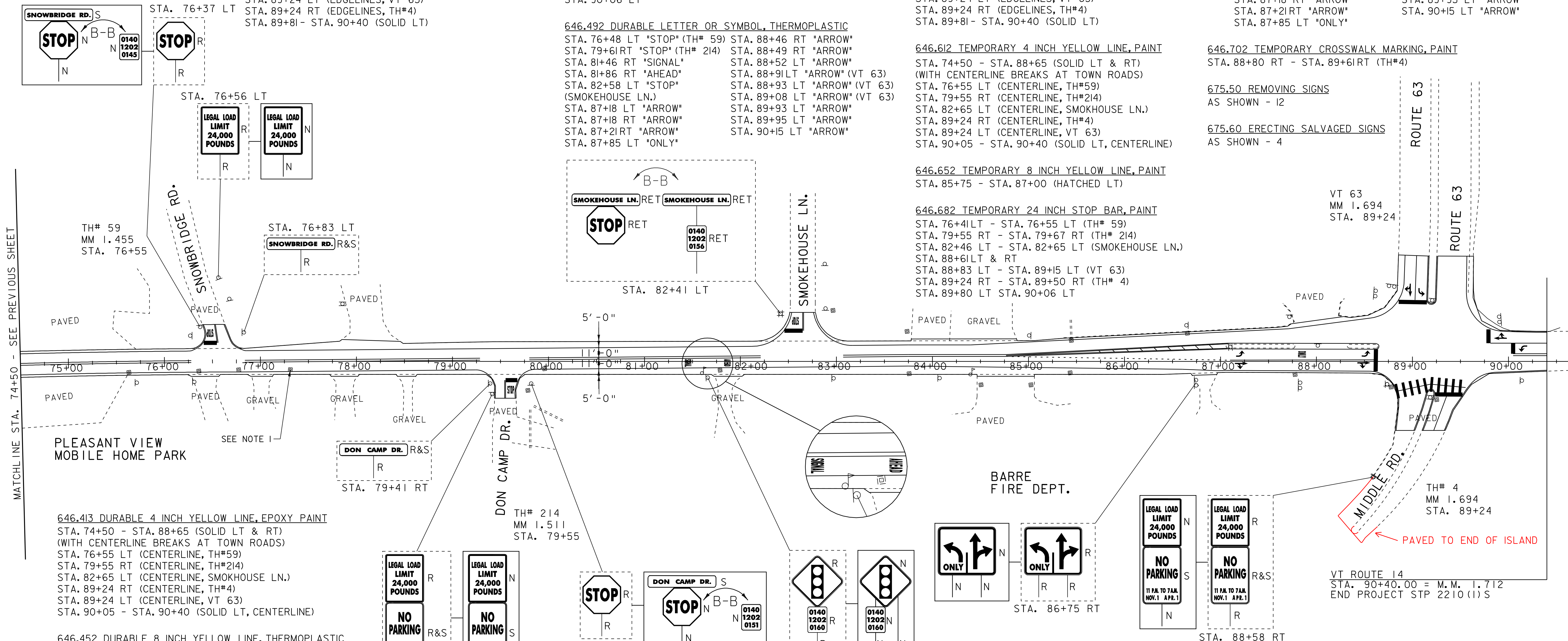
646.702 TEMPORARY CROSSWALK MARKING, PAINT  
 STA. 88+80 RT - STA. 89+61RT (TH#4)

675.50 REMOVING SIGNS  
 AS SHOWN - 12

675.60 ERECTING SALVAGED SIGNS  
 AS SHOWN - 4

646.652 TEMPORARY 8 INCH YELLOW LINE, PAINT  
 STA. 85+75 - STA. 87+00 (HATCHED LT)

646.682 TEMPORARY 24 INCH STOP BAR, PAINT  
 STA. 76+41LT - STA. 76+55 LT (TH# 59)  
 STA. 79+55 RT - STA. 79+67 RT (TH# 214)  
 STA. 82+46 LT - STA. 82+65 LT (SMOKEHOUSE LN.)  
 STA. 88+61LT & RT  
 STA. 88+83 LT - STA. 89+15 LT (VT 63)  
 STA. 89+24 RT - STA. 89+50 RT (TH# 4)  
 STA. 89+80 LT STA. 90+06 LT



646.413 DURABLE 4 INCH YELLOW LINE, EPOXY PAINT  
 STA. 74+50 - STA. 88+65 (SOLID LT & RT)  
 (WITH CENTERLINE BREAKS AT TOWN ROADS)  
 STA. 76+55 LT (CENTERLINE, TH#59)  
 STA. 79+55 RT (CENTERLINE, TH#214)  
 STA. 82+65 LT (CENTERLINE, SMOKEHOUSE LN.)  
 STA. 89+24 RT (CENTERLINE, TH#4)  
 STA. 89+24 LT (CENTERLINE, VT 63)  
 STA. 90+05 - STA. 90+40 (SOLID LT, CENTERLINE)

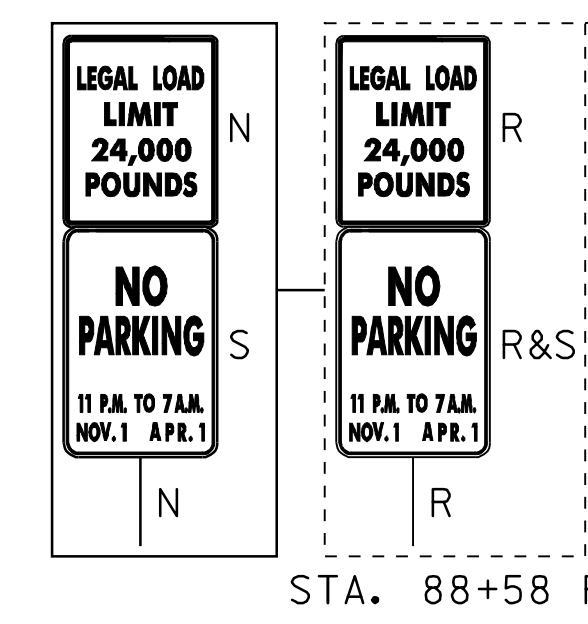
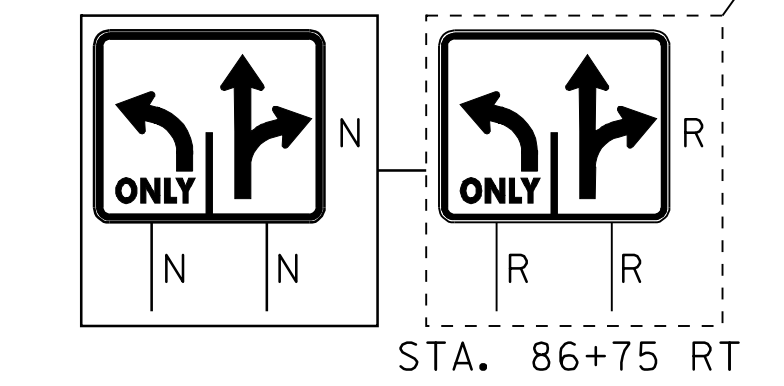
646.452 DURABLE 8 INCH YELLOW LINE, THERMOPLASTIC  
 STA. 85+75 - STA. 87+00 (HATCHED LT)

604.412 REHAB. DROP INLETS, CATCH BASINS,  
 OR MANHOLES, CLASS I  
 STA. 75+60 RT STA. 83+76 RT  
 STA. 76+35 RT STA. 85+43 RT  
 STA. 76+77 RT STA. 85+45 LT  
 STA. 77+31RT STA. 86+65 LT  
 STA. 79+96 RT STA. 87+78 RT  
 STA. 81+79 RT STA. 89+25 RT

616.47 BITUMINOUS CONCRETE GUTTERS AND TRAFFIC ISLANDS  
 STA. 76+60 RT - STA. 76+77 RT  
 STA. 77+15 RT - STA. 77+71RT  
 STA. 78+03 RT - STA. 78+43 RT  
 STA. 78+65 RT - STA. 79+37 RT  
 STA. 79+75 RT - STA. 81+70 RT  
 STA. 82+66 RT - STA. 84+44 RT

617.10 RELOCATE MAILBOX, SINGLE SUPPORT  
 STA. 81+60 RT  
 STA. 84+40 RT

646.502 DURABLE CROSSWALK MARKING, THERMOPLASTIC  
 STA. 88+80 RT - STA. 89+61RT (TH#4)

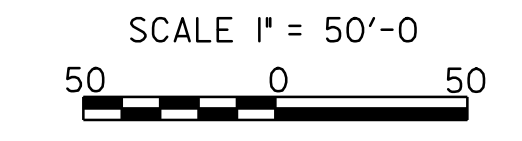


**NOTE:**

1. EXISTING DRAINAGE GRATE IS LOCATED BENEATH THE SIDEWALK. THE CONTRACTOR SHALL REMOVE THE SIDEWALK SLAB ABOVE THE EXISTING DRAINAGE STRUCTURE, REFORM STRUCTURE TO ALLOW FRAME AND GRATE TO BE ADJACENT TO SIDEWALK, AND RECONSTRUCT SIDEWALK. REMOVAL OF THE SIDEWALK SHALL BE PAID UNDER ITEM 203.16, "SOLID ROCK EXCAVATION". REFORMING THE STRUCTURE SHALL BE PAID UNDER ITEM 604.412, "REHAB. DROP INLETS, CATCH BASINS, OR MANHOLES, CLASS I". NEW SIDEWALK WILL BE PAID UNDER ITEM 618.10, "PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH".

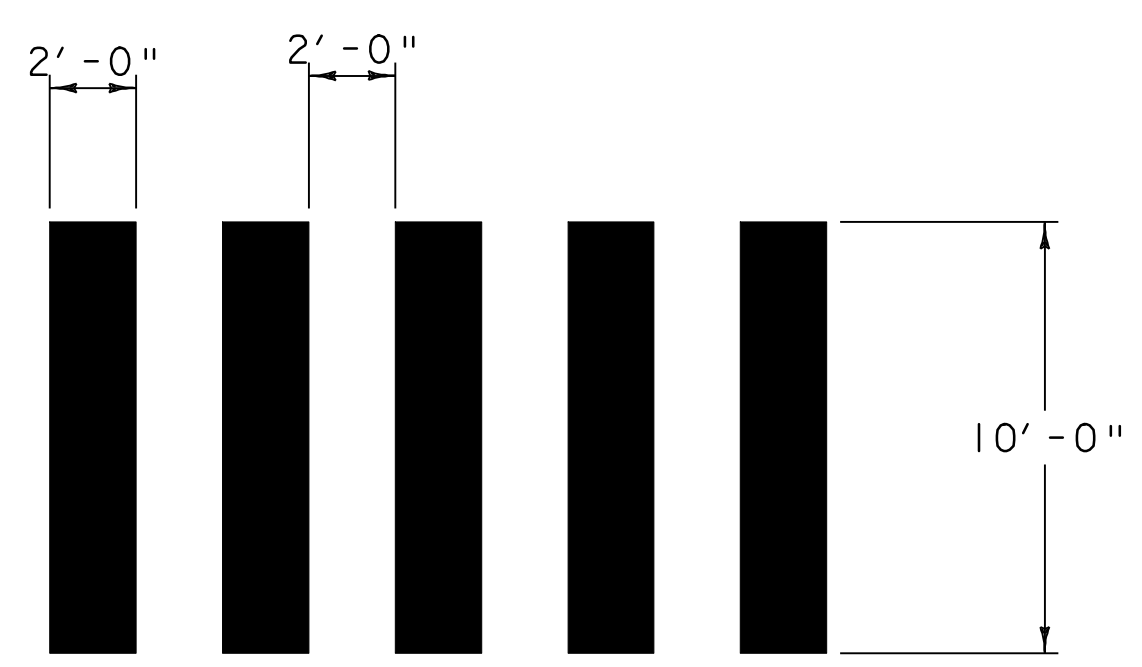
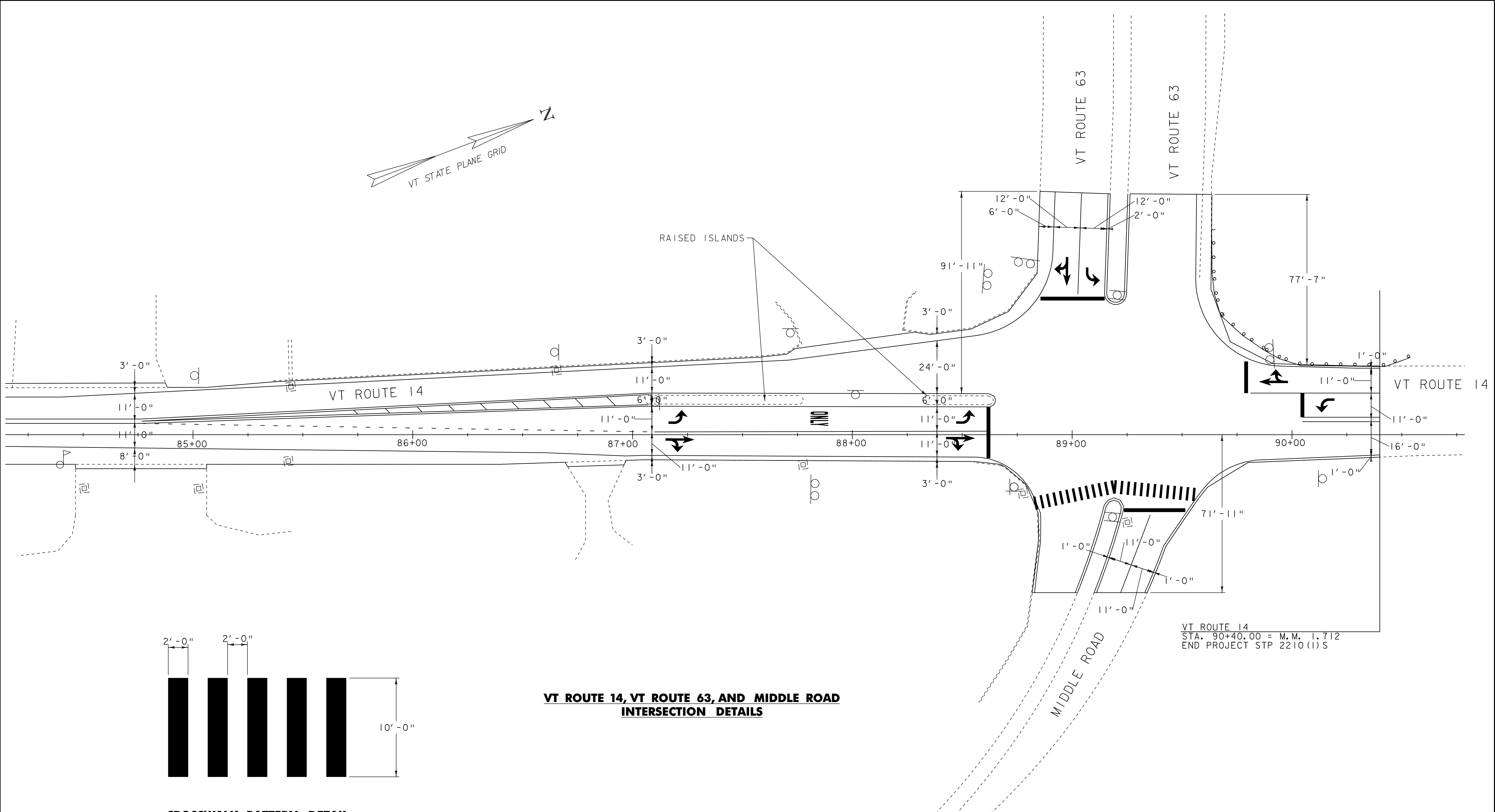
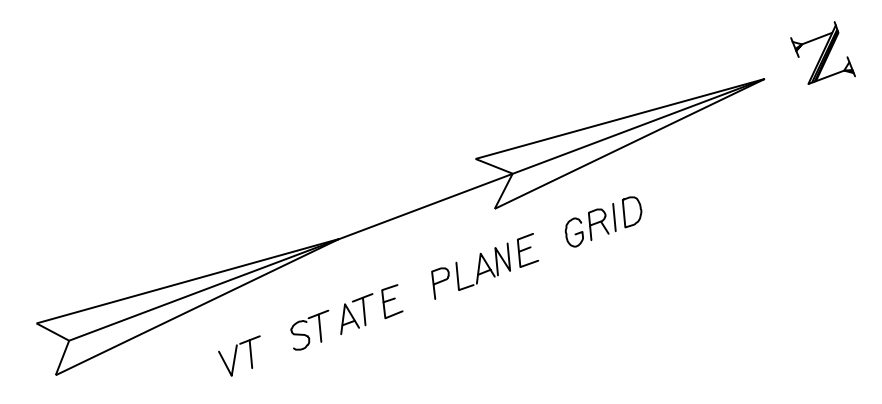
**SIGN LEGEND**  
 R = REMOVE  
 S = SALVAGE  
 N = NEW  
 RET = RETAIN  
 B-B = BACK TO BACK

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



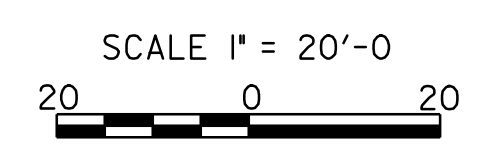
**PROJECT LAYOUT SHEET 11**

PROJECT NAME:	WILLIAMSTOWN-BARRE TOWN
PROJECT NUMBER:	STP 2210(1)S
FILE NAME:	p99c172.dgn
PROJECT LEADER:	EPD
DESIGNED BY:	CDL
PLOT FILE:	p99c172III.I
PLOT DATE:	29-APR-2009 13:30
DRAWN BY:	RHB
CHECKED BY:	EPD
SHEET	23 OF 32

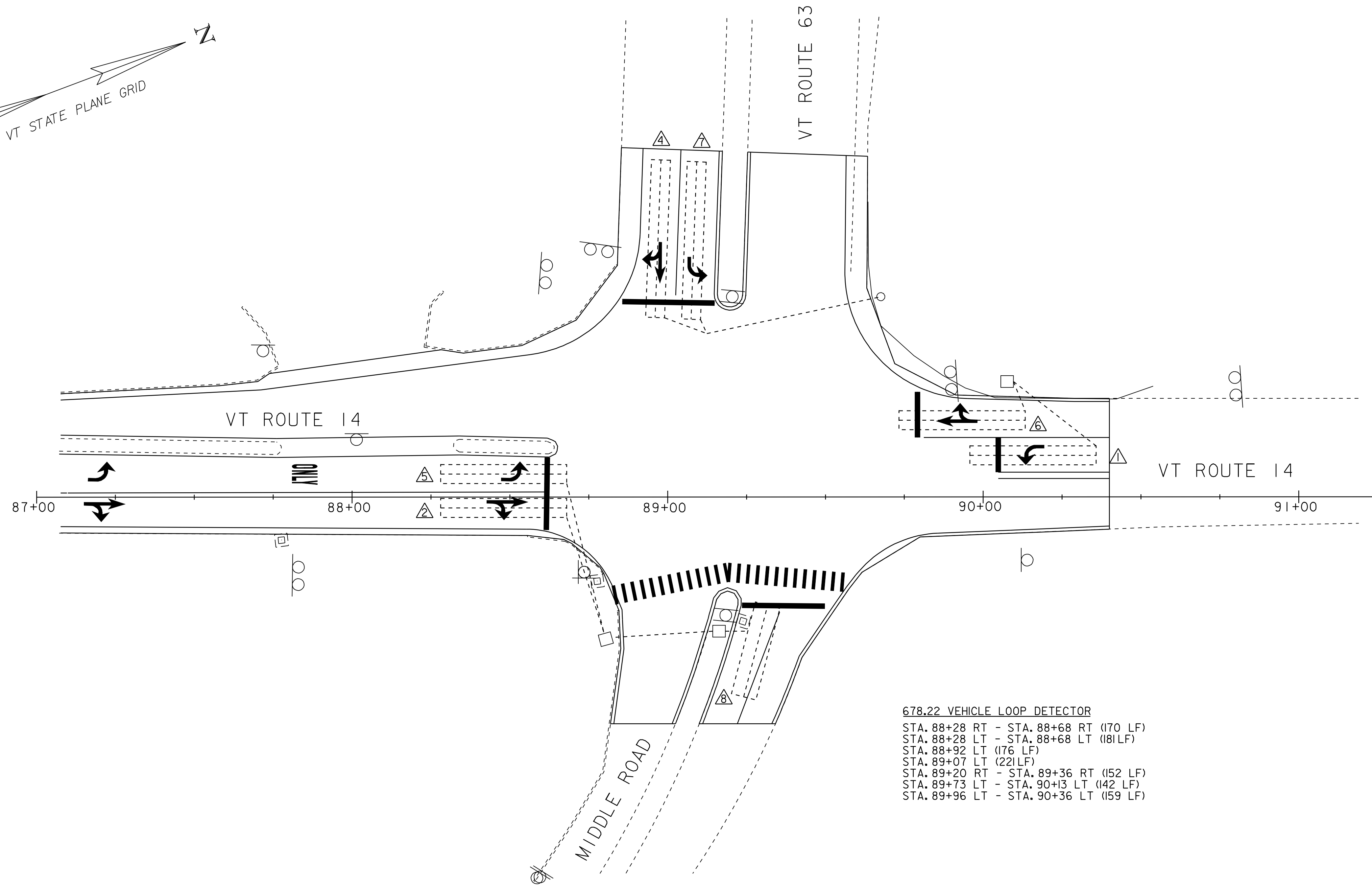
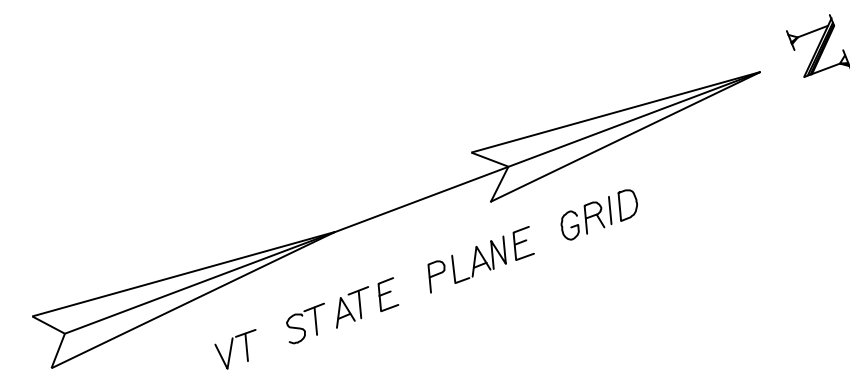


**CROSSWALK PATTERN DETAIL**  
NOT TO SCALE

**VT ROUTE 14, VT ROUTE 63, AND MIDDLE ROAD  
INTERSECTION DETAILS**



<b>INTERSECTION DETAIL SHEET</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN	PLOT DATE: 29-APR-2009 13:3
	PROJECT NUMBER: STP 2210 (1)S	DRAWN BY: RHB
	FILE NAME: p99cl72.dgn	CHECKED BY: EPD
	DESIGNED BY: CDL	SHEET 24 OF 32
	PLOT FILE: p99cl721ds.l	



**678.22 VEHICLE LOOP DETECTOR**  
 STA. 88+28 RT - STA. 88+68 RT (170 LF)  
 STA. 88+28 LT - STA. 88+68 LT (181 LF)  
 STA. 88+92 LT (176 LF)  
 STA. 89+07 LT (221 LF)  
 STA. 89+20 RT - STA. 89+36 RT (152 LF)  
 STA. 89+73 LT - STA. 90+13 LT (142 LF)  
 STA. 89+96 LT - STA. 90+36 LT (159 LF)

**SIGNAL LAYOUT PLAN**  
 NOT TO SCALE

**GENERAL NOTES**

1. THIS PLAN IS NOT TO SCALE AND SHALL BE USED AS A GUIDE. THE CONTRACTOR SHALL CONFIRM ALL LOCATIONS, INCLUDING, BUT NOT LIMITED TO, UTILITIES, POLES, PULL BOXES, STRIPING, AND LOOP DETECTORS. THE CONTRACTOR SHALL CONFIRM ALL LOCATIONS IN THE FIELD WITH THE RESIDENT ENGINEER PRIOR TO INSTALLATION.
2. PRIOR TO COLD PLANING, THE CONTRACTOR SHALL DISCONNECT THE VEHICLE DETECTOR LOOP IN THE CONTROLLER CABINET AND CUT IT AT THE CURB OR SHOULDER. ONCE THE VEHICLE DETECTOR LOOP IS DISCONNECTED, THE SIGNAL PHASE THAT IT WAS CALLING SHALL BE SET ON MAXIMUM RECALL OR THE SIGNAL SHALL BE SET TO FLASH WHILE TRAFFIC IS BEING CONTROLLED BY A UNIFORMED TRAFFIC OFFICER. DETECTOR AND SIGNAL WORK SHALL BE INCIDENTAL TO PAY ITEM 678.22, 'VEHICLE LOOP DETECTOR'. UNIFORMED TRAFFIC OFFICERS WILL BE PAID FOR UNDER CONTRACT ITEM 630.10.
3. ALL PROPOSED VEHICLE DETECTOR LOOPS SHALL BE INSTALLED IN THE LEVELING COURSE, WHICH IS IMMEDIATELY BELOW THE WEARING SURFACE. ONCE THE PROPOSED VEHICLE DETECTOR LOOP IS INSTALLED, THE INDUCTANCE, RESISTANCE AND LEAKAGE TO GROUND MUST BE TESTED USING PROPERLY CALIBRATED EQUIPMENT. THESE TEST RESULTS SHALL BE COMPARED WITH THE CALCULATED VALUES SHOWN ON THE LAYOUT PLANS AND THE FIELD MEASURED VALUES SHALL BE RECORDED ON THE LAYOUT PLANS. UPON COMPLETION OF THE INSTALLATION OF A PROPOSED VEHICLE LOOP DETECTOR, THE SIGNAL SHALL BE RETURNED TO NORMAL OPERATION.
4. THE CONTRACTOR SHALL USE THE EXISTING CONDUIT WHICH RUNS FROM THE CURB TO THE CONTROLLER PANEL FOR THE NEW LOOP DETECTORS.
5. EXISTING TIMINGS WILL BE USED.
6. WORK IMPROVEMENTS CONSISTING OF THOSE SHOWN ON PLANS SHALL BE PERFORMED ACCORDING TO SPECIFICATIONS AND STANDARD DRAWINGS OF VERMONT AGENCY OF TRANSPORTATION. VEHICLE DETECTOR LOOPS SHALL COMPLY WITH VTRANS STANDARD E-172.

**VEHICLE LOOP DETECTOR SCHEDULE**

STREET	LANE	STATION	LOOP #	SIZE	TYPE	# OF TURNS	CALL PHASE	MODE	INDUCTANCE		RESISTANCE		LEAKAGE TO GROUND
									CALC.	MEAS.	CALC.	MEAS.	
VT 14	SB LT	89+96 - 90+36 LT	1	6' X 40'	LONG	2	1 + 5	PRES.	409		1.47		
VT 14	NB TH/RT	88+28 - 88+68 RT	2	6' X 40'	LONG	2	2 + 6	PRES.	351		0.72		
VT 63	EB TH/RT	88+92 LT	4	6' X 50'	LONG	1	4 + 8	PRES.	199		1.22		
VT 14	NB LT	88+28 - 88+68 LT	5	6' X 40'	LONG	2	1 + 5	PRES.	352		0.74		
VT 14	SB TH/RT	89+73 - 90+13 LT	6	6' X 40'	LONG	2	2 + 6	PRES.	404		1.41		
VT 63	EB LT	89+07 LT	7	6' X 50'	LONG	1	4 + 8	PRES.	196		1.18		
MIDDLE RD.	WB LT/TH/RT	89+20 - 89+36 RT	8	8' X 30'	LONG	2	4 + 8	PRES.	280		0.43		

**LOOP DETECTOR LAYOUT SHEET**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
 PROJECT NUMBER: STP 2210 (1)S

FILE NAME: p99cl72.dgn  
 PROJECT LEADER: EPD  
 DESIGNED BY: CDL  
 PLOT FILE: p99cl72lds.i

PLOT DATE: 29-APR-2009 13:3  
 DRAWN BY: RHB  
 CHECKED BY: EPD  
 SHEET 25 OF 32

# TRAFFIC SIGN SUMMARY SHEET 1

















MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST. POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL								
		E	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN			SALV TIS	RETAIN	SALVAGE	FLANGED CHANNEL				SQUARE STEEL (In)				TUBULAR ALUMINUM Ø (In)				TUBULAR STEEL Ø (In)				W-SHAPE STEEL		REQUIRE	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER
													lb/ft		lb/ft		3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0		5.0	FTG. SIZE	WEIGHT	POST SIZE					
													1.12	2.0	3.0	1.88													2.16	3.35			
WILLIAMSTOWN STA. 206+05 LT		I	30	30	6.25		I																			RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 206+78 LT		I	24	30	5.00				I		X		X													VR-017 MOUNT SPEED LIMIT SIGN BELOW LEGAL LOAD LIMIT		E-141					
STA. 215+36 RT		I	24	30	5.00				I		X		X													VR-017		E-141					
STA. 215+92 RT		I	30	30	6.25																					RI-I		E-143					
		I	6	10	0.42				I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 220+85 LT		I	30	30	6.25																					RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 221+15 LT		I	24	30	5.00				I		X		X													VR-017		E-141					
STA. 226+56 LT		I	30	30	6.25																					RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 227+03 LT		I	24	30	5.00				I		X		X													VR-017		E-141					
STA. 233+33 LT		I	30	30	6.25																					RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 233+50 LT		I	24	30	5.00				I		X		X													VR-017 MOUNT SPEED LIMIT SIGN BELOW LEGAL LOAD LIMIT		E-141					

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."

										FT	FT	FT	FT	FT	FT	EA	LB	LB	LB										
										FT				FT															
										58.35	SF	EA.	SF																

<b>TRAFFIC SIGN SUMMARY SHEET 1</b>	PROJECT NAME: WILLIAMSTOWN-BARRE TOWN
	PROJECT NUMBER: STP 2210 (1)S
	FILE NAME: p99cl72.dgn
	PLOT DATE: 29-APR-2009 13:3
DESIGNED BY: CDL	DRAWN BY: RHB
CHECKED BY: EPD	CHECKED BY: EPD
PLOT FILE: p99cl72+ssl.1	SHEET 26 OF 32

# TRAFFIC SIGN SUMMARY SHEET 2

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST. POST	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL					
		E A	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (In)			TUBULAR ALUMINUM Ø (In)			TUBULAR STEEL Ø (In)				W-SHAPE STEEL			REQUIRE	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER			
											1.2	2.0	3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0	5.0	FTG. SIZE					WEIGHT	POST SIZE	
																									lb/ft							lb/ft
STA. 233+53 RT	 	I	24	30	5.00				I			X		X												VR-017 MOUNT SPEED LIMIT SIGN BELOW LEGAL LOAD LIMIT		E-141				
STA. 233+94 RT	 	I	30	30	6.25																				RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X												MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 236+92 LT	 	I	30	30	6.25																				RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
		I	6	10	0.42				I		X		X												MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 240+79 RT	 	I	24	30	5.00				I		X		X												VR-017 MOUNT SPEED LIMIT SIGN BELOW LEGAL LOAD LIMIT		E-141					
STA. 241+34 RT	 	I	30	30	6.25				I		X		X												RI-I MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-143					
		I	6	10	0.42				I		X		X												MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138					
STA. 241+63 LT	 	I	30	30	6.25																				RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143					
STA. 241+86 LT		I	24	30	5.00				I		X		X												VR-017		E-141					

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."

FT	FT	FT	FT	FT	FT	EA	LB	LB	LB	LB	EA	EA	LB
		105		105									
	SF	SF	EA.	SF			FT		FT		LB		EA.
	41.68		5				105		105				EA.

**TRAFFIC SIGN SUMMARY SHEET 2**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (1)S  
FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl72+ss2.i  
PLOT DATE: 29-APR-2009 13:3  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 27 OF 32

# TRAFFIC SIGN SUMMARY SHEET 3

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL		
		E A	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (In)			TUBULAR ALUMINUM Ø (In)			TUBULAR STEEL Ø (In)				W-SHAPE STEEL			REQUIRE	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER
											lb/ft	lb/ft	lb/ft	ANCHOR	SLEEVE	FOUND-ATION	FTG. SIZE	WEIGHT	POST SIZE	24"	30"								
																						1.2	2.0	3.0	1.75				
STA. 248+29 LT		I	30	30	6.25																					RI-I	E-143		
		I	6	10	0.42				I		X		X														MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.	E-138	
STA. 248+62 LT	 	I	24	30	5.00				I		X		X													VR-017	E-141		
STA. 254+60 RT		I	24	30	5.00				I		X		X													VR-017	E-141		
STA. 254+82 RT		I	30	30	6.25																					RI-I	E-143		
		I	6	10	0.42				I		X		X														MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.	E-138	
STA. 273+89 LT	 	I	30	30	6.25																					RI-I	E-143		
		I	6	10	0.42				I		X		X														MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.	E-143	
		I	6	10	0.42				I		X		X														MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.	E-138	
STA. 274+42 LT		I	24	30	5.00				I		X		X													VR-017	E-141		
STA. 280+25 LT	 	I	30	30	6.25																					RI-I	E-143		
		I	6	10	0.42				I		X		X														MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.	E-143	
STA. 284+95 RT		I	24	30	5.00				I		X		X													VR-017	E-141		

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."	FT	FT	FT	FT	FT	FT	EA	LB	LB	LB	LB	EA	EA	LB
	120	120	120	120	120	120								
	SF	SF	EA.	SF										
	46.26		3											




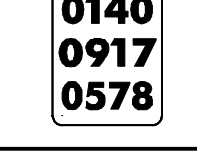
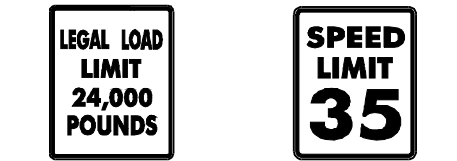


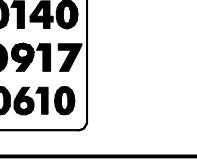


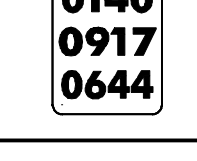


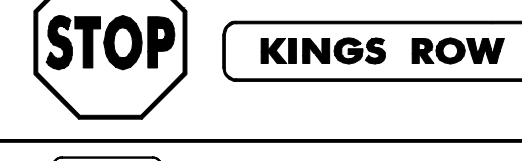
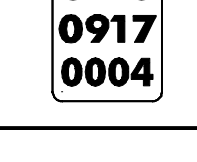
**TRAFFIC SIGN SUMMARY SHEET 3**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S

FILE NAME: p99ci72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99ci72+ss3.i

PLOT DATE: 29-APR-2009 13:3  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 28 OF 32

# TRAFFIC SIGN SUMMARY SHEET 4

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXISTING POSTS			NEW SIGN POSTS													REMARKS	SIGN DETAIL						
		E A	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN	SALV TIS	RETAIN	SALVAGE	NO. OF POSTS	FLANGED CHANNEL			SQUARE STEEL (In)			TUBULAR ALUMINUM Ø (In)			TUBULAR STEEL Ø (In)				W-SHAPE STEEL			REQUIRE D	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER	
												lb/ft	lb/ft	lb/ft	3.0	4.0	4.0 MOD	FOUND- ATION	3.0	3.5	4.0	5.0	FTG. SIZE		WEIGHT	POST SIZE					
																							1.2				2.0				3.0
OPTION ITEMS																															
STA. 285+30 RT		I	30	30	6.25																						RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42						I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138	
STA. 305+26 LT		I	30	30	6.25																						RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42						I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138	
STA. 305+77 LT		I	24	30	5.00						I		X		X												VR-017 MOUNT SPEED LIMIT SIGN BELOW LEGAL LOAD LIMIT		E-141		
STA. 321+74 RT		I	24	30	5.00						I		X		X												VR-017		E-141		
STA. 322+04 RT		I	30	30	6.25																						RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42						I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138	
STA. 339+24 RT		I	24	30	5.00						I		X		X												VR-017		E-141		
STA. 339+51 RT		I	30	30	6.25																						RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42						I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138	
STA. 353+82 RT		I	30	30	6.25								X		X												RI-I		E-143		
BARRE STA. 1+90 RT		I	24	30	5.00						I		X		X												VR-017		E-141		
STA. 2+15 RT		I	30	30	6.25																						RI-I MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42						I		X		X													MOUNT REF. PLAQUE BACK TO BACK WITH STOP SIGN.		E-138	

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."

	SF	SF	EA.	SF		FT	FT	FT	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
	59.60		6			150	150	150																											

**TRAFFIC SIGN SUMMARY SHEET 4**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN

PROJECT NUMBER: STP 2210 (1)S

FILE NAME: p99cl72.dgn

PROJECT LEADER: EPD

DESIGNED BY: CDL

PLOT FILE: p99cl72+ss4.i

PLOT DATE: 29-APR-2009 13:35

DRAWN BY: RHB

CHECKED BY: EPD

SHEET 29 OF 32

# TRAFFIC SIGN SUMMARY SHEET 5

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS																REMARKS	SIGN DETAIL				
		E A	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN			SALV TIS	RETAIN	SALVAGE	FLANGED CHANNEL			SQUARE STEEL (In)			TUBULAR ALUMINUM Ø (In)			TUBULAR STEEL Ø (In)					W-SHAPE STEEL		REQUIRE	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER
													lb/ft	lb/ft	lb/ft	ANCHOR	SLEEVE	FOUND-ATION	FTG. SIZE	WEIGHT	POST SIZE	lb/ft									
																						1.2	2.0	3.0	1.75		2.0	2.5			
STA. 6+14 RT		I	24	30	5.00				I			X		X												VR-017		E-141			
STA. 6+41RT		I	30	30	6.25				I			X		X												RI-I	MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42																								E-138		
STA. 8+67 RT		I	24	30	5.00				I			X		X												VR-017		E-141			
STA. 9+00 RT		I	30	30	6.25				I			X		X												RI-I	MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
STA. 42+57 RT		I	30	30	6.25				I			X		X												RI-I	MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42				I			X		X															E-138		
STA. 46+16 RT		I	24	30	5.00				I			X		X												VR-017		E-141			
STA. 46+53 RT		I	30	30	6.25				I			X		X												RI-I	MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
	 	I	6	10	0.42				I			X		X															E-138		
STA. 76+37 LT		I	30	30	6.25				I			X		X												RI-I	MOUNT STREET SIGN OVER STOP SIGN. POST TOP MOUNTING BRACKET REQUIRED FOR FLANGED CHANNEL POST OPTION.		E-143		
		I	6	10	0.42				I			X		X															E-138		
STA. 76+56 LT		I	24	30	5.00				I			X		X												VR-017		E-141			

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."	FT	FT	FT	FT	FT	FT	EA	LB	LB	LB	LB	EA	EA	LB
	135	135	135	135	135	135								
	SF	SF	EA.	SF										
	52.93		6											

**TRAFFIC SIGN SUMMARY SHEET 5**

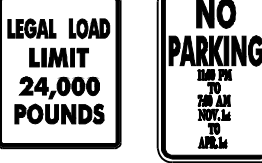




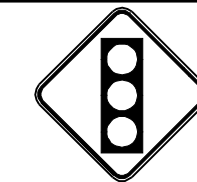
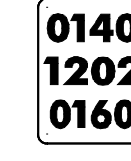

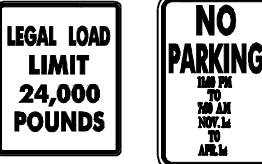

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S

FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl72+ss5.i

PLOT DATE: 29-APR-2009 13:30  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 30 OF 32

# TRAFFIC SIGN SUMMARY SHEET 6

MILEMARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS														REMARKS	SIGN DETAIL							
		EA	WIDTH (In)	HEIGHT (In)	"A"	"B"	SALV SIGN			SALV TIS	RETAIN	SALVAGE	FLANGED CHANNEL			SQUARE STEEL (In)			TUBULAR ALUMINUM ∅ (In)			TUBULAR STEEL ∅ (In)				W-SHAPE STEEL		REQUIRE	DETAIL ON SHEET NUMBER	STD. SHEET NUMBER		
								1.2	2.0				3.0	1.75	2.0	2.5	3.0	4.0	4.0 MOD	FOUND-ATION	3.0	3.5	4.0		5.0	FTG. SIZE					WEIGHT	POST SIZE
																										lb/ft	lb/ft					

OPTION ITEMS																																
STA. 79+44 RT	 	I	24	30	5.00					I			X		X													VR-017	E-141			
STA. 79+82 RT	 	I	30	30	6.25																								RI-I	E-143		
		I	6	10	0.42					I		X		X																E-138		
STA. 81+66 RT	 	I	36	36	9.00					2		X		X																E-152 E-138		
STA. 86+75 RT		I	36	30	7.50					2		X		X																VR-922	E-145A	
STA. 88+58 RT	 	I	24	30	5.00					I		X		X																VR-017	E-141	
SHEET 26 TOTAL					58.35								150		150																	
SHEET 27 TOTAL					41.68								105		105																	
SHEET 28 TOTAL					46.26								120		120																	
SHEET 29 TOTAL					59.60								150		150																	
SHEET 30 TOTAL					52.93								135		135																	
SHEET 31 TOTAL					33.59								105		105																	

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE ROADWAY, TRAFFIC & SAFETY SECTIONS "SIGN POST DESIGN GUIDELINE."

SUB TOTAL				292.41																												
ROUNDING				2.59																												
	SF	SF	EA.	SF																												
	295		29																													

**TRAFFIC SIGN SUMMARY SHEET 6**

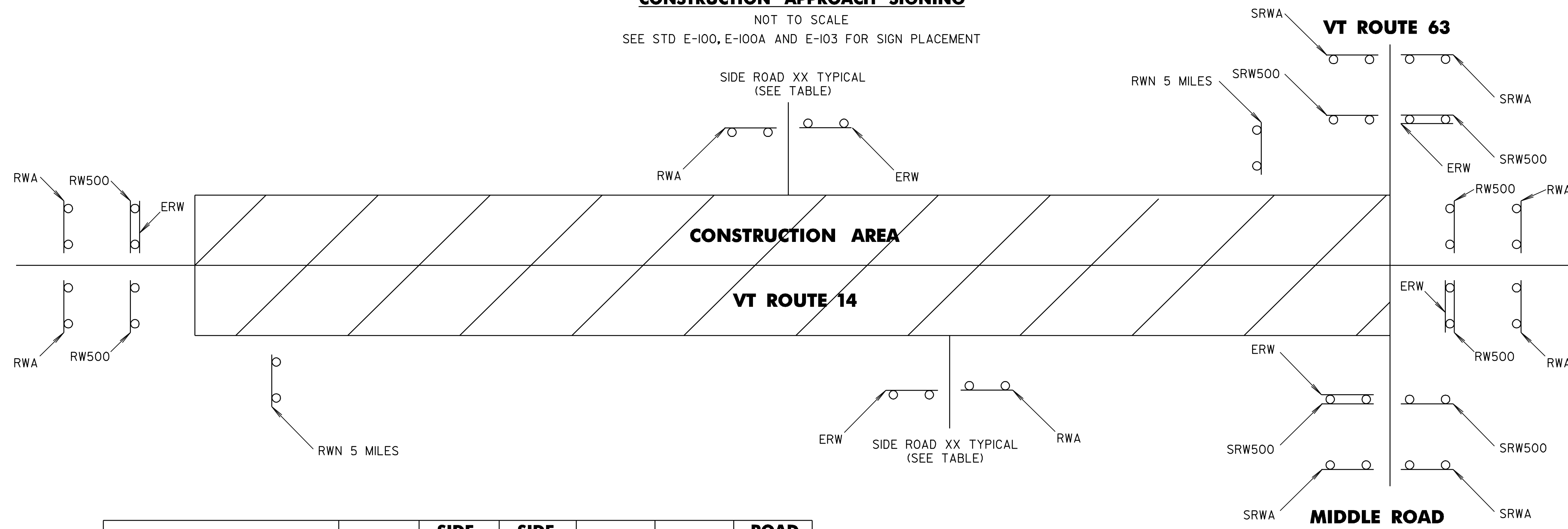
PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (I)S  
FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl72+ss6.l  
PLOT DATE: 29-APR-2009 13:30  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 31 OF 32

**CONSTRUCTION APPROACH SIGNING**

NOT TO SCALE  
SEE STD E-100, E-100A AND E-103 FOR SIGN PLACEMENT

**NOTES:**

1. THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN FOR APPROVAL PRIOR TO THE START OF CONSTRUCTION. THE COST OF PREPARING THIS PLAN (AND MAKING CHANGES IF NECESSARY) SHALL NOT BE PAID SEPARATELY BUT SHALL BE INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".
2. THE CONTRACTOR SHALL INCLUDE A CONSTRUCTION SIGN APPROACH PACKAGE FOR EXPECTED LANE CLOSURES AND WORK ZONE SPEED REDUCTIONS IN COMPLIANCE WITH VTRANS STANDARD E-103, EXCEPT REFER TO PART\*6 OF THE MUTCD FOR PROPER SIGN SPACING. PAYMENT FOR PROVIDING THIS PACKAGE SHALL BE INCIDENTAL TO ITEM 641.10, "TRAFFIC CONTROL".
3. THE BID PRICE FOR "TRAFFIC CONTROL", ITEM 641.10, SHALL INCLUDE ALL OF THE FOLLOWING, AS NEEDED: APPROACH AND ON-PROJECT CONSTRUCTION SIGNING, PORTABLE ARROW BOARDS, BARRIERS, BARRELS, CONES, BARRICADES, TEMPORARY REGULATORY AND WARNING SIGNS, AND POSTS AS DETAILED IN VTRANS STANDARDS. ALL ADJUSTING, RELOCATING, AND REMOVING OF THESE DEVICES AS DIRECTED BY THE RESIDENT ENGINEER SHALL ALSO BE INCLUDED. THE FOLLOWING ITEMS WILL BE PAID FOR SEPARATELY:  
646.602, 646.612, 646.652, 646.682, 646.692, AND 646.702 - TEMPORARY PAVEMENT MARKINGS  
630.10 AND 630.15 - UNIFORMED TRAFFIC OFFICERS AND FLAGGERS
4. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) WILL BE PROVIDED FOR USE ALONG THIS PROJECT. THE PLACEMENT OF THESE UNITS AS WELL AS THE MESSAGE WILL BE APPROVED BY THE RESIDENT ENGINEER. THESE SIGNS WILL BE PAID FOR UNDER ITEM 641.15, "PORTABLE CHANGEABLE MESSAGE SIGN".  
  
PCMS SHOULD NOT REPLACE ANY OF THE SIGNING DETAILED IN THE MUTCD AND SHOULD NOT BE USED IF STANDARD TRAFFIC CONTROL DEVICES ADEQUATELY PROVIDE THE INFORMATION THE MOTORISTS NEED TO TRAVEL SAFELY.  
  
THE PCMS SHALL CONSIST OF EITHER ONE OR TWO PHASES. TYPICALLY, A PHASE SHALL CONSIST OF UP TO THREE LINES OF EIGHT CHARACTERS PER LINE. THE PCMS SHOULD BE USED AS A SUPPLEMENT AND NOT AS A SUBSTITUTE FOR CONVENTIONAL SIGNS AND PAVEMENT MARKINGS.  
  
THE PCMS SHOULD COMMUNICATE WHAT INFORMATION MOTORISTS NEED TO KNOW. UNNECESSARY INFORMATION SHOULD BE AVOIDED. MESSAGES SHOULD BE UPDATED PERIODICALLY TO DESCRIBE THE WORK ACTIVITY OCCURRING SO THAT THE PCMS CONTINUES TO COMMAND THE ATTENTION OF MOTORISTS.
5. THE MUTCD 2003 SHALL BE THE STANDARD FOR ALL TRAFFIC CONTROL DEVICES. EXISTING SIGNS, SIGNALS AND MARKINGS SHALL BE VALID UNTIL SUCH TIME AS THEY ARE REPLACED OR RECONSTRUCTED. WHEN NEW TRAFFIC CONTROL DEVICES ARE ERECTED OR PLACED OR EXISTING TRAFFIC CONTROL DEVICES ARE REPLACED OR REPAIRED THE EQUIPMENT, DESIGN, METHOD OF INSTALLATION, PLACEMENT OR REPAIR SHALL CONFORM WITH SUCH STANDARDS.
6. NO CONSTRUCTION SIGNS SHALL BE INSTALLED AS TO INTERFERE OR OBSTRUCT THE VIEW OF EXISTING TRAFFIC CONTROL DEVICES, STOPPING SIGHT DISTANCE, AND CORNER SIGHT DISTANCE FROM DRIVES AND TOWN HIGHWAYS.



SIDE ROAD NAME	ROAD WORK AHEAD	SIDE ROAD WORK AHEAD	SIDE ROAD WORK 500 FT	END ROAD WORK	ROAD WORK 500 FT	ROAD WORK NEXT 5 MILES
<b>WILLIAMSTOWN</b>						
BEGIN PROJECT (VT 14)	2			1	2	1
TH #100 (INDUSTRY ST)	1			1		
TH #99 (BUSINESS CENTER RD)	1			1		
TH #40 (SPIDER WEB FARM RD)	1			1		
TH #39 (PLEASANT ST)	1			1		
TH #73 (GARDEN ST)	1			1		
TH #72 (DEPOT ST)	1			1		
TH #71 (BECKETT ST)				1		
VT 64 (BROOK ST)	1			1		
TH #1 (MEADOW ST)	1			1		
TH #24 (CONSTRUCTION HILL)	1			1		
TH #78 (ELM ST)	1			1		
TH #25 (CIRCLE ST)	1			1		
TH #55 (OLD TOWN RD)	1			1		
(SOUCY LN)	1			1		
TH #12 (BROCKWAY HILL RD)	1			1		
TH #8 (FALLS BRIDGE RD)	1			1		
TH #89 (HULBERT RD)	1			1		
TH #75 (FARNHAM RD)	1			1		
<b>BARRE</b>						
TH #124 (KINGS ROW)	1			1		
TH #130 (MORIN RD)	1			1		
TH #167 (LEO AVE)	1			1		
TH #104 (JACKSON ST)	1			1		
TH #60 (HOLDEN RD)	1			1		
TH #59 (SNOWBRIDGE RD)	1			1		
TH #214 (DON CAMP DR)	1			1		
(SMOKEHOUSE LN)	1			1		

SIDE ROAD NAME	ROAD WORK AHEAD	SIDE ROAD WORK AHEAD	SIDE ROAD WORK 500 FT	END ROAD WORK	ROAD WORK 500 FT	ROAD WORK NEXT 5 MILES
VT 63		2	2	1		
TH #4 (MIDDLE RD)		2	2	1		
END PROJECT (VT 14)	2			1	2	1
TOTALS	29	4	4	30	4	2

**LEGEND**

- RWA = ROAD WORK AHEAD
- RW500 = ROAD WORK 500 FT
- SRWA = SIDE ROAD WORK AHEAD
- SRW500 = SIDE ROAD WORK 500 FT
- ERW = END ROAD WORK
- RWN = ROAD WORK NEXT 5 MILES

**CONSTRUCTION APPROACH SIGNING SHEET**

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN  
PROJECT NUMBER: STP 2210 (1)S  
FILE NAME: p99cl72.dgn  
PROJECT LEADER: EPD  
DESIGNED BY: CDL  
PLOT FILE: p99cl72cas.1  
PLOT DATE: 29-APR-2009 13:3  
DRAWN BY: RHB  
CHECKED BY: EPD  
SHEET 32 OF 32