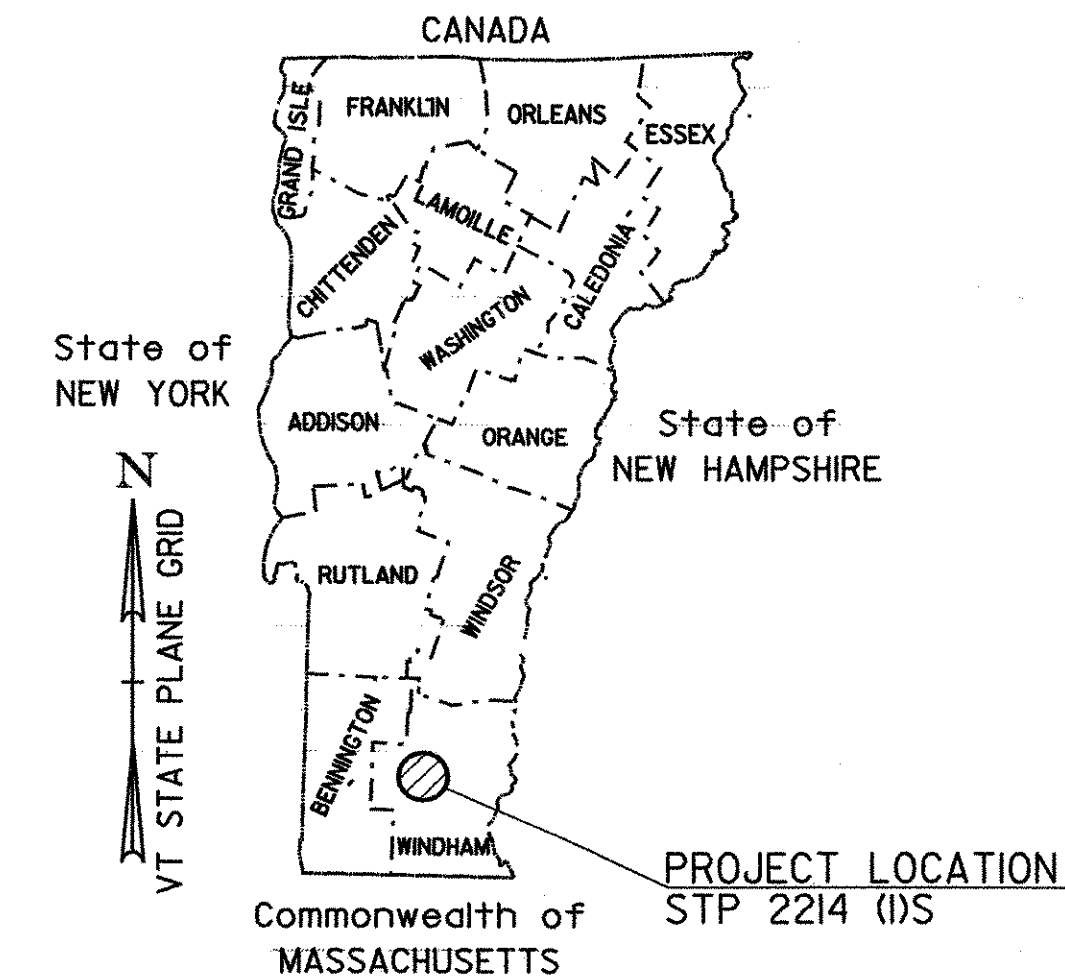
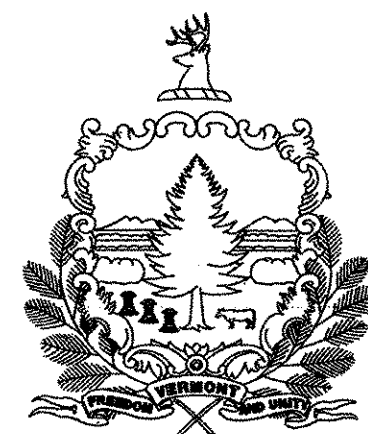


STATE OF VERMONT AGENCY OF TRANSPORTATION



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E-101M	CONSTRUCTION SIGN DETAILS	05/30/03
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PROPOSED IMPROVEMENT TOWNS OF DOVER & STRATTON COUNTY OF WINDHAM VT ROUTE 100

BEGINNING IN DOVER ON VT ROUTE 100 AT THE WILMINGTON/DOVER TOWN LINE, STATION 0+000.00 (MM 0.000) AND EXTENDING NORTHERLY ALONG VT ROUTE 100 A DISTANCE OF 11078.57 METERS (6.884 MILES) TO THE STRATTON/WARDSBORO TOWNLINE, STATION 2+151.89 (MM 1.337).

PROJECT DATA

FROM STA 0+000.00 (MM 0.000) TO STA 2+151.89 (MM 1.337)
LENGTH OF ROADWAY = 11 078.57 METERS (6.884 MILES)
LENGTH OF PROJECT = 11 078.57 METERS (6.884 MILES)

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECLAIMING AND RESURFACING OF THE EXISTING HIGHWAY WITH A BASE COURSE, WEARING COURSE, NEW PAVEMENT MARKINGS, GUARDRAIL, SIGNS AND INCIDENTAL ITEMS AS SHOWN IN THE PROJECT QUANTITIES.

TRAFFIC DATA

DOVER MM 0.00 TO MM 2.16			
2002	ADT	=	7000
2002	DHV	=	1300
2012	ADT	=	8200
2012	DHV	=	1500
2002-2012 ESALS = 700 000			
DOVER MM 2.16 TO MM 3.59			
2002	ADT	=	5900
2002	DHV	=	1100
2012	ADT	=	6900
2012	DHV	=	1300
2002-2012 ESALS = 561 000			
DOVER MM 3.59 TO MM 1.34 STRATTON			
2002	ADT	=	1600
2002	DHV	=	300
2012	ADT	=	1800
2012	DHV	=	340
2002-2012 ESALS = 254 000			

BITUMINOUS CONCRETE PAVEMENT SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE/DESIGN LIFE ESAL	820,500
DESIGN NUMBER OF GYRATIONS	75
PERFORMANCE GRADED ASPHALT BINDER	58-34

RECORD PLANS

CONTRACTOR: FW WHITCOMB CONSTRUCTION CORP. - WALPOLE, NH
RESIDENT ENGINEER: MARK HAUGHWOUT
CONSTRUCTION BEGAN: JULY 12, 2004
CONSTRUCTION COMPLETE: NOVEMBER 5, 2005
RECORD PLANS BY: MARK HAUGHWOUT & JUDY GILMORE

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.
BY *Mark W. Haughwout* RESIDENT ENGINEER
DATE *3/23/07*

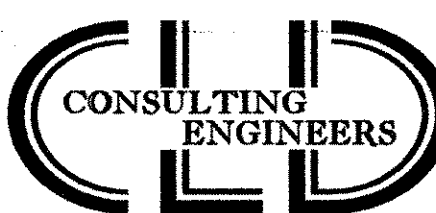
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.

BEGIN STP 2214(1)S
STA 0+000.00 11+000 WILMINGTON
(MM 0.000)

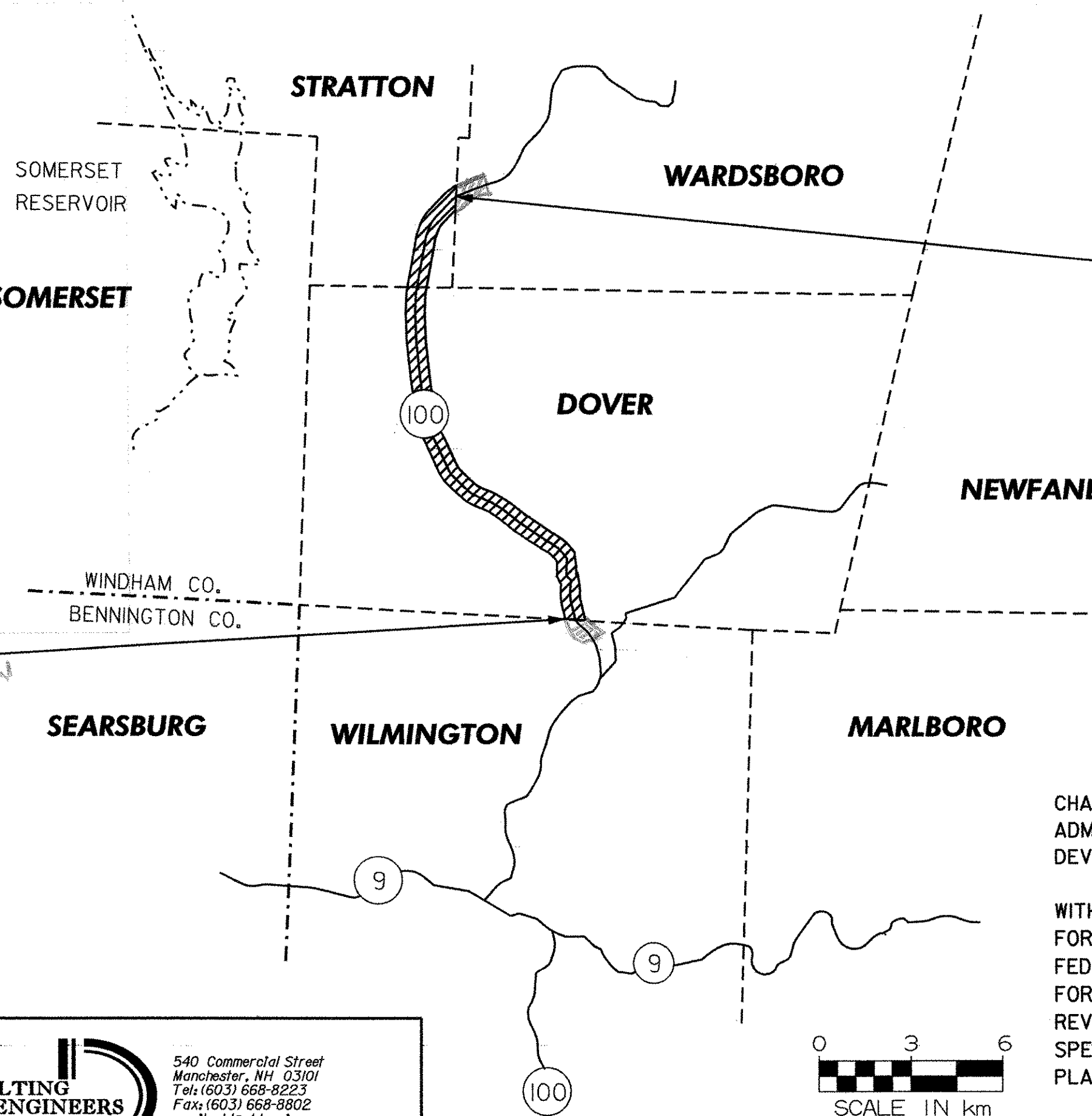
UNLESS OTHERWISE INDICATED, THE DRAWINGS AND DETAILS OF THESE PLANS ARE NOT TO SCALE.

SURVEYED BY :
SURVEYED DATE :
DATUM
VERTICAL N/A
HORIZONTAL N/A

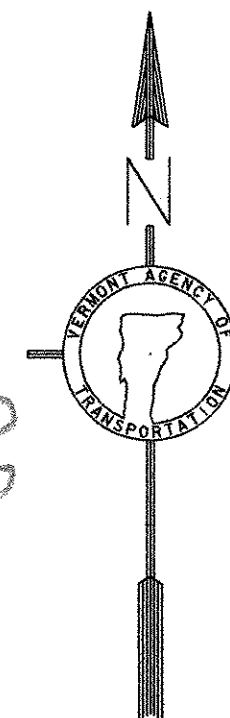
METRIC PLANS
PREPARED BY:



540 Commercial Street
Manchester, NH 03101
Tel: (603) 668-8223
Fax: (603) 668-8802
email: alo@cdengineering.com
Metric - New Hampshire - Vermont



END STP 2214(1)S
STA 2+151.89
(MM 1.337)
0+104.00
WARDSBORO



RIGHT-OF-WAY LIMITS, IF APPLICABLE, ARE PROVIDED SOLELY FOR THE CONVENIENCE OF THE STATE AND ITS CONTRACTOR DURING THE COURSE OF THIS PAVING PROJECT. ANY REFERENCES TO OFFSETS ON THESE PLANS ARE APPROXIMATE AND SHOULD NOT BE RELIED UPON FOR ANY OTHER PURPOSES.

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROJECT DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2001, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JANUARY 4, 2001 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

/pave/99b180/pb180.dgn
pb180+1



UNLESS NOTED OTHERWISE
STATIONS ARE IN KILOMETERS
ELEVATIONS ARE IN METERS
DIMENSIONS ARE IN MILLIMETERS

DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATOR	
APPROVED _____	DATE _____
DIRECTOR OF PROJECT DEVELOPMENT	
APPROVED <i>[Signature]</i>	DATE <i>3/14/04</i>
PROJECT MANAGER :	
PROJECT NAME : DOVER - STRATTON	
PROJECT NUMBER : AC STP 2214(1)S	
SHEET 1 OF 49 SHEETS	

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	

NOTES:

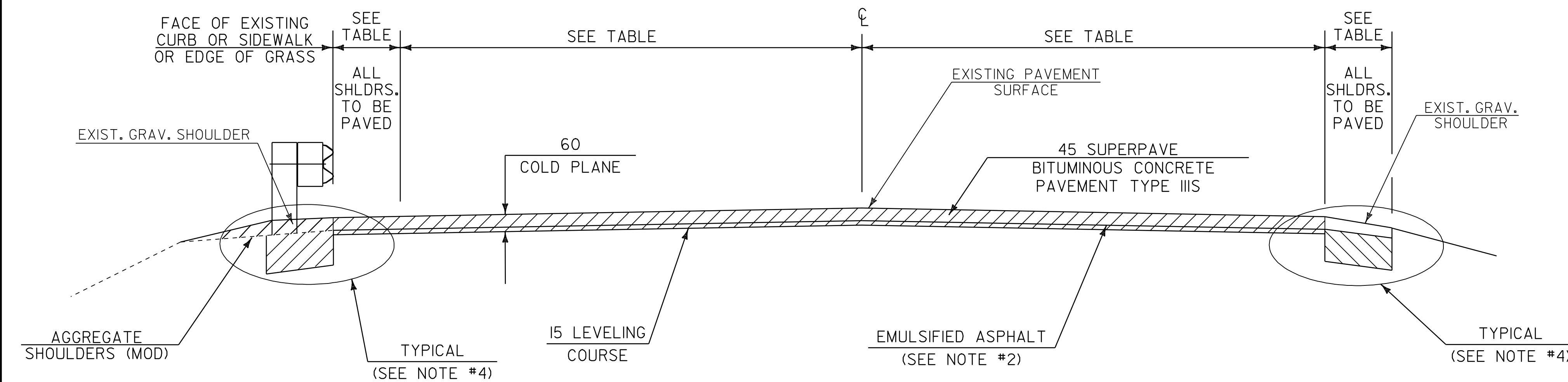
- THE PAVEMENT WEARING COURSE SHALL BE TYPE IIIS EXCEPT FOR BRIDGES 48, 50 AND 51 WHICH WILL BE TYPE IVS, THE BASE COURSE SHALL BE TYPE IS, AND THE LEVELING COURSE SHALL BE TYPE IVS ITEM 490.30, AS SHOWN ON THE TYPICALS, UNLESS DIRECTED BY THE RESIDENT ENGINEER. ALL LIQUID ASPHALT USED IN SUPERPAVE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 58-34.
- EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, ON COLD PLANED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT AT THE RATE OF 0.12 L/m² OR AS DIRECTED BY THE RESIDENT ENGINEER.
- SUPERPAVE BITUMINOUS CONCRETE PAVEMENT TOLERANCE = 5 mm +/- (TOTAL THICKNESS EXCLUDING LEVEL COURSE).
- EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 75 mm +/- OR AS DIRECTED BY THE ENGINEER. EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT AS DIRECTED BY THE RESIDENT ENGINEER. THIS WORK WILL BE PAID FOR USING THE APPROPRIATE RENTAL ITEMS. THE METHOD OF REMOVAL AND THE USE OF RENTAL ITEMS SHALL BE APPROVED BY THE RESIDENT ENGINEER PRIOR TO ANY WORK BEING DONE. MATERIAL REMOVED SHALL BE REPLACED WITH SUBBASE OF CRUSHED GRAVEL (FINE GRADED), ITEM 301.28(MOD).
- COLD PLANING SHALL BE COMPLETED ACCORDING TO TYPICAL OR AS DENOTED OTHERWISE ON THE PLANS. A FULL DEPTH BUTT JOINT SHALL BE CONSTRUCTED AT THE PROJECT BEGIN/END AND AT ALL SIDE ROAD APPROACHES AS SHOWN ON THE PROJECT PLANS OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. ALL BUTT JOINTS SHALL BE SAW CUT, SUBSIDIARY TO ITEM 490.30.
- ALL DRIVES SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER. ANY AND ALL REQUIRED EXCAVATION IN DRIVE AREAS SHALL BE AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 210.10 OR THE APPLICABLE RENTAL ITEM(S). IF REQUIRED, A NEW DRIVEWAY SUBBASE SHALL BE CONSTRUCTED AND WILL BE PAID FOR UNDER ITEM 301.28, SUBBASE OF CRUSHED GRAVEL (FINE GRADED) (MOD). A NEW BITUMINOUS SURFACE SHALL BE CONSTRUCTED AS DIRECTED AND WILL BE PAID FOR UNDER ITEM 490.30, QUANTITIES OF THE ABOVE ITEMS HAVE BEEN INCLUDED TO PAY FOR THIS WORK.
- ALL EDGES OF PAVEMENT SHALL BE BACKED UP FULL HEIGHT WITH COLD PLANE GRINDINGS AS DIRECTED BY THE RESIDENT ENGINEER AND WILL BE PAID FOR UNDER ITEM 402.12, AGGREGATE SHOULDERS (MOD). COLD PLANE GRINDINGS TO REMAIN THE PROPERTY OF THE STATE OF VERMONT.
- ESTIMATED QUANTITIES OF ITEM 608.25, EXCAVATOR RENTAL AND 608.37 TRUCK RENTAL HAVE BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING GUARDRAIL FLARES WITH EXCAVATED DITCHING MATERIAL. THE GUARDRAIL FLARES SHALL BE CAPPED WITH AN ESTIMATED 75 mm DEPTH OF AGGREGATE SHOULDERS (MOD) UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 5 TONS OF AGGREGATE SHOULDERS (MOD) FOR EACH GUARDRAIL TERMINAL. AN ESTIMATED QUANTITY OF EARTH BORROW HAS BEEN INCLUDED TO PROVIDE FOR ADDITIONAL MATERIAL IF NEEDED, FOR FLARE CONSTRUCTION.
- ALL BITUMINOUS CONCRETE PAVEMENT WORK, WHICH WILL INVOLVE SOME HAND-WORK (SUCH AS DRIVE AND SIDE ROAD APPROACHES AND AROUND DRAINAGE/UTILITY STRUCTURES), SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR ITEM 490.30, SUPERPAVE BITUMINOUS CONCRETE PAVEMENT.
- COMPACTION, GRADING, AND CLEAN UP OF ITEM 301.28, SUBBASE OF CRUSHED GRAVEL (MOD), ITEM 402.12, AGGREGATE SHOULDERS (MOD) AND ITEM 651.35, TOPSOIL, IS TO BE INCLUDED IN THE CONTRACT UNIT PRICE OF EACH ITEM.
- GRASS GROWING ADJACENT TO PAVEMENT, OR THROUGH CRACKS IN THE PAVEMENT, WHICH MAY HAMPER THE PLACEMENT OF NEW BITUMINOUS CONCRETE, SHALL BE REMOVED BY THE CONTRACTOR, AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT FOR THIS WORK WILL NOT BE MADE DIRECTLY, BUT WILL BE CONSIDERED SUBSIDIARY TO ITEM 490.30, SUPERPAVE BITUMINOUS CONCRETE PAVEMENT PG 58-34.
- YIELDING MARKER POSTS TO BE USED ONLY TO DELINEATE PIPE INLETS & OUTLETS.
- ONE METER OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 1.8 m POSTS. IF THIS CAN NOT BE OBTAINED THEN 2.4 m POSTS SHALL BE USED.
- PIPE INLET AND OUTLET AREAS, AND DITCH CLEANING THROUGH THE PROJECT, SHALL BE PERFORMED AT LOCATIONS AS DIRECTED ON THE DITCH CLEANING DETAIL SHEET AND AS DIRECTED BY THE RESIDENT ENGINEER. PAYMENT WILL BE UNDER THE APPLICABLE EQUIPMENT RENTAL ITEM(S).
- SPECIAL PRECAUTIONS MUST BE TAKEN TO PREVENT DAMAGE TO THE EXISTING MEMBRANES ON THE BRIDGES, THE CONTRACTOR SHALL PERFORM COLD PLANING WITH CAUTION AT THE LISTED LOCATIONS. ANY DAMAGES WHICH MAY OCCUR TO THE MEMBRANE AS A RESULT OF THIS PROJECT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR AND WILL BE REPLACED AT NO EXPENSE TO THE STATE OF VT.

NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

**PROJECT
TYPICAL
SHEET #1**

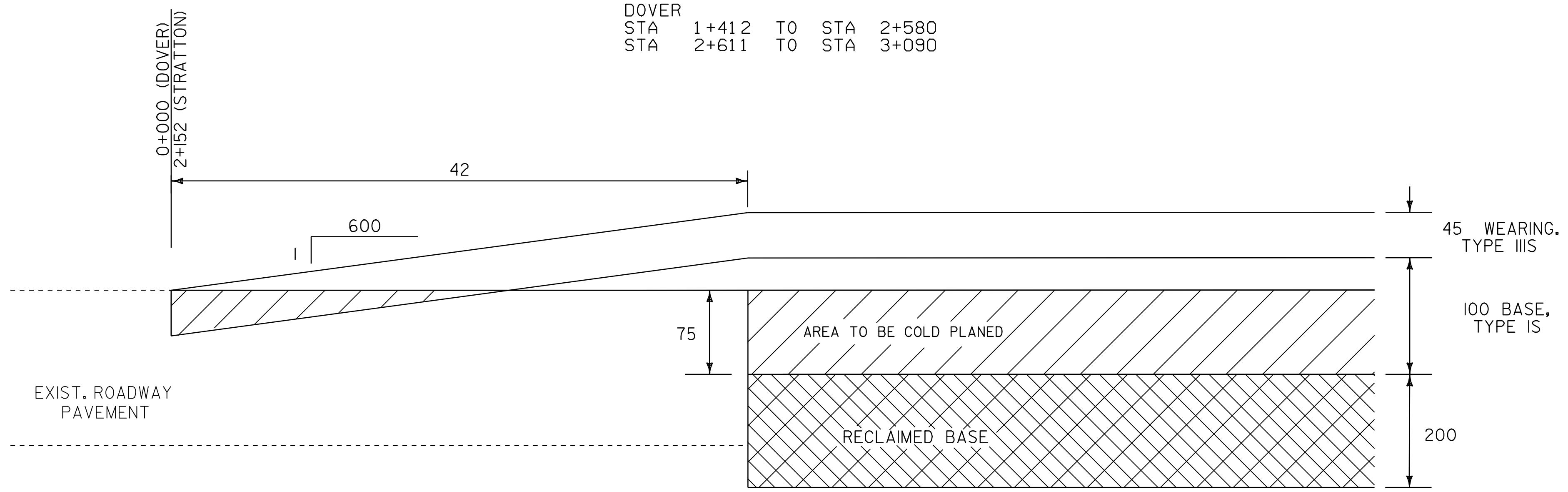
DOVER - STRATTON		PROJECT NO. : STP 2214(I)S
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IPARM FILE NAME: pbl80ty01	SURVEY DATE: 5/00	
SURVEYED BY: CLD_ENGINEERS_INC	DRAWN BY: MPS	
SQUAD LEADER: WRH	SHEET: 2 OF 49	

NOT TO SCALE



COLD PLANE TYPICAL SECTION

DOVER
STA 1+412 TO STA 2+580
STA 2+611 TO STA 3+090



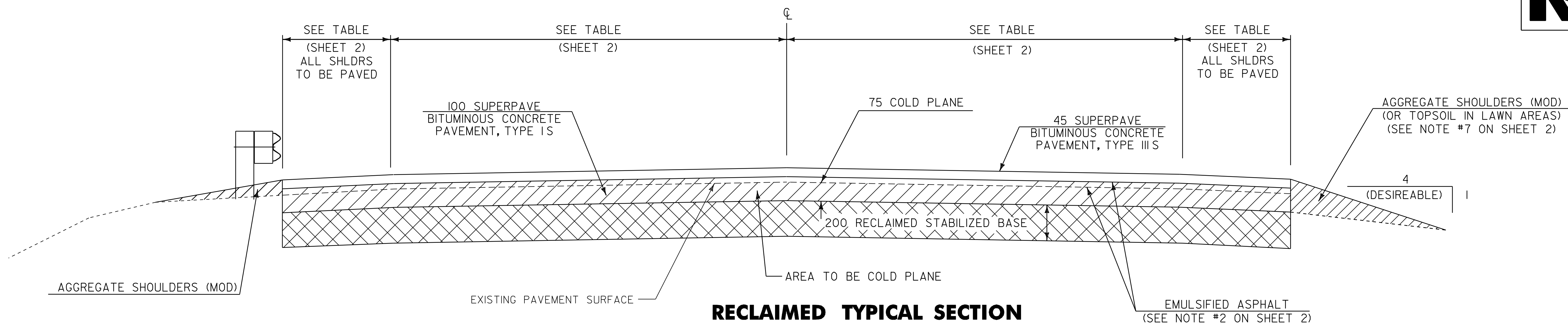
APPROACH AREA DETAIL

STA 0+000 DOVER BEGIN PROJECT
STA 2+152 STRATTON END PROJECT

PROJECT PAVING LIMITS

TOWN	BEGIN STATION	END STATION	LANE TYPICAL (m)	WEARING DEPTH (mm)	BASE DEPTH (mm)	LEVELING DEPTH (mm)	TOTAL (+)	NOTES
VT ROUTE 100								
DOVER	0+000	0+500	1.2-3.3-3.3-1.2	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	0+500	1+000	1.6-3.3-3.3-1.6	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	1+000	1+379	1.3-3.3-3.3-1.3	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	1+379	1+412	0.7-3.3-3.3-0.7	30		15	10	COLD PLANE 25 mm, LEVEL, OVERLAY, BRIDGE #56
	1+412	1+500	1.5-3.3-3.3-1.5	45		15	30	COLD PLANE 60 mm, LEVEL, OVERLAY
	1+500	2+090	1.1-3.3-3.3-1.1	45		15	187	COLD PLANE 60 mm, LEVEL, OVERLAY
	2+090	2+500	1.2-3.3-3.3-1.2	45		15	133	COLD PLANE 60 mm, LEVEL, OVERLAY
	2+500	2+580	1.4-3.3-3.3-1.4	45		15	27	COLD PLANE 60 mm, LEVEL, OVERLAY
	2+580	2+611	1.4-3.3-3.3-1.4	30		15	10	COLD PLANE 25 mm, LEVEL, OVERLAY, BRIDGE #58
	2+611	3+000	1.4-3.3-3.3-1.4	45		15	132	COLD PLANE 60 mm, LEVEL, OVERLAY
	3+000	3+090	1.3-3.3-3.3-1.3	45		15	30	COLD PLANE 60 mm, LEVEL, OVERLAY
	3+090	3+430	1.3-3.3-3.3-1.3	45	100			COLD PLANE 75, RECLAIM 200, LEVEL, OVERLAY
	3+430	3+446	1.3-3.3-3.3-1.3	30		15	5	COLD PLANE 25 mm, LEVEL, OVERLAY, BRIDGE #59
	3+446	5+600	1.3-3.3-3.3-1.3	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	5+600	5+770	VARIES SEE LAYOUT	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	5+770	5+839	2.6-3.3-3.3-2.6	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	5+839	5+948	VARIES SEE LAYOUT	45	100			COLD PLANE 75, RECLAIM 200, OVERLAY
	5+948	8+927	1.2-3.3-3.3-1.2	45	100			COLD PLANE 75, RECLAIM 200, LEVEL, OVERLAY, BRIDGES #60, 61, 62 & 63
STRATTON	0+000	2+152	1.0-3.3-3.3-1.0	45	100			COLD PLANE 75, RECLAIM. 200, LEVEL, OVERLAY BRIDGE#64

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A



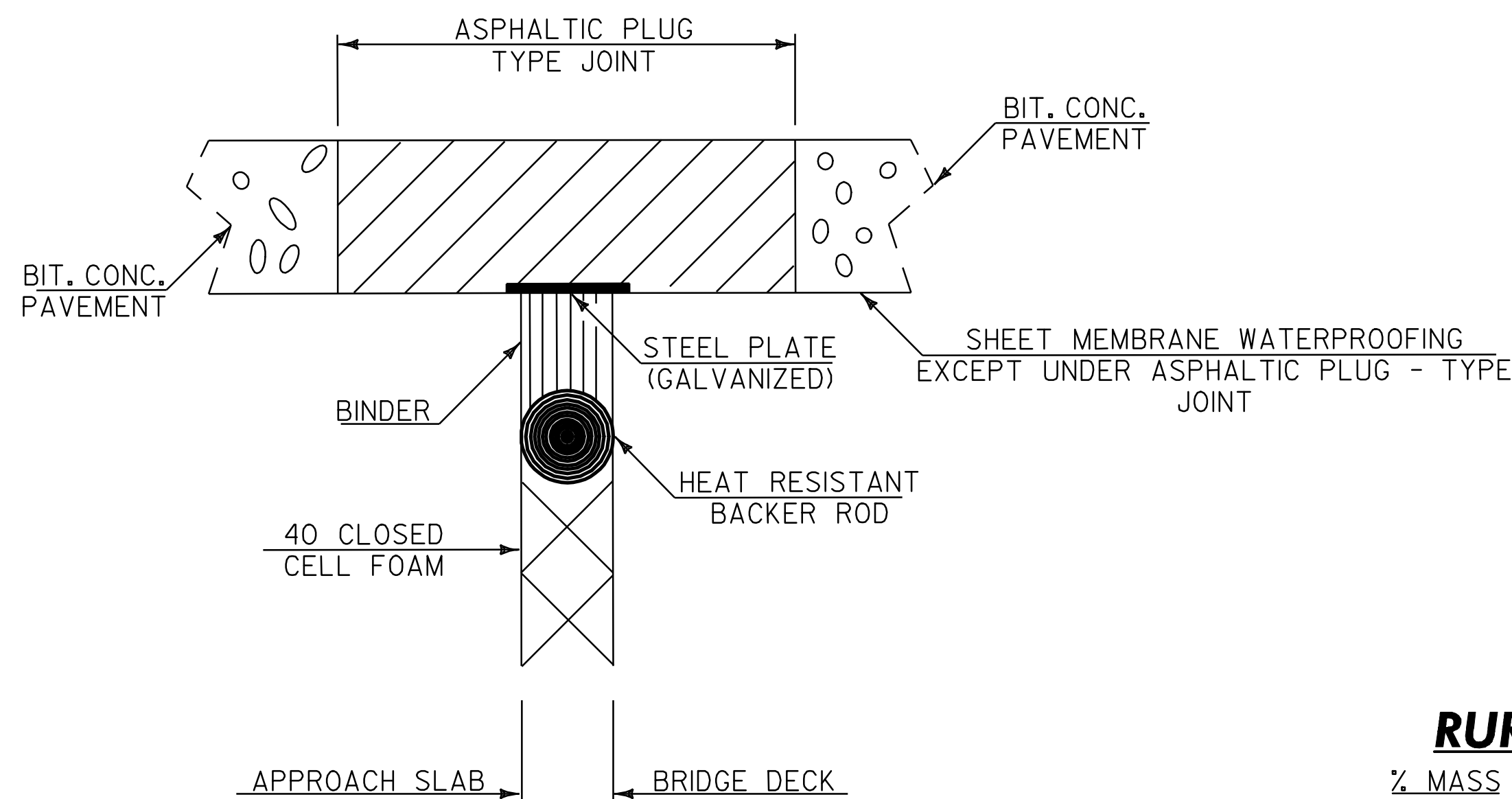
RECLAIMED TYPICAL SECTION

DOVER
 STA 0+000 TO STA 1+379
 STA 3+090 TO STA 3+430
 STA 3+446 TO STA 8+927
 STRATTON
 STA 0+000 TO STA 2+152

* REFER TO SPECIAL PROVISIONS FOR BASIS OF PAYMENT

NOTE:

I. ITEM 301.28, SUBBASE OF CRUSHED GRAVEL, (FINE GRADED) (MOD) FOR STABILIZATION HAS BEEN CALCULATED AT A DEPTH OF 25 mm FOR THE ENTIRE PROJECT EXCLUDING BRIDGES, TO BE USED AT THE RESIDENT ENGINEERS DISCRETION. THIS ITEM SHALL ALSO BE USED TO CORRECT MINOR VERTICAL ALIGNMENT AND DIPS IN THE ROADWAY AS DIRECTED BY THE RESIDENT ENGINEER.



ASPHALTIC PLUG-TYPE JOINT DETAIL

LOCATION

BRIDGE #58 (9.4m) @ STA 2+594.46 (MM 1.612) (@ 2 EXPANSION JOINTS)
 BRIDGE #59 (9.2m) @ STA 3+436.92 (MM 2.136) (@ 2 EXPANSION JOINTS)
 BRIDGE #56 (9.4m) @ STA 1+399.03 - MM 0.869 (@ 2 EXPANSION JOINTS)

RURAL AREA - SEED MIXTURE

% MASS	kg/ha	NAME	PUR %	GERM %
37.50	26	CREeping RED FESCUE	98	85
37.50	26	TALL FESCUE	95	90
5.00	4	RED TOP	95	90
15.00	10	BIRDSFOOT TREFOIL	98	85
5.00	4	ANNUAL RYE GRASS	95	85
100.00	70			

SEED MIXTURE:
 SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY MASS AND SHALL BE FREE OF ALL NOXIOUS SEED.
 SEED:
 TO BE APPLIED PER SEEDING FORMULAS OR AS DIRECTED BY THE RESIDENT ENGINEER.
 FERTILIZER:
 FORMULA 10-20-10 TO BE USED WITH SEED, APPLIED AT THE RATE OF 560 kg/ha. (HYDRO SEEDERS MAY USE 19-19-19 FORMULA)
 AGRICULTURAL LIMESTONE:
 TO BE APPLIED AT THE RATE OF 4500 kg/ha OR AS DIRECTED BY THE RESIDENT ENGINEER.
 HAY MULCH:
 TO BE PLACED ON EARTH SLOPES AT THE RATE OF 4500 kg/ha OR AS DIRECTED BY THE RESIDENT ENGINEER.
 TOPSOIL:
 TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.

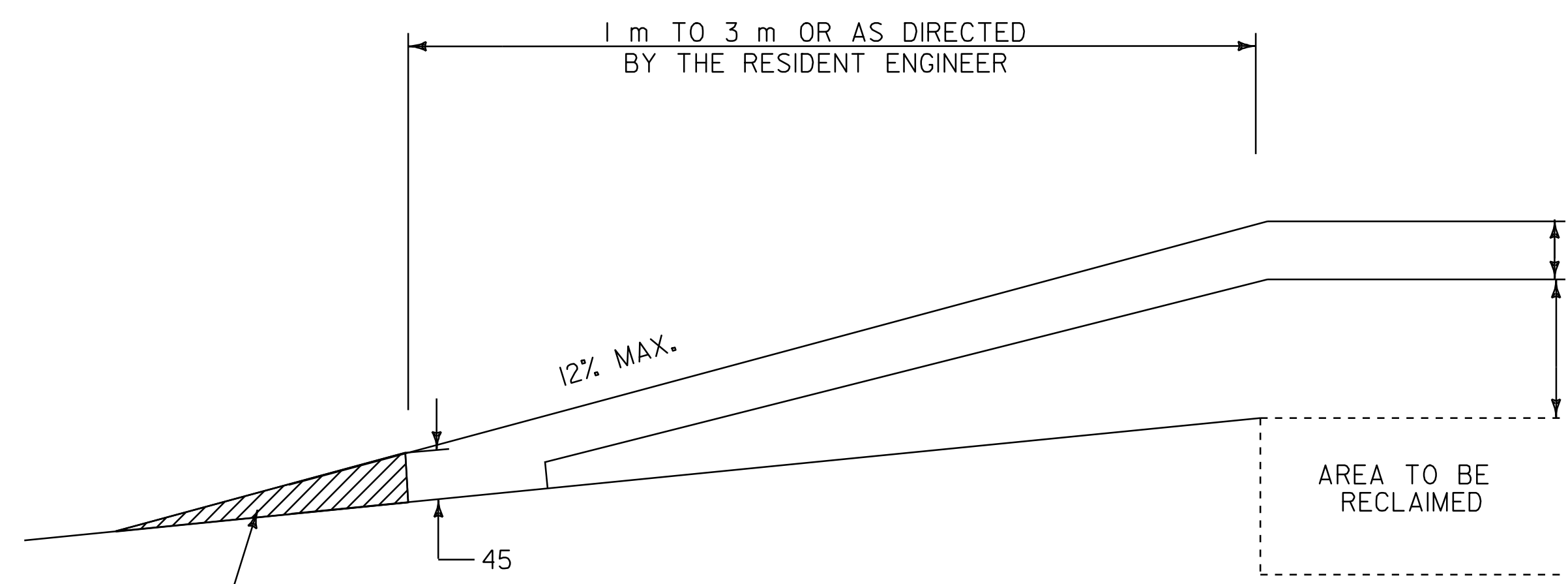
NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PROJECT TYPICAL SHEET #2

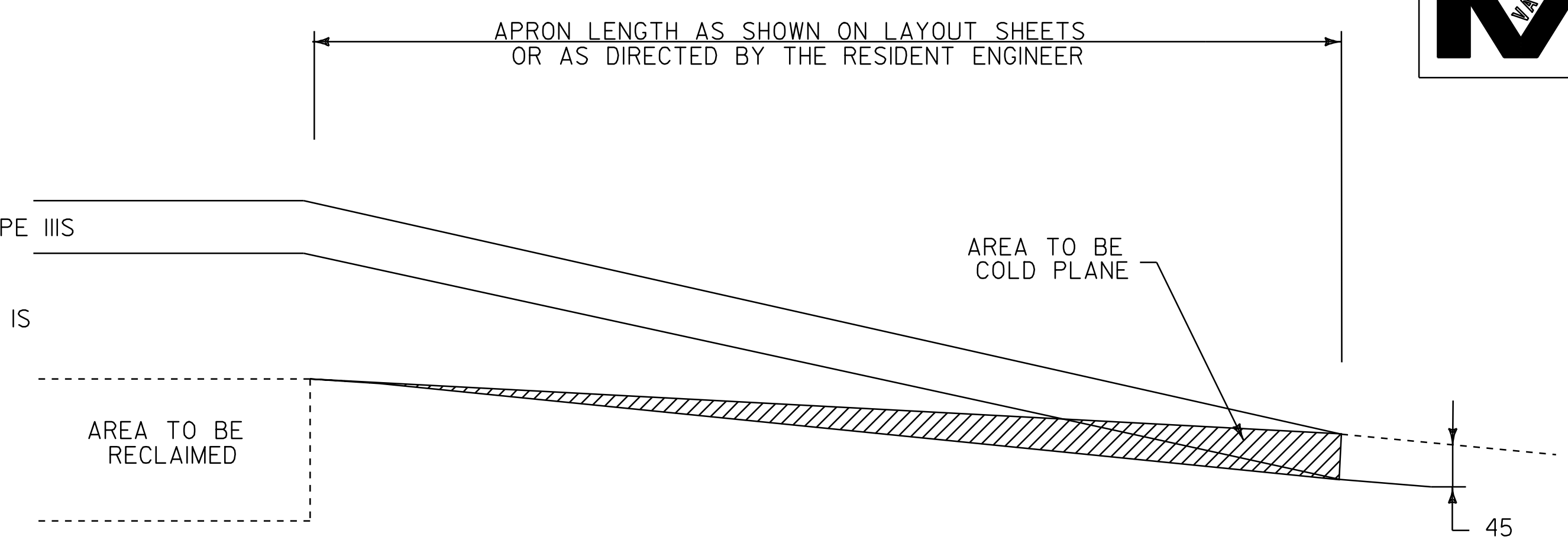
PROJECT: DOVER-STRATTON	PROJECT NO. : STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80+y02.i	SURVEY DATE: 5/00
SURVEYED BY: CLD_ENGINEERS_INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 3 OF 49

NOT TO SCALE

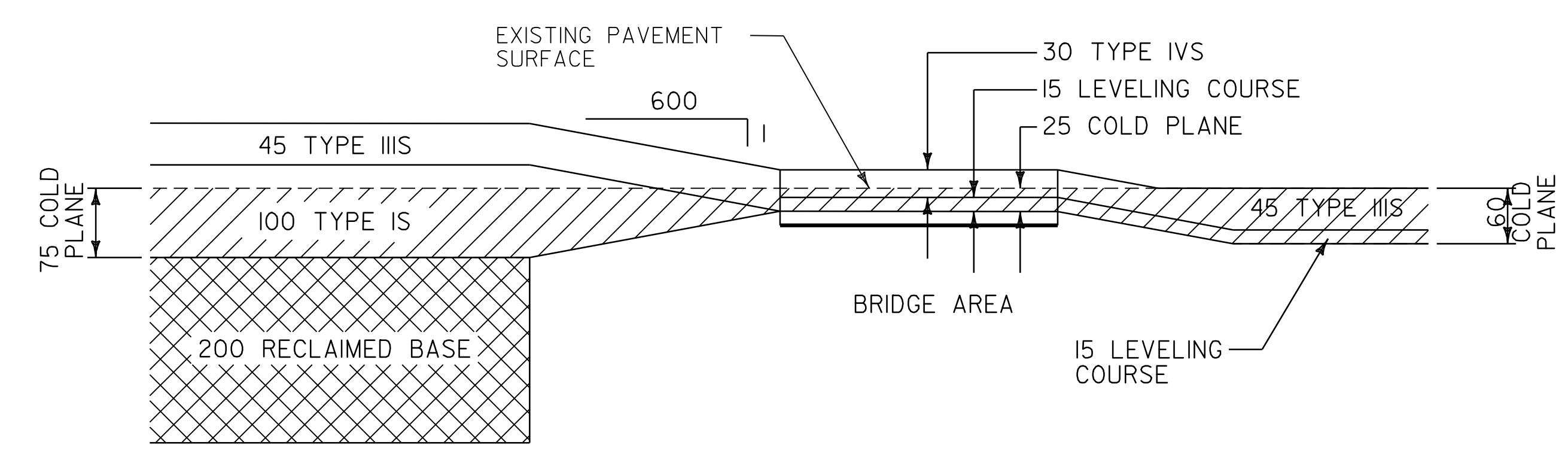


AGGREGATE SHOULDERS (MOD) @ GRAVEL DRIVE, PAVED DRIVES WILL BE SAW CUT, COLD PLANED AND NEW PAVEMENT WILL BE TAPERED FOR PROPER MATCH AS DETERMINED BY THE RESIDENT ENGINEER

DRIVEWAY TRANSITION DETAIL @ RECLAIMED STABILIZED BASE AREAS

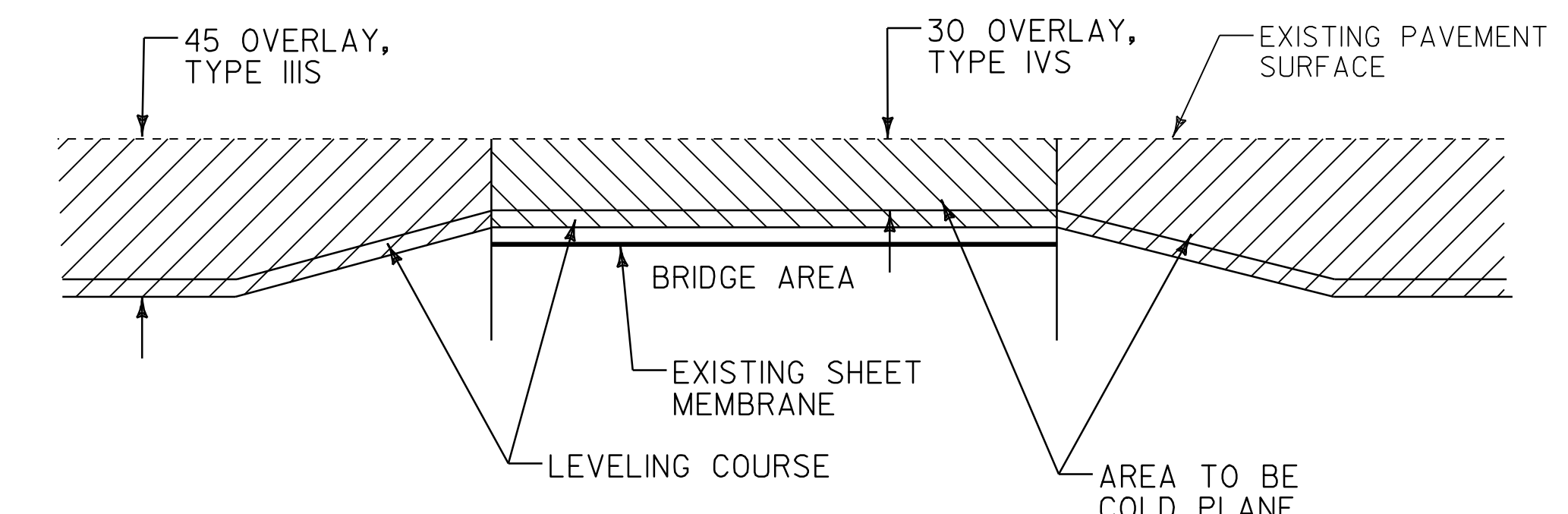


TOWN HIGHWAY TRANSITION DETAIL @ RECLAIMED STABILIZED BASE AREAS



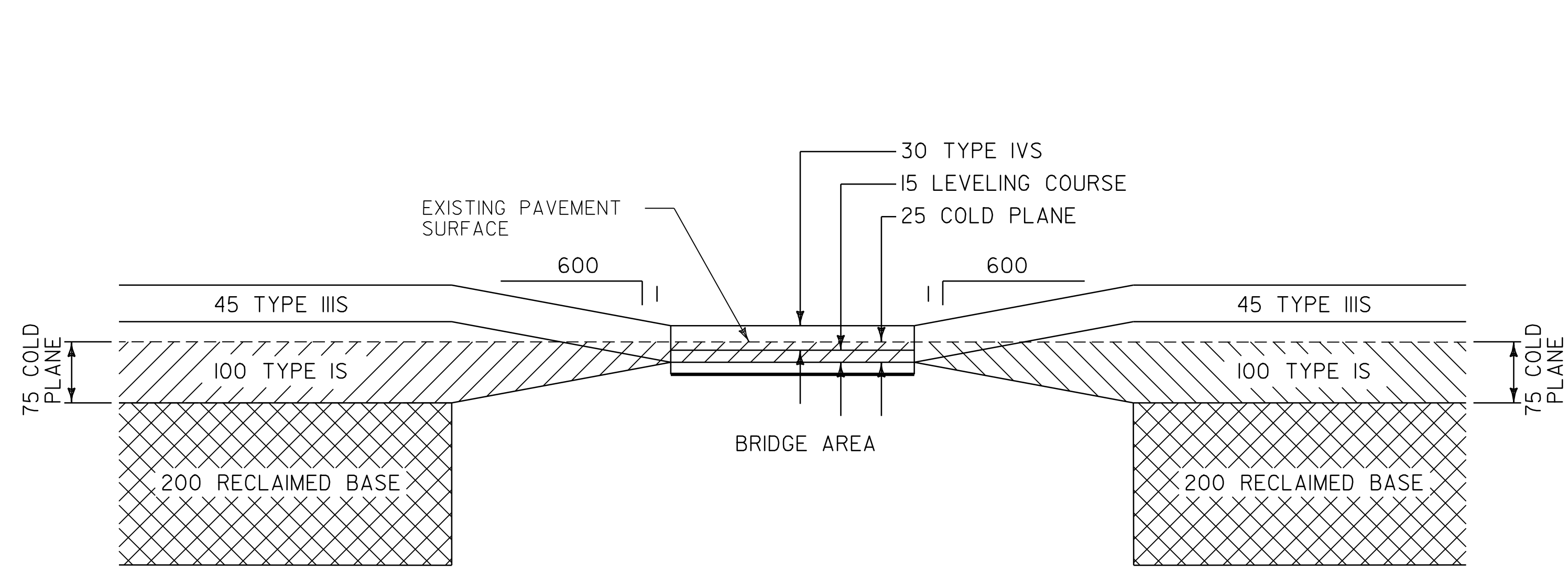
RECLAIMING/COLD PLANING DETAIL @ BRIDGE 56

DOVER
BRIDGE # 56 STA 1+379 TO STA 1+412



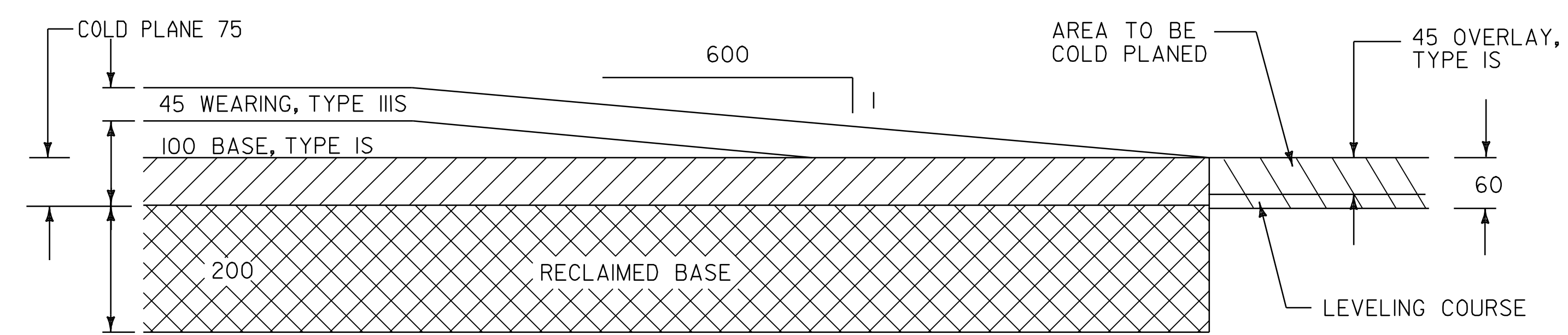
COLD PLANING DETAIL @ BRIDGE 58

DOVER
STA 2+580 TO STA 2+611



RECLAIMING DETAIL @ BRIDGE 59

DOVER
BRIDGE # 59 STA 3+430 TO STA 3+446



TRANSITION AREA DETAIL

DOVER
STA 3+090 END COLD PLANING/BEGIN RECLAIMING

NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

DATUM
VERTICAL _____
HORIZONTAL _____

**PROJECT
TYPICAL
SHEET #3**

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80+y03.i	SURVEY DATE:
SURVEYED BY: CLD	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 4 OF 49

NOT TO SCALE

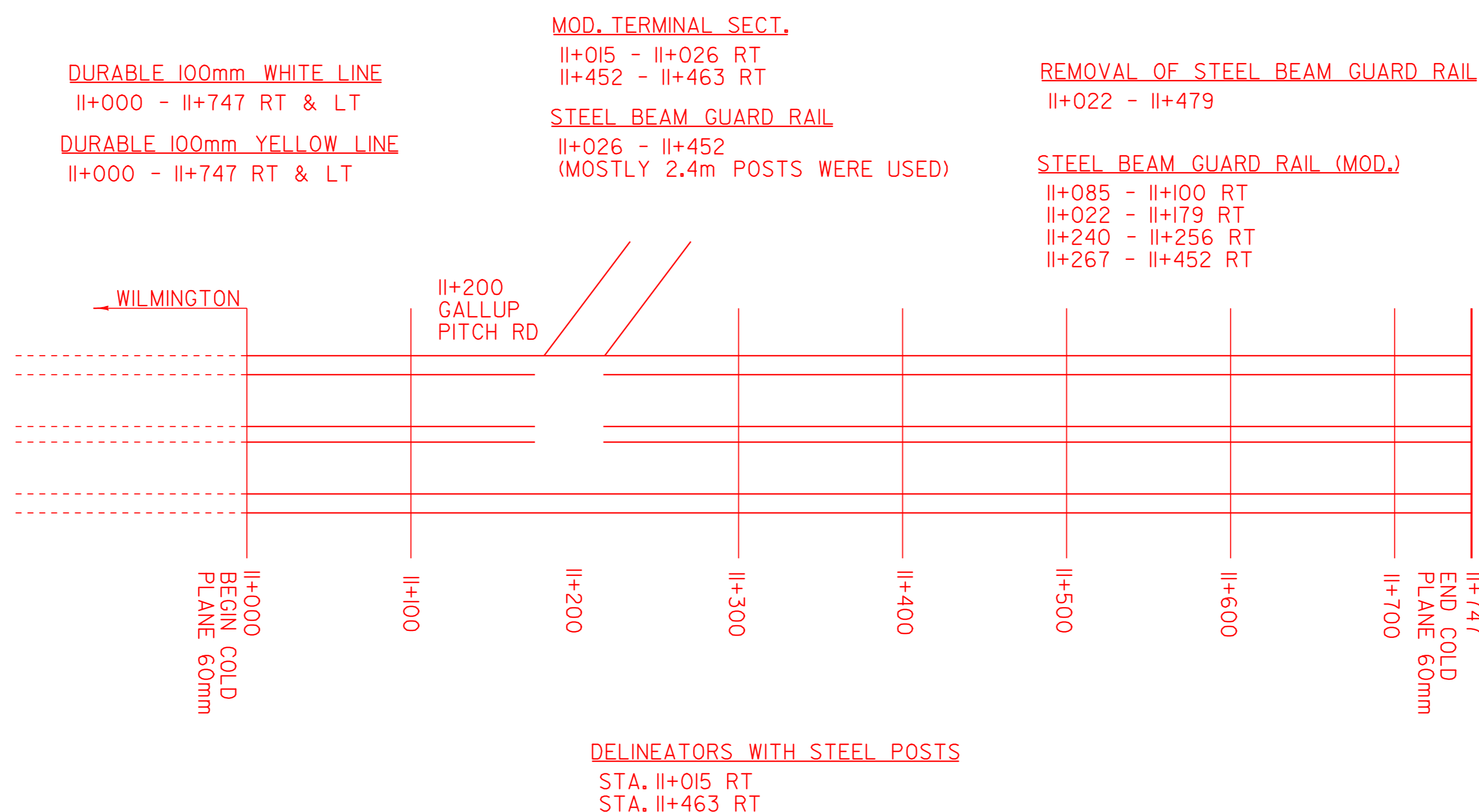
MARKER POST LOCATIONS FOR CULVERTS (CROSS-PIPES ONLY)
PAID AS ITEM 619.17, YEILDING MARKER POSTS

EACH STATION HAS 1 POST

WILMINGTON STA.

- II+047 RT
- II+050 LT
- II+111 RT
- II+115 LT
- II+163 RT SOUTH OF GALLUP PITCH ROAD
- II+168 LT SOUTH OF GALLUP PITCH ROAD
- II+237 RT NORTH OF GALLUP PITCH ROAD
- II+238 LT NORTH OF GALLUP PITCH ROAD
- II+335 RT
- II+337 LT
- II+403 LT
- II+470 RT
- II+472 LT
- II+503 RT
- II+507 LT
- II+551 RT
- II+555 LT
- II+616 RT
- II+619 LT
- II+653 RT
- II+661 LT
- II+701 RT
- II+703 LT
- II+737 RT
- II+739 LT
- II+403 RT

STOP SIGN AND LLL SIGN
WERE IN GOOD CONDITION -
NOT REPLACED



PLEASE NOTE:
THIS SECTION OF ROAD WAS NOT
STRAIGHT IT WAS ONLY DRAWN AS
SUCH FOR PURPOSES OF SIMPLICATION

NOT TO SCALE

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PROJECT: DOVER-STRATTON	PROJECT NO. : STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn IPARM FILE NAME: pbl80i01.i	PLOT DATE: 24-MAY-2007 1 SURVEY DATE: 5/00 DRAWN BY: J GILMORE SHEET: 9A OF 49

WILM STA. 11+779 - 11+790 RT
DOVER STA. 0+165 - 0+176 RT

TEMPORARY AND DURABLE 100 mm YELLOW LINE
STA 0+000 TO 0+219 SOLID LT & RT
STA 0+225 DOUBLE SOLID LT (TH #11)
STA 0+231 TO 0+600 SOLID LT & RT
STA 0+234 DOUBLE SOLID LT (TH #32)

MANUFACTURED TERMINAL SECTION (FLARED)
STA 0+565 RT
STEEL BEAM GUARDRAIL (2.4 m POST) (MOD)
STA 0+565 TO 0+600 RT
REMOVAL & DISPOSAL OF GUARDRAIL
STA 0+554 TO 0+600 RT

REMOVING SIGNS
AS SHOWN - 5
ERECTING SALVAGED SIGNS
AS SHOWN - 3
DELINEATORS W/ STEEL POSTS
STA. 11+779 RT
STA. 0+165 RT
STA. 0+565 RT

CHANGING ELEVATION OF SMH
STA. 0+440 LT

YIELDING MARKER POSTS

STA. 0+002 RT
STA. 0+010 LT
STA. 0+053 RT
STA. 0+057 LT
STA. 0+113 RT
STA. 0+117 LT
STA. 0+161 RT
STA. 0+164 LT
STA. 0+190 RT
STA. 0+194 LT
STA. 0+247 RT
STA. 0+250 LT
STA. 0+330 RT
STA. 0+334 LT
STA. 0+405 RT
STA. 0+416 LT

STONE FILL, TYPE J
GEOTEXTILE UNDER STONE FILL

STA. 0+040 TO 0+048 LT
STA. 0+155 TO 0+159 LT
STA. 0+247 TO 0+285 LT

TEMPORARY AND DURABLE 100 mm WHITE LINE
STA 0+000 TO 0+219 SOLID LT 197
STA 0+000 TO 0+600 SOLID RT
STA 0+237 TO 0+600 SOLID LT
46

STEEL BEAM GUARDRAIL
WILM. SB. 11+790 - 11+843 RT
*DOVER STA. 0+000 - 0+165 RT
(THIS RUN WAS ADDED TO ORIGINAL PLANS BY C.O. #2)

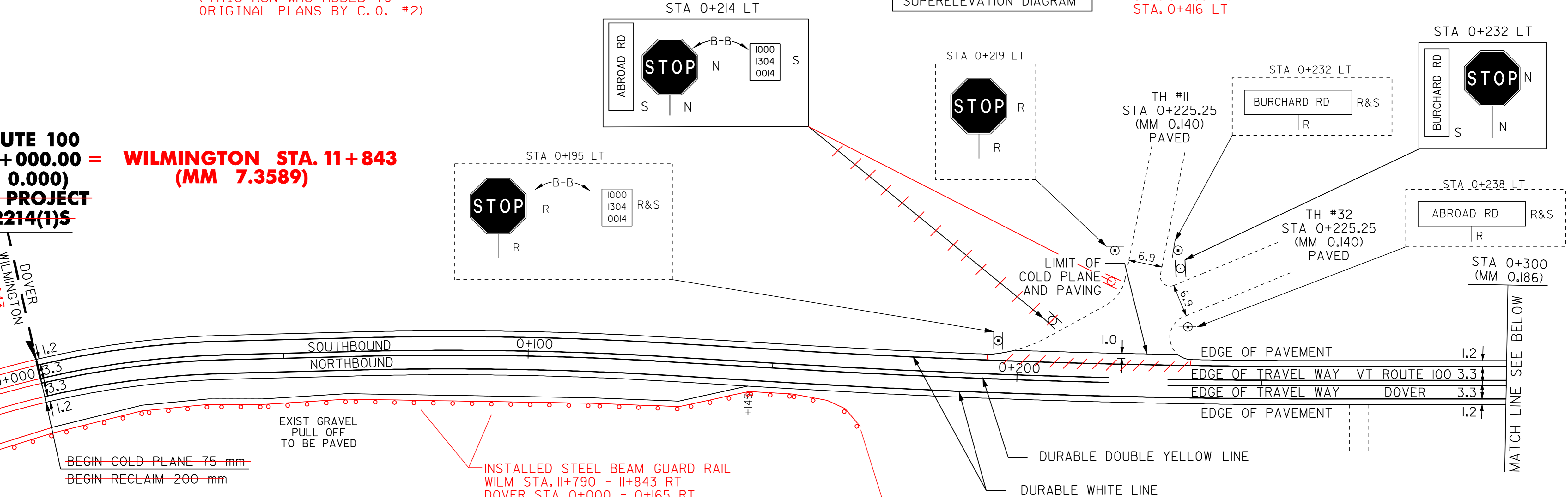
PC = 0+154
PT = 0+264
R = 1845
SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

VT ROUTE 100
DOVER STA 0+000.00 = WILMINGTON STA. 11+843
(MM 0.000)
BEGIN PROJECT
STP 2214(1)S

THE PROJECT WAS EXTENDED TO 11+747 (WILM. STA.) TO INCLUDE THE TRUCK PARKING AREA. THE FURTHER EXT. TO 11+000 WAS PART OF C.O. #2.

WILMINGTON BEGIN
STA 11+747
COLD PLANE 75 mm
RECLAIM 200 mm

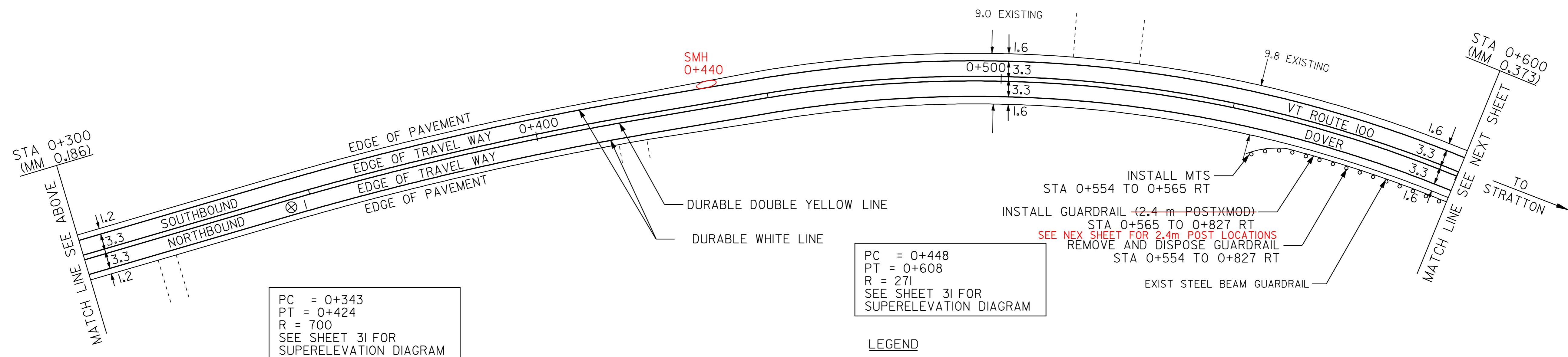
INSTALLED MTS
WILMINGTON STA. 11+779 - 11+790 RT



PC = -0+091
PT = 0+038
R = 185
SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

PC = 0+064
PT = 0+129
R = 750
SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

INSTALL MTS
DOVER 0+165 - 0+176 RT



PC = 0+343
PT = 0+424
R = 700
SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

PC = 0+448
PT = 0+608
R = 271
SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

- LEGEND
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	STA	TOTAL DEPTH (MM)	PCC
VERTICAL	N/A		
HORIZONTAL	N/A		

PAVEMENT CORES	STA	TOTAL DEPTH (MM)	PCC
I	STA 0+345 RT	146	NO

NOT TO SCALE

<h3>PAVING PROJECT LAYOUT #1</h3>	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(1)S
	DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbi80i01.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 10 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 0+600 TO 1+200 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 0+600 TO 1+200 SOLID LT
 STA 0+600 TO 1+200 SOLID RT

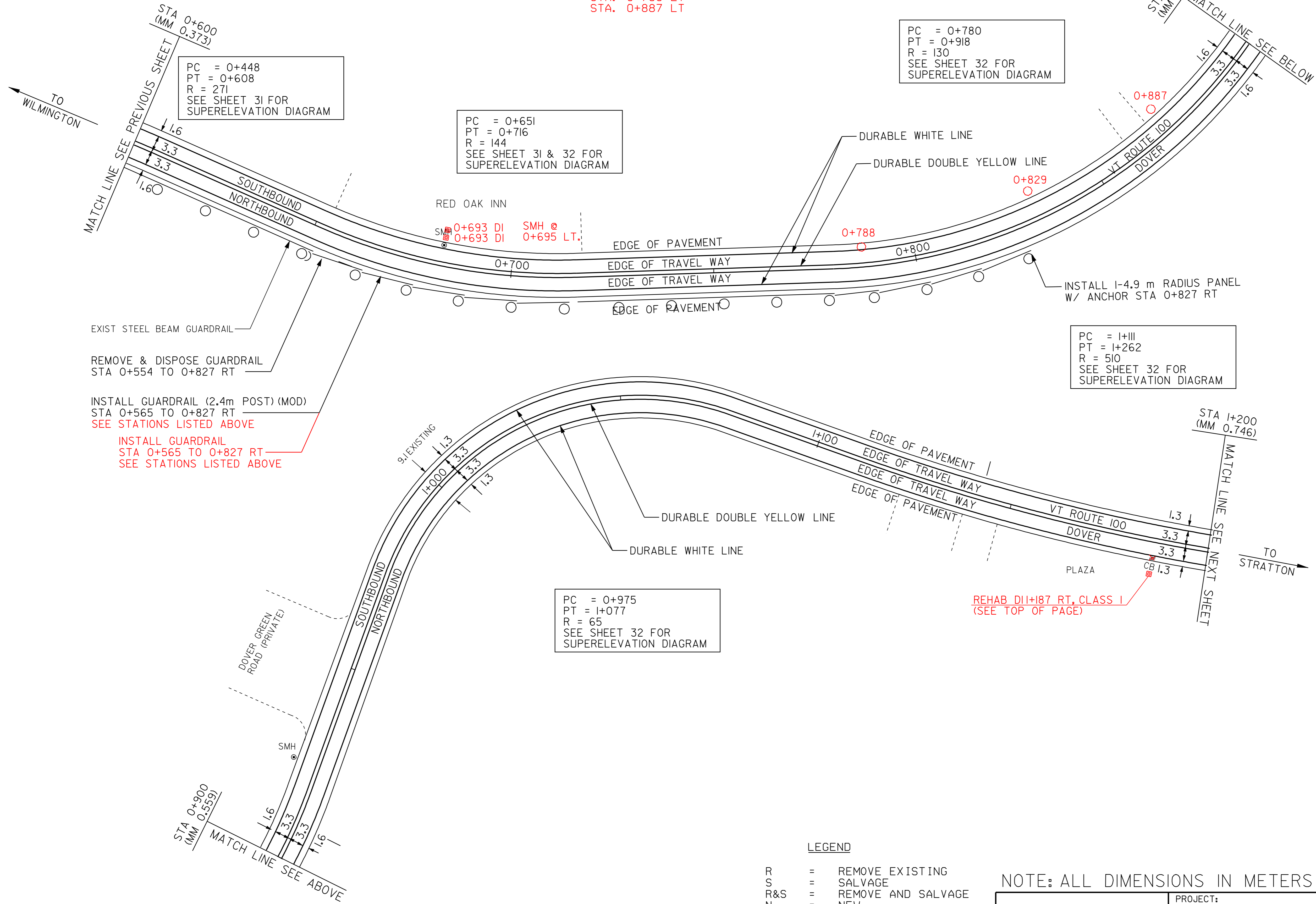
STEEL BEAM GUARDRAIL (2.4 m POST)(MOD)
 STA 0+600 TO 0+827 RT
 STA 0+750 TO 0+782 RT (UTILITY POLE)
 REMOVAL & DISPOSAL OF GUARDRAIL (FACTORED SPACING)
 STA 0+600 TO 0+827 RT
 ANCHOR FOR STEEL BEAM RAIL
 STA 0+827 RT

REHABILITATION OF DI, CB OR MH, CLASS I
 STA 1+187 RT
 STA. 0+693 LT - 2 EA (2 GRATES)
 CHANGING ELEVATION OF SMH
 STA 0+682 LT
 STA. 0+695 LT
 STA. 0+829 LT
 STA. 0+788 LT
 STA. 0+887 LT

STEEL BEAM GUARDRAIL
 STA. 0+600 TO 0+674 RT
 STA. 0+681 TO 0+750 RT
 STA. 0+782 TO 0+827 RT

DELINEATORS WITH STEEL POSTS
 STA. 0+827 RT

YIELDING MARKER POSTS
 STA. 0+689 RT
 STA. 0+777 LT
 STA. 0+873 RT
 STA. 0+874 LT
 STA. 1+085 LT
 STA. 1+085 RT
 STA. 1+129 LT
 STA. 1+168 RT
 STA. 1+178 LT



PC = 0+448
 PT = 0+608
 R = 271
 SEE SHEET 31 FOR SUPERELEVATION DIAGRAM

PC = 0+651
 PT = 0+716
 R = 144
 SEE SHEET 31 & 32 FOR SUPERELEVATION DIAGRAM

PC = 0+780
 PT = 0+918
 R = 130
 SEE SHEET 32 FOR SUPERELEVATION DIAGRAM

PC = 1+111
 PT = 1+262
 R = 510
 SEE SHEET 32 FOR SUPERELEVATION DIAGRAM

PC = 0+975
 PT = 1+077
 R = 65
 SEE SHEET 32 FOR SUPERELEVATION DIAGRAM

EXIST STEEL BEAM GUARDRAIL
 REMOVE & DISPOSE GUARDRAIL
 STA 0+554 TO 0+827 RT
 INSTALL GUARDRAIL (2.4m POST)(MOD)
 STA 0+565 TO 0+827 RT
 SEE STATIONS LISTED ABOVE
 INSTALL GUARDRAIL
 STA 0+565 TO 0+827 RT
 SEE STATIONS LISTED ABOVE

REHAB DI+187 RT, CLASS I
 (SEE TOP OF PAGE)

INSTALL 1-4.9 m RADIUS PANEL
 W/ ANCHOR STA 0+827 RT

- LEGEND
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE

PAVING PROJECT LAYOUT #2	PROJECT: DOVER-STRATTON	PROJECT NO. : STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i02.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: II OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 1+200 TO 1+344 SOLID LT & RT
 STA 1+356 TO 1+441 SOLID LT & RT
 STA 1+448 DOUBLE SOLID RT (SA #4)
 STA 1+453 TO 1+500 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 1+200 TO 1+338 SOLID LT
 STA 1+200 TO 1+435 SOLID RT
 STA 1+362 TO 1+500 SOLID LT
 STA 1+459 TO 1+500 SOLID RT

TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 1+448 RT (SA #4)

TEMPORARY AND DURABLE LETTER OR SYMBOL
 STA 1+448 RT - "STOP" (SA #4)

STEEL BEAM GUARDRAIL
 STA 1+354 TO 1+369 RT
 STA 1+375 TO 1+379 LT
 STA 1+413 TO 1+424 RT
 STA 1+422 TO 1+430 LT

HEAVY DUTY STEEL BEAM GUARDRAIL
 STA 1+369 TO 1+378 RT
 STA 1+379 TO 1+387 LT
 STA 1+405 TO 1+413 RT
 STA 1+414 TO 1+422 LT

REMOVAL & DISPOSAL OF GUARDRAIL
 STA 1+354 TO 1+378 RT
 STA 1+370 TO 1+387 LT
 STA 1+405 TO 1+420 RT
 STA 1+414 TO 1+440 LT

ANCHOR FOR STEEL BEAM RAIL

STA 1+424 RT
STA. 1+364 LT
MANUFACTURED TERMINAL SECTION (FLARED)
 STA 1+354 RT
 STA ~~1+375 LT~~ **CHANGED TO ANCHOR**
 STA 1+430 LT

REMOVING SIGNS
 AS SHOWN - 5

ERECTING SALVAGED SIGNS
 AS SHOWN - 2

DELINEATORS WITH STEEL POSTS

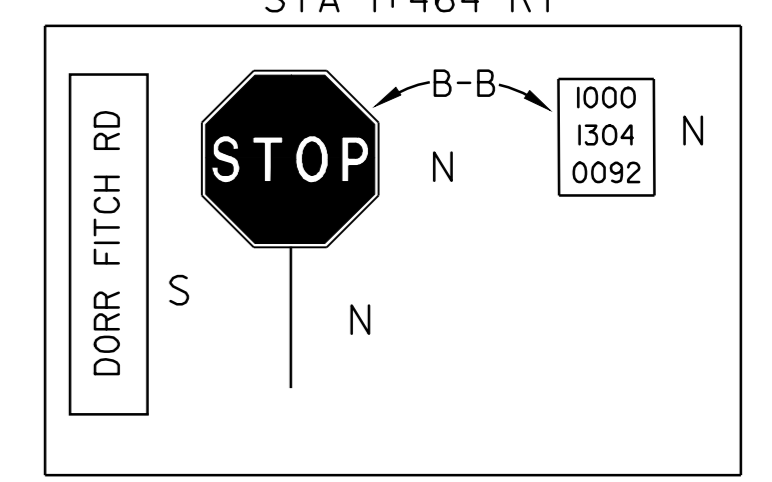
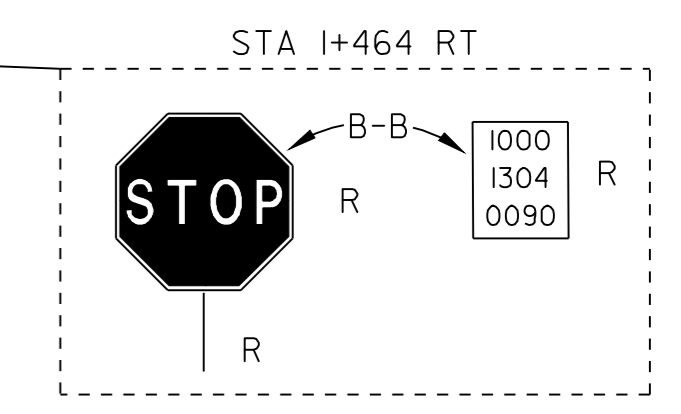
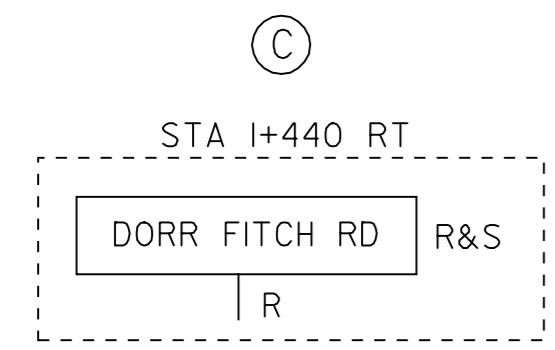
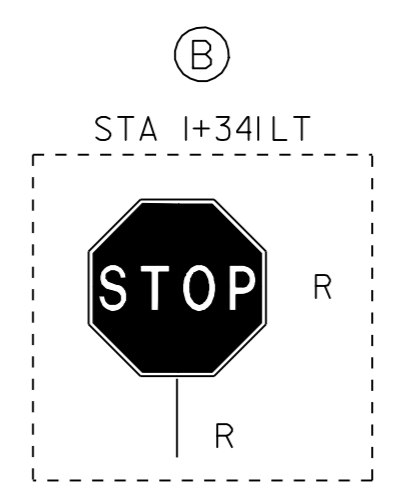
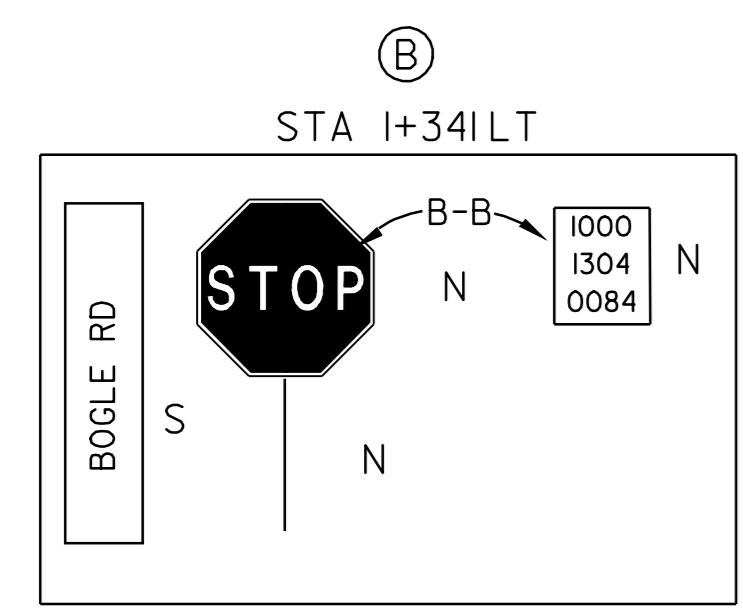
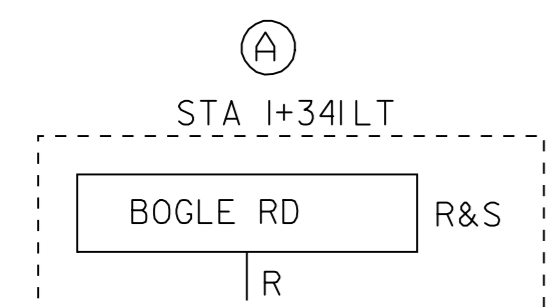
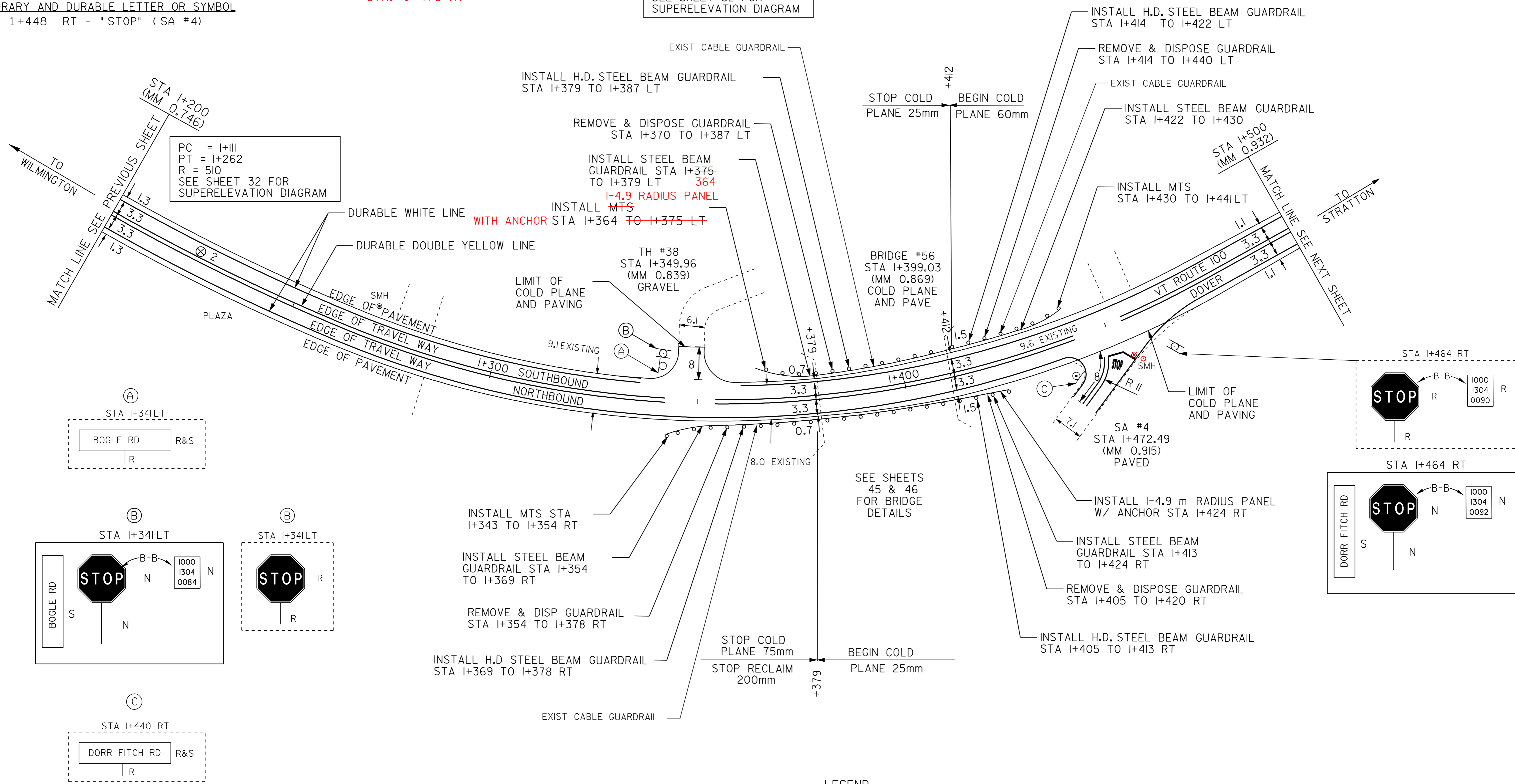
STA. 1+343 RT
 STA. 1+364 LT
 STA. 1+424 RT
 STA. 1+430 LT

YIELDING MARKER POSTS

STA. 1+225 RT
 STA. 1+226 LT

CHANGING ELEVATION OF SMH
 STA. 1+470 RT

PC = I+262
 PT = I+476
 R = 237
 SEE SHEET 32 FOR SUPERELEVATION DIAGRAM



- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	STA	TOTAL DEPTH (MM)	PCC
VERTICAL	N/A		
HORIZONTAL	N/A		
	2 STA I+222 LT	165	N0

NOT TO SCALE

PAVING PROJECT LAYOUT #3	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbi80i03.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 12 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 1+500 TO 1+583 SOLID LT & RT
 STA 1+595 TO 1+769 SOLID LT & RT
 STA 1+775 DOUBLE SOLID LT (TH #9)
 STA 1+775 DOULBE SOLID RT (TH #15)
 STA 1+781 TO 1+800 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 1+500 TO 1+772 SOLID LT
 STA 1+500 TO 1+577 SOLID RT
 STA 1+601 TO 1+772 SOLID RT
 STA 1+778 TO 1+800 SOLID LT
 STA 1+778 TO 1+800 SOLID RT

TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 1+775 LT (TH #9)
 STA 1+775 RT (TH #15)

TEMPORARY AND DURABLE LETTER OR SYMBOL
 STA 1+775 LT - "STOP" (TH #9)
 STA 1+775 RT - "STOP" (TH #15)

STEEL BEAM GUARDRAIL
 STA 1+686 TO 1+771 LT
 STA 1+779 TO 1+800 LT

REMOVAL & DISPOSAL OF GUARDRAIL
 STA 1+762 TO 1+771 LT
 STA 1+779 TO 1+800 LT

REMOVAL & DISPOSAL OF GUIDE POSTS
 STA 1+730 TO 1+748 LT (4)

HEAVY DUTY STEEL BEAM GUARDRAIL
 T.H. #9 STA 1+771 LT (10M)
 T.H. #9 STA 1+779 LT (10M)

STEEL BEAM GUARDRAIL (2.4m POSTS) (MOD.)
 STA. 1+686 TO 1+755 LT

DELINEATORS WITH STEEL POSTS
 STA. 1+686 LT
 STA. 1+762 LT
 STA. 1+788 LT

REHABILITATING
CHANGING ELEV. OF DI, CB OR MH, CLASS I

~~STA 1+539 RT~~
 STA 1+628 RT
 STA 1+716 RT
 STA 1+765 RT

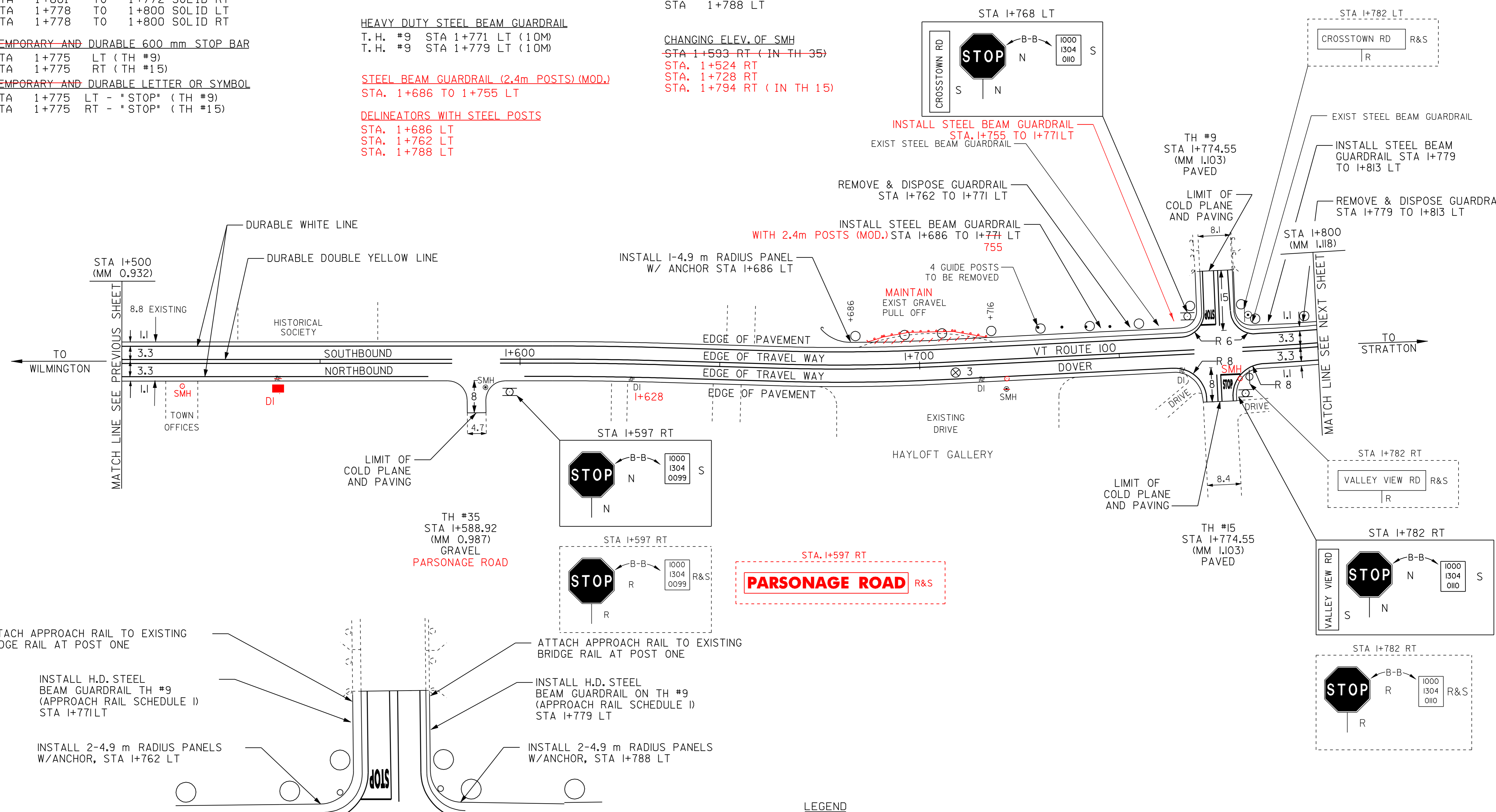
ANCHOR FOR STEEL BEAM RAIL
 STA 1+686 LT
 STA 1+762 LT
 STA 1+788 LT

CHANGING ELEV. OF SMH
~~STA 1+593 RT (IN TH 35)~~
 STA. 1+524 RT
 STA. 1+728 RT
 STA. 1+794 RT (IN TH 15)

REMOVING SIGNS
 AS SHOWN - 8

ERECTING SALVAGED SIGNS
 AS SHOWN - 5

YIELDING MARKER POSTS
 STA. 1+721 LT
 STA. 1+770 RT
 STA. 1+772 LT



DETAIL OF GUARDRAIL ON TH #9

DATUM	STA	TOTAL DEPTH (MM)	PCC
VERTICAL	N/A		
HORIZONTAL	N/A		
	3 STA 1+709 RT	140	N0

NOT TO SCALE

- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #4	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i04.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 13 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 2+400 TO 2+545 SOLID LT & RT
 STA 2+557 TO 2+800 SOLID LT & RT

STEEL BEAM GUARDRAIL
 STA 2+543 TO 2+570 LT
 STA 2+578 TO 2+586 RT
 STA 2+605 TO 2+608 LT
 STA 2+621 TO 2+632 RT
 40

HEAVY DUTY STEEL BEAM GUARDRAIL
 STA 2+570 TO 2+578 LT
 STA 2+586 TO 2+594 RT
 STA 2+597 TO 2+605 LT
 STA 2+613 TO 2+621 RT

REMOVING SIGNS
 AS SHOWN - 3

DELINEATORS WITH STEEL POSTS
 STA. 2+532 LT
 STA. 2+567 RT
 STA. 2+608 LT
 STA. 2+632 RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 2+400 TO 2+800 SOLID LT
 STA 2+400 TO 2+539 SOLID RT
 STA 2+563 TO 2+800 SOLID RT

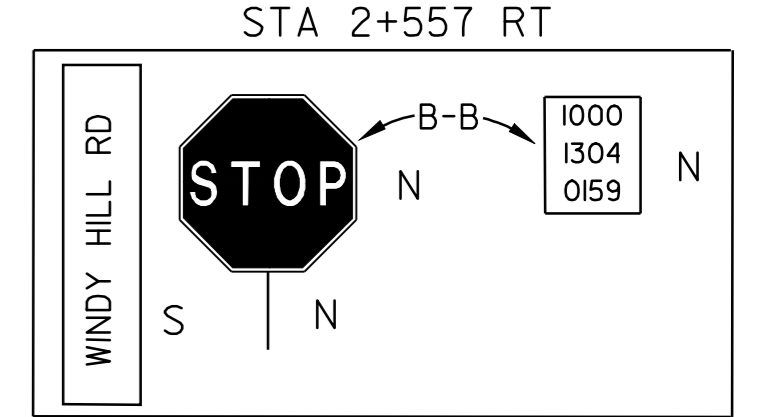
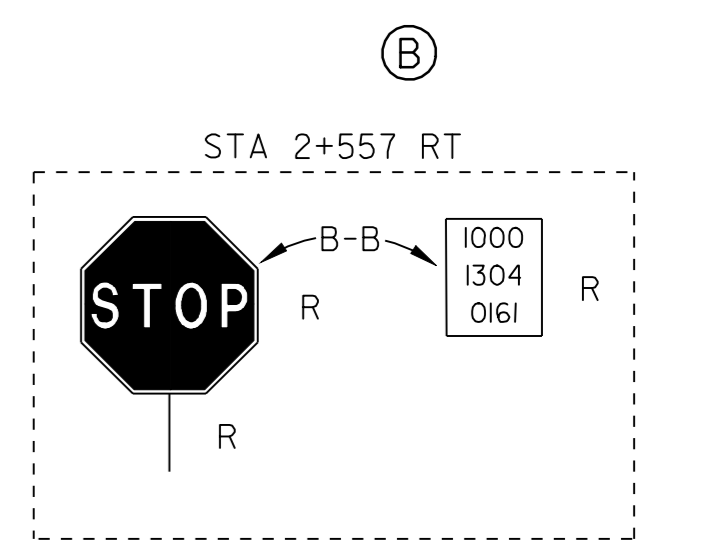
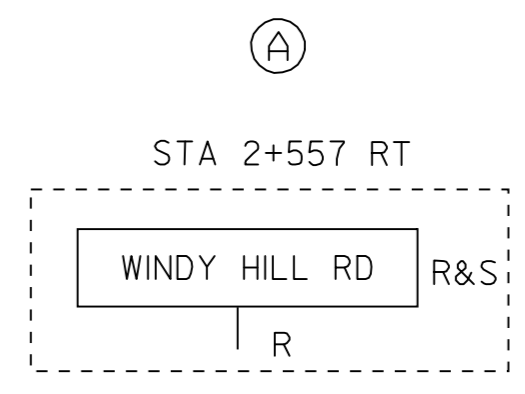
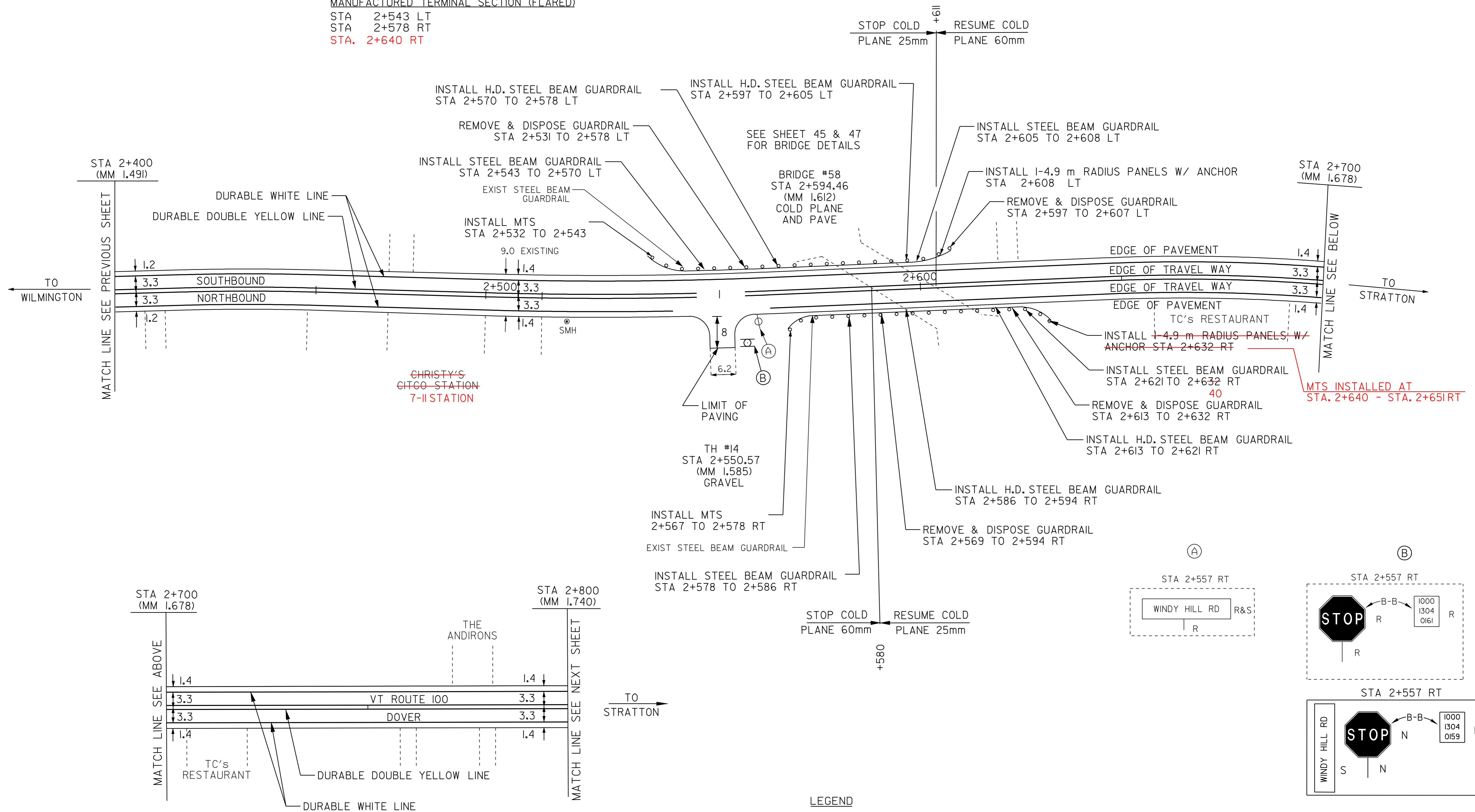
REMOVAL & DISPOSAL OF GUARDRAIL
 STA 2+531 TO 2+578 LT
 STA 2+569 TO 2+594 RT
 STA 2+597 TO 2+607 LT
 STA 2+613 TO 2+632 RT

ANCHOR FOR STEEL BEAM RAIL
 STA 2+608 LT
 STA 2+632 RT

ERECTING SALVAGED SIGNS
 AS SHOWN - 1

YIELDING MARKER POSTS
 STA. 2+522 RT

MANUFACTURED TERMINAL SECTION (FLARED)
 STA 2+543 LT
 STA 2+578 RT
 STA. 2+640 RT



- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - NEW RAIL = NEW RAIL
 - EXISTING RAIL = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE

PAVING PROJECT LAYOUT #6	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbi80i06.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 15 OF 49	

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 2+800 TO 2+844 SOLID LT & RT
 STA 2+850 DOUBLE SOLID LT (TH #9)
 STA 2+856 TO 3+400 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 2+800 TO 2+847 SOLID LT
 STA 2+800 TO 3+400 SOLID RT
 STA 2+853 TO 3+400 SOLID LT

TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 2+850 LT (TH #9)

REMOVING SIGNS
 AS SHOWN - 2

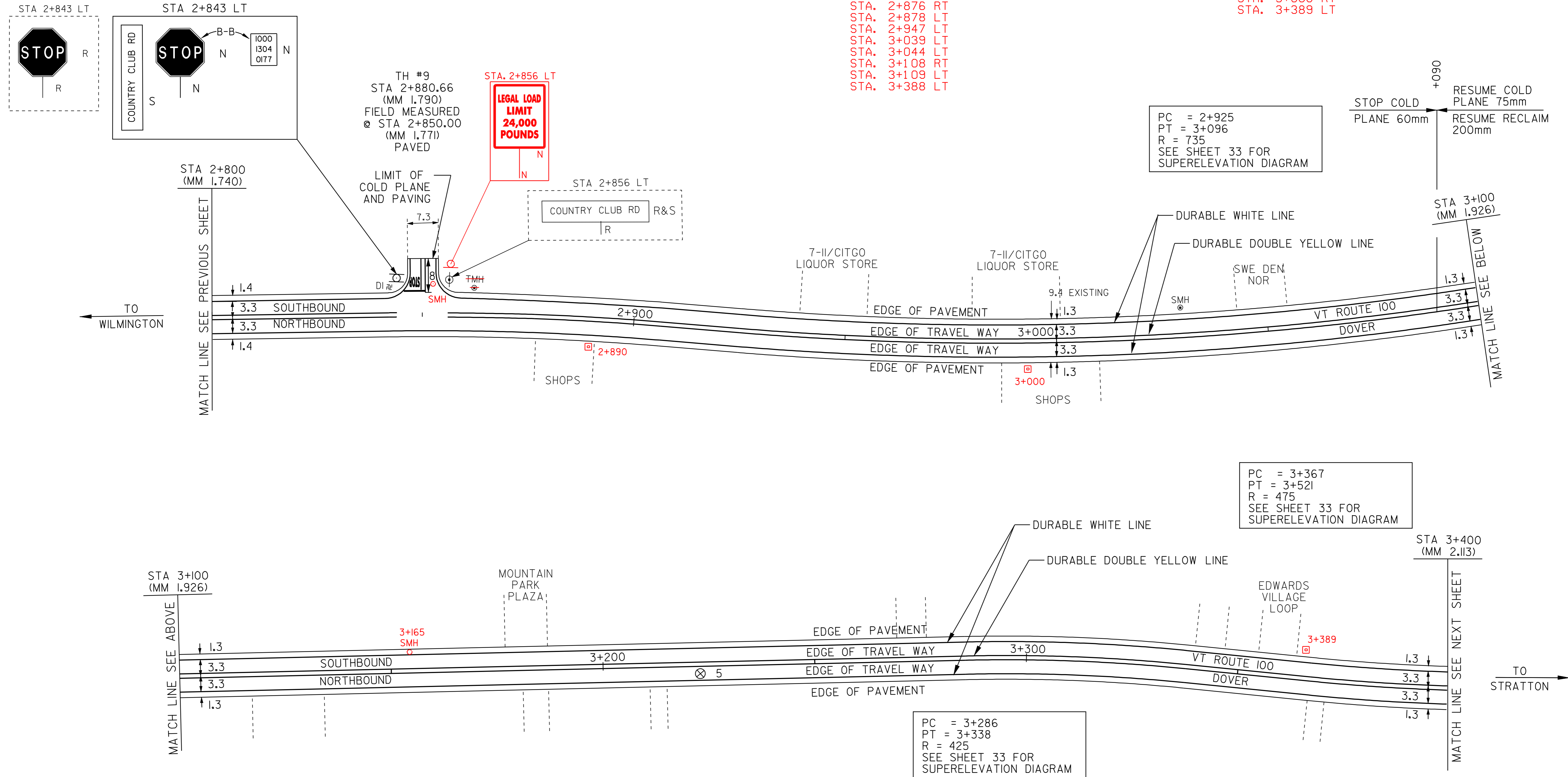
CHANGING ELEVATION OF SMH
 STA. 3+165 LT
 STA. 2+893 LT

TEMPORARY AND DURABLE LETTER OR SYMBOL
 STA 2+850 LT - "STOP" (TH #9)

ERECTING SALVAGED SIGNS
 AS SHOWN - 1

REHABILITATING DI, CB, OR MH, CLASS I
 STA. 2+890 RT
 STA. 3+000 RT
 STA. 3+389 LT

YIELDING MARKER POSTS
 STA. 2+876 RT
 STA. 2+878 LT
 STA. 2+947 LT
 STA. 3+039 LT
 STA. 3+044 LT
 STA. 3+108 RT
 STA. 3+109 LT
 STA. 3+388 LT



DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVEMENT CORES	STA	TOTAL DEPTH (MM)	PCC
5	STA 3+223 RT	165	NO

NOT TO SCALE

- LEGEND
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #7	PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
	DESIGN FILE NAME:	/pave/99bl80/pbl80.dgn		
	IPARM FILE NAME:	pbl80i07.i		
	SURVEYED BY:	CLD ENGINEERS, INC		
	SQUAD LEADER:	WRH		
		PLOT DATE:	24-MAY-2007	
		SURVEY DATE:	5/00	
		DRAWN BY:	MPS	
		SHEET:	16 OF 49	

PC = 3+367
PT = 3+521
R = 475
SEE SHEET 33 FOR
SUPERELEVATION DIAGRAM

TEMPORARY AND DURABLE 100 mm YELLOW LINE
STA 3+400 TO 3+486 SOLID LT & RT
STA 3+495 TO 3+564 SOLID LT & RT
STA 3+570 TO 3+737 SOLID LT & RT
STA 3+743 TO 3+876 SOLID LT & RT
STA 3+888 TO 4+000 SOLID LT & RT

TEMPORARY AND DURABLE 600 mm STOP BAR
STA 3+549 RT (TH #8)
STA 3+570 RT (TH #8)
STA 3+743 LT (TH #66)

MANUFACTURED TERMINAL SECTION (FLARED)
STA 3+416 RT 420
STA 3+424 LT
STA 3+451 RT 447
STA 3+474 LT

REMOVAL & DISPOSAL OF GUARDRAIL
STA 3+407 TO 3+428 RT
STA 3+413 TO 3+436 LT
STA 3+439 TO 3+454 RT
STA 3+447 TO 3+486 LT

TEMPORARY AND DURABLE 100 mm WHITE LINE
STA 3+400 TO 3+735 SOLID LT
STA 3+400 TO 3+480 SOLID RT
STA 3+504 TO 3+567 SOLID RT
STA 3+573 TO 4+000 SOLID RT
STA 3+746 TO 3+870 SOLID LT
STA 3+894 TO 4+000 SOLID LT

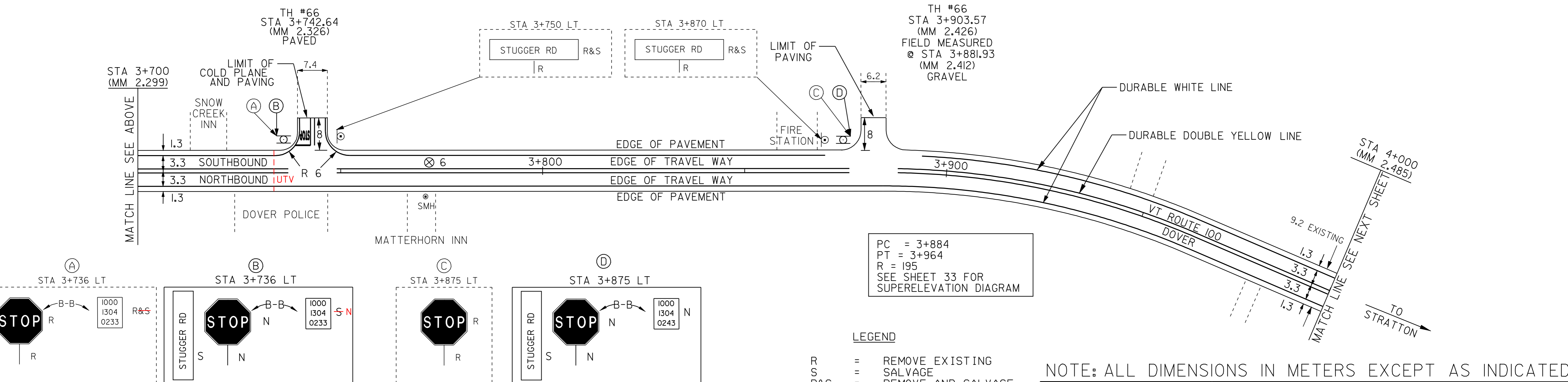
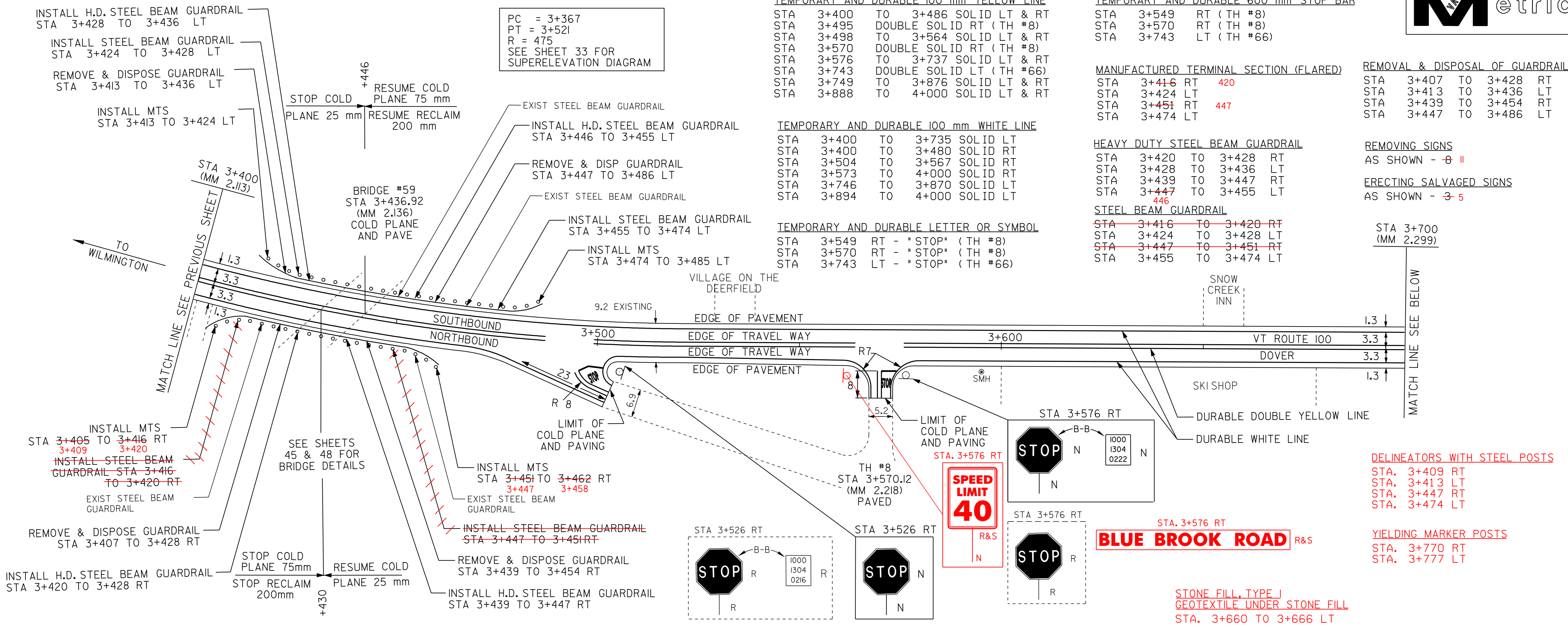
HEAVY DUTY STEEL BEAM GUARDRAIL
STA 3+420 TO 3+428 RT
STA 3+428 TO 3+436 LT
STA 3+439 TO 3+447 RT
STA 3+447 TO 3+455 LT

REMOVING SIGNS
AS SHOWN - 8 II
ERECTING SALVAGED SIGNS
AS SHOWN - 3-5

TEMPORARY AND DURABLE LETTER OR SYMBOL
STA 3+549 RT - "STOP" (TH #8)
STA 3+570 RT - "STOP" (TH #8)
STA 3+743 LT - "STOP" (TH #66)

STEEL BEAM GUARDRAIL
STA 3+416 TO 3+420 RT
STA 3+424 TO 3+428 LT
STA 3+447 TO 3+451 RT
STA 3+455 TO 3+474 LT

STA 3+700
(MM 2.299)



DATUM VERTICAL N/A HORIZONTAL N/A		STA 3+772 LT 146 NO	PCC NO
--	--	---------------------	--------

NOT TO SCALE

- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - NEW RAIL = NEW RAIL
 - EXISTING RAIL = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #8	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i08.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC SQUAD LEADER: WRH	DRAWN BY: MPS SHEET: 17 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 4+000 TO 4+400 SOLID LT & RT

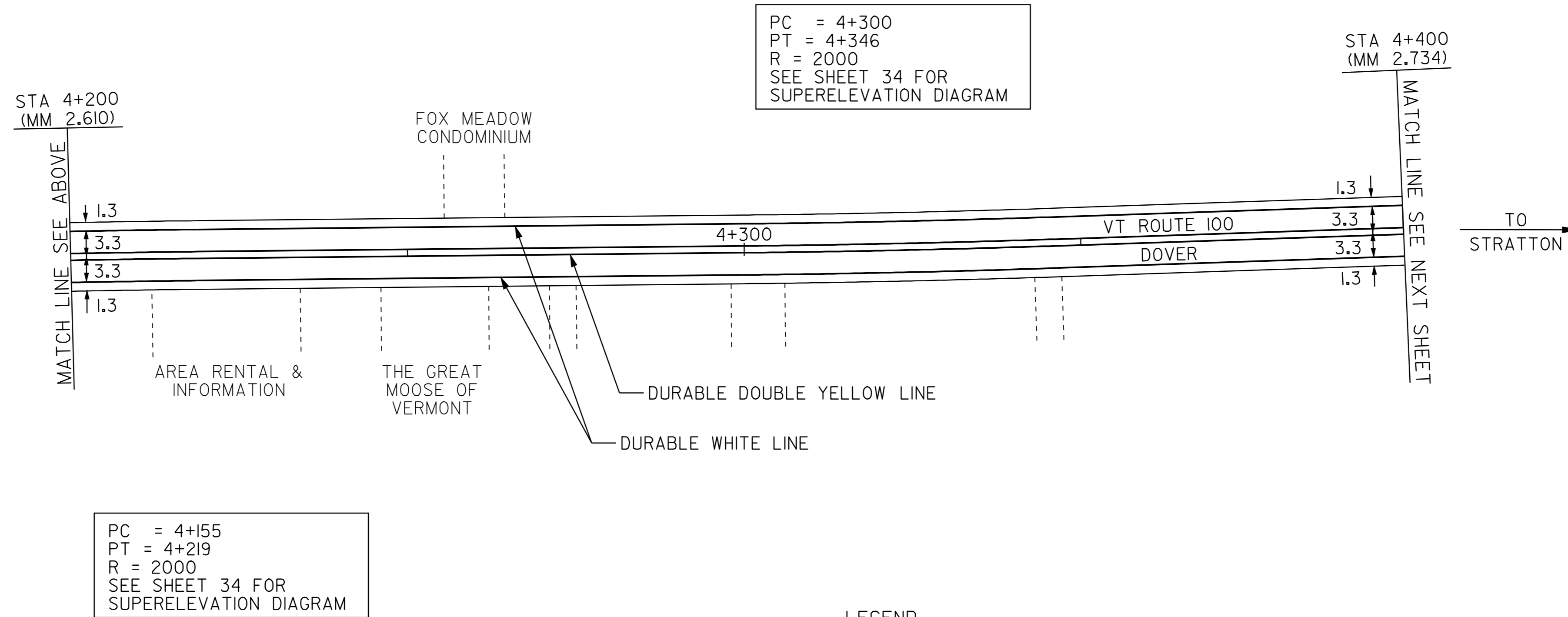
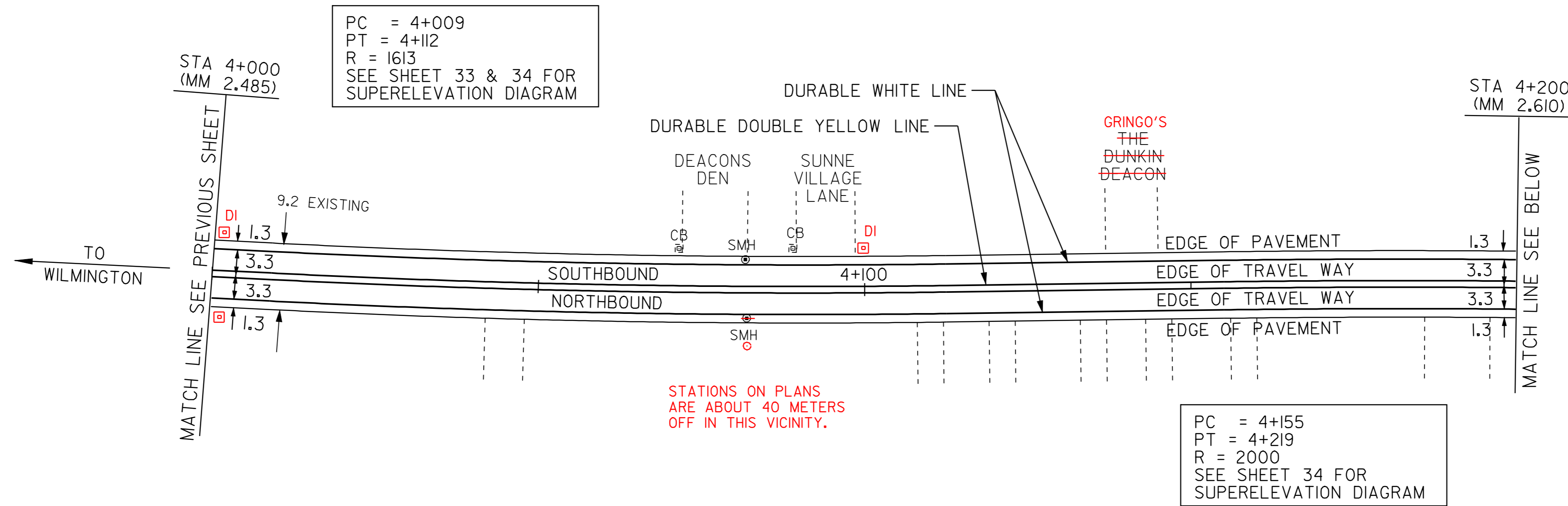
TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 4+000 TO 4+400 SOLID LT
 STA 4+000 TO 4+400 SOLID RT

CHANGING ELEV. OF SMH
~~STA 4+087 LT~~
 STA 4+087 RT
 4+129

CHANGING ELEV. OF DI, CATCH BASINS, OR MANHOLES
 STA. 4+003 LT

REHABILITATING DI, CB, OR MH, CLASS I
 STA. 4+126 LT
 STA. 4+139 LT

YIELDING MARKER POSTS
 STA. 4+007 LT
 STA. 4+090 RT
 STA. 4+126 LT
 STA. 4+139 LT
 STA. 4+206 RT
 STA. 4+211 LT



LEGEND

- R = REMOVE EXISTING
- S = SALVAGE
- R&S = REMOVE AND SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK
- NEW RAIL = NEW RAIL
- EXISTING RAIL = EXISTING RAIL
- ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE

**PAVING
 PROJECT
 LAYOUT #9**

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80i09.i	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 18 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE

STA 4+400	TO	4+520	SOLID LT & RT
STA 4+527	TO	4+825	DOUBLE SOLID LT (SA 3)
STA 4+533	TO	4+831	SOLID LT & RT
STA 4+843	TO	4+939	SOLID LT & RT
STA 4+951	TO	5+000	SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE

STA 4+400	TO	4+527	SOLID LT
STA 4+400	TO	4+825	SOLID RT
STA 4+549	TO	5+000	SOLID LT
STA 4+849	TO	4+933	SOLID RT
STA 4+957	TO	5+000	SOLID RT

DURABLE 200 mm WHITE LINE

STA 4+500	TO	4+532	LT (GORE MARKINGS)
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YIELDING MARKER POSTS
 STA. 4+671 LT
 STA. 4+675 RT
 STA. 4+818 RT
 STA. 4+820 LT

EROSION MATTING
 STA. 4+839 TO 4+865 RT
 STA. 4+878 TO 4+971 RT

CHANGING ELEV. OF DI, CATCH BASINS, OR MANHOLES
 STA. 4+820 RT

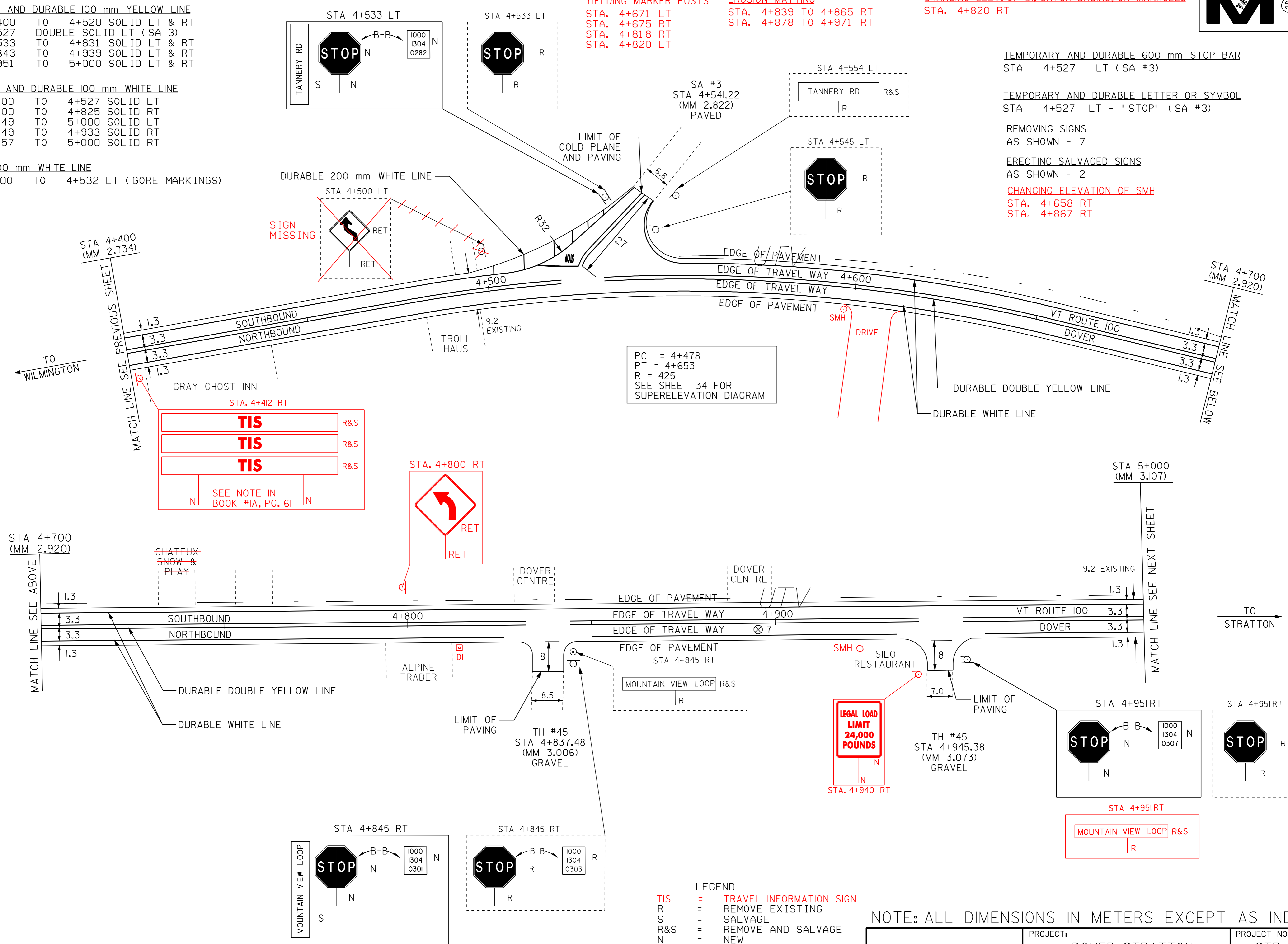
TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 4+527 LT (SA #3)

TEMPORARY AND DURABLE LETTER OR SYMBOL
 STA 4+527 LT - "STOP" (SA #3)

REMOVING SIGNS
 AS SHOWN - 7

ERECTING SALVAGED SIGNS
 AS SHOWN - 2

CHANGING ELEVATION OF SMH
 STA. 4+658 RT
 STA. 4+867 RT



PC = 4+478
 PT = 4+653
 R = 425
 SEE SHEET 34 FOR SUPERELEVATION DIAGRAM

TIS R&S
TIS R&S
TIS R&S
 SEE NOTE IN BOOK #1A, PG. 61

LEGAL LOAD LIMIT 24,000 POUNDS

- LEGEND**
- TIS = TRAVEL INFORMATION SIGN
 - R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - NEW RAIL = NEW RAIL
 - EXISTING RAIL = EXISTING RAIL
 - ⊗ = BORING LOCATION
 - UTV = BURIED CABLE

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM

VERTICAL	N/A
HORIZONTAL	N/A

STA	TOTAL DEPTH (MM)	PCC
7 STA 4+895 RT	197	NO

NOT TO SCALE

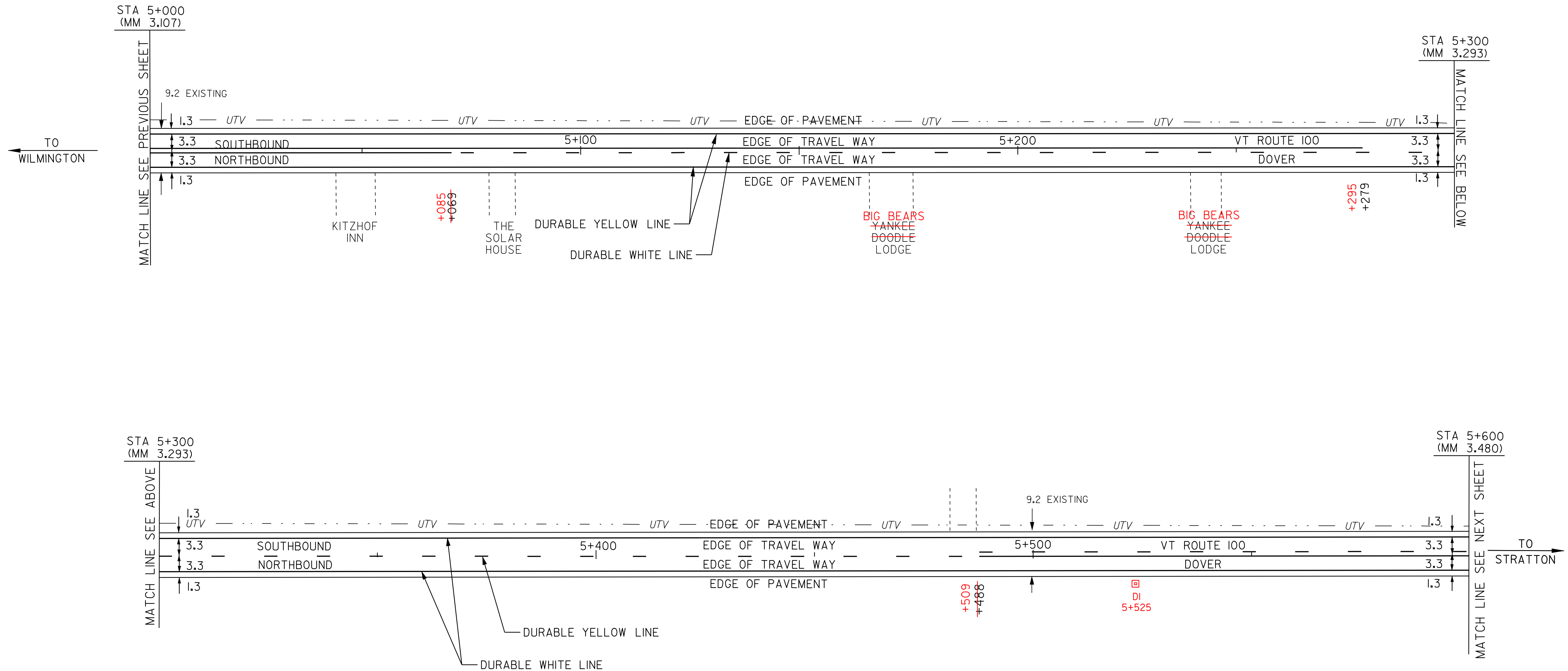
PAVING PROJECT LAYOUT #10	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i0.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 19 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
~~STA 5+000 TO 5+069 SOLID LT & RT~~
~~STA 5+069 TO 5+279 SOLID LT, DASHED RT~~
~~STA 5+279 TO 5+488 DASHED~~
~~STA 5+488 TO 5+600 DASHED LT, SOLID RT~~
 STA 5+000 TO 5+086 SOLID LT & RT
 STA 5+086 TO 5+295 SOLID LT, DASHED RT
 STA 5+295 TO 5+509 DASHED
 STA 5+509 TO 5+600 DASHED LT, SOLID RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 5+000 TO 5+600 SOLID LT
 STA 5+000 TO 5+600 SOLID RT

REHABILITATING DI, CB, OR MH, CLASS I
 STA. 5+525 RT

YIELDING MARKER POSTS
 STA. 5+027 LT
 STA. 5+040 RT
 STA. 5+126 RT
 STA. 5+127 LT
 STA. 5+218 RT
 STA. 5+220 LT
 STA. 5+291 RT
 STA. 5+293 LT
 STA. 5+375 LT
 STA. 5+385 RT
 STA. 5+524 RT
 STA. 5+525 LT



LEGEND

- R = REMOVE EXISTING
- S = SALVAGE
- R&S = REMOVE AND SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK
- = NEW RAIL
- - - = EXISTING RAIL
- ⊗ = BORING LOCATION
- UTV = BURIED CABLE

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE

PAVING PROJECT LAYOUT #11	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	
	IPARM FILE NAME: pbl80III.I	
	SURVEYED BY: CLD ENGINEERS, INC	
SQUAD LEADER: WRH		PLOT DATE: 24-MAY-2007
		SURVEY DATE: 5/00
		DRAWN BY: MPS
		SHEET: 20 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 5+600 TO 5+750 DASHED LT, SOLID RT
 STA 5+850 TO ~~5+898~~ SOLID LT & RT 5+910
 STA 5+898 910 TO 6+164 SOLID LT, DASHED RT 6+187
 STA 6+164 87 TO 6+200 DASHED

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 5+600 TO 5+750 SOLID LT
 STA 5+600 TO 5+750 SOLID RT
 STA 5+850 TO 6+200 SOLID LT
 STA 5+850 TO 6+200 SOLID RT

REMOVAL & DISPOSAL OF GUARDRAIL
 STA 5+697 TO 5+750 LT
 STA 6+056 TO 6+200 RT
 STA 6+158 TO 6+200 LT

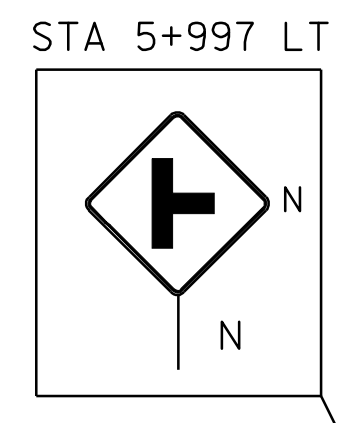
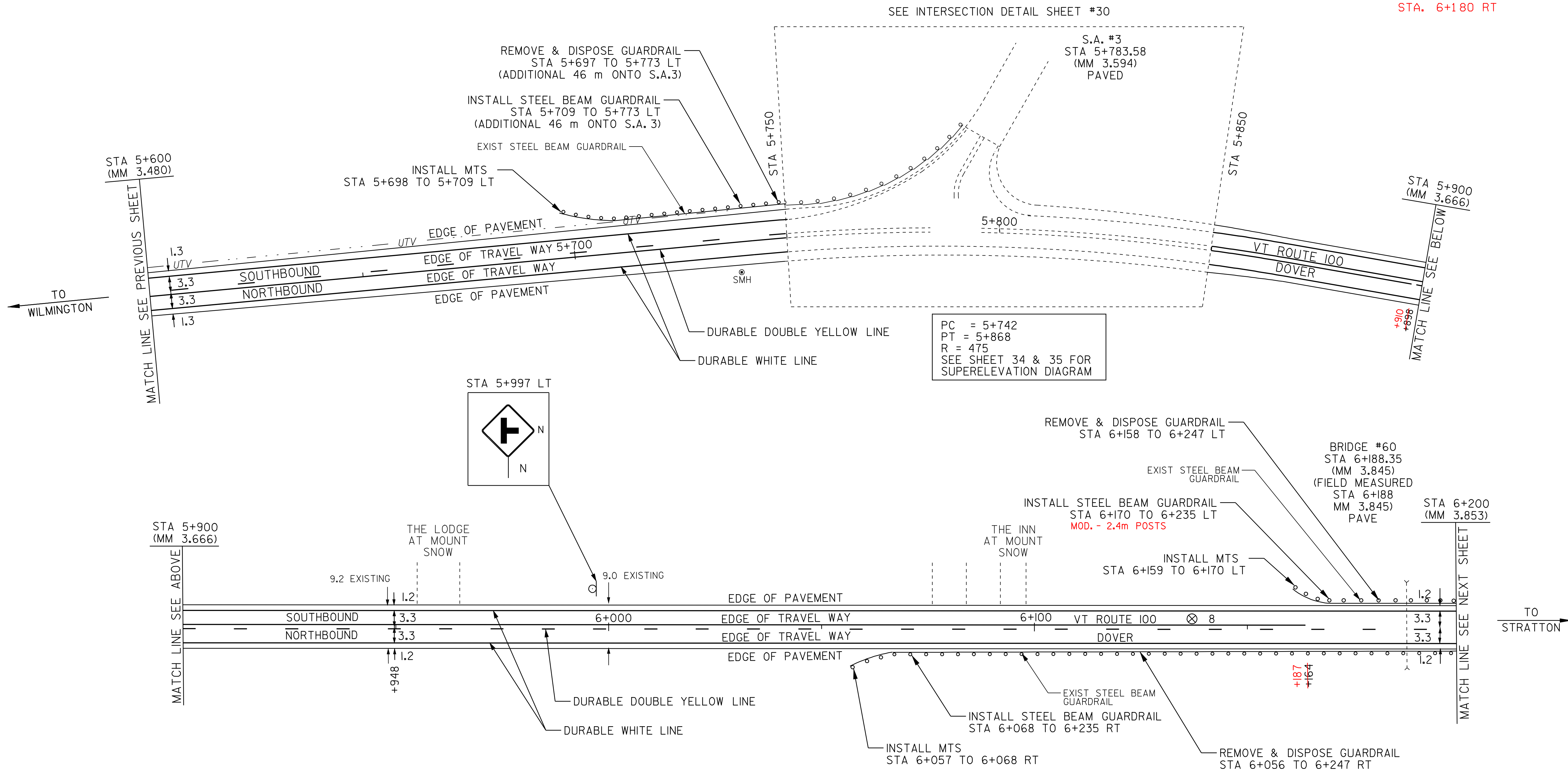
STEEL BEAM GUARDRAIL
 STA 5+709 TO 5+750 LT
 STA 6+068 TO 6+200 RT
~~STA 6+170 TO 6+200 LT~~

STEEL BEAM GUARDRAIL - 2.4m POSTS (MOD.)
 STA. 6+170 TO 6+200 LT

MANUFACTURED TERMINAL SECTION (FLARED)
 STA 5+709 LT
 STA 6+068 RT
 STA 6+170 LT

DELINEATORS WITH STEEL POSTS
 STA. 5+698 LT
 STA. 5+057 RT
 STA. 6+159 LT

YIELDING MARKER POSTS
 STA. 5+691 RT
 STA. 5+693 LT
 STA. 6+180 RT



- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - = EXISTING RAIL
 - ⊗ = BORING LOCATION
 - UTV = BURIED CABLE

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM			
VERTICAL	N/A		
HORIZONTAL	N/A		

PAVEMENT CORES - ⊗			
STA	TOTAL DEPTH (MM)	PCC	
8 STA 6+136 LT	140	NO	

NOT TO SCALE

PAVING PROJECT LAYOUT #12

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80i12.1	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 21 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 6+200 TO 6+341 DASHED 357
 STA 6+341 57 TO 6+620 DASHED LT, SOLID RT 654
 STA 6+620 54 TO 6+636 SOLID LT & RT
 STA 6+642 DOUBLE SOLID LT (SKI AREA ROAD)
 STA 6+648 TO 6+727 SOLID LT & RT 751
 STA 6+727 51 TO 6+800 SOLID LT, DASHED RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 6+200 TO 6+800 SOLID RT
 STA 6+200 TO 6+636 SOLID LT
 STA 6+648 TO 6+800 SOLID LT

REMOVAL & DISPOSAL OF GUARDRAIL
 STA 6+200 TO 6+247 RT
 STA 6+200 TO 6+247 LT

REMOVING SIGNS
 AS SHOWN - 2-3

ERECTING SALVAGED SIGNS
 AS SHOWN - 1

YIELDING MARKER POSTS
 STA. 6+394 LT
 STA. 6+405 RT
 STA. 6+487 LT
 STA. 6+517 RT
 STA. 6+641 RT

TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 6+638 LT

STEEL BEAM GUARDRAIL
 STA 6+200 TO 6+235 RT
 STA 6+200 TO 6+235 LT

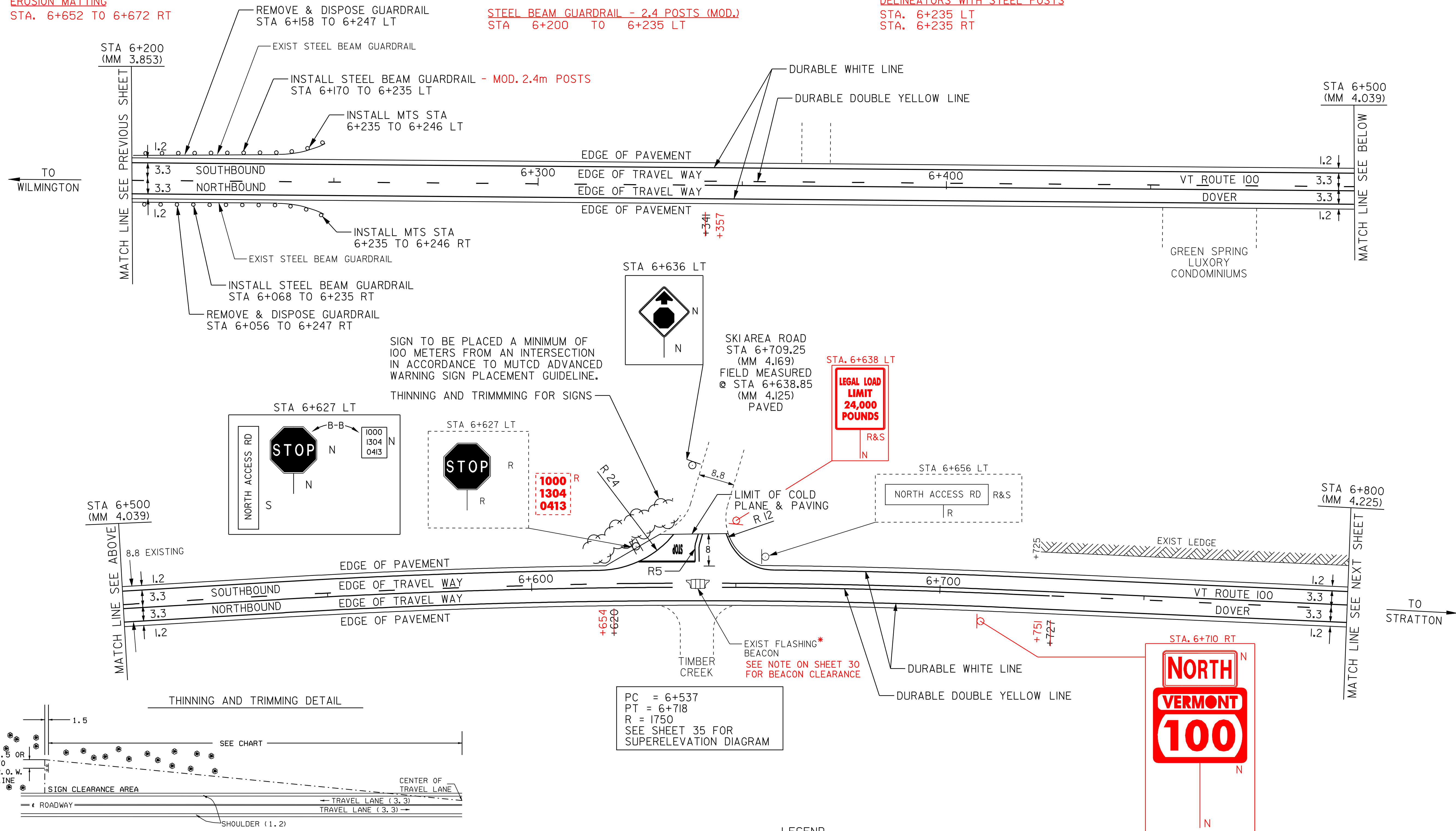
STEEL BEAM GUARDRAIL - 2.4 POSTS (MOD.)
 STA 6+200 TO 6+235 LT

MANUFACTURED TERMINAL SECTION (FLARED)
 STA 6+235 RT
 STA 6+235 LT

THINNING AND TRIMMING FOR SIGNS
 STA 6+636 LT

DELINEATORS WITH STEEL POSTS
 STA. 6+235 LT
 STA. 6+235 RT

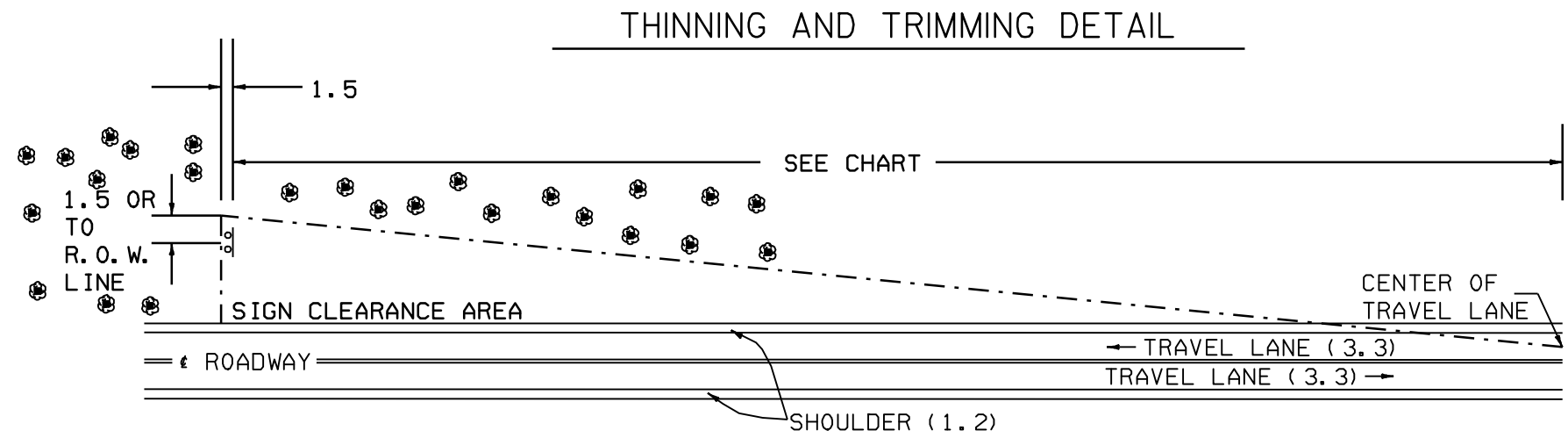
EROSION MATTING
 STA. 6+652 TO 6+672 RT



SIGN TO BE PLACED A MINIMUM OF 100 METERS FROM AN INTERSECTION IN ACCORDANCE TO MUTCD ADVANCED WARNING SIGN PLACEMENT GUIDELINE.

THINNING AND TRIMMING FOR SIGNS

PC = 6+537
 PT = 6+718
 R = 1750
 SEE SHEET 35 FOR SUPERELEVATION DIAGRAM



MINIMUM SIGN SIGHT DISTANCE CHART

APPROACH SPEED (km/hr)	SIGHT DISTANCE (meters)
50 OR LESS	91
55	107
65	122
70	137
80	152
90	168

NOT TO SCALE

LEGEND

- R = REMOVE EXISTING
- S = SALVAGE
- R&S = REMOVE AND SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK
- = NEW RAIL
- - - = EXISTING RAIL
- ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM

VERTICAL	N/A
HORIZONTAL	N/A

PAVING PROJECT LAYOUT #13	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i13.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 22 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 6+800 TO 6+952 SOLID LT, DASHED RT 967
 STA 6+952.67 TO 7+051 SOLID LT & RT
 STA 7+057 DOUBLE SOLID LT (TH #53)
 STA 7+063 TO 7+188 SOLID LT & RT
 STA 7+200 TO 7+355 SOLID LT & RT 373
 STA 7+355.73 TO 7+400 SOLID LT, DASHED RT

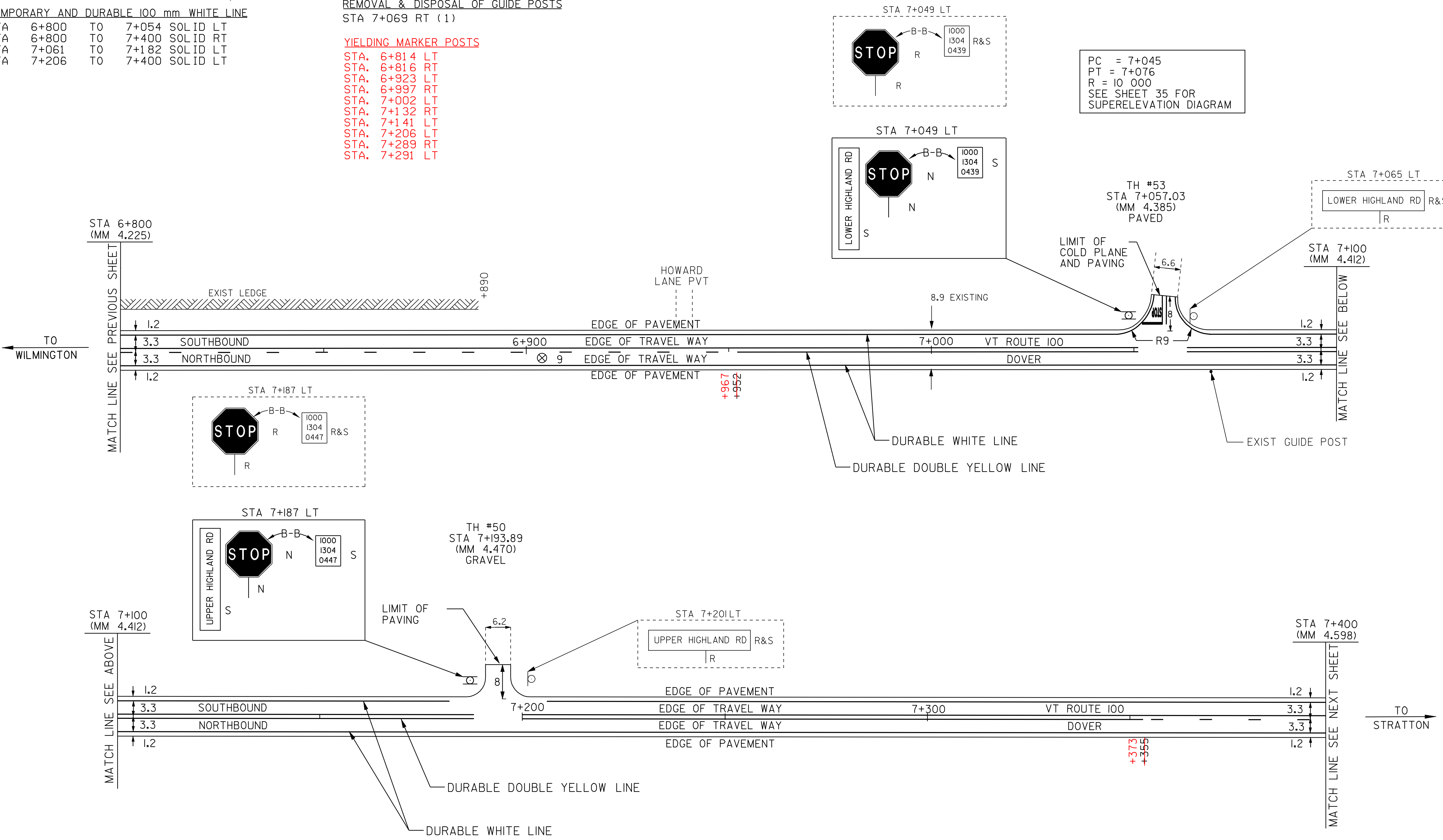
TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 6+800 TO 7+054 SOLID LT
 STA 6+800 TO 7+400 SOLID RT
 STA 7+061 TO 7+182 SOLID LT
 STA 7+206 TO 7+400 SOLID LT

TEMPORARY AND DURABLE 600 mm STOP BAR
 STA 7+057 LT (TH #53)
TEMPORARY AND DURABLE LETTER OR SYMBOL
 STA 7+057 LT - "STOP" (TH #53)

REMOVING SIGNS
 AS SHOWN - 6
ERECTING SALVAGED SIGNS
 AS SHOWN - 4

REMOVAL & DISPOSAL OF GUIDE POSTS
 STA 7+069 RT (1)

YIELDING MARKER POSTS
 STA. 6+814 LT
 STA. 6+816 RT
 STA. 6+923 LT
 STA. 6+997 RT
 STA. 7+002 LT
 STA. 7+132 RT
 STA. 7+141 LT
 STA. 7+206 LT
 STA. 7+289 RT
 STA. 7+291 LT



DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVEMENT CORES	STA	TOTAL DEPTH (MM)	PCC
9	STA 6+904 RT	140	NO

NOT TO SCALE

- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #14	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i14.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 23 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE

STA 7+400	TO	7+580	SOLID LT, DASHED RT	605
STA 7+580	TO	7+661	DASHED	685
STA 7+661	TO	7+780	DASHED LT SOLID RT	807
STA 7+780	TO	7+799	SOLID LT & RT	
STA 7+812	TO	7+926	SOLID LT, DASHED RT	951
STA 7+926	TO	8+000	DASHED	

STEEL BEAM GUARDRAIL

STA 7+831	TO	7+937	RT
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REMOVAL & DISPOSAL OF GUARDRAIL

STA 7+820	TO	7+950	RT
-----------	----	-------	----

MANUFACTURED TERMINAL SECTION (FLARED)

STA 7+831	RT
STA 7+937	RT

REMOVING SIGNS

AS SHOWN - 3

ERECTING SALVAGED SIGNS

AS SHOWN - 2

YIELDING MARKER POSTS

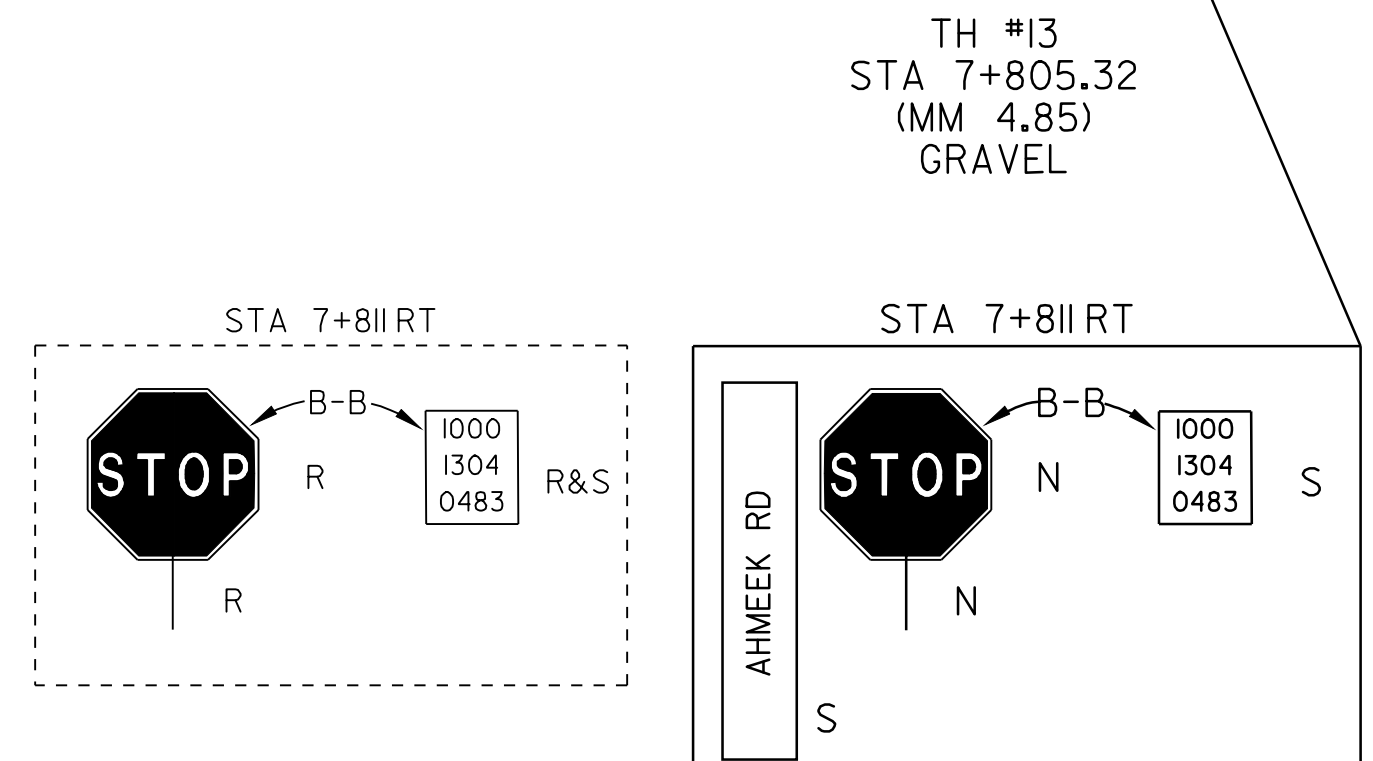
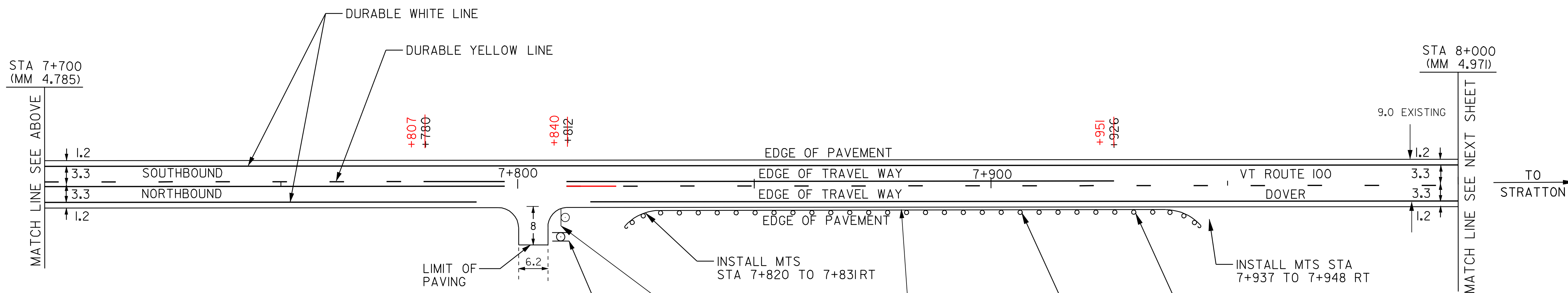
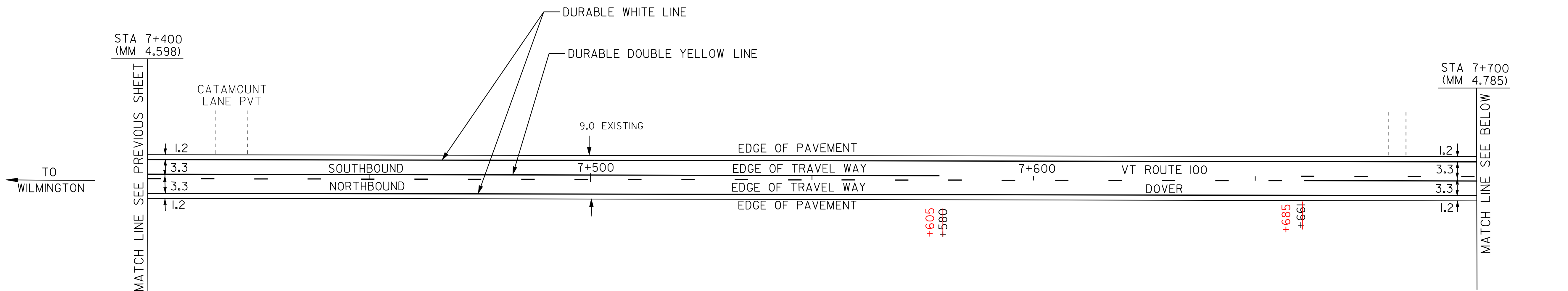
STA. 7+413	RT
STA. 7+415	LT
STA. 7+537	RT
STA. 7+538	LT
STA. 7+660	RT
STA. 7+662	LT
STA. 7+780	LT
STA. 7+780	RT

TEMPORARY AND DURABLE 100 mm WHITE LINE

STA 7+400	TO	8+000	LT
STA 7+400	TO	7+793	RT
STA 7+817	TO	8+000	RT

DELINEATORS WITH STEEL POSTS

STA. 7+820	RT
STA. 7+948	RT



- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM

VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE

PAVING PROJECT LAYOUT #15	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i15.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 24 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE

STA 8+000	TO	8+433	DASHED	462
STA 8+433	TO	8+514	DASHED LT, SOLID RT	560
STA 8+514	TO	8+529	SOLID LT & RT	
STA 8+535	TO	8+600	DOUBLE SOLID RT (TH #8)	
STA 8+541	TO	8+600	SOLID LT & RT	

TEMPORARY AND DURABLE 100 mm WHITE LINE

STA 8+000	TO	8+600	LT
STA 8+000	TO	8+532	RT
STA 8+538	TO	8+600	RT

TEMPORARY AND DURABLE 600 mm STOP BAR

STA 8+535	RT (TH #8)
-----------	------------

TEMPORARY AND DURABLE LETTER OR SYMBOL

STA 8+535	RT - "STOP" (TH 8)
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STEEL BEAM GUARDRAIL

STA 8+044	TO	8+188	RT
STA 8+387	TO	8+520	RT 531
STA 8+390	TO	8+436	LT
STA 8+500	TO	8+515	LT

DELINEATORS WITH STEEL POSTS

STA. 8+033	RT
STA. 8+199	RT
STA. 8+376	RT
STA. 8+379	LT
STA. 8+436	LT
STA. 8+495	LT
STA. 8+526	LT
STA. 8+531	RT

REMOVAL & DISPOSAL OF GUARDRAIL

STA 8+033	TO	8+201	LT
STA 8+377	TO	8+531	RT
STA 8+381	TO	8+419	LT
STA 8+489	TO	8+527	LT

REMOVING SIGNS
AS SHOWN - 3

ERECTING SALVAGED SIGNS
AS SHOWN - 2-1

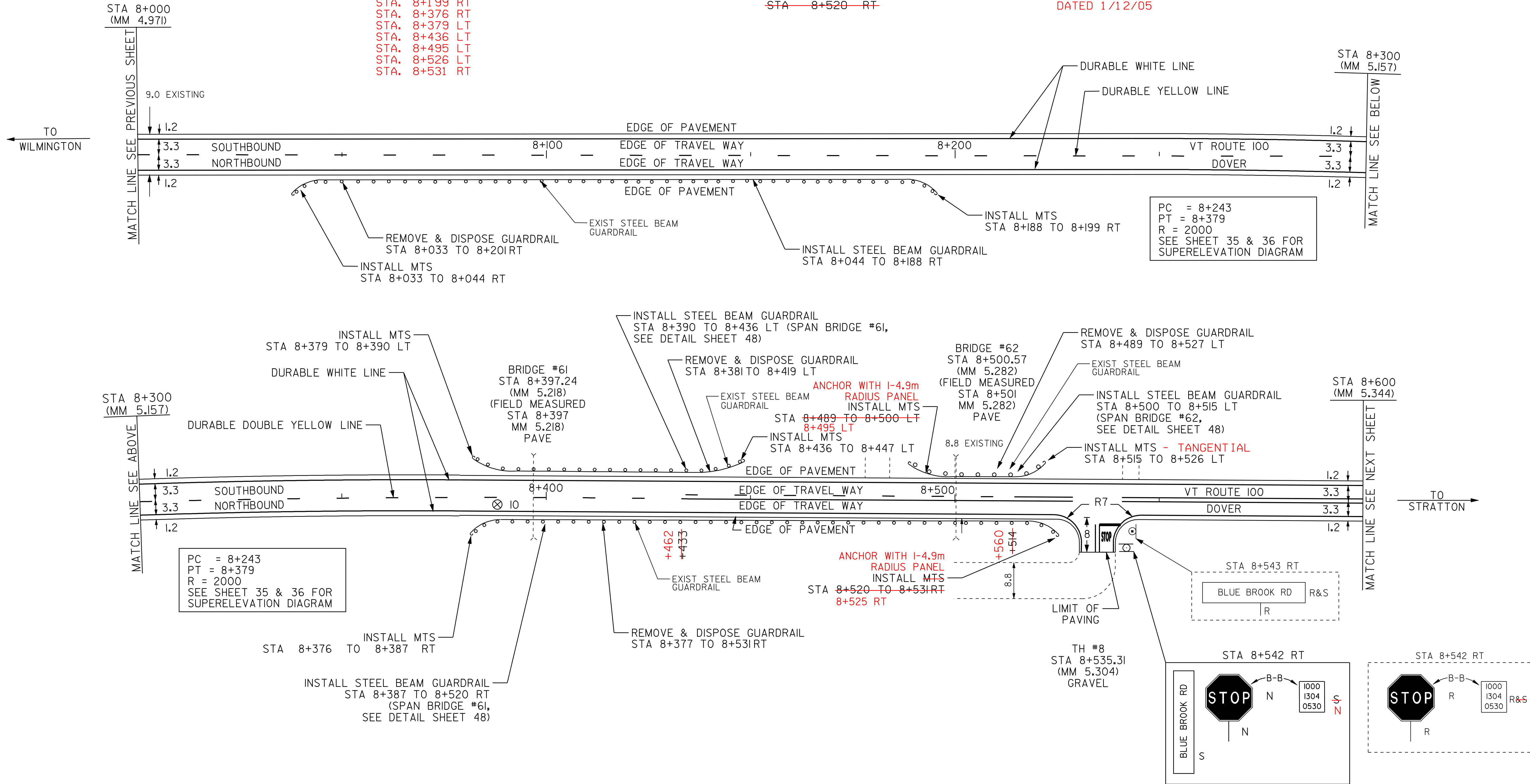
ANCHOR FOR STEEL BEAM RAIL
STA. 8+495 LT
STA. 8+525 RT

YIELDING MARKER POSTS

STA. 8+025	RT
STA. 8+027	LT
STA. 8+117	LT
STA. 8+117	RT
STA. 8+222	RT
STA. 8+241	LT

MANUFACTURED TERMINAL SECTION (FLARED)

STA 8+044	RT
STA 8+188	RT
STA 8+387	RT
STA 8+390	LT
STA 8+436	LT
STA 8+500	LT
STA 8+515	LT - TANGENTIAL, CHANGED BY S.A./C.O. #3 DATED 1/12/05
STA 8+520	RT



LEGEND

R	=	REMOVE EXISTING
S	=	SALVAGE
R&S	=	REMOVE AND SALVAGE
N	=	NEW
RET	=	RETAIN
B-B	=	BACK TO BACK
○	=	NEW RAIL
○	=	EXISTING RAIL
⊗	=	BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #16	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i16.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 25 OF 49

DATUM

VERTICAL	N/A
HORIZONTAL	N/A

PAVEMENT CORES - ⊗

STA	TOTAL DEPTH (MM)	PCC
10 STA 8+388 RT	178	NO

NOT TO SCALE

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 DOVER
 STA 8+600 TO 8+777 SOLID LT & RT
 STA 8+789 TO 8+927 SOLID LT & RT
 STRATTON
 STA 0+000 TO 0+300 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 DOVER
 STA 8+600 TO 8+927 LT
 STA 8+600 TO 8+771 RT
 STA 8+795 TO 8+927 RT
 STRATTON
 STA 0+000 TO 0+300 LT
 STA 0+000 TO 0+300 RT

STONE FILL, TYPE I
 GEOTEXTILE UNDER STONE FILL
~~STA 0+095 TO 0+300 RT~~
 STA. 0+197 TO 0+300 RT
 STA. 0+250 TO 0+264 LT

STEEL BEAM GUARDRAIL
~~STA 8+692 TO 8+814 LT~~
 STA 8+713 TO 8+736 RT

REMOVAL & DISPOSAL OF GUARDRAIL
 STA 8+692 TO 8+825 LT
 STA 8+702 TO 8+748 RT

~~ANCHORS FOR STEEL BEAM GUARDRAIL~~
 STA 8+692 LT

MANUFACTURED TERMINAL SECTION (FLARED)
 STA 8+713 RT
 STA 8+736 RT
 STA 8+814 LT
 STA. 8+696 LT

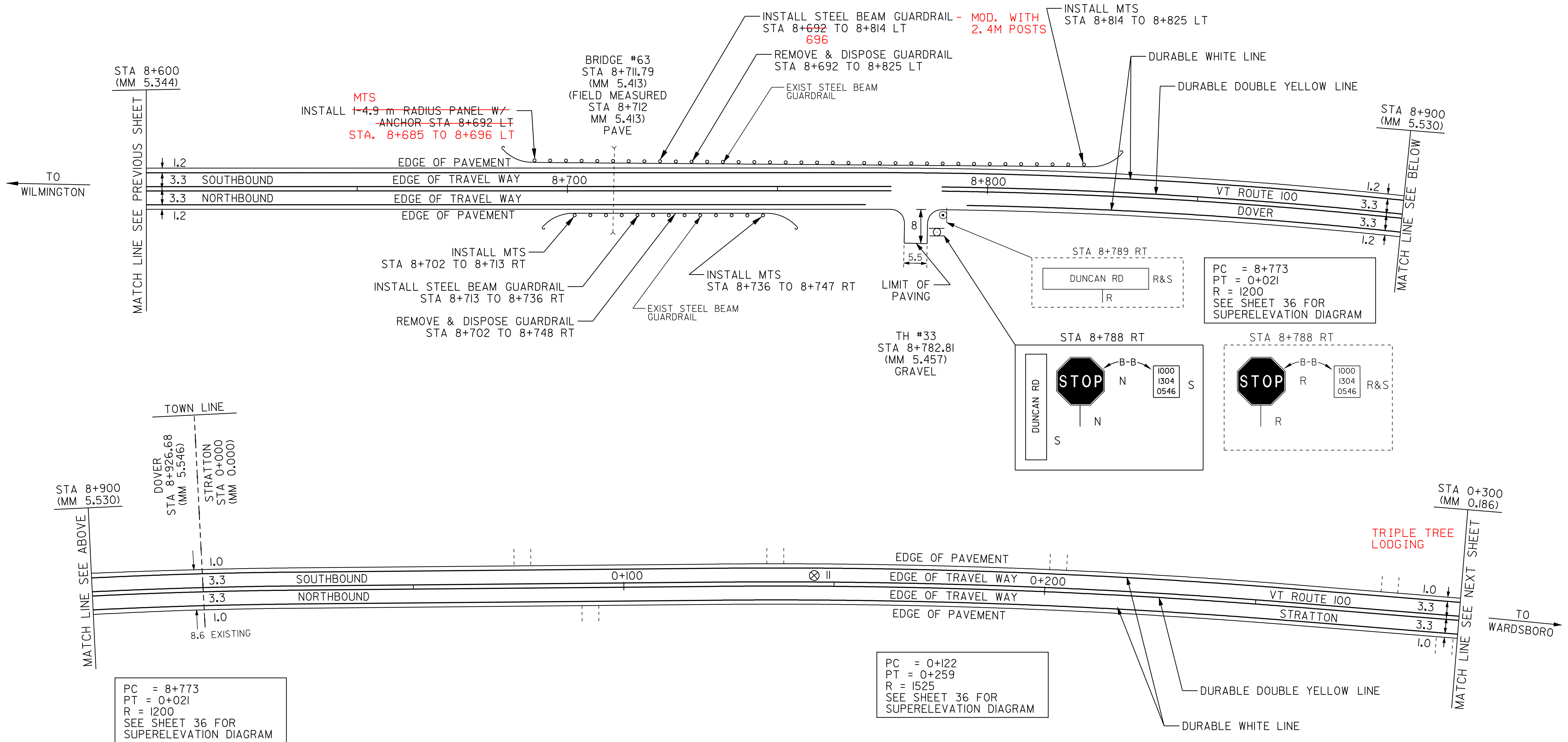
REMOVING SIGNS
 AS SHOWN - 3

ERECTING SALVAGED SIGNS
 AS SHOWN - 2

YIELDING MARKER POSTS
 STA. 0+092 LT
 STA. 0+101 RT
 STA. 0+183 LT
 STA. 0+196 RT
 STA. 0+282 LT
 STA. 0+289 RT

STEEL BEAM GUARDRAIL 2.4m POSTS (MOD.)
 STA. 8+696 TO 8+814 LT

DELINEATORS WITH STEEL POSTS
 STA. 8+685 LT
 STA. 8+702 RT
 STA. 8+747 RT
 STA. 8+825 LT



PC = 8+773
 PT = 0+021
 R = 1200
 SEE SHEET 36 FOR SUPERELEVATION DIAGRAM

PC = 0+122
 PT = 0+259
 R = 1525
 SEE SHEET 36 FOR SUPERELEVATION DIAGRAM

LEGEND

- R = REMOVE EXISTING
- S = SALVAGE
- R&S = REMOVE AND SALVAGE
- N = NEW
- RET = RETAIN
- B-B = BACK TO BACK
- = NEW RAIL
- - - = EXISTING RAIL
- ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #17	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i17.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 26 OF 49

DATUM	STA	TOTAL DEPTH (MM)	PCC
VERTICAL	N/A		
HORIZONTAL	N/A		
	II STA 0+145 LT	229	NO

NOT TO SCALE

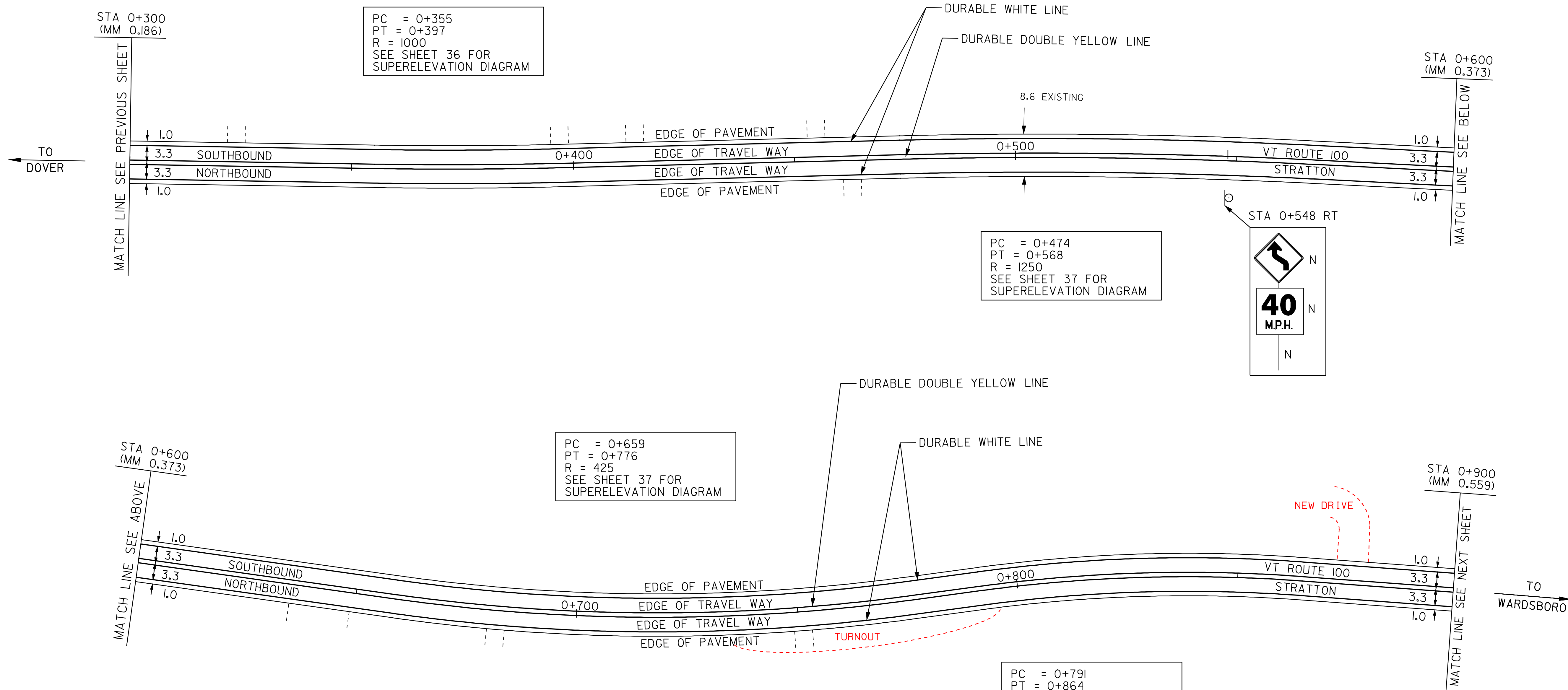
TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 0+300 TO 0+900 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 0+300 TO 0+900 LT
 STA 0+300 TO 0+900 RT

STONE FILL, TYPE I
 GEOTEXTILE UNDER STONE FILL
 STA 0+300 TO 0+630 RT 635
 STA 0+346 TO 0+410 LT
 STA 0+422 TO 0+507 LT 433
 STA 0+870 TO 0+900 LT
 STA 0+875 TO 0+900 RT
 STA. 0+300 TO 0+301 RT
 STA. 0+476 TO 0+507 LT
 STA. 0+600 TO 0+610 LT
 STA. 0+700 TO 0+740 LT

YIELDING MARKER POSTS
 STA. 0+322 LT
 STA. 0+334 RT
 STA. 0+433 LT
 STA. 0+440 RT
 STA. 0+518 LT
 STA. 0+525 RT
 STA. 0+592 LT
 STA. 0+595 RT
 STA. 0+690 LT
 STA. 0+697 RT

EROSION MATTING
 STA. 0+595 TO 0+608 RT
 STA. 0+710 TO 0+730 LT



STA 0+300
(MM 0.186)

PC = 0+355
 PT = 0+397
 R = 1000
 SEE SHEET 36 FOR
 SUPERELEVATION DIAGRAM

STA 0+600
(MM 0.373)

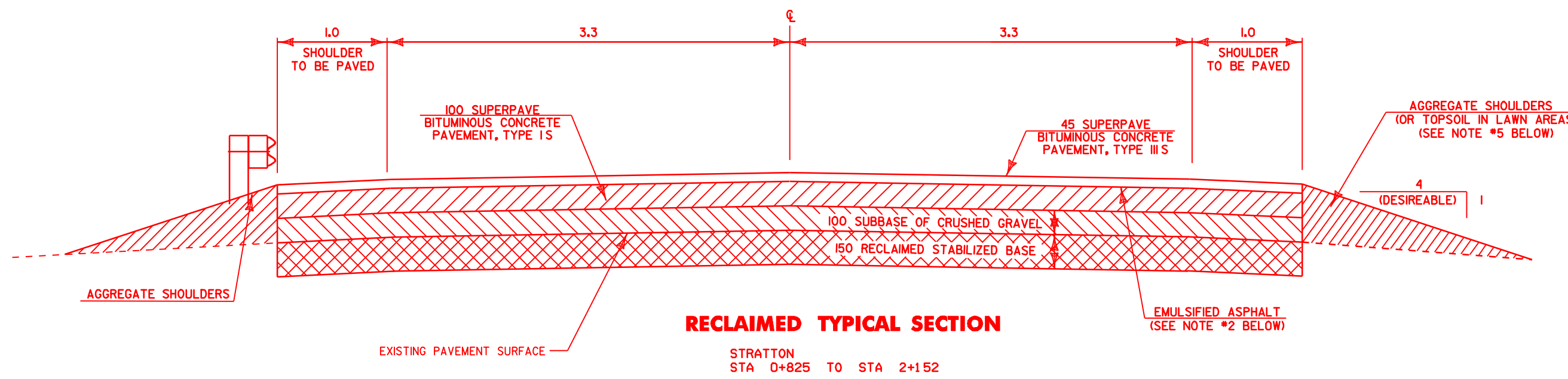
PC = 0+474
 PT = 0+568
 R = 1250
 SEE SHEET 37 FOR
 SUPERELEVATION DIAGRAM

STA 0+600
(MM 0.373)

PC = 0+659
 PT = 0+776
 R = 425
 SEE SHEET 37 FOR
 SUPERELEVATION DIAGRAM

STA 0+900
(MM 0.559)

PC = 0+791
 PT = 0+864
 R = 350
 SEE SHEET 37 FOR
 SUPERELEVATION DIAGRAM



- LEGEND
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #18	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i18.1	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 27 OF 49

DATUM
 VERTICAL N/A
 HORIZONTAL N/A

ACTUAL WORK: STATION 0+846 TO WARDSBORO 0+015 LT & RT

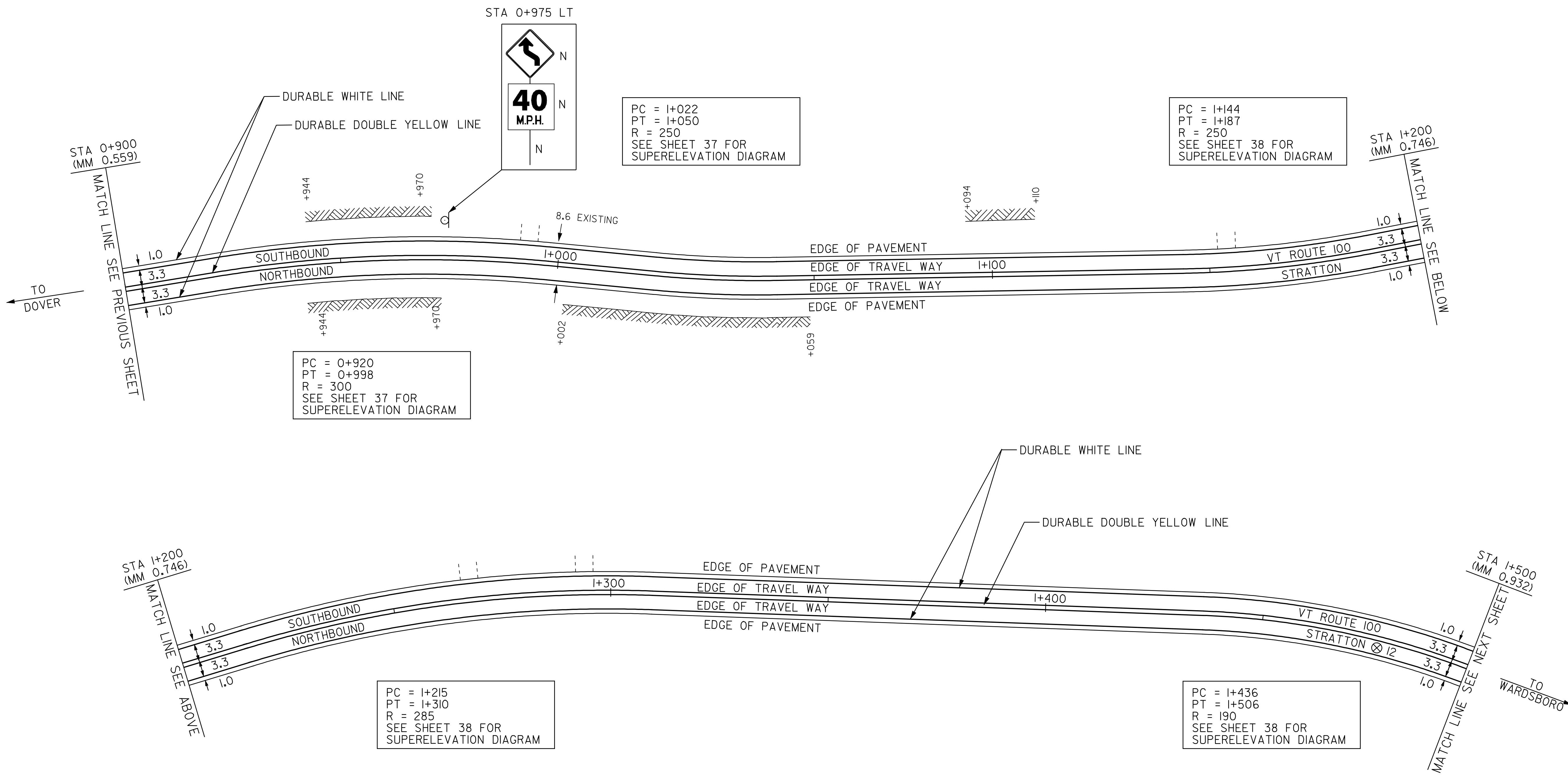
NOT TO SCALE

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 0+900 TO 1+500 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 0+900 TO 1+500 LT
 STA 0+900 TO 1+500 RT

STONE FILL, TYPE I
 GEOTEXTILE UNDER STONE FILL
 STA 0+900 TO 0+945 LT 955
 STA 0+900 TO 0+975 RT
 STA 0+975 TO 1+000 RT
 STA 1+020 TO 1+500 RT 167
~~STA 1+159 TO 1+175 LT~~
~~STA 1+305 TO 1+360 LT~~
 STA. 1+187 TO 1+360 RT

YIELDING MARKER POSTS
 STA. 1+025 RT
 STA. 1+035 LT
 STA. 1+080 LT
 STA. 1+106 RT
 STA. 1+113 LT
 STA. 1+181 RT
 STA. 1+189 LT



PC = 0+920
 PT = 0+998
 R = 300
 SEE SHEET 37 FOR
 SUPERELEVATION DIAGRAM

PC = 1+022
 PT = 1+050
 R = 250
 SEE SHEET 37 FOR
 SUPERELEVATION DIAGRAM

PC = 1+144
 PT = 1+187
 R = 250
 SEE SHEET 38 FOR
 SUPERELEVATION DIAGRAM

PC = 1+215
 PT = 1+310
 R = 285
 SEE SHEET 38 FOR
 SUPERELEVATION DIAGRAM

PC = 1+436
 PT = 1+506
 R = 190
 SEE SHEET 38 FOR
 SUPERELEVATION DIAGRAM

- LEGEND
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVEMENT CORES - ⊗			
STA	TOTAL DEPTH (MM)	PCC	
I2 STA 1+473 RT	I78	NO	

NOT TO SCALE

PAVING PROJECT LAYOUT #19

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80i19.1	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 28 OF 49

TEMPORARY AND DURABLE 100 mm YELLOW LINE
 STA 1+500 TO 1+762 SOLID LT & RT
 STA 1+774 TO 2+152 SOLID LT & RT

STONE FILL, TYPE I
 GEOTEXTILE UNDER STONE FILL
 STA 1+500 TO 1+552 RT
 STA 1+696 TO 1+706 LT
 STA. 1+518 TO 1+570 RT
 STA. 1+664 TO 1+710 RT
 STA. 1+675 TO 1+700 LT
 STA. 1+750 TO 1+764 LT

REMOVING SIGNS
 AS SHOWN - 3
~~ERECTING SALVAGED SIGNS~~
 AS SHOWN - 2

TEMPORARY AND DURABLE 100 mm WHITE LINE
 STA 1+500 TO 1+756 LT
 STA 1+500 TO 2+152 RT
 STA 1+780 TO 2+152 LT

DELINEATORS WITH STEEL POSTS

STA. 1+700 LT
 STA. 1+700 RT
 STA. 1+727 RT
 STA. 1+736 LT

STEEL BEAM GUARDRAIL WITH 2.4 POSTS (MOD.)
 STA. 1+698 TO 1+725 LT (BR. #64)
 STA. 1+698 TO 1+729 RT (BR. #64)

MANUFACTURED TERMINAL SECTION
 STA. 1+725 LT

ANCHORS FOR STEEL BEAM RAIL
 STA. 1+700 LT
 STA. 1+700 RT
 STA. 1+727 RT

YIELDING MARKER POSTS

DOVER:
 STA. 1+676 LT
 STA. 1+754 RT
 STA. 1+772 LT
 STA. 1+825 RT
 STA. 1+833 LT
 STA. 1+926 RT
 STA. 1+938 LT
 STA. 1+987 RT
 STA. 2+000 LT
 STA. 2+068 RT
 STA. 2+104 RT
 STA. 2+113 LT
 STA. 2+140 RT
 STA. 2+148 LT
 WARDSBORO:
 STA. 0+034 RT
 STA. 0+041 LT

PC = I+553
 PT = I+771
 R = I360
 SEE SHEET 38 & 39 FOR SUPERELEVATION DIAGRAM

PC = I+436
 PT = I+506
 R = I90
 SEE SHEET 38 FOR SUPERELEVATION DIAGRAM

PC = I+884
 PT = I+949
 R = 700
 SEE SHEET 39 FOR SUPERELEVATION DIAGRAM

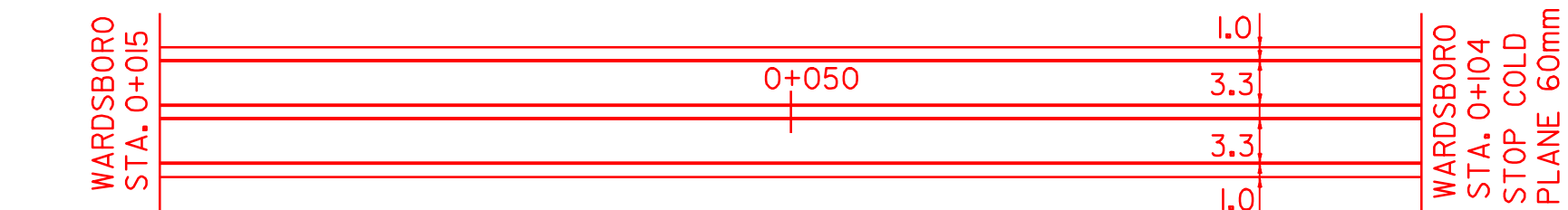
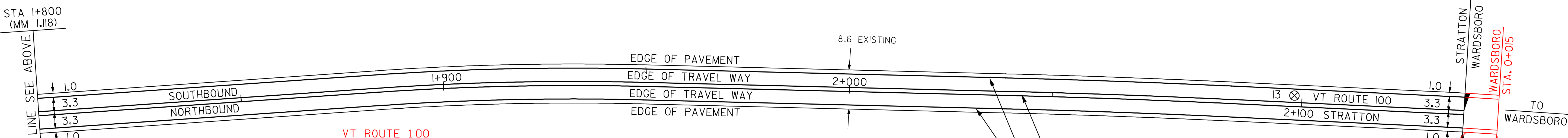
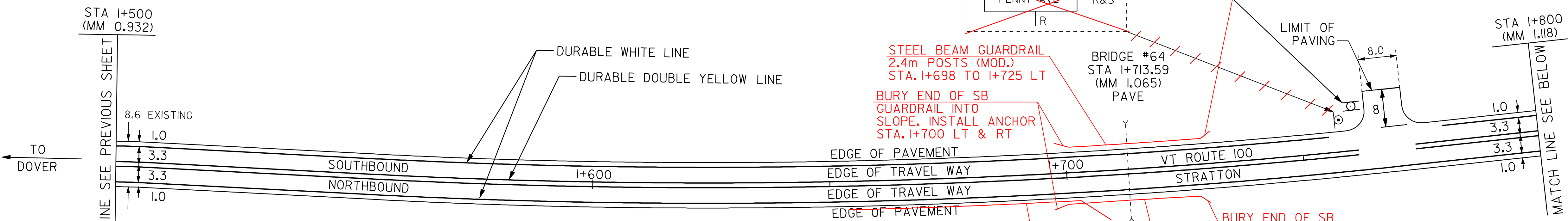
PC = 2+038
 PT = 2+094
 R = 2500
 SEE SHEET 39 FOR SUPERELEVATION DIAGRAM

PC = 2+148
 PT = 2+367
 R = 290
 SEE SHEET 39 FOR SUPERELEVATION DIAGRAM

- LEGEND**
- R = REMOVE EXISTING
 - S = SALVAGE
 - R&S = REMOVE AND SALVAGE
 - N = NEW
 - RET = RETAIN
 - B-B = BACK TO BACK
 - = NEW RAIL
 - - - = EXISTING RAIL
 - ⊗ = BORING LOCATION

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

PAVING PROJECT LAYOUT #20	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i20.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 29 OF 49



DATUM	STA	TOTAL DEPTH (MM)	PCC
VERTICAL	N/A		
HORIZONTAL	N/A		
	I3 STA 2+098 LT	I78	NO

NOT TO SCALE

TEMPORARY AND DURABLE 100 mm YELLOW LINE

STA 5+750 TO 5+770 DASHED LT SOLID RT 797
 STA 5+770 TO 5+793 SOLID LT & RT 797
 STA 5+790 TO SOLID LT (SA #3)
 STA 5+805 TO 5+850 SOLID LT & RT

TEMPORARY AND DURABLE 100 mm WHITE LINE

STA 5+750 TO 5+850 SOLID RT
 STA 5+750 TO 5+850 SOLID LT 800
 STA. 5+810 TO 5+850 SOLID LT

TEMPORARY AND DURABLE 600 mm STOP BAR

STA 5+783 LT (SA #3)

TEMPORARY AND DURABLE LETTER OR SYMBOL

STA 5+795 LT - "STOP" (SA #3)
 STA. 5+785 LT - "STOP" (SA #3)

REMOVAL & DISPOSAL OF GUARDRAIL

STA 5+750 TO 5+773 LT (PLUS 46M ONTO S.A. 3)

STEEL BEAM GUARDRAIL

STA 5+750 TO 5+773 LT (PLUS 46M ONTO S.A. 3)

EROSION MATTING

STA. 5+837 TO 5+843 RT

ANCHOR FOR STEEL BEAM RAIL

STA 5+773 LT
 STA 5+780 LT (END OF GUARDRAIL ON S.A. 3)

DELINEATORS WITH STEEL POSTS

STA. 5+780 LT

REMOVING SIGNS

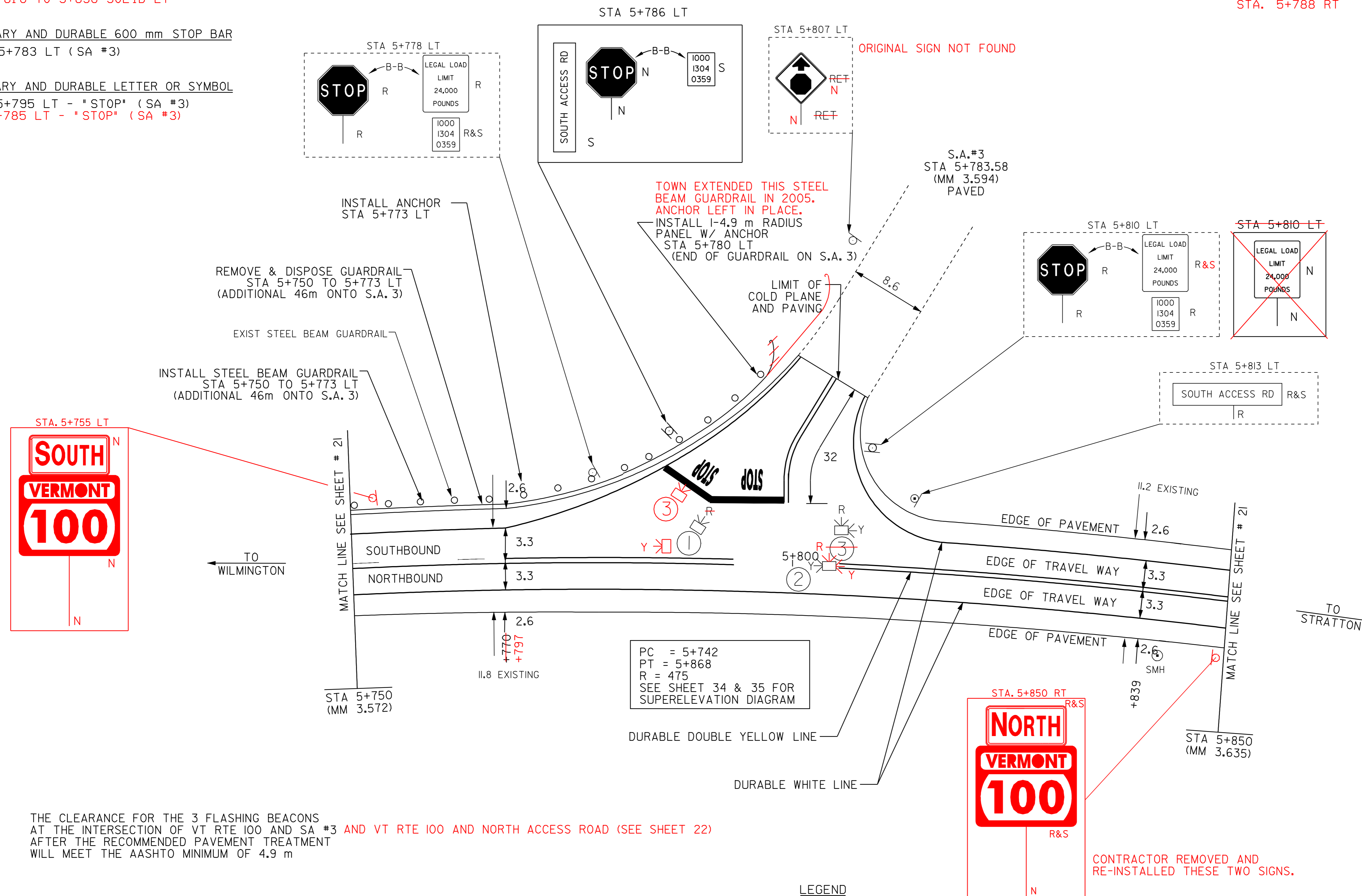
AS SHOWN - 7

ERECTING SALVAGED SIGNS

AS SHOWN - 2

YIELDING MARKER POSTS

STA. 5+782 LT
 STA. 5+788 RT



THE CLEARANCE FOR THE 3 FLASHING BEACONS AT THE INTERSECTION OF VT RTE 100 AND SA #3 AND VT RTE 100 AND NORTH ACCESS ROAD (SEE SHEET 22) AFTER THE RECOMMENDED PAVEMENT TREATMENT WILL MEET THE AASHTO MINIMUM OF 4.9 m

LEGEND

R	= REMOVE EXISTING
S	= SALVAGE
R&S	= REMOVE AND SALVAGE
N	= NEW
RET	= RETAIN
B-B	= BACK TO BACK
—	= NEW RAIL
- - -	= EXISTING RAIL
⊗	= BORING LOCATION

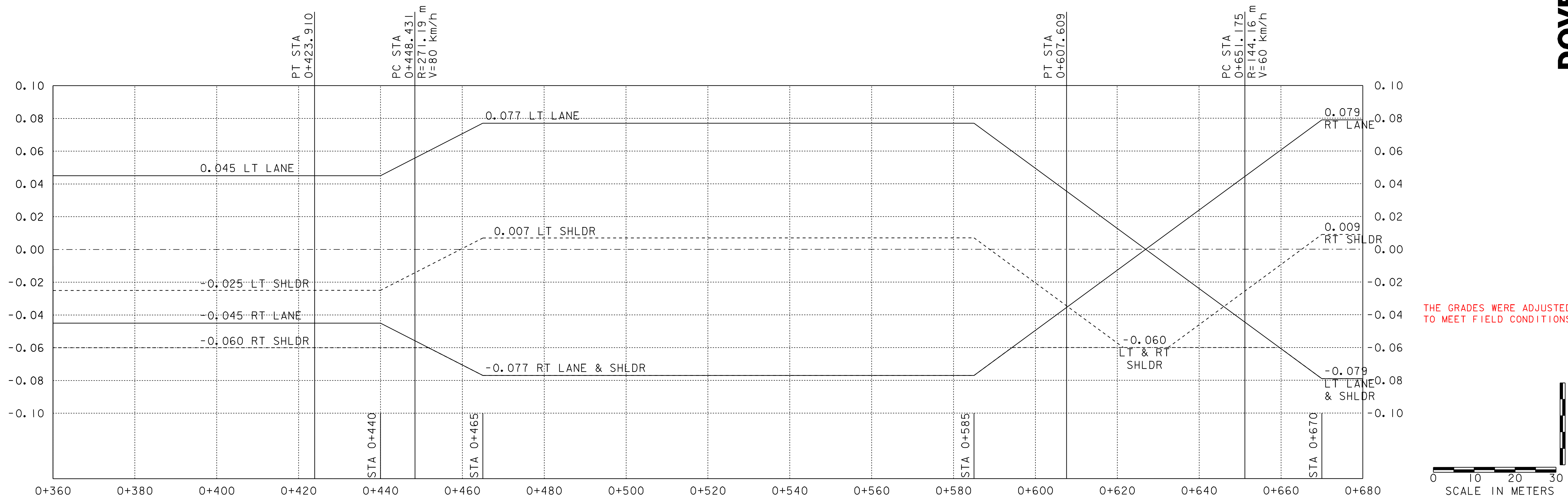
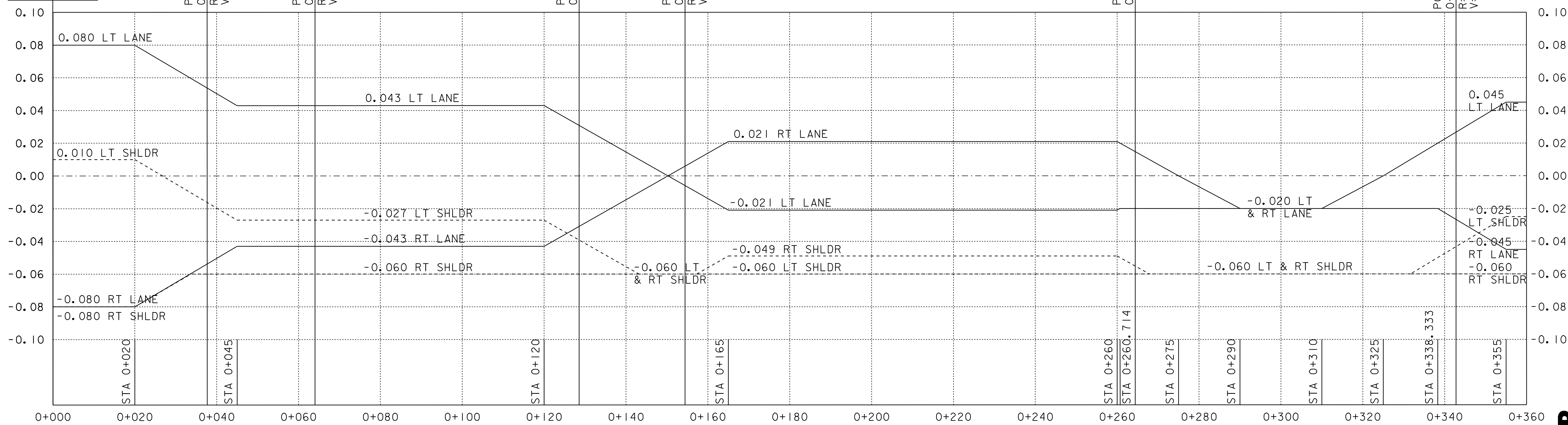
NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

NOT TO SCALE

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

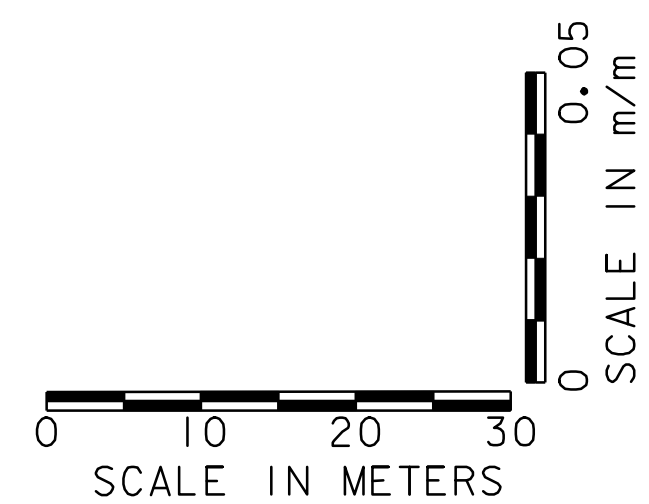
INTERSECTION DETAIL SHEET	PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80i2.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD_ENGINEERS INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 30 OF 49

VT ROUTE 100
 STA 0+000.00
 (MM 0.000)
 BEGIN PROJECT
 STP 2214(I)S



DOVER

THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

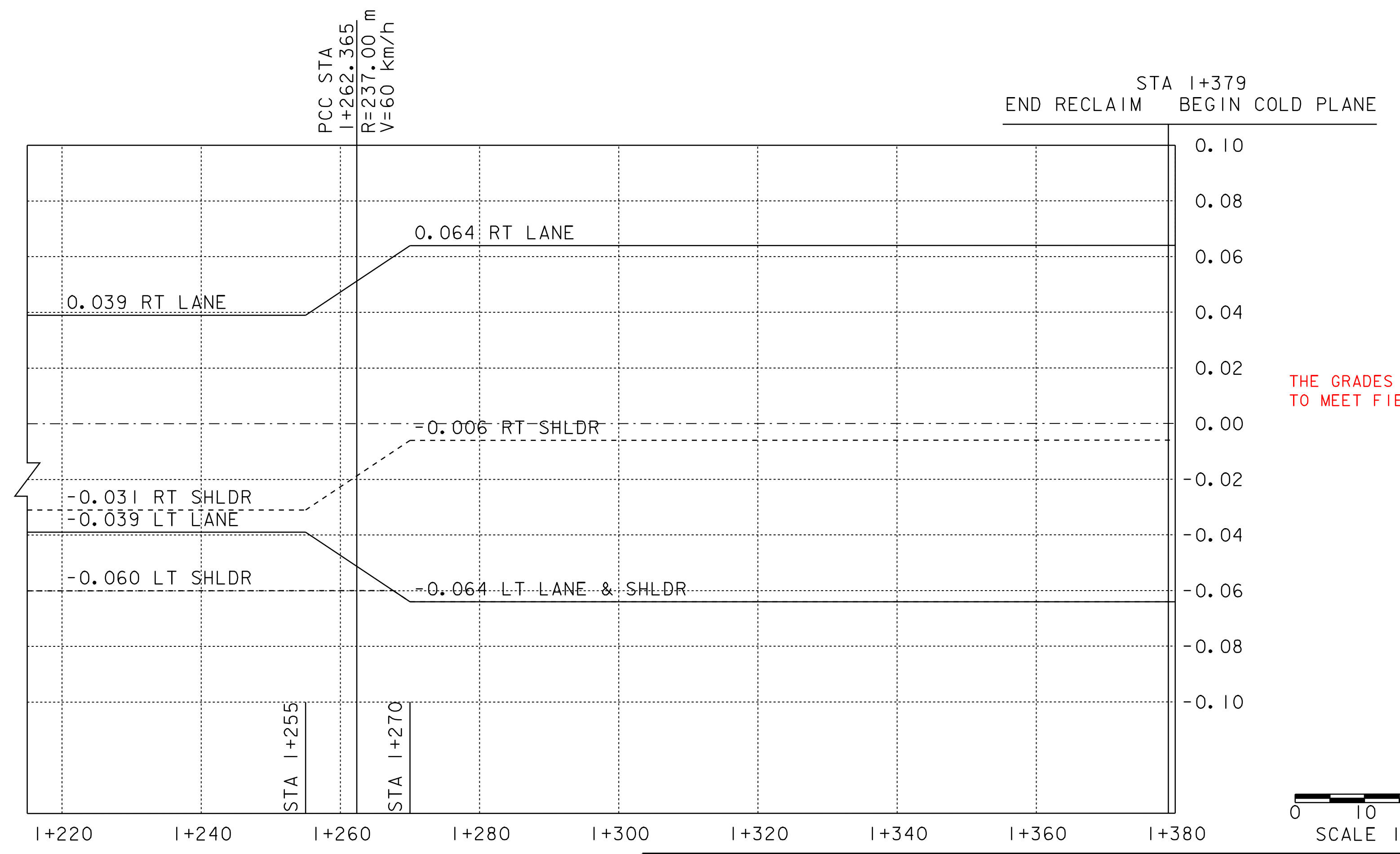
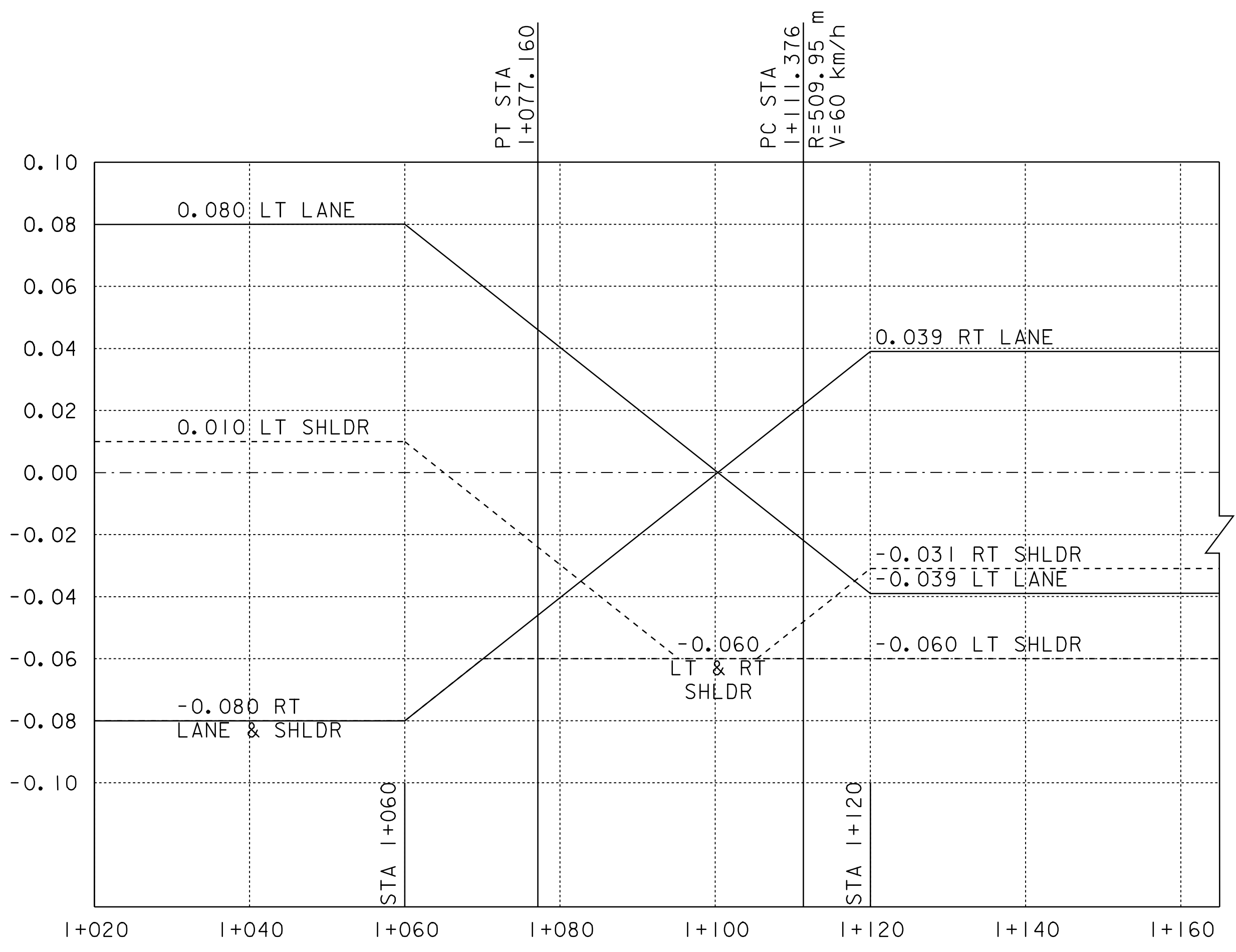
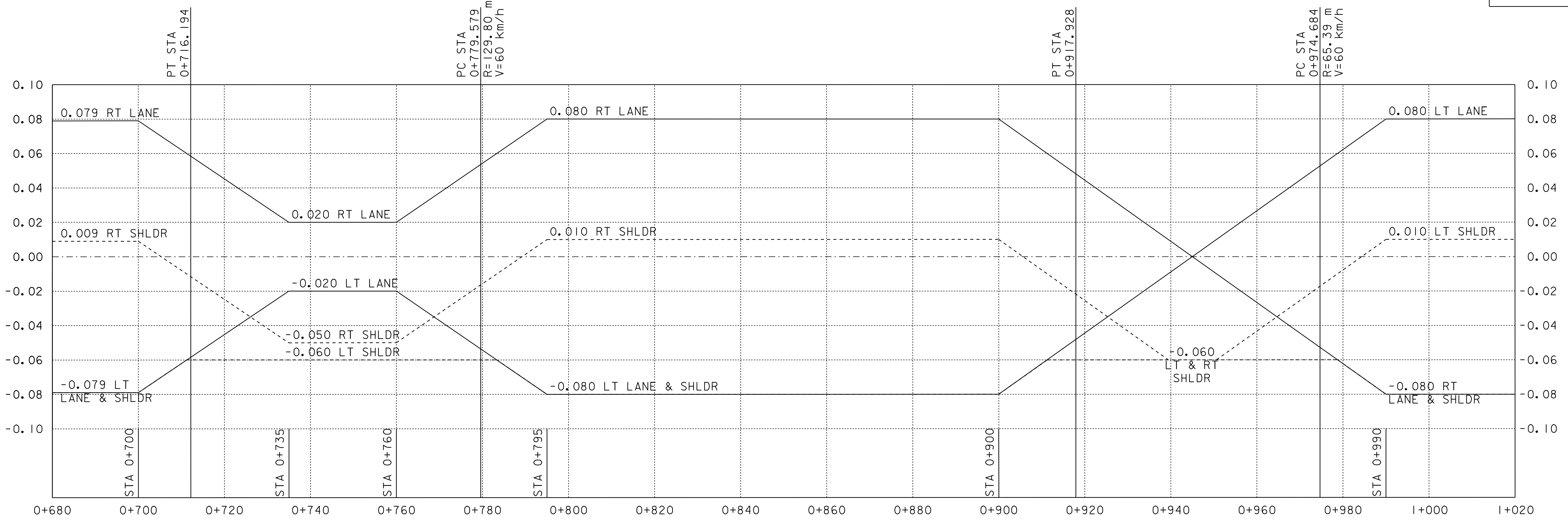


DATUM
 VERTICAL _____ N/A
 HORIZONTAL _____ N/A

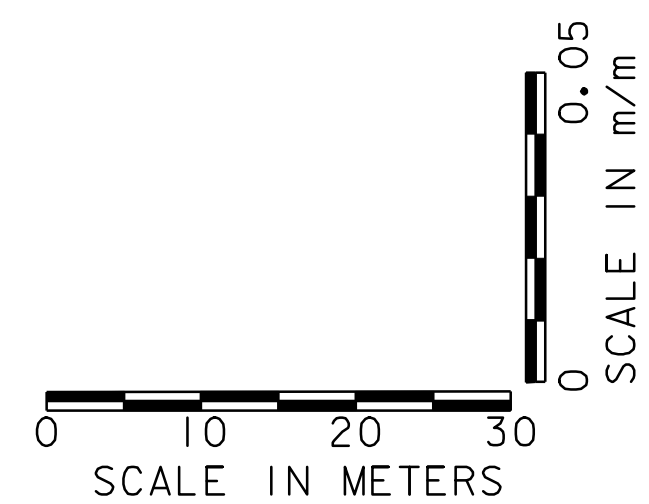
NOTE:
 THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK
 REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE
 REGRADING OF THE RECLAIMED AREAS.

**BANKING
 DIAGRAM
 SHEET 1**

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbi80bd01.i	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: PTS
SQUAD LEADER: WRH	SHEET: 31 OF 49



THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS



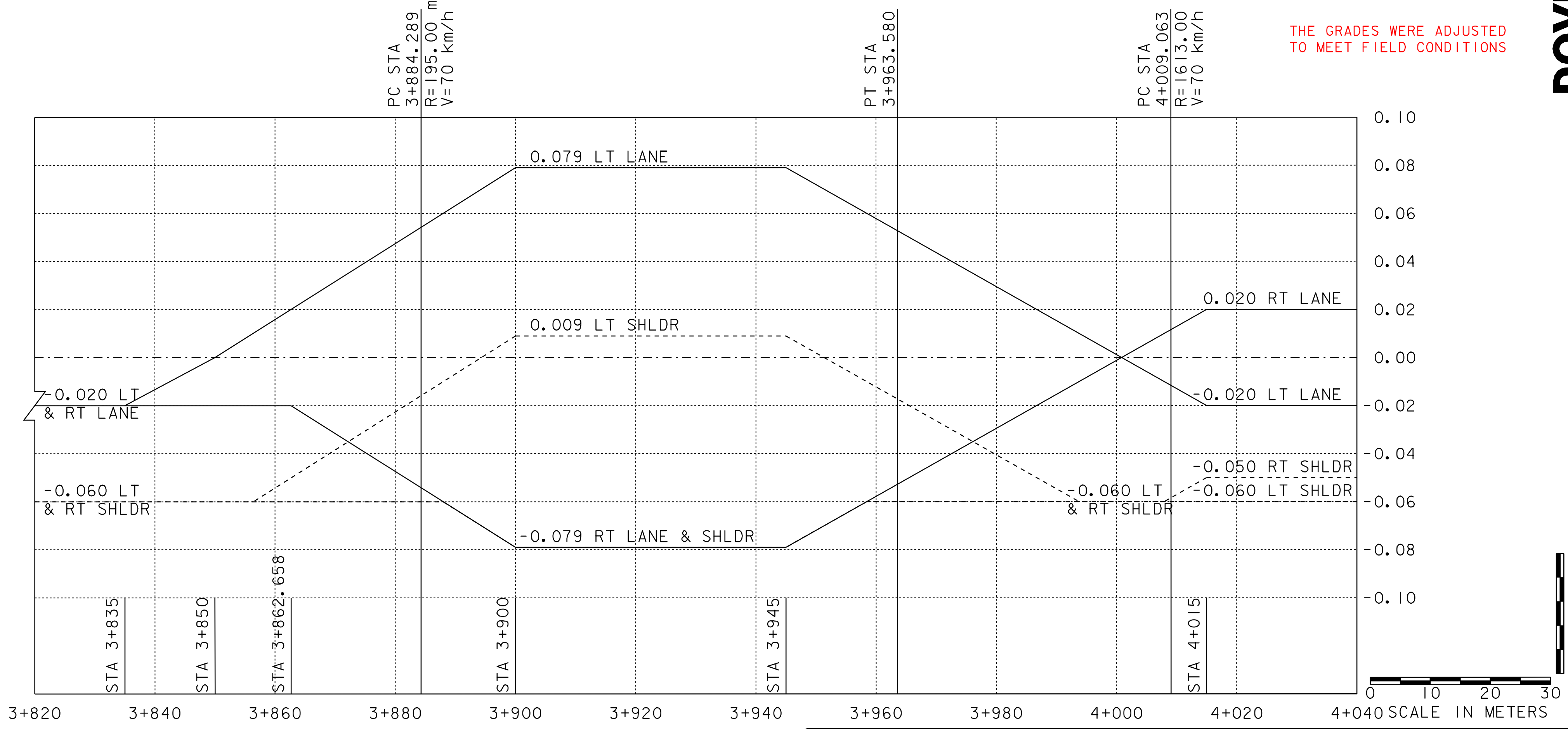
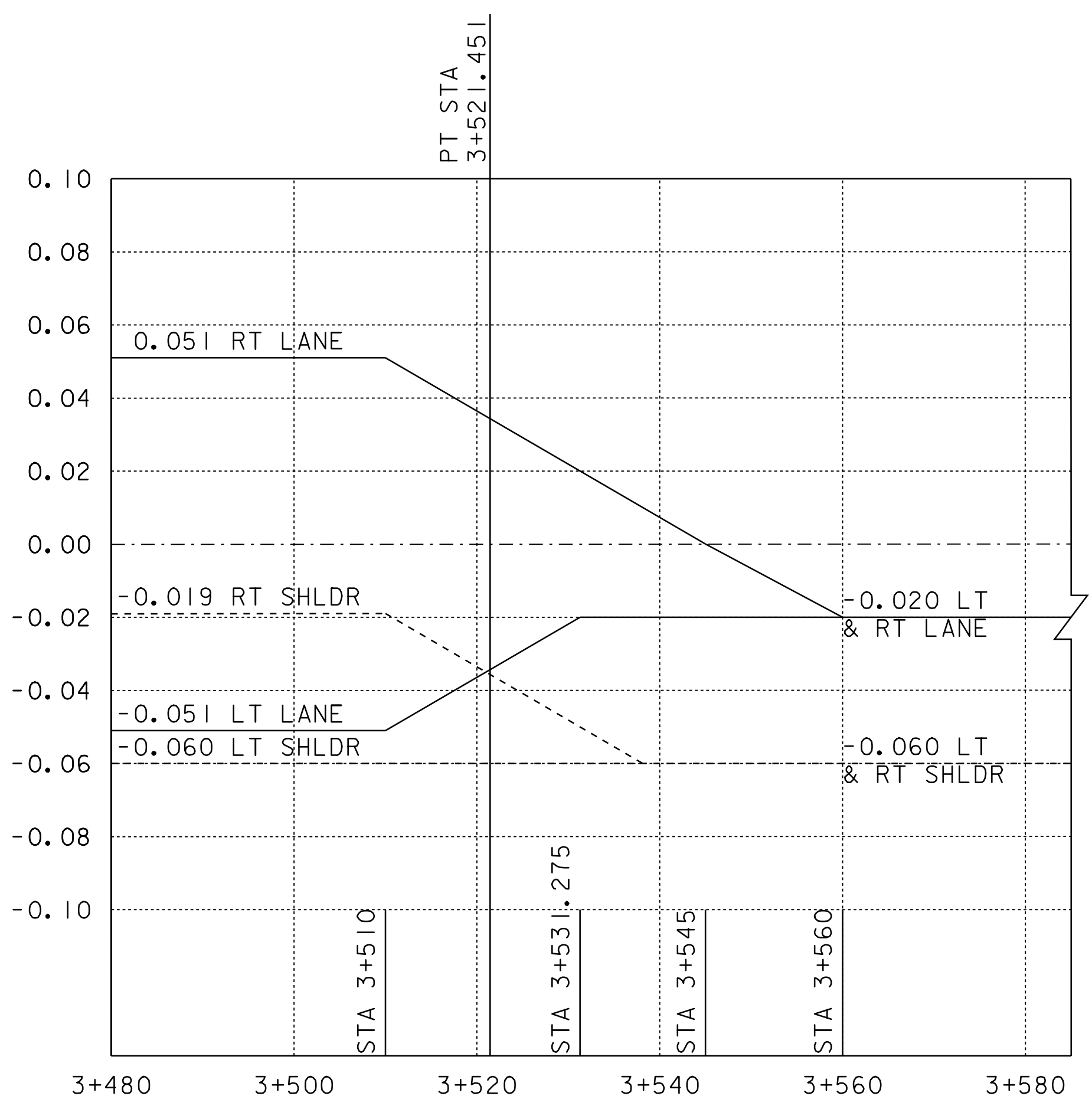
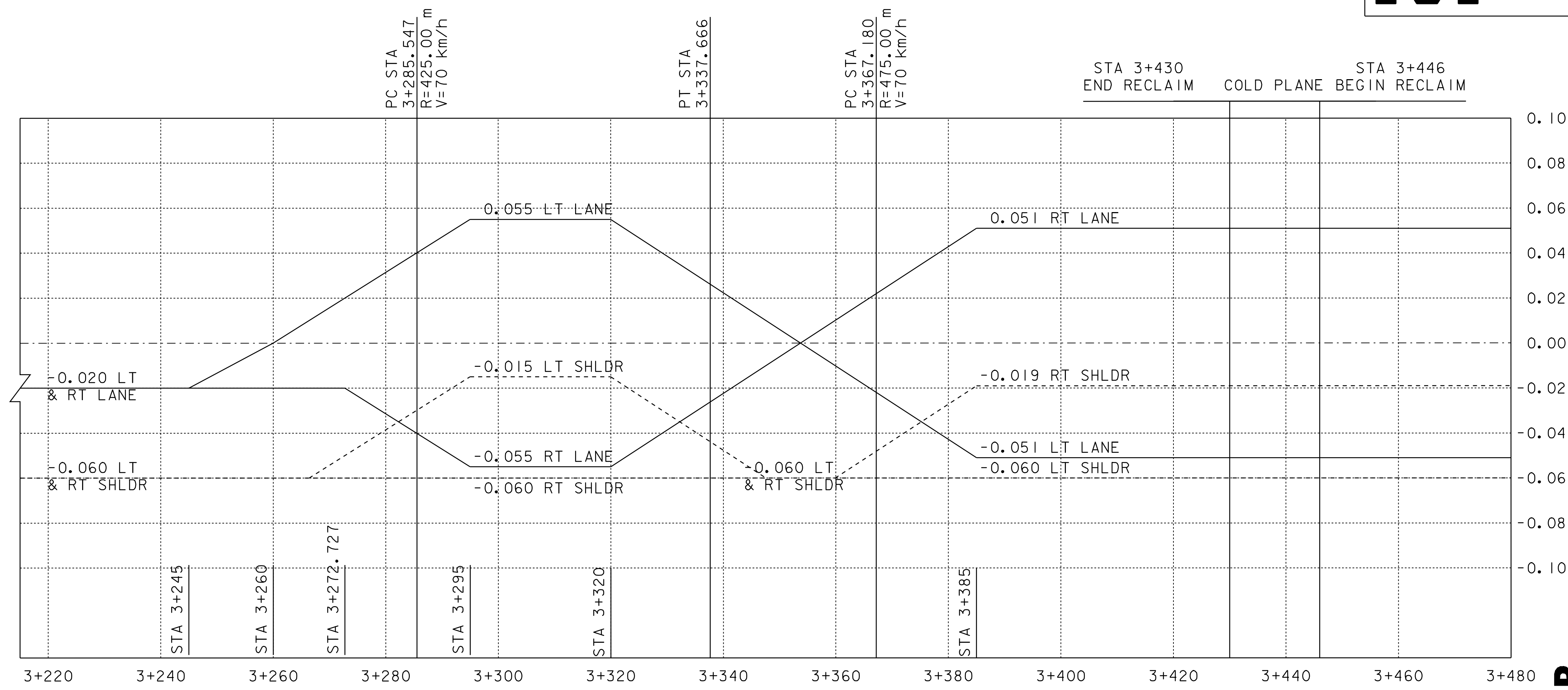
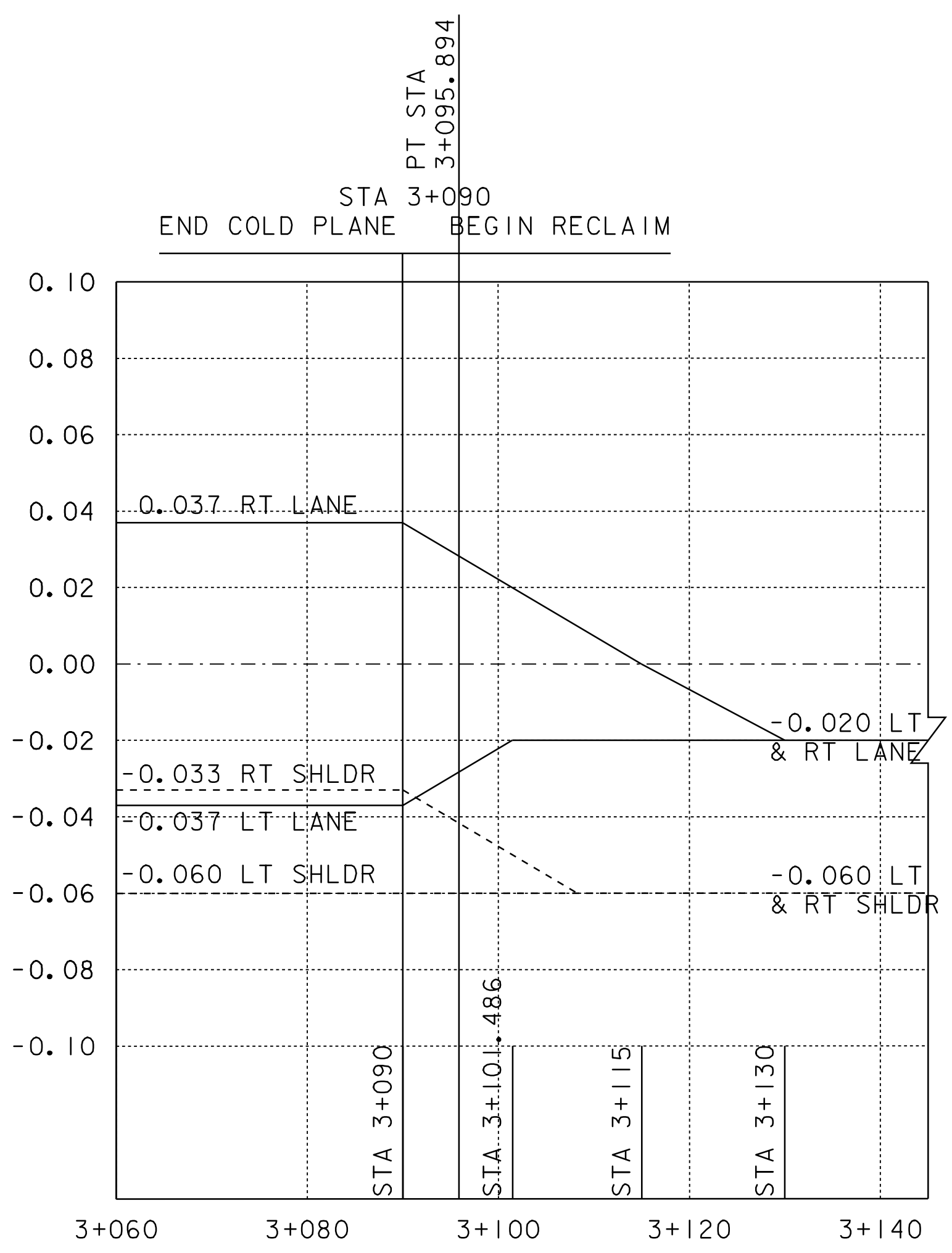
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

**BANKING
DIAGRAM
SHEET 2**

PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
DESIGN FILE NAME:	/pave/99bi80/pbi80.dgn		
IPARM FILE NAME:	pbi80bd02.i		
SURVEYED BY:	CLD ENGINEERS, INC		
SQUAD LEADER:	WRH		
PLOT DATE:	24-MAY-2007		
SURVEY DATE:	5/00		
DRAWN BY:	PTS		
SHEET:	32 OF 49		

DOVER



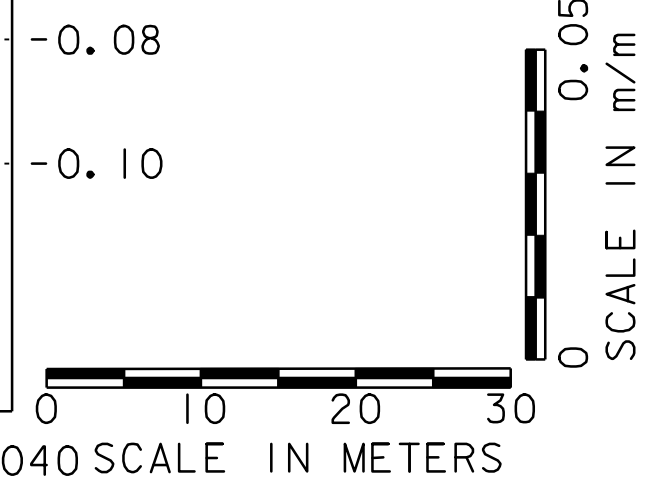
THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

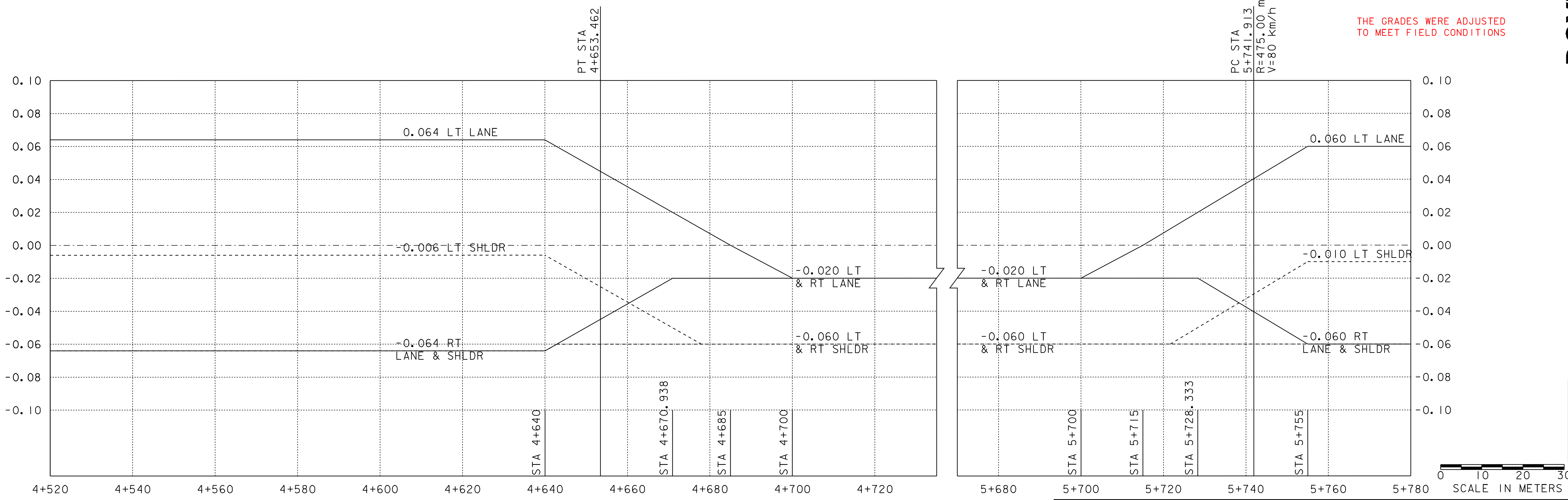
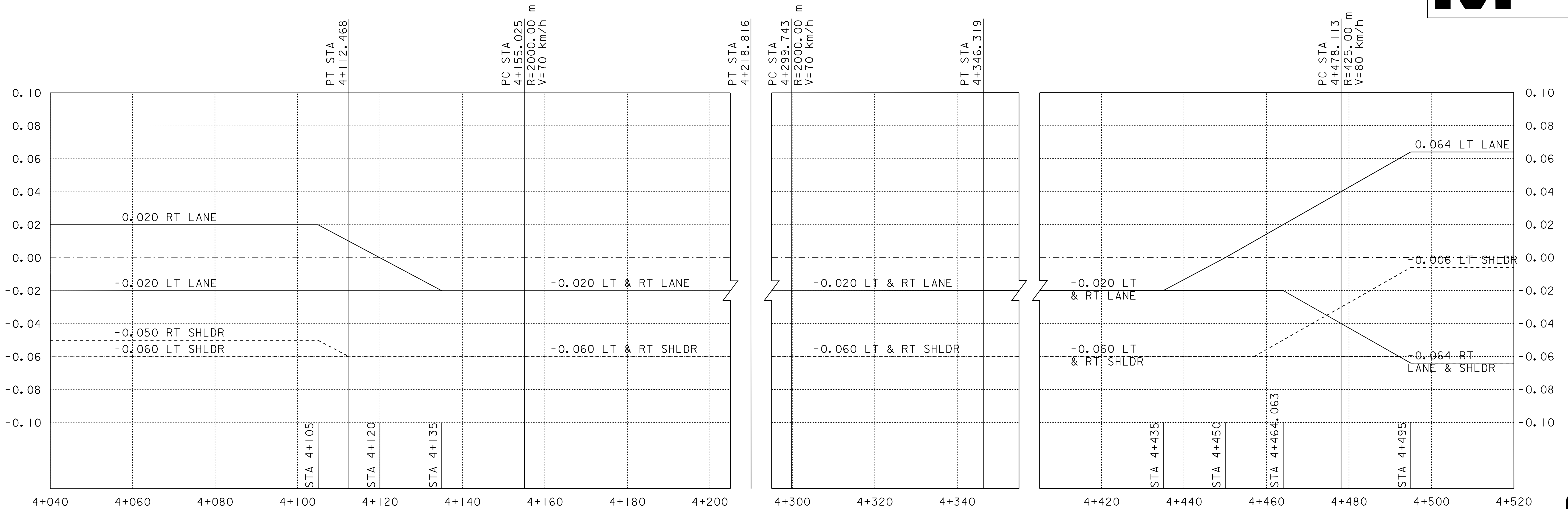
DOVER

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

BANKING DIAGRAM SHEET 3	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbi80bd03.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: PTS
	SQUAD LEADER: WRH	SHEET: 33 OF 49





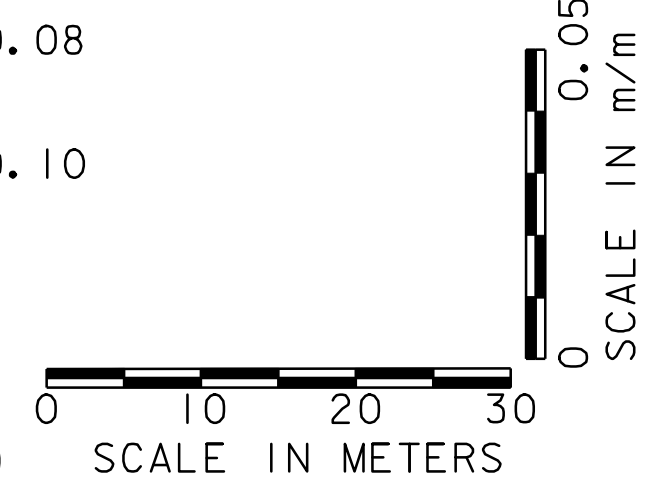
THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

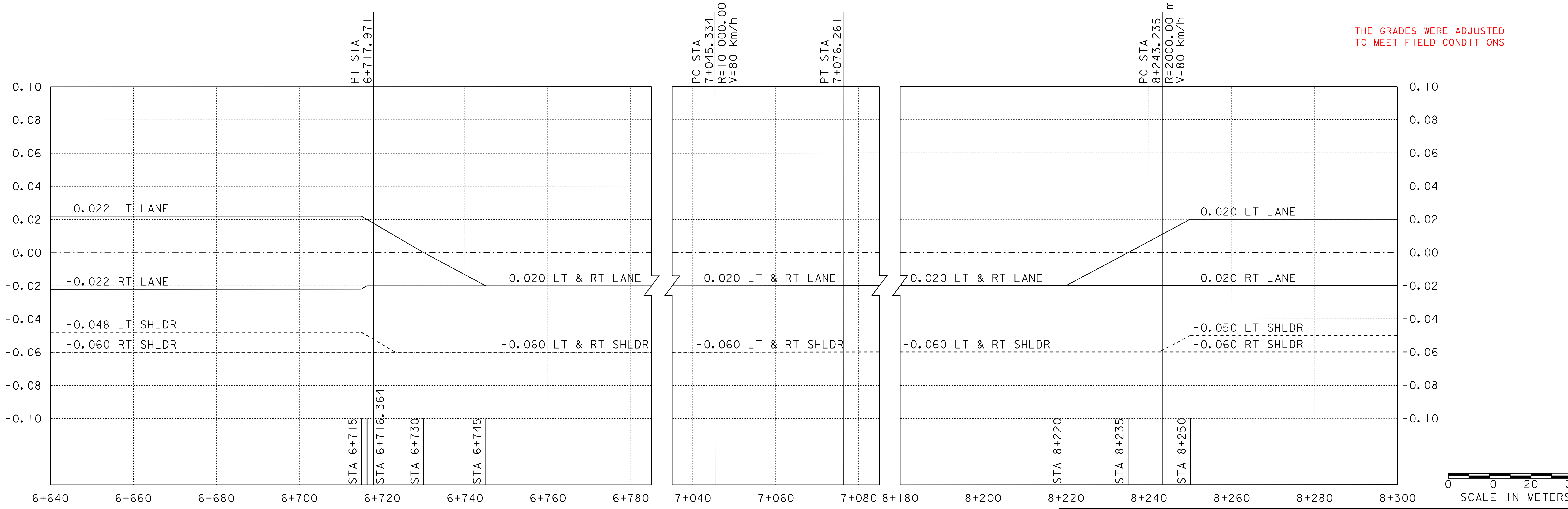
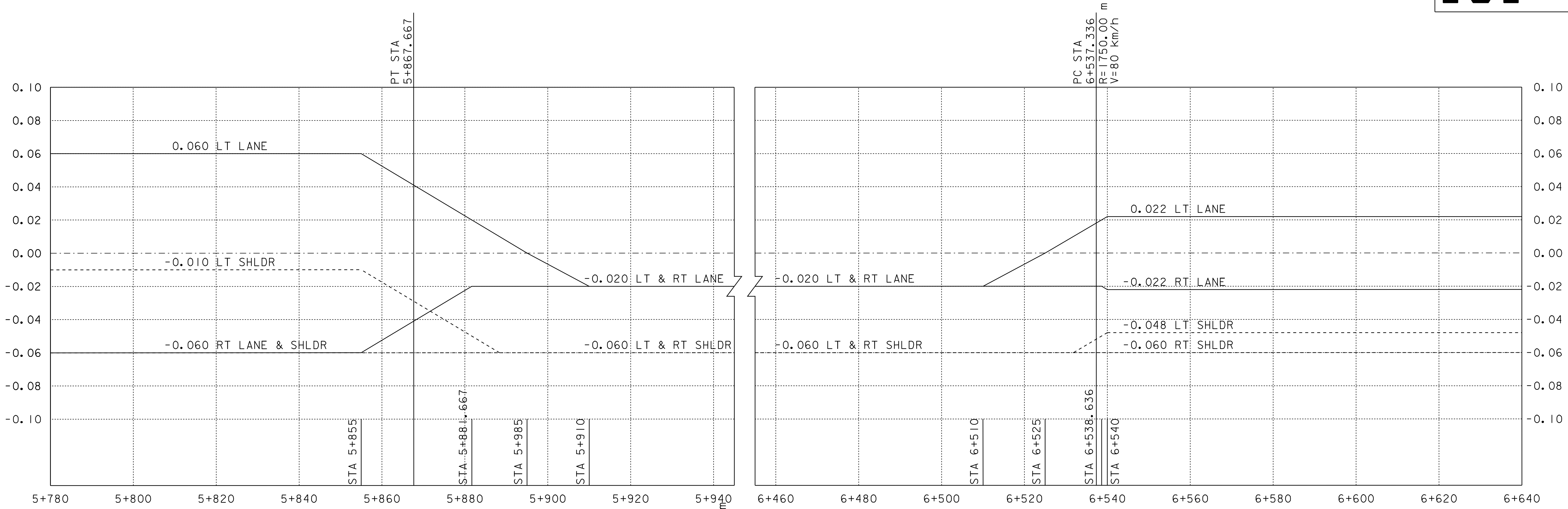
DOVER

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

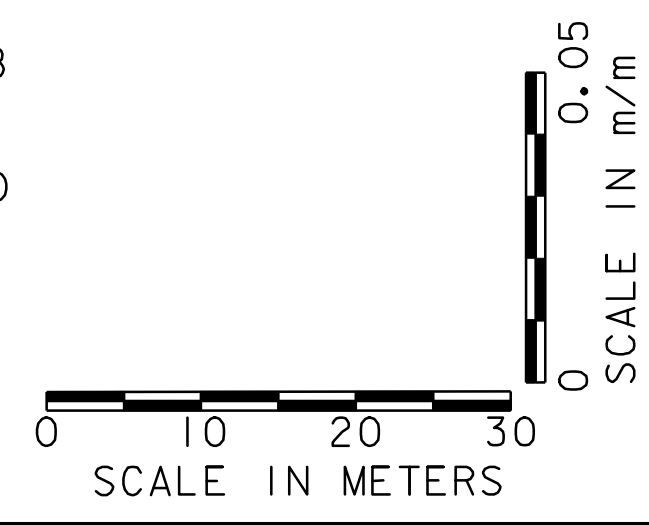
BANKING DIAGRAM SHEET 4	PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbi80bd04.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: PTS
	SQUAD LEADER: WRH	SHEET: 34 OF 49





THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

DOVER

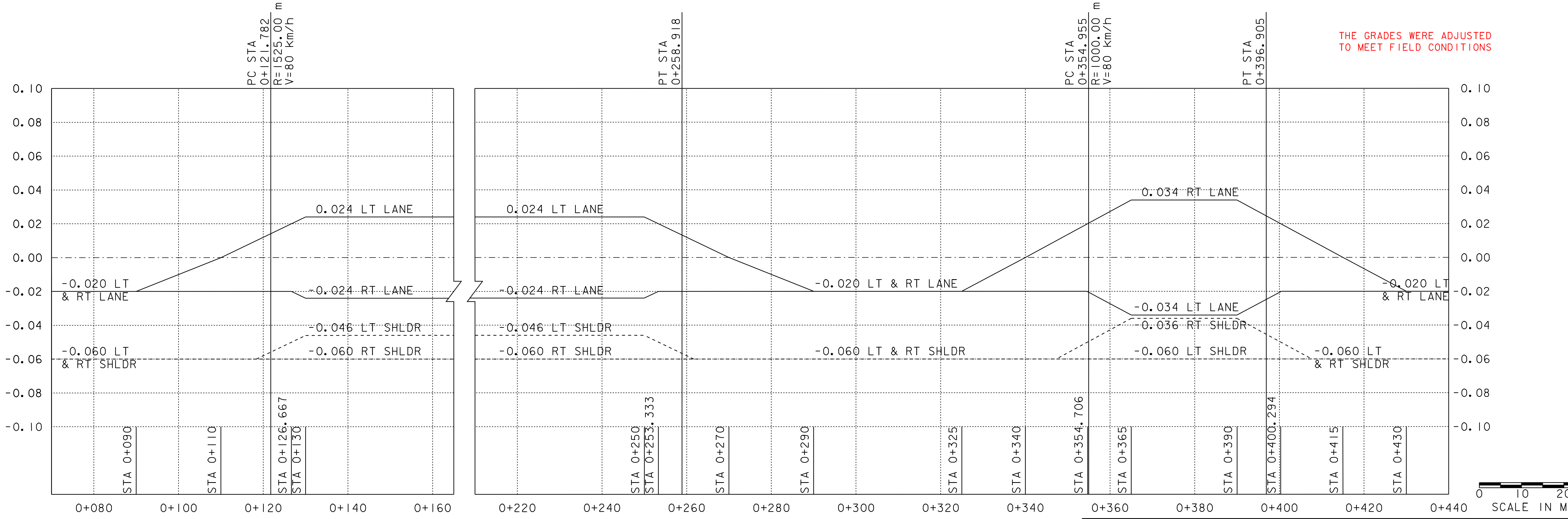
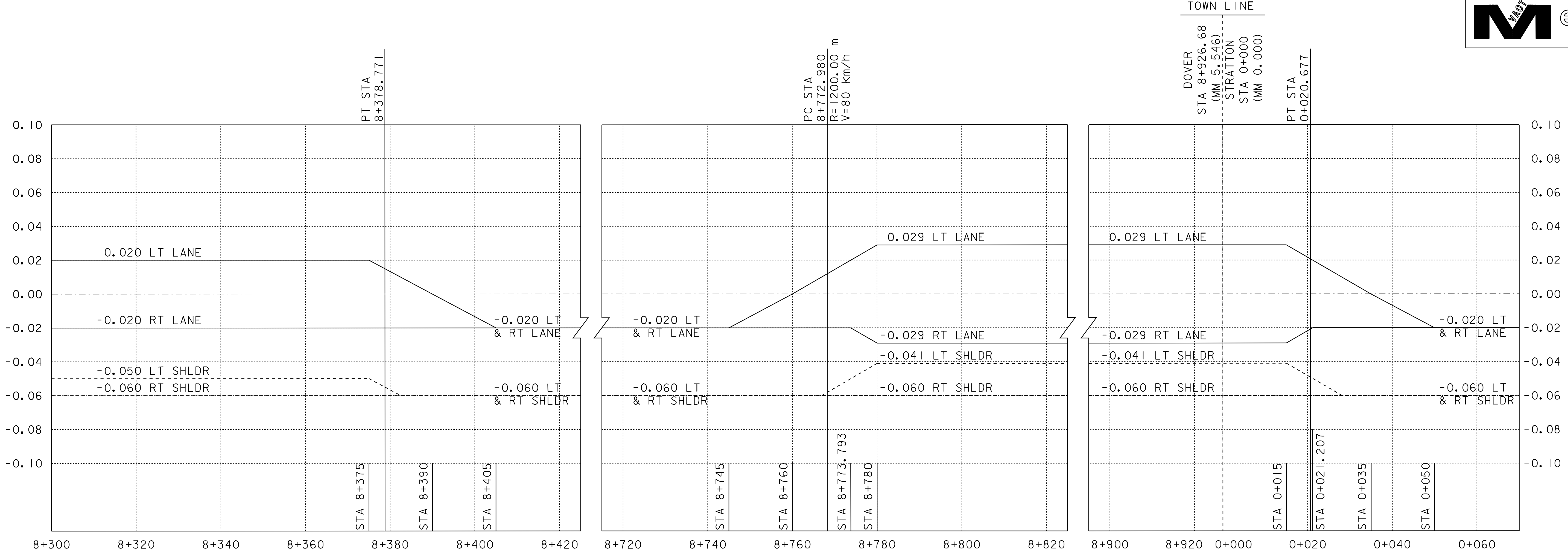


DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

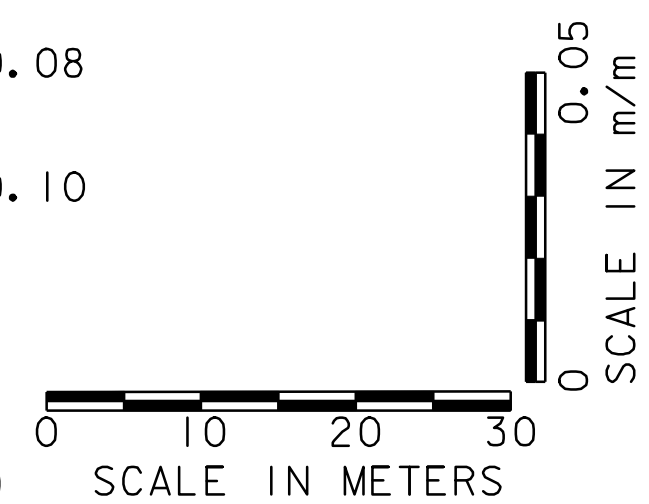
NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

**BANKING
DIAGRAM
SHEET 5**

PROJECT: DOVER-STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bi80/pbi80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbi80bd05.i	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS, INC	DRAWN BY: PTS
SQUAD LEADER: WRH	SHEET: 35 OF 49



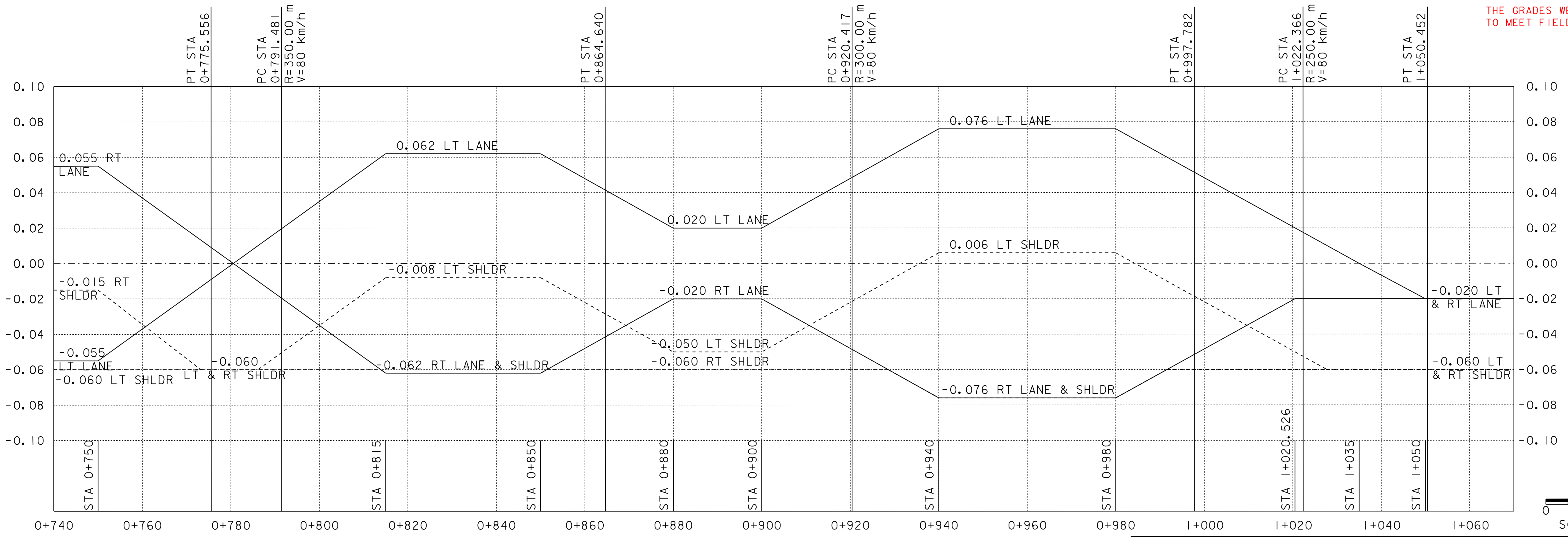
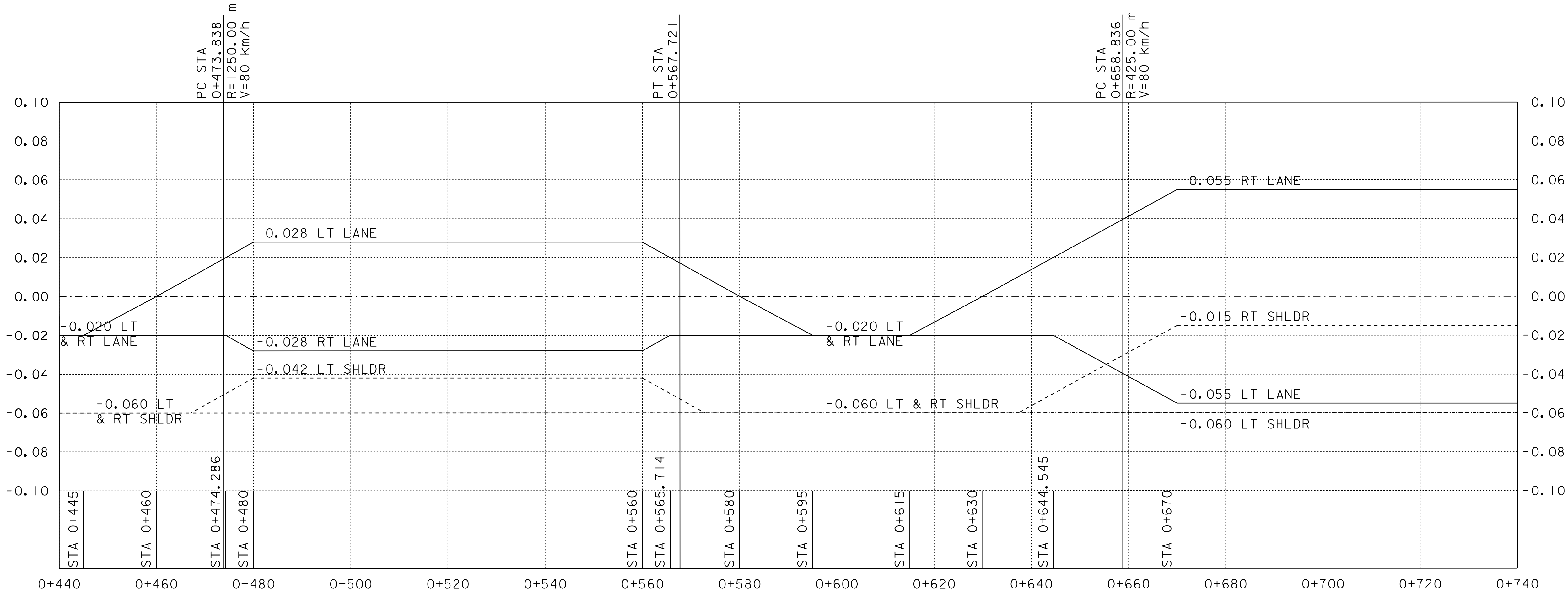
DOVER / STRATTON



DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

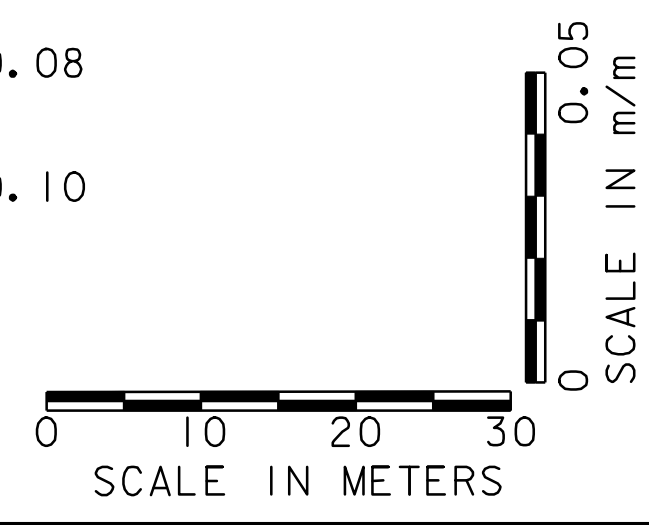
NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

BANKING DIAGRAM SHEET 6	PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
	DESIGN FILE NAME:	/pave/99bi80/pbi80.dgn		
	IPARM FILE NAME:	pbi80bd06.i		
	SURVEYED BY:	CLD ENGINEERS, INC		
SQUAD LEADER:	WRH			PLOT DATE: 24-MAY-2007
				SURVEY DATE: 5/00
				DRAWN BY: PTS
				SHEET: 36 OF 49



THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

STRATTON

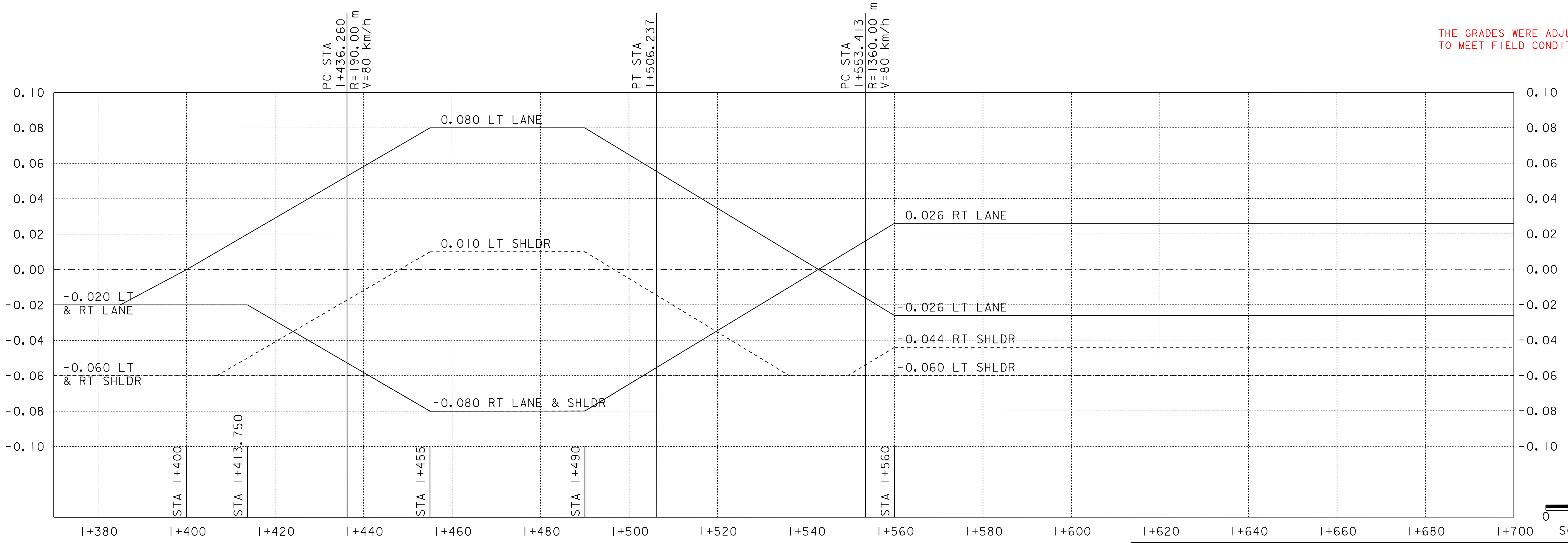
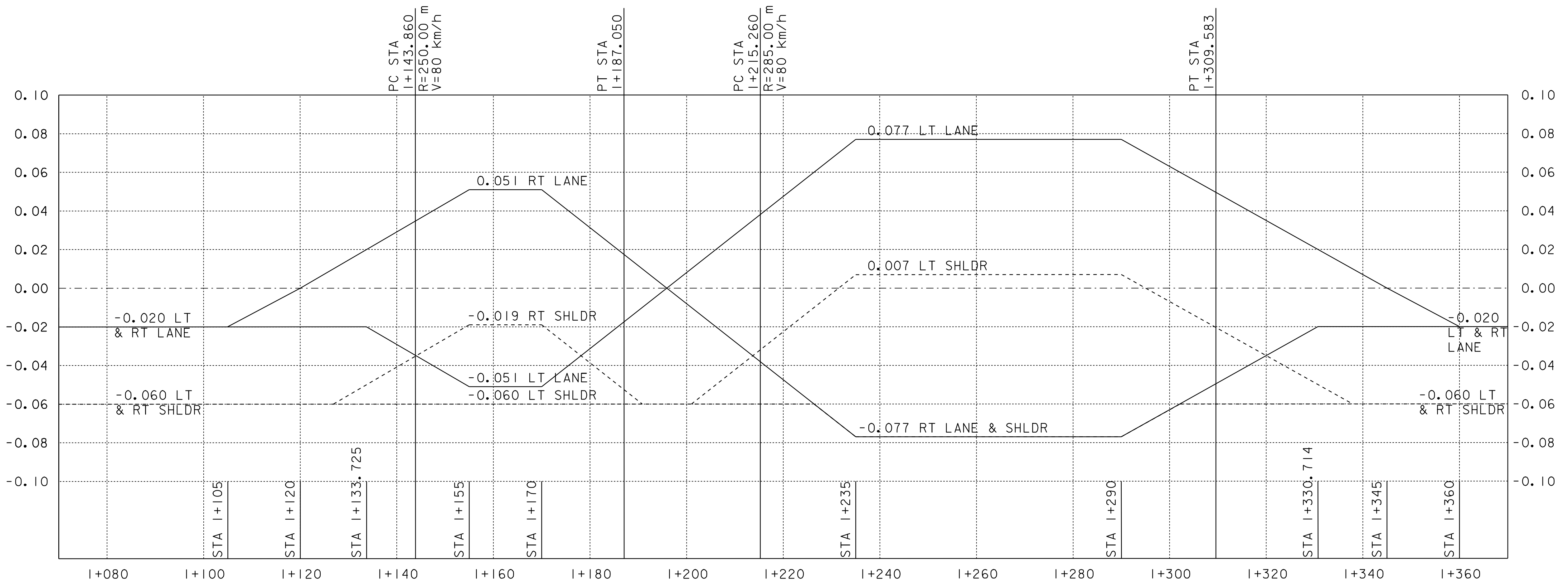


DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

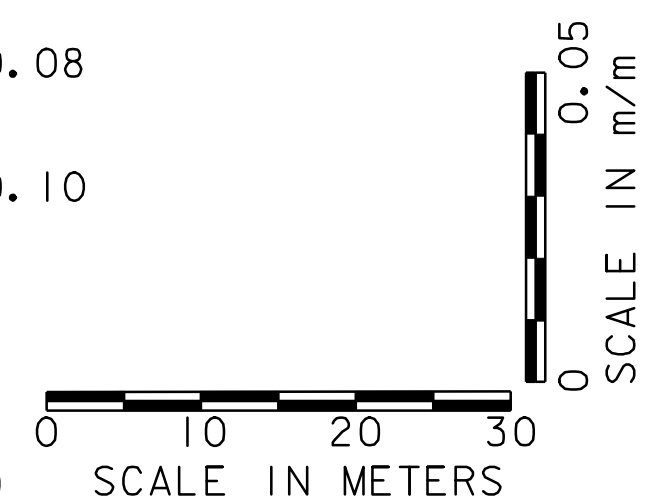
**BANKING
DIAGRAM
SHEET 7**

PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
DESIGN FILE NAME:	/pave/99bi80/pbi80.dgn	PLOT DATE:	24-MAY-2007
IPARM FILE NAME:	pbi80bd07.i	SURVEY DATE:	5/00
SURVEYED BY:	CLD ENGINEERS, INC	DRAWN BY:	PTS
SQUAD LEADER:	WRH	SHEET:	37 OF 49



THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS

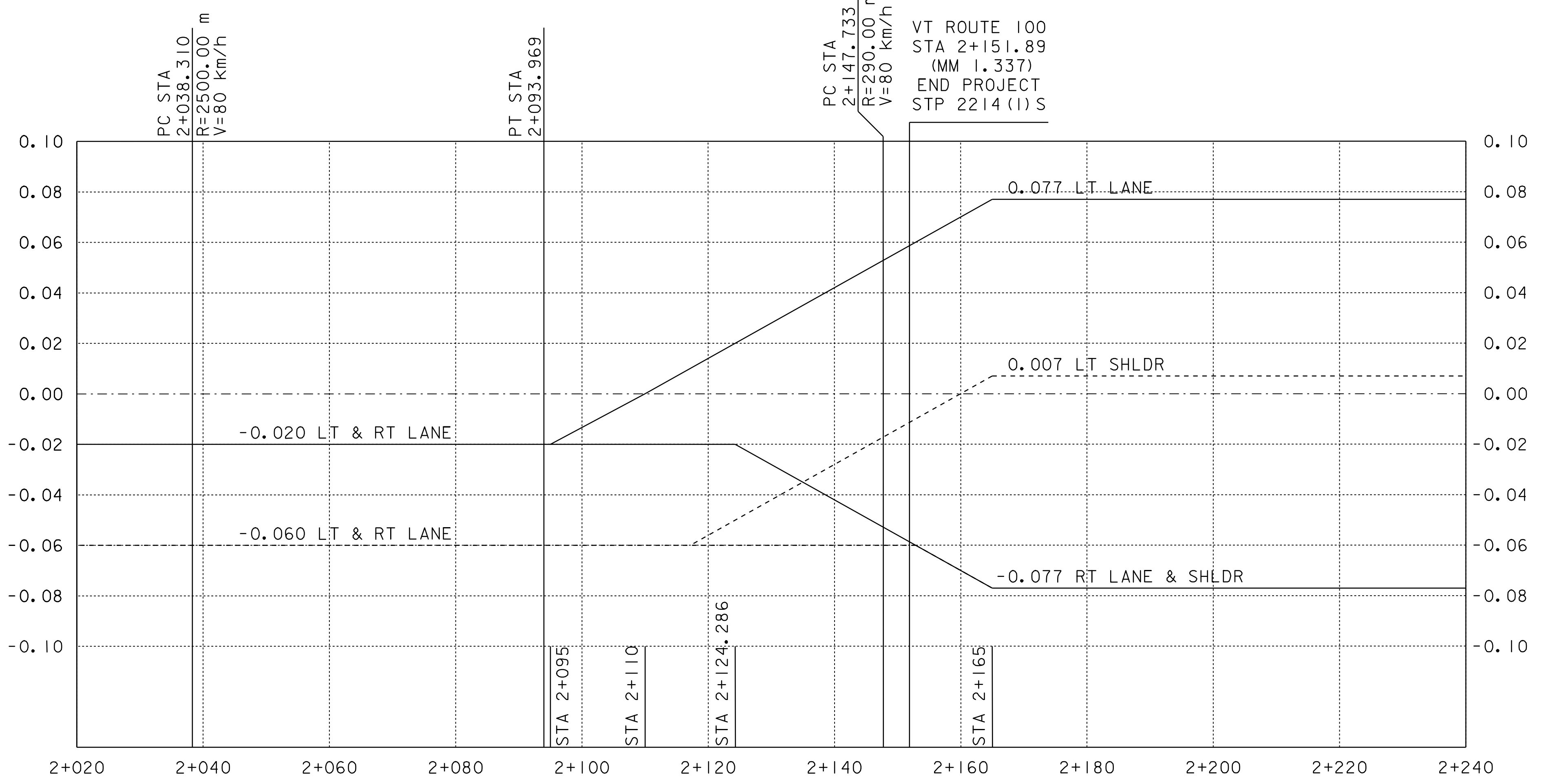
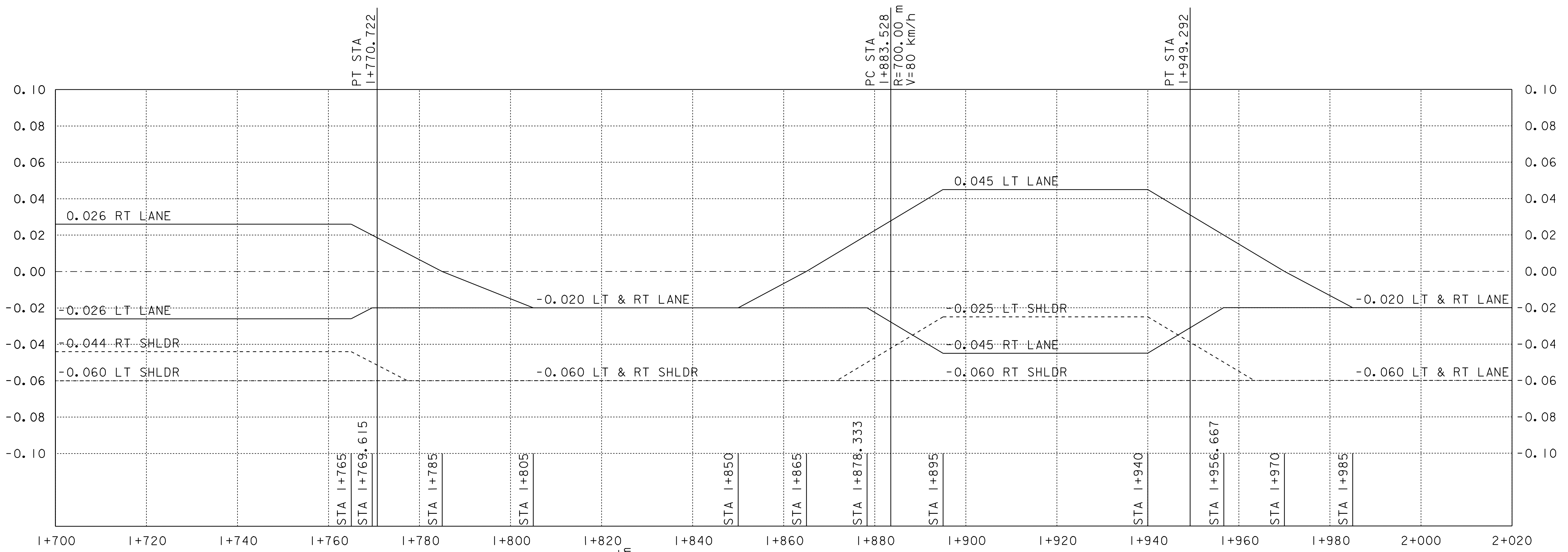
STRATTON



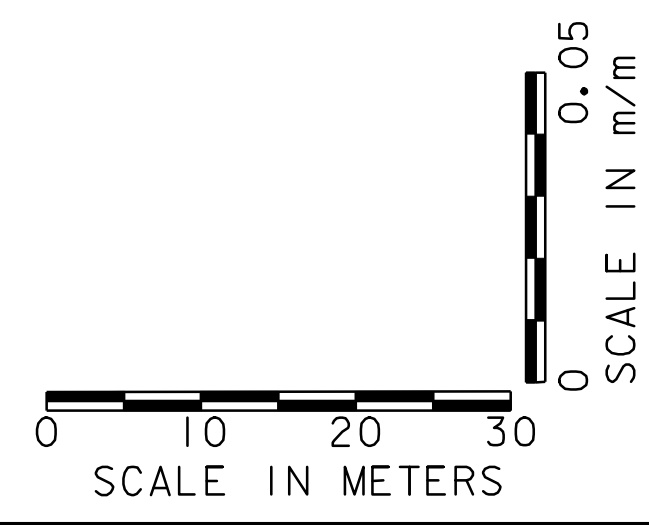
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

BANKING DIAGRAM SHEET 8	PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
	DESIGN FILE NAME:	/pave/99bi80/pbi80.dgn		
	IPARM FILE NAME:	pbi80bd08.i		
	SURVEYED BY:	CLD ENGINEERS, INC		
SQUAD LEADER:	WRH			PLOT DATE: 24-MAY-2007
				SURVEY DATE: 5/00
				DRAWN BY: PTS
				SHEET: 38 OF 49



THE GRADES WERE ADJUSTED TO MEET FIELD CONDITIONS



STRATTON

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
THE CONTRACTOR IS RESPONSIBLE FOR THE ENGINEERING WORK REQUIRED TO LAYOUT AND MAINTAIN THE CROSS SLOPES IN THE REGRADING OF THE RECLAIMED AREAS.

BANKING DIAGRAM SHEET 9	PROJECT:	DOVER-STRATTON	PROJECT NO.:	STP 2214(I)S
	DESIGN FILE NAME:	/pave/99bi80/pbi80.dgn		
	IPARM FILE NAME:	pbi80bd09.i		
	SURVEYED BY:	CLD ENGINEERS, INC		
SQUAD LEADER:	WRH			PLOT DATE: 24-MAY-2007
				SURVEY DATE: 5/00
				DRAWN BY: PTS
				SHEET: 39 OF 49

KILOMETER MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN	SALVAGED	NO. OF POSTS	NEW SIGN POSTS														REQUIRE SIGN	REMARKS	SIGN DETAIL						
		EA	WIDTH (mm)	HEIGHT (mm)	"A"	"B"	SALV SIGN				SALV TIS	FLANGED CHANNEL			SQUARE STEEL (mm)			TUBULAR ALUMINUM Ø (mm)			TUBULAR STEEL Ø (mm)						W-SHAPE STEEL				DETAIL ON SHEET NUMBER	STD. SHEET NUMBER	
												1.7	3.0	4.5	44	50	63	75	100	100 MOD	FOUND-ATION	75	89	100			125	FTG. SIZE		WEIGHT			POST SIZE
																												kg/m	kg/m				
STA 1+782 RT	VALLEY VIEW ROAD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																						BACK TO BACK		E-143M				
* STA 2+856 LT		1	600	750	0.45								X 4.27														ORIGINAL SIGN NOT FOUND. NEW SIGN INSTALLED.		E-141M				
STA 2+557 RT	WINDY HILL RD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																						BACK TO BACK		E-143M E-138M				
* SEE ABOVE STA 2+843 LT	COUNTRY CLUB RD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF STOP SIGN						
		1	750	750	0.56																						BACK TO BACK		E-143M E-138M				
STA 3+526 RT		1	750	750	0.56					1			X 4.27	X																E-143M			
STA 3+576 RT *RD SIGN SEE SH 43		1	750	750	0.56					1			X 4.27	X													BACK TO BACK		E-143M E-138M				
STA 3+736 LT	STUGGER RD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																						BACK TO BACK NEW MM		E-143M E-138M				
STA 3+576 RT							1						X 4.27																				
STA 3+875 LT	STUGGER RD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																						BACK TO BACK		E-143M E-138M				
STA 6+638 LT							1						X 4.27														ORIGINAL SIGN MISSING. NEW SIGN INSTALLED.		E-141M				
STA 4+533 LT	TANNERY RD						1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF STOP SIGN						
		1	750	750	0.56																						BACK TO BACK		E-143M E-138M				

OPTION ITEMS

m m m 46.97 36.8 m m 0 36.8 EA kg kg kg kg kg kg kg kg

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S "SIGN POST DESIGN GUIDELINE."

TOTALS	m ² 5.11 4.63	m ²	EA.	m ²		m	46.97 36.8	m	0 36.8		EA.	kg		EA.	kg	EA.	EA.	kg
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PROJECT :	DOVER - STRATTON	PROJECT NO. :	STP 2214(I) S
DESIGN FILE NAME:	/pave/99b180/pb180.dgn	PLOT DATE:	24-MAY-2007
IPARM FILE NAME:	pb180ss02.1	SURVEY DATE:	05/00
SURVEYED BY:	CLD_ENGINEERS_INC	DRAWN BY:	MPS
SQUAD LEADER:	WRH	SHEET:	41 OF 49

KILOMETER MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST RETAIN SALVAGE	NO. OF POSTS	NEW SIGN POSTS												REQUIRE FRAMING SIGN	REMARKS	SIGN DETAIL								
		EA	WIDTH (mm)	HEIGHT (mm)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL			SQUARE STEEL (mm)			TUBULAR ALUMINUM Ø (mm)			TUBULAR STEEL Ø (mm)				W-SHAPE STEEL		DETAIL ON SHEET NUMBER	STD. SHEET NUMBER					
											1.7	3.0	4.5	44	50	63	75	100	100 MOD	FOUND-ATION	75			89	100			125	FTG. SIZE		WEIGHT	POST SIZE
																													kg/m	kg/m		
STA 4+845 RT	MOUNTAIN VIEW LOOP					1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF STOP SIGN						
		1	750	750	0.56																					BACK TO BACK		E-143M E-138M				
STA 4+951 RT		1	750	750	0.56				1			X 4.27	X													BACK TO BACK		E-143M E-138M				
STA 5+786 LT	SOUTH ACCESS RD					1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF STOP SIGN						
		1	750	750	0.56																					BACK TO BACK		E-143M				
STA 5+755 LT	 	1	300	600	0.18							X 4.88														ORIGINAL SIGNS DESTROYED IN 2004. REPLACED IN 2005.		E-136BM				
STA 4+940 RT STA 5+810 LT		1	600	750	0.45				1			X 4.27	X															E-141M				
STA 5+997 LT		1	750	750	0.56				1			X 4.88	X													STATE ROUTE/TOWN HIGHWAY INTERSECTION SIGN		E-155M				
STA 6+627 LT	NORTH ACCESS RD					1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF STOP SIGN						
		1	750	750	0.56																					BACK TO BACK		E-143M E-138M				
STA 6+636 LT		1	750	750	0.56				1			X 4.88	X													SIGN TO BE PLACED A MINIMUM OF 100 METERS FROM AN INTERSECTION IN ACCORDANCE TO MUTCD ADVANCED WARNING SIGN PLACEMENT GUIDELINE.		E-150M				
STA 5+807 LT		1	750	750	0.56							X 4.27														ORIGINAL SIGN NOT FOUND. NEW SIGN INSTALLED.		E-150M				
STA 7+049 LT	LOWER HIGHLAND RD					1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																					BACK TO BACK		E-143M				
STA 7+187 LT	UPPER HIGHLAND RD					1			1			X 4.27	X													SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN						
		1	750	750	0.56																					BACK TO BACK		E-143M				
STA 6+710 RT	 	1	300	600	0.18							X 4.27														ORIGINAL SIGNS DESTROYED IN 2004. REPLACED IN 2005.		E-136BM				
		1	600	600	0.36																											

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S "SIGN POST DESIGN GUIDELINE."										m		m		m		m		EA		kg		kg		kg		kg		EA.		EA.		kg	
										53.07 41.4		0 41.4		EA		kg		kg		kg		kg		EA.		EA.		kg					
TOTALS										6.66 5.02		m ²		EA.		m ²		EA		m		0 41.4		kg		EA.		EA.		kg			

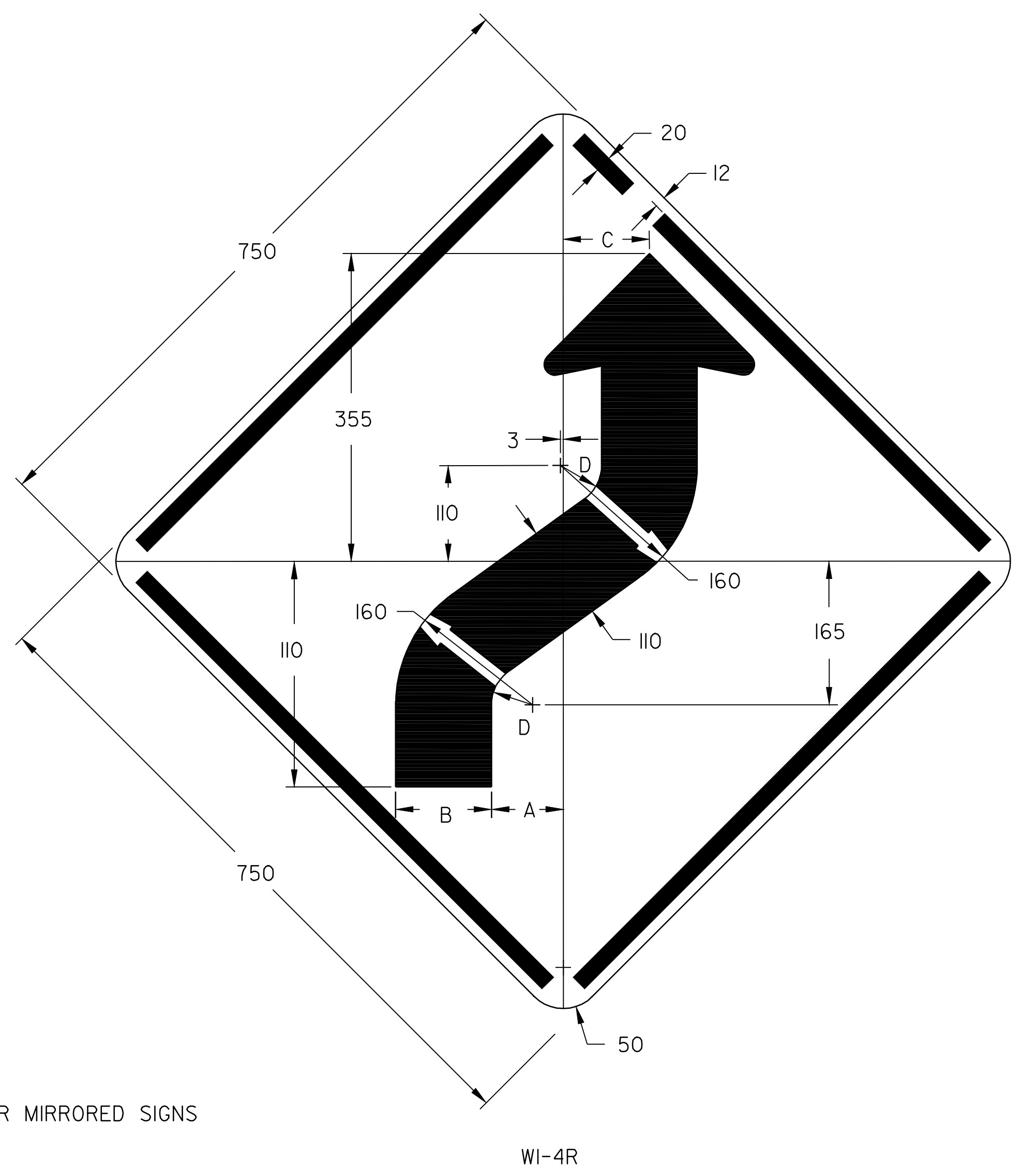
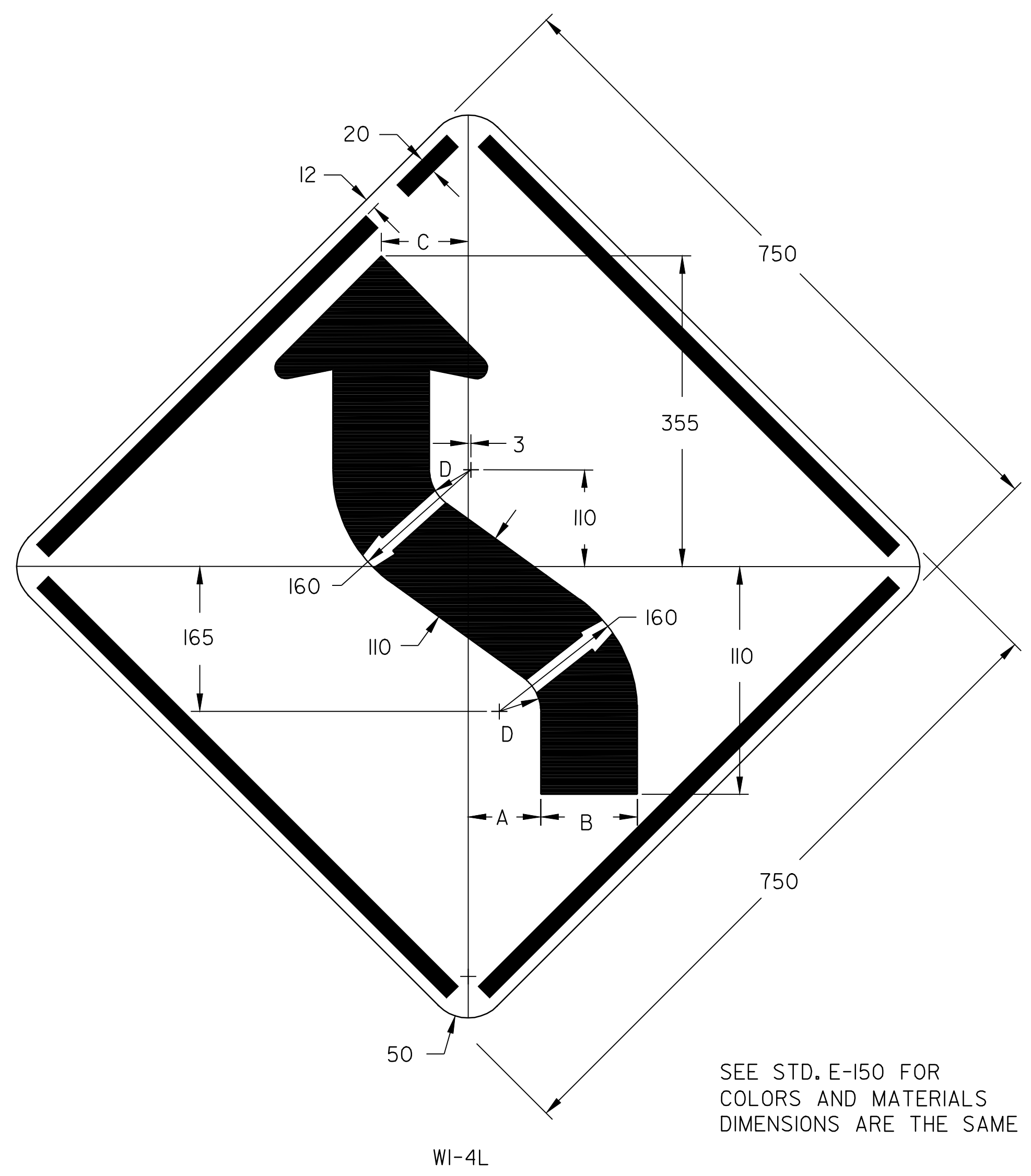
PROJECT :	DOVER - STRATTON	PROJECT NO. :	STP 2214(I) S
DESIGN FILE NAME:	/pave/99bl80/pbl80.dgn	PLOT DATE:	24-MAY-2007
IPARM FILE NAME:	pbl80ss03.i	SURVEY DATE:	05/00
SURVEYED BY:	CLD_ENGINEERS_INC	DRAWN BY:	MPS
SQUAD LEADER:	WRH	SHEET:	42 OF 49

KILOMETER MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXIST POST	NO. OF POSTS	NEW SIGN POSTS												REQUIREMENTS	REMARKS	SIGN DETAIL								
		EA	WIDTH (mm)	HEIGHT (mm)	"A"	"B"	SALV SIGN			SALV TIS	FLANGED CHANNEL		SQUARE STEEL (mm)			TUBULAR ALUMINUM Ø (mm)			TUBULAR STEEL Ø (mm)					W-SHAPE STEEL		DETAIL ON SHEET NUMBER	STD. SHEET NUMBER					
											1.7	3.0	4.5	44	50	63	75	100	100 MOD	FOUND-ATION	75			89	100			125	FTG. SIZE		WEIGHT	POST SIZE
																													kg/m	kg/m		
STA 7+811 RT	AHMEEK RD					1			1		X		X														SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN					
		1	750	750	0.56																						BACK TO BACK		E-143M			
STA 8+542 RT STA 3+57 RT*	BLUE BROOK RD					1			1		X		X														SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN					
		1	750	750	0.56																						BACK TO BACK NEW MM INSTALLED ALSO		E-143M			
STA 8+788 RT	DUNCAN RD					1			1		X		X														SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN					
		1	750	750	0.56																						BACK TO BACK		E-143M			
STRATTON																																
STA 0+548 RT		1	750	750	0.56				1		X		X																44			
		1	450	450	0.20																									E-155M		
STA 0+975 LT		1	750	750	0.56				1		X		X																44			
		1	450	450	0.20																									E-155M		
STA 1+761 LT	PENNY AVE	1	750	150	0.11		±		1		X		X														NEW SALVAGED SIGN TO BE INSTALLED USING MOUNTING BRACKET AS SHOWN ON E-160M, MOUNTED 0.3M FROM AND PERPENDICULAR TO TOP OF EXISTING STOP SIGN		E-123M			
		1	750	750	0.56																						BACK TO BACK NEW MM INSTALLED ALSO		E-143M			
		1	150	200	0.03		±																									
	SUBTOTALS SHEET 40				3.42																											
	SUBTOTALS SHEET 41				4.94		8																									
	SUBTOTALS SHEET 42				5.11		8																									
	SUBTOTALS SHEET 43				4.63		8																									
	SUBTOTALS SHEET 42				6.66		8																									
	SUBTOTALS SHEET 43				5.02		8																									
					3.76		8																									
					3.93																											

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S "SIGN POST DESIGN GUIDELINE."

PROJECT TOTALS	m ²	m ²	EA.	m ²		m	kg	kg	kg	EA.	kg	EA.	EA.	kg
	19.12	18.35	34	32		166.40	142.6	142.6	0					

PROJECT : DOVER - STRATTON PROJECT NO. : STP 2214(I) S
 DESIGN FILE NAME: /pave/99bl80/pbl80.dgn PLOT DATE: 24-MAY-2007
 IPARM FILE NAME: pbl80ss04.i SURVEYED BY: CLD_ENGINEERS_INC SURVEY DATE: 05/00
 SQUAD LEADER: WRH DRAWN BY: MPS SHEET: 43 OF 49



SEE STD. E-150 FOR
COLORS AND MATERIALS
DIMENSIONS ARE THE SAME FOR MIRRORED SIGNS

SIGN	DIMENSIONS			
	A	B	C	D
STD.	85	110	100	50

TEXT LAYOUT DIMENSIONS ARE BASED ON THE "LETTER & NUMERAL WIDTHS AND SPACE" TABLES FOUND IN THE "STANDARD HIGHWAY SIGNS" BOOKLET. MINOR VARIATIONS IN TEXT DIMENSIONS ARE ACCEPTABLE BASED ON INDIVIDUAL MANUFACTURER'S LETTER FABRICATION. SIGNIFICANT CHANGES WHICH AFFECT SIGN APPEARANCE SHALL BE BROUGHT TO THE ATTENTION OF THE VTRAN'S TRAFFIC AND SAFETY DIVISION BEFORE FABRICATION.

NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

SIGN DETAIL SHEET	PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(1)S
	DESIGN FILE NAME: /pave/99b180/pb180.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pb180sd.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD_ENGINEERS_INC	DRAWN BY: HJC
	SQUAD LEADER: WRH	SHEET: 44 OF 49

BRIDGE QUANTITY SUMMARY

STATION	STATION	POS.	BRIDGE NO.	OFFSET BLOCK	525.10 REMOVAL OF EXIST. RAILING	525.40 H.D.S.B. CURB MTD. (MOD 1)	525.40 H.D.S.B. CURB MTD. (MOD 2)	525.40 H.D.S.B. CURB MTD. (MOD 3)	525.41 H.D.S.B. FASCIA MTD.	525.41 H.D.S.B. FASCIA MTD. (MOD 2)	525.41 H.D.S.B. FASCIA MTD. (MOD 3)	529.25 REMOVAL OF CONC. OR MASONRY	REMARKS
DOVER													
1+378	1+405	RT	56	150	27				26.6				SEE BRIDGE DETAIL SHEET 2
1+387	1+414	LT	56	150	27				27				SEE BRIDGE DETAIL SHEET 2
									26.6				
2+578	2+597	LT	58	150	19			19.1					SEE BRIDGE DETAIL SHEET 3
2+594	2+613	RT	58	150	19			19.1					SEE BRIDGE DETAIL SHEET 3
3+428	3+439	RT	59	150	11				11.4				SEE BRIDGE DETAIL SHEET 4
3+436	3+447	LT	59	150	11				11.4				SEE BRIDGE DETAIL SHEET 4
SUBTOTAL					114			38.2	76.0				
ROUNDING					0			0	0				
TOTALS					114			38.2	76.0				

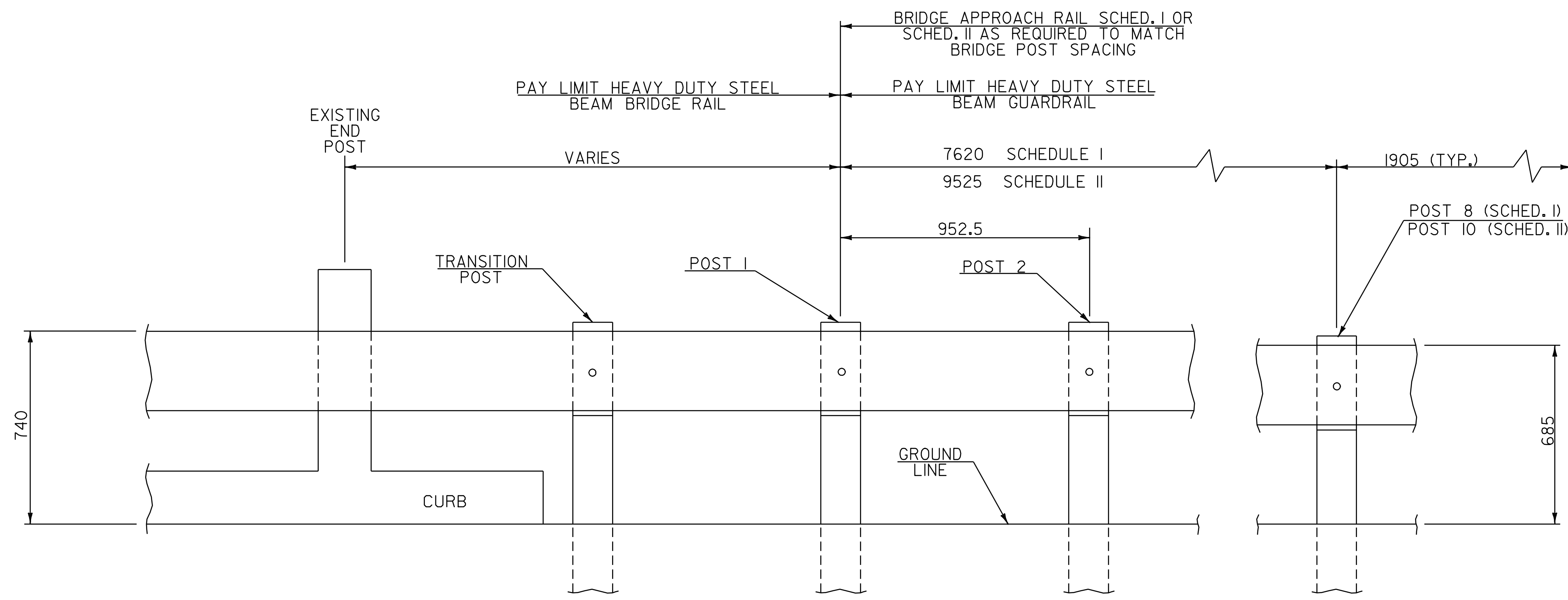
NOTES

- BRIDGE RAIL SHALL BE HEAVY DUTY STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 7.62 METERS.
- APPROACH RAILING SHALL BE HEAVY DUTY STEEL BEAM FOR 7.62 METERS FOR SCHEDULE I OR 9.525 METERS FOR SCHEDULE II FROM THE ENDS OF THE BRIDGE.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICAL TO THE MID-POINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST 1.
- SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.
- SEE STANDARD SHEET G-1M FOR DELINEATION DETAILS AND PLACEMENT.
- ERECT DELINEATORS ON EVERY FIFTH POST OR APPROXIMATELY 9 METERS APART PAYMENT SHALL BE SUBSIDIARY TO THE GUARDRAIL ITEMS IN THE CONTRACT.
- AN ESTIMATED QUANTITY OF ITEM 501.22 CONCRETE CLASS A AND ITEM 507.15 REINFORCING STEEL HAVE BEEN ADDED TO REPAIR BRIDGE DAMAGE CAUSED BY REMOVING EXISTING BRIDGE RAIL.

ITEM 501.22 CONCRETE CLASS A 1 M3 (EST) NOT USED ON PROJECT
 ITEM 507.15 REINFORCING STEEL 100 KG (EST) NOT USED ON PROJECT

BRIDGE APPROACH RAILING

WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1, USE SCHEDULE I FOR APPROACH RAILING. WHEN A RAIL PANEL SPLICE OCCURS AT BRIDGE END POST USE SCHEDULE II FOR APPROACH RAILING.



BRIDGE APPROACH RAILING

NOT TO SCALE

POST NO.	SPACING	PAYMENT FACTOR
1	952.5	1.4 x 3810
2	952.5	
3	952.5	
4	952.5	
5	952.5	
6	1270	1.2 x 3810
7	1270	
8	1270	
9	1905 (TYP.)	1.0 (TYP.)

POST NO.	SPACING	PAYMENT FACTOR
1	952.5	1.4 x 5715
2	952.5	
3	952.5	
4	952.5	
5	952.5	
6	952.5	
7	952.5	
8	1270	1.2 x 3810
9	1270	
10	1270	
11	1905 (TYP.)	1.0 (TYP.)

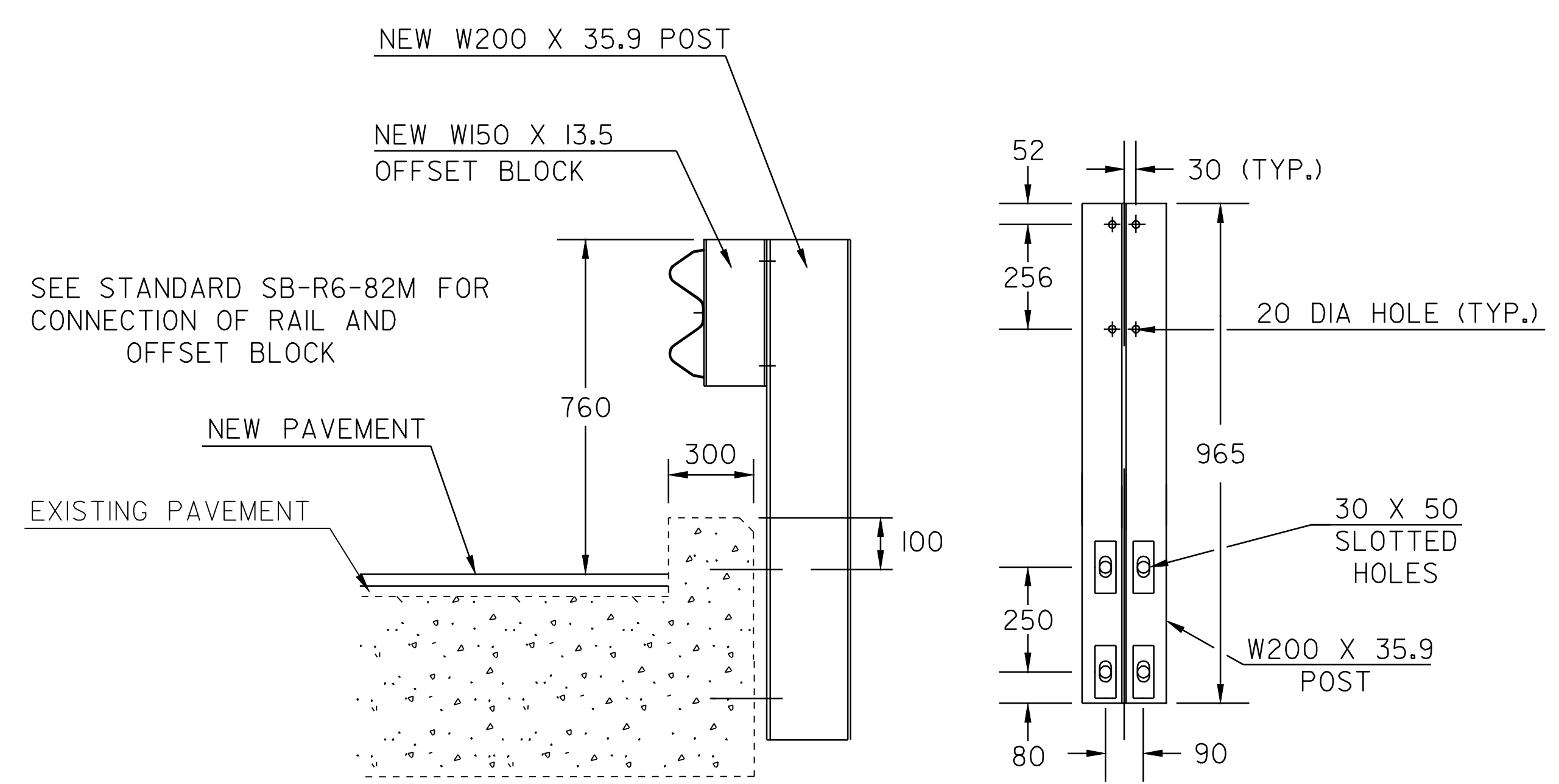
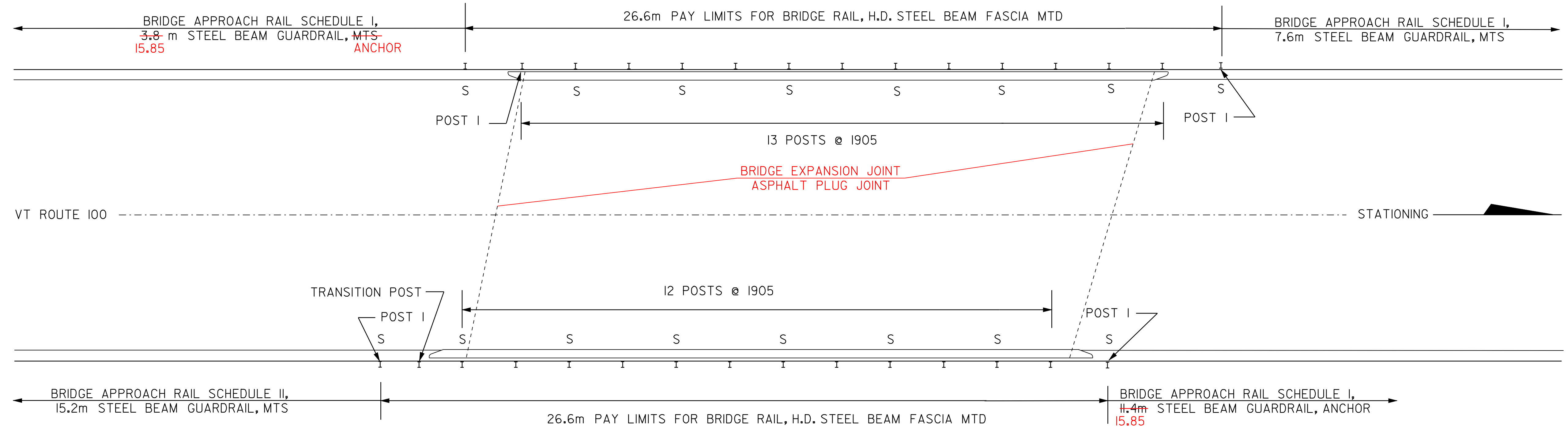
NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

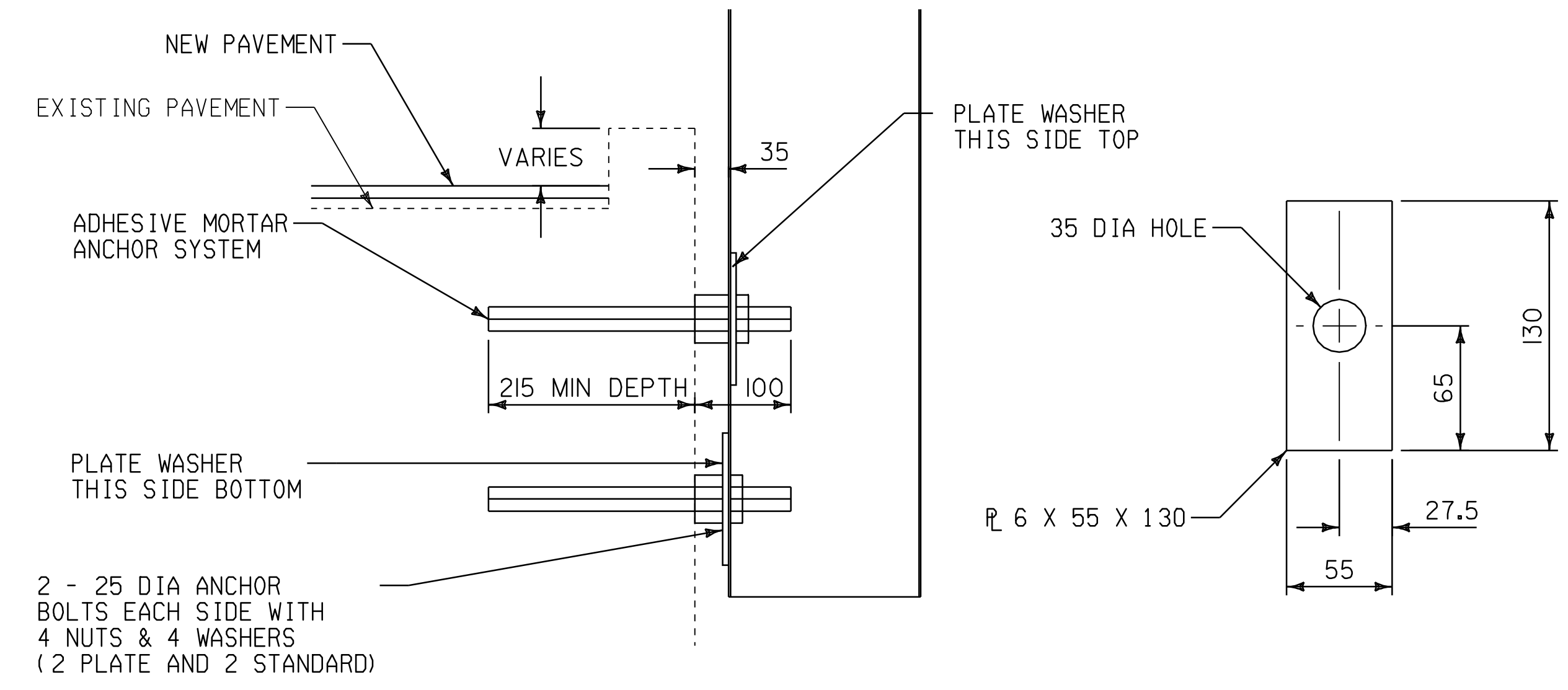
BRIDGE DETAIL SHEET #1	PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99b180/pb180.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pb180br01.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 45 OF 49

NOT TO SCALE

**BRIDGE #56
DOVER
STA 1+399.03
(MM 0.869)**



FASCIA MOUNTED STEEL POST DETAIL
NOT TO SCALE



ANCHORAGE DETAIL
NOT TO SCALE

PLATE WASHER DETAIL
NOT TO SCALE

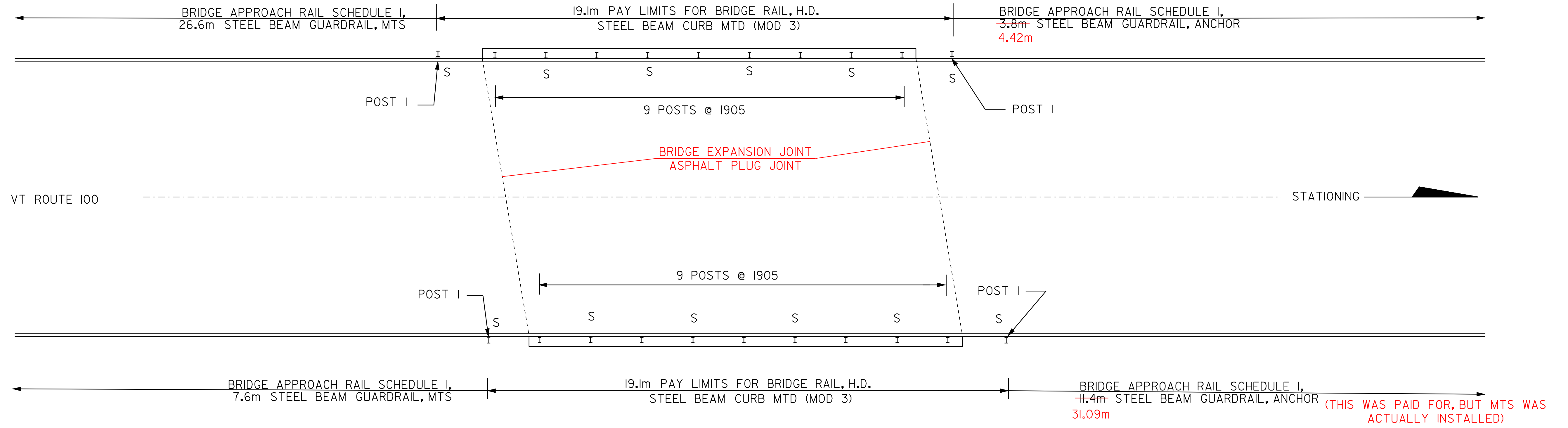
NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

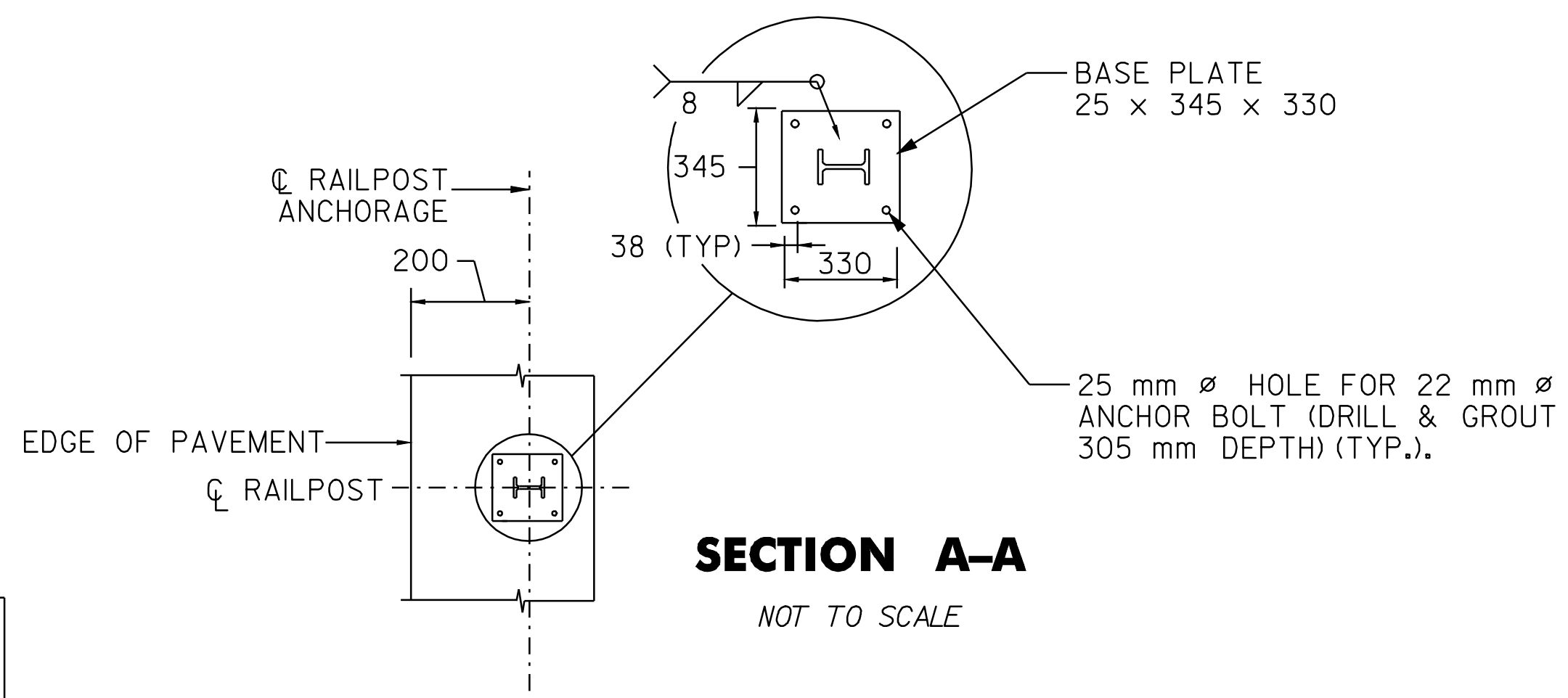
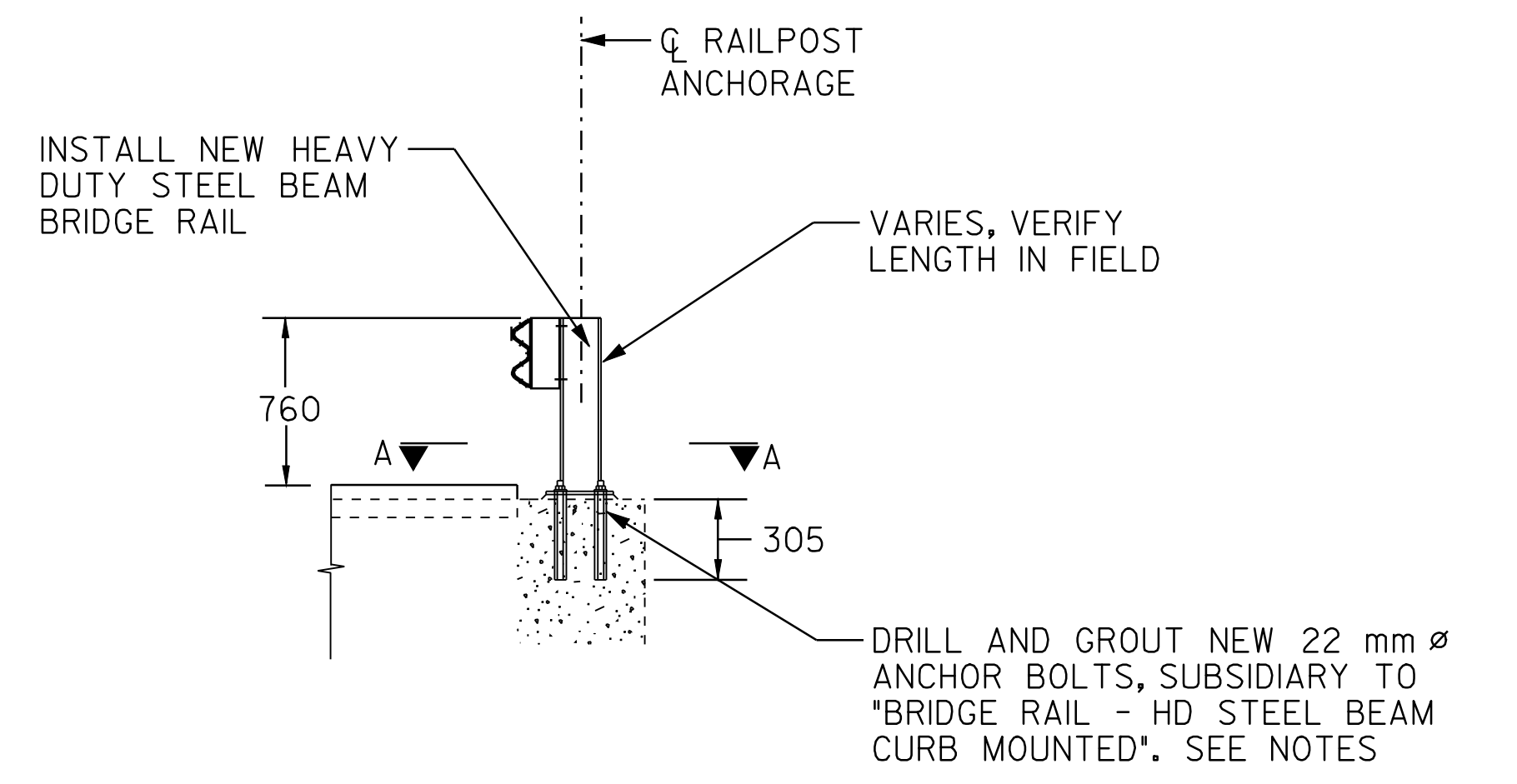
**BRIDGE
DETAIL
SHEET #2**

PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80br02.i	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 46 OF 49

NOT TO SCALE



**BRIDGE #58
DOVER
STA 2+594.46
(MM 1.612)**



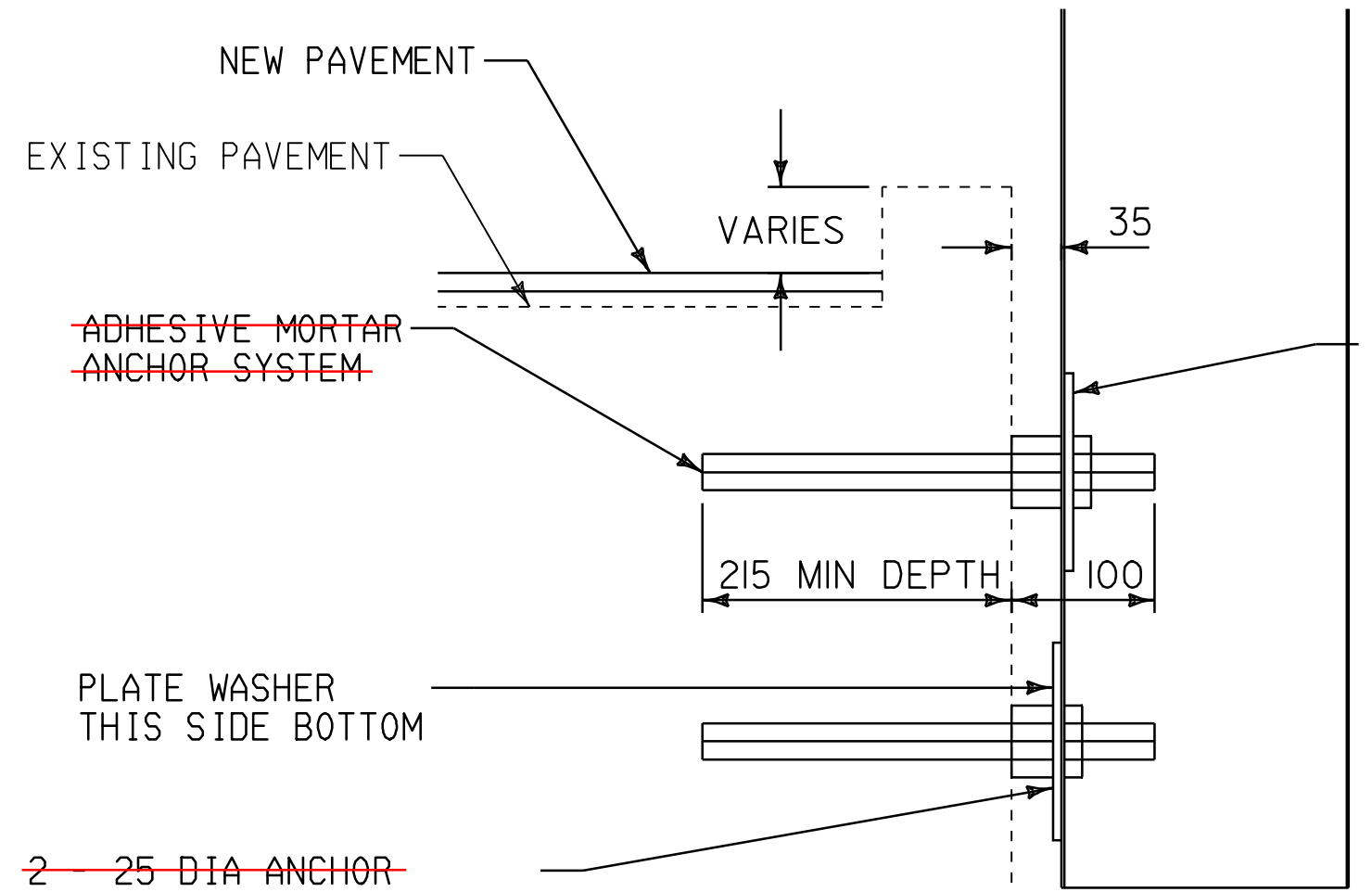
NOTES

1. NEW BRIDGE RAILING POSTS SHALL BE SET NORMAL TO GRADE.
2. ALL WELDING SHALL CONFORM TO THE PROVISIONS OF SUBSECTION 506.10.
3. NEW ASTM A-449-M 22 mm DIAMETER ANCHOR BOLTS TO BE GROUTED IN PLACE SHALL BE FURNISHED WITH TWO NUTS AND ONE WASHER. BOLTS, NUTS AND WASHERS SHALL BE FURNISHED UNDER "HEAVY DUTY STEEL BEAM BRIDGE RAIL."
4. GROUT NEW BOLTS WITH A TWO COMPONENT ADHESIVE MORTAR. DRILLING AND GROUTING NEW ANCHOR BOLTS SHALL BE SUBSIDIARY TO "HEAVY DUTY STEEL BEAM BRIDGE RAIL".
5. A MINIMUM PULLOUT STRENGTH OF 13 600 kg SHALL BE ATTAINED ON ALL NEW BOLTS THAT ARE GROUTED IN PLACE. A SAMPLE GROUTED BOLT WILL BE TESTED BEFORE MATERIALS ARE APPROVED FOR USE, AND THEN RANDOM BOLTS WILL BE FIELD TESTED BY VTRANS TO ENSURE THIS STRENGTH IS BEING ATTAINED. 48-HOURS ADVANCE NOTICE IS REQUIRED TO ALERT VTRANS AS TO WHEN THE CONTRACTOR WILL BE READY FOR THIS TESTING.

NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

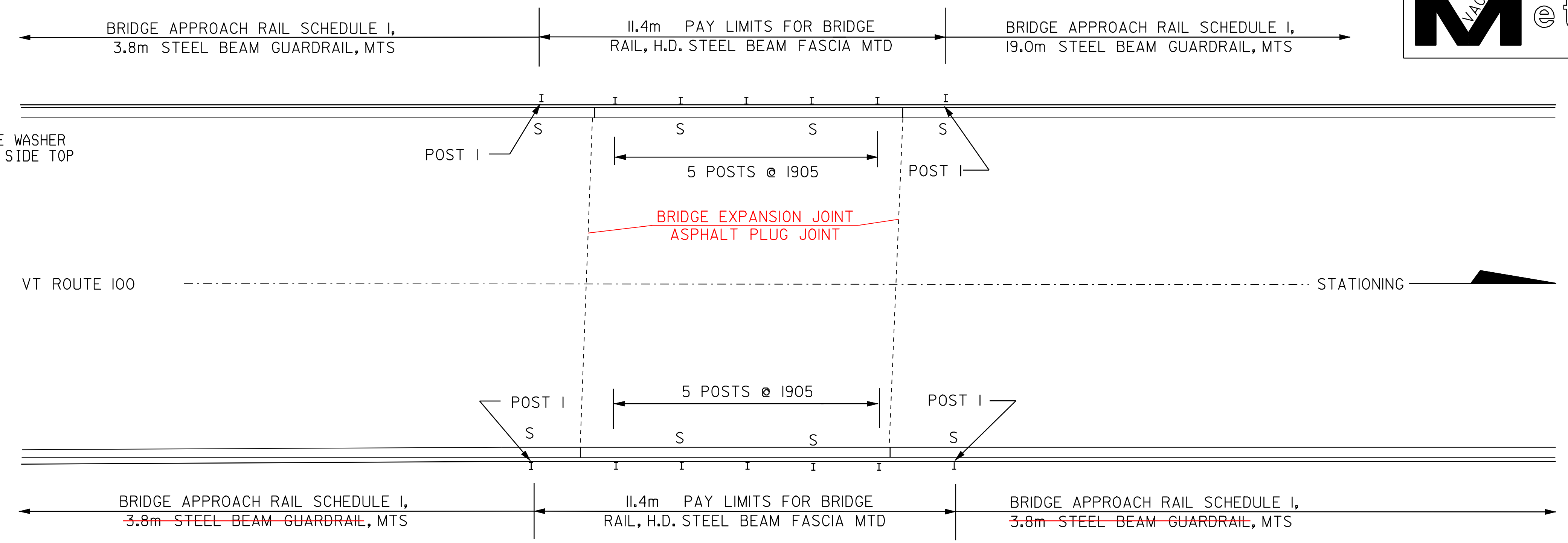
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

BRIDGE DETAIL SHEET #3	PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
	IPARM FILE NAME: pbl80br03.i	SURVEY DATE: 5/00
	SURVEYED BY: CLD ENGINEERS INC	DRAWN BY: MPS
	SQUAD LEADER: WRH	SHEET: 47 OF 49



ANCHORAGE DETAIL

~~2 - 25 DIA ANCHOR BOLTS EACH SIDE WITH 4 NUTS & 4 WASHERS (2 PLATE AND 2 STANDARD) NOT DONE~~



**BRIDGE #59
DOVER
STA 3+436.92
(MM 2.136)**

NOT TO SCALE

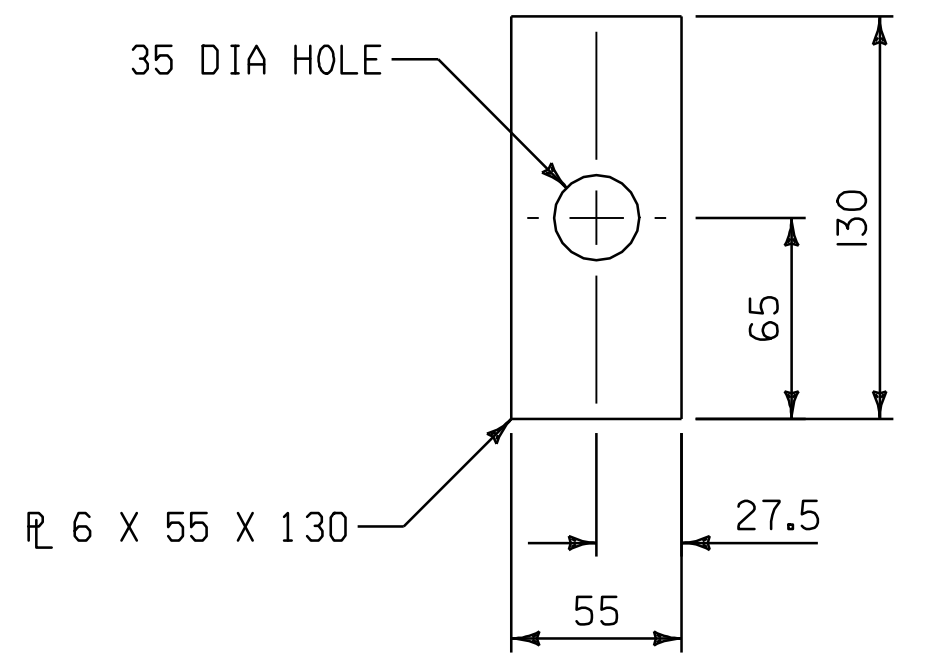
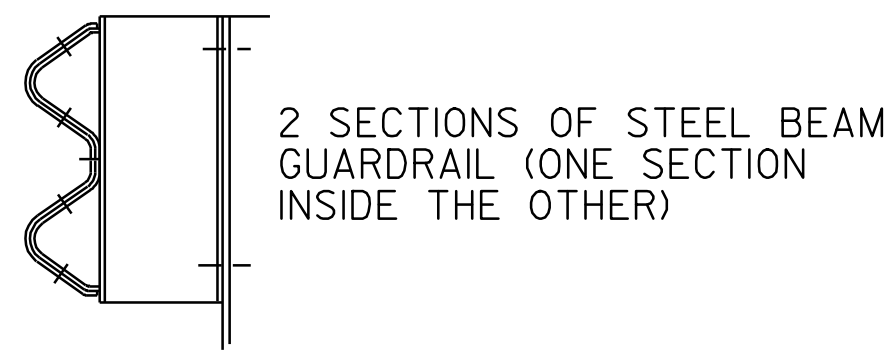


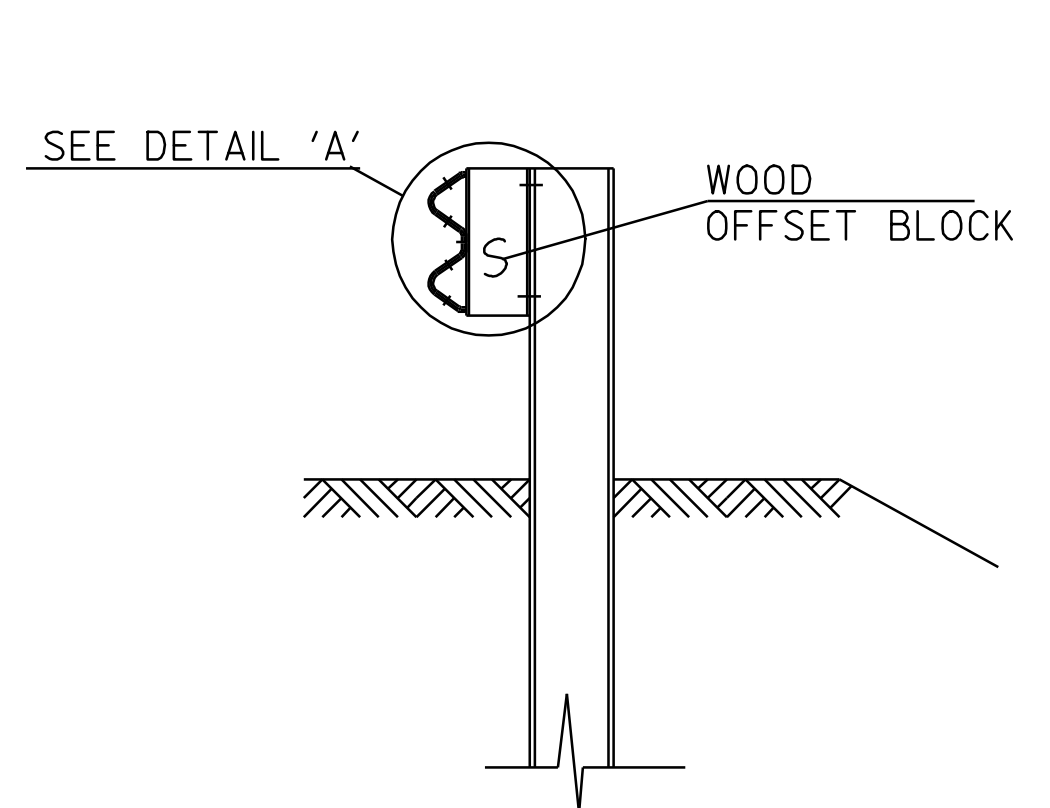
PLATE WASHER DETAIL



DETAIL A

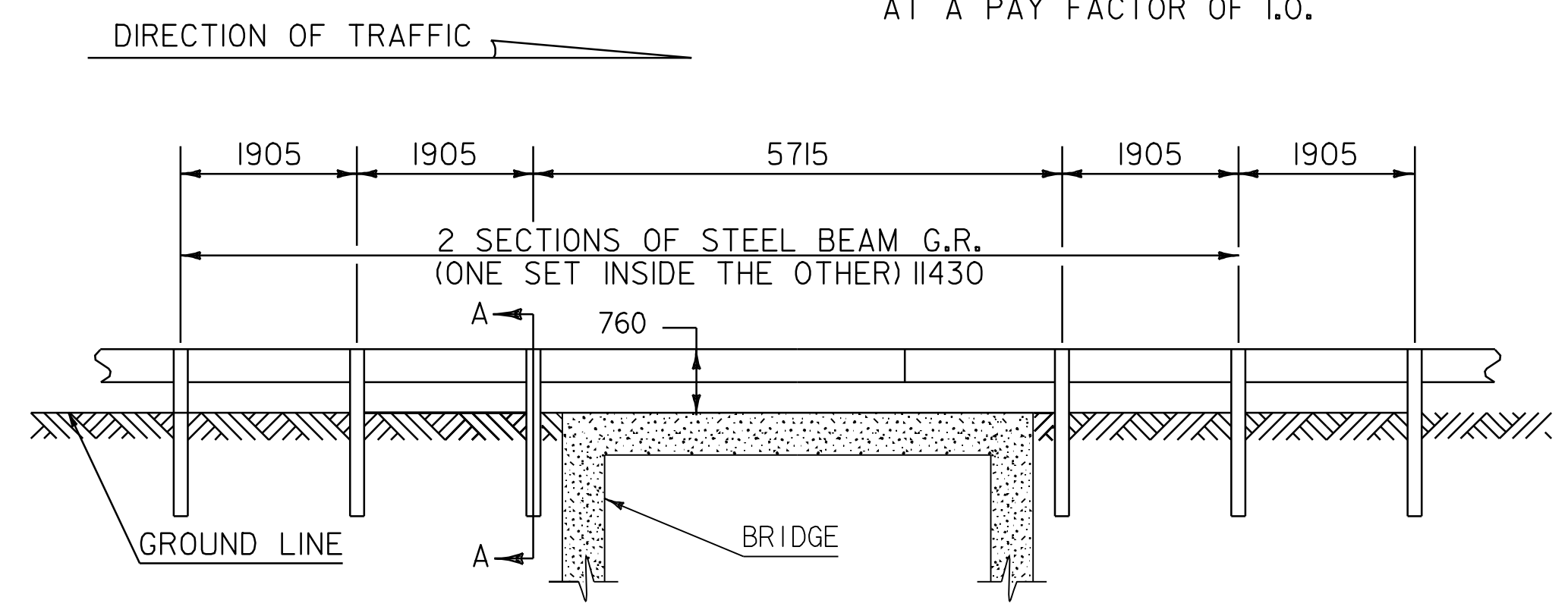
NOTES

1. SEE STANDARD G-IM & G-IDM FOR STEEL BEAM GUARDRAIL DETAILS.
2. THIS DETAIL TO BE USED AS INDICATED ON THE ITEM DETAIL SHEET OR AS DIRECTED BY THE ENGINEER.
3. THIS WORK SHALL BE PAID FOR AS STEEL BEAM GUARDRAIL AT A PAY FACTOR OF 1.0.



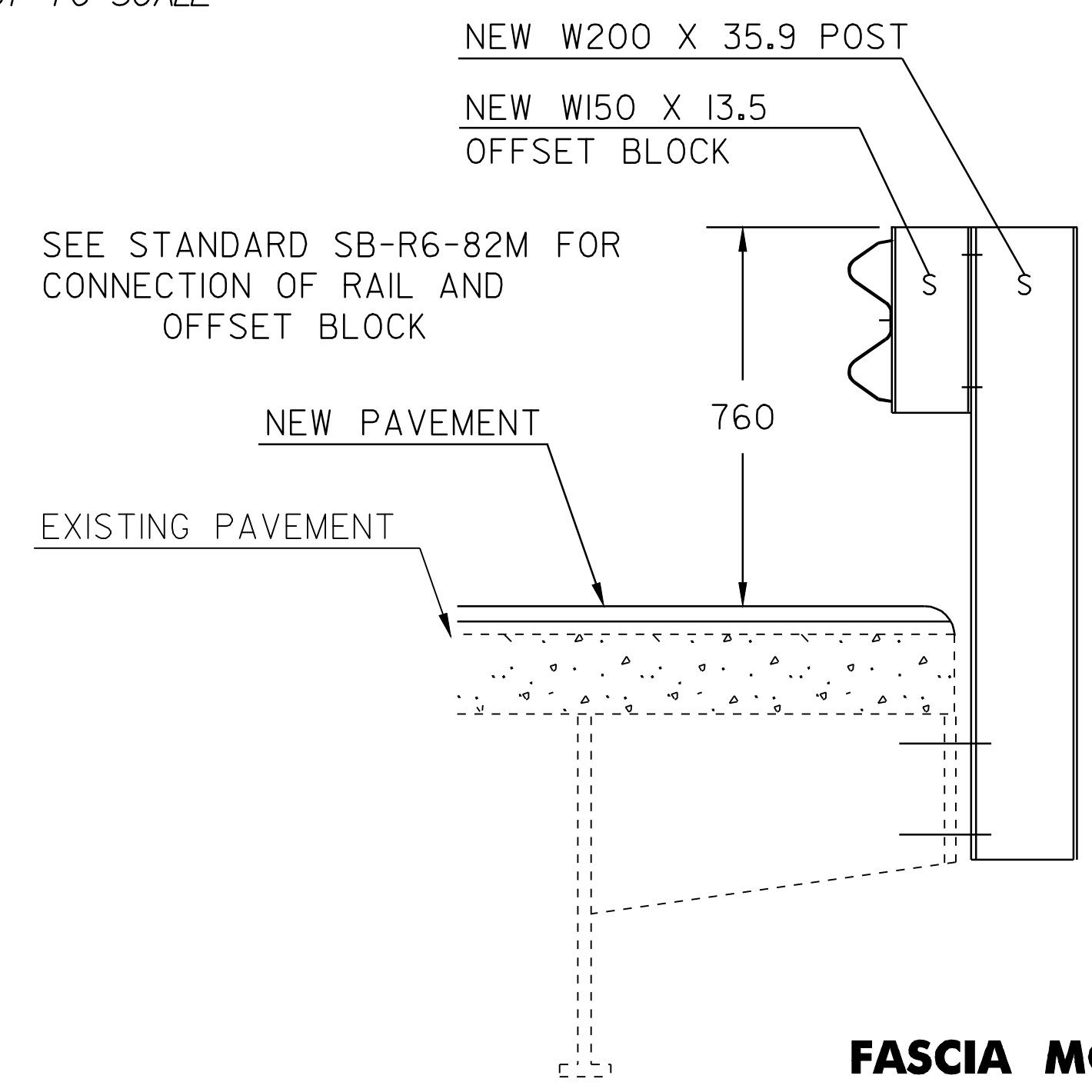
SECTION A-A

NOT TO SCALE

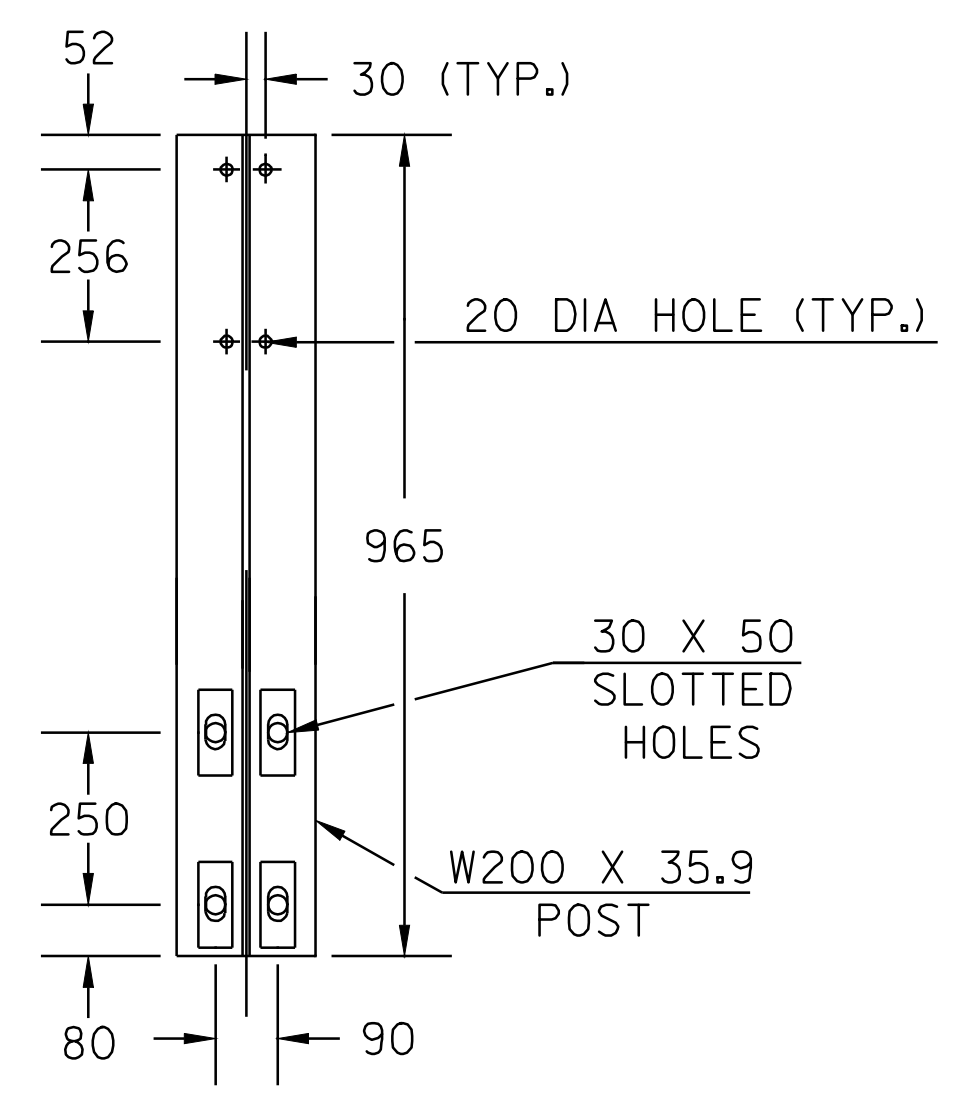


**DETAIL OF STEEL BEAM GUARDRAIL AT
BRIDGE #61 @ STA 8+397.24, DOVER (MM 5.218)
BRIDGE #62 @ STA 8+500.57, DOVER (MM 5.282)**

NOT TO SCALE



FASCIA MOUNTED STEEL POST DETAIL
NOT TO SCALE



NOTE: ALL DIMENSIONS IN MILLIMETERS EXCEPT AS INDICATED

**BRIDGE
DETAIL
SHEET #4**

PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	PLOT DATE: 24-MAY-2007
IPARM FILE NAME: pbl80br04.i	SURVEY DATE: 5/00
SURVEYED BY: CLD ENGINEERS INC	DRAWN BY: MPS
SQUAD LEADER: WRH	SHEET: 48 OF 49

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

CONSTRUCTION APPROACH SIGNING

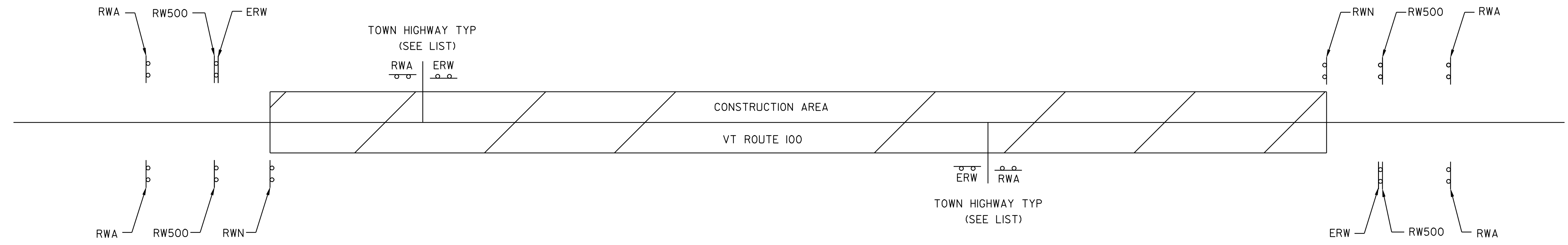
NOT TO SCALE

SEE STD E-100M FOR SIGN PLACEMENT

NOTE: WHEN DIRECTED BY THE RESIDENT ENGINEER, "NO OUTLET" SIDE ROADS WILL NOT REQUIRE CONSTRUCTION SIGNING

LEGEND

- RWA = ROAD WORK AHEAD
- RW500 = ROAD WORK 500 FT
- RWN = ROAD WORK NEXT 7 MILES
- ERW = END ROAD WORK



LIST OF TOWN/STATE HIGHWAYS FOR CONSTRUCTION SIGNS

TOWN/STATE HIGHWAY NAME	ROAD WORK AHEAD	END ROAD WORK	ROAD WORK 500 FT	ROAD WORK NEXT 7 MILES	SIDE ROAD WORK AHEAD	SIDE ROAD WORK 500 FT	OTHER
DOVER							
TH #11	1	1					
TH #38	1	1					
SA #4	1	1					
TH #35	1	1					
TH #9	1	1					
TH #15	1	1					
TH #14	1	1					
TH #9	1	1					
TH #8	1	1					
TH #66	1	1					
TH #66	1	1					
SA #3	1	1					
TH #45	1	1					
TH #45	1	1					
SA #3	1	1					
SKIAREA RD.	1	1					
TH #53	1	1					
TH #50	1	1					
TH #13	1	1					
TH #8	1	1					
TH #33	1	1					
STRATTON							
TH #20	1	1					
BEGINNING OF PROJECT	2	1	2	1			
END OF PROJECT	2	1	2	1			
TOTALS	26	24	4	2			

NOTE: ALL DIMENSIONS IN METERS EXCEPT AS INDICATED

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

CONSTRUCTION APPROACH SIGNING	PROJECT: DOVER - STRATTON	PROJECT NO.: STP 2214(I)S
	DESIGN FILE NAME: /pave/99bl80/pbl80.dgn	
	IPARM FILE NAME: pbl80cs.1	PLOT DATE: 24-MAY-2007
	SURVEYED BY: CLD ENGINEERS INC	SURVEY DATE: 5/00
	SQUAD LEADER: WRH	DRAWN BY: MPS
		SHEET: 49 OF 49