

ASPHALTIC PLUG TYPE JOINT DETAIL

WILLIAMSTOWN
STA. 306+68
~~STA. 306+96~~

**ASPHALTIC PLUG BRIDGE JOINT
GENERAL NOTES**

INSTALLATION:

THE JOINT SHALL BE LOCATED CENTRALLY OVER THE DECK EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.

THE JOINT SHALL BE EXCAVATED AS SHOWN ON THE PLANS BY USE OF SAWS AND PNEUMATIC HAMMER OR A HAMMER AND CHISEL.

THE JOINT AREA SHALL BE BLAST CLEANED OF DEBRIS AND ASPHALT. THE JOINT AREA SHALL BE THOROUGHLY DRIED USING HOT COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.

SPALLED AND DEFECTIVE CONCRETE SHALL BE REPAIRED WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.

PROPERLY SIZED HEAT RESISTANT BACKER ROD SHALL BE PLACED IN THE MOVEMENT GAP ALLOWING FOR 25MM (1 INCH) +/- OF BINDER ABOVE THE ROD.

THE BINDER MATERIAL SHALL BE HEATED AND PLACED AS RECOMMENDED BY THE MANUFACTURER.

PLACE 6 MM (1/4 INCH) THICK BY 200 MM (8 INCH) WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRESTAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER.

A. THE STEEL PLATES MAY BE OMITTED WHERE THE APPROACH SLAB IS COVERED WITH A STONE BASE OR BITUMINOUS PAVEMENT, AND VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.

THE BINDER MATERIAL AND AGGREGATE SHALL BE HEATED AND MIXED AS RECOMMENDED BY THE MANUFACTURER.

THE INSTALLATION OF MATERIAL, COMPACTION, AND TOPCOATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.

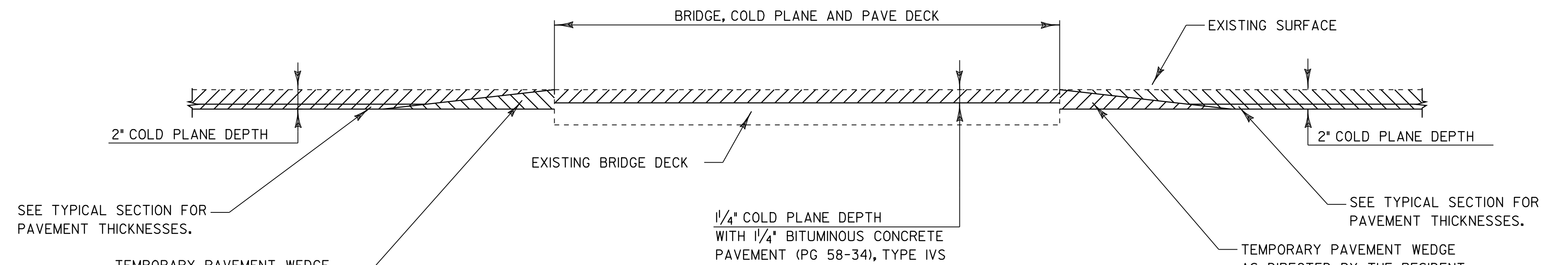
IMMEDIATELY AFTER TOPCOATING, AN ANTI-SKID MATERIAL SHALL BE CAST OVER THE JOINT TO REDUCE THE RISK OF TRACKING.

JOINT SHALL BE PROTECTED FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 52 C +/- (125 F).

WEATHER LIMITATIONS:

BINDER MATERIAL SHALL BE APPLIED ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL:

- (A) THE AMBIENT AIR TEMPERATURE IS AT LEAST 10° C (50° F) AND RISING.
- (B) THE ROAD SURFACE IS SUFFICIENTLY DRY.
- (C) WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF THE SATISFACTORY WORK.



BRIDGE COLD PLANE DETAIL

BRIDGE #59, #60 AND #64

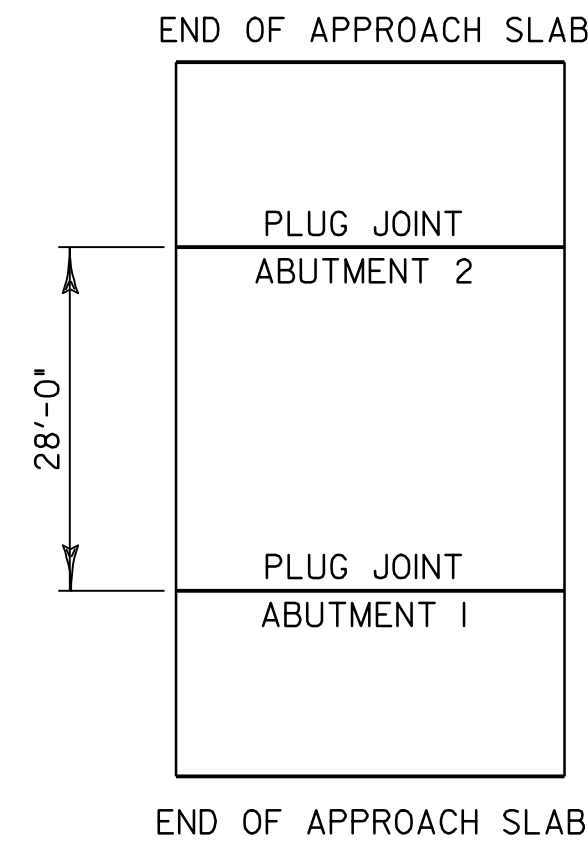
SEE TYPICAL SECTION FOR PAVEMENT THICKNESSES.

TEMPORARY PAVEMENT WEDGE AS DIRECTED BY THE RESIDENT ENGINEER

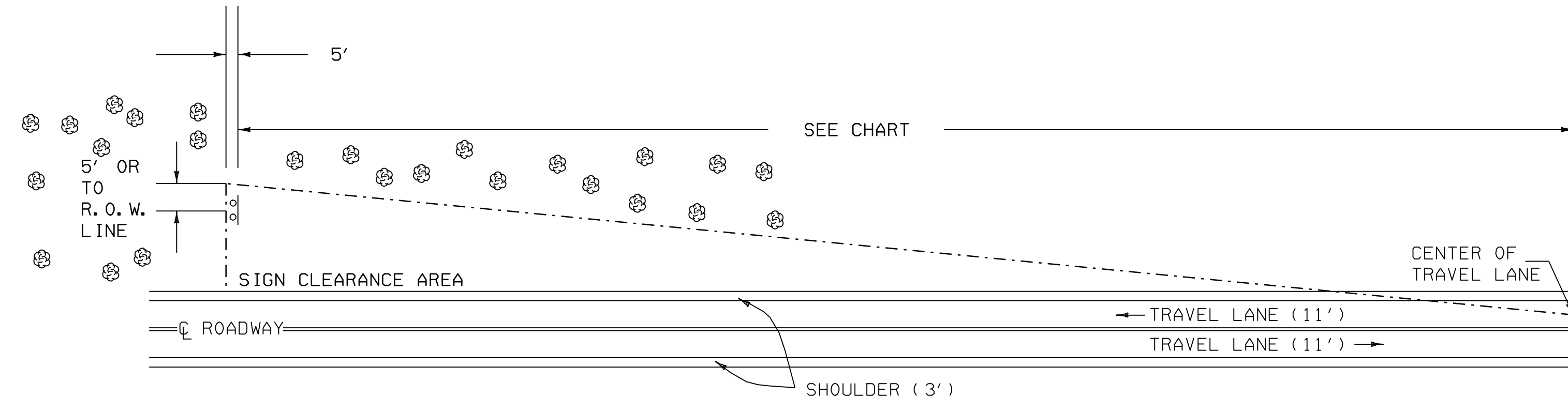
SEE TYPICAL SECTION FOR PAVEMENT THICKNESSES.

TEMPORARY PAVEMENT WEDGE AS DIRECTED BY THE RESIDENT ENGINEER

| JOINT | STATION | LENGTH |
|-----------------------|---------|----------|
| 1 | 306+68 | 36' - 0" |
| 2 | 306+96 | 36' - 0" |
| BRIDGE #60 | | 72' - 0" |
| TOTAL LENGTH OF JOINT | | |



**BRIDGE #60
FIELD STA. 306+85 (MM 5.812)
ROUTE LOG STA. 306+99 (MM 5.814)
COLD PLANE AND PAVE**



THE CONTRACTOR SHALL REMOVE ALL WOODY STEMMED GROWTH INCLUDING BRUSH, SAPLINGS, TREE LIMBS GROWING WITHIN OR PROJECTING INTO THE CLEARANCE AREA AND DOWN TO GROUND LEVEL. PAYMENT WILL BE FOR THINNING AND TRIMMING FOR SIGNS, ITEM 201.31, AND PAID FOR PER EACH. NO CHEMICALS (POISONS OR DEFOLIANTS) ALLOWED.

MINIMUM SIGN SIGHT DISTANCE CHART

| APPROACH SPEED (mph) | SIGHT DISTANCE (feet) |
|----------------------|-----------------------|
| 30 OR LESS | 300 |
| 35 | 350 |
| 40 | 400 |
| 45 | 450 |
| 50 | 500 |
| 55 | 550 |

THINNING AND TRIMMING FOR SIGNS DETAIL

| WILLIAMSTOWN | BARRE |
|----------------|---------------|
| STA. 212+96 LT | STA. 53+05 LT |
| STA. 220+82 LT | STA. 86+65 LT |
| STA. 221+03 RT | STA. 86+75 RT |
| STA. 259+55 RT | STA. 88+79 LT |
| STA. 347+05 RT | |

BRIDGE LOCATION AND DETAIL SHEET

PROJECT NAME: WILLIAMSTOWN-BARRE TOWN
PROJECT NUMBER: STP 2210 (I)S

FILE NAME: p99cl72.dgn
PROJECT LEADER: EPD
DESIGNED BY: CDL
PLOT FILE: p99cl72blds.1
PLOT DATE: 29-APR-2009 13:3
DRAWN BY: RHB
CHECKED BY: EPD
SHEET 7 OF 32

DETAILS ARE NOT TO SCALE