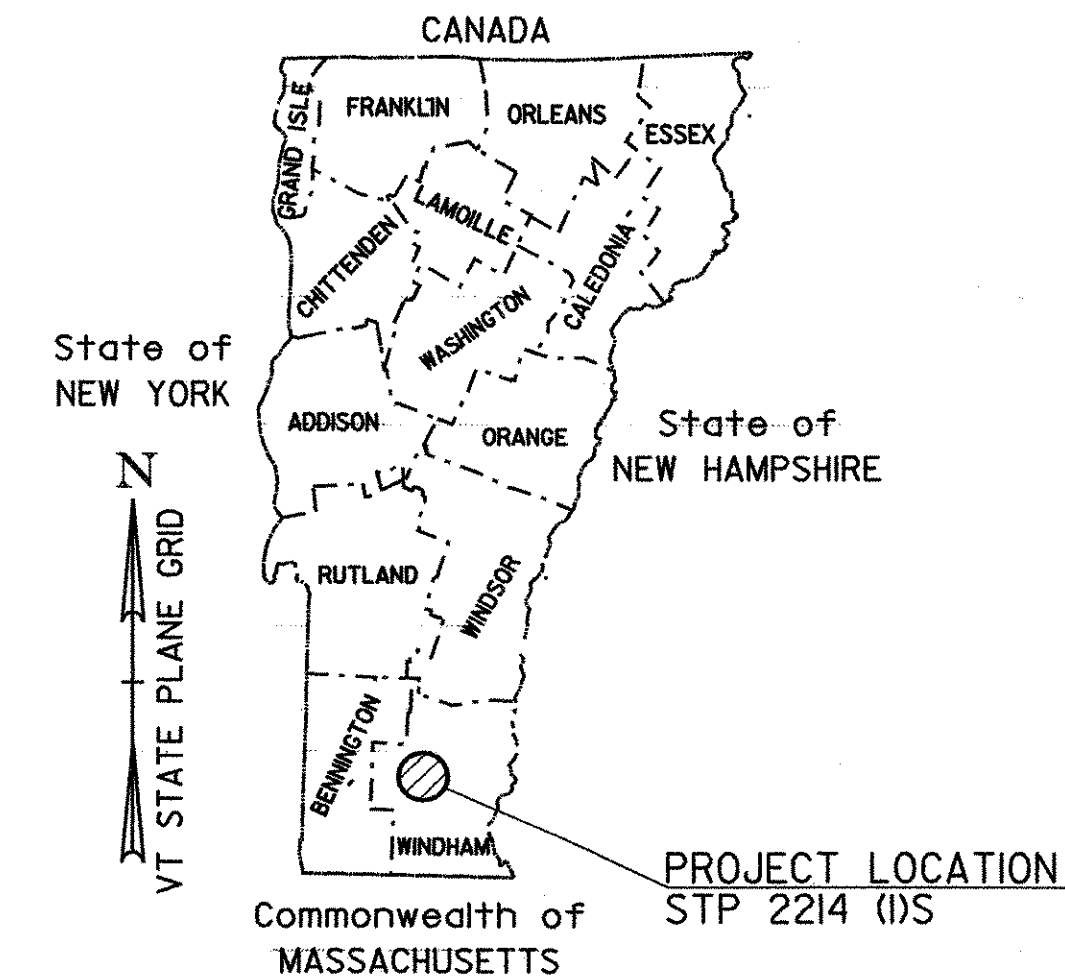
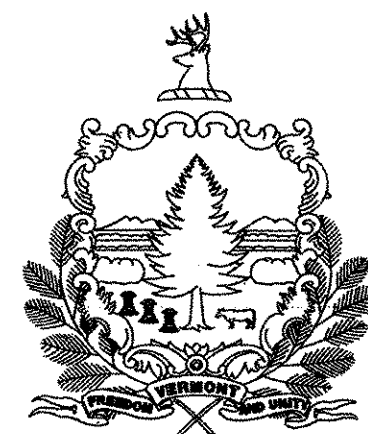


STATE OF VERMONT AGENCY OF TRANSPORTATION



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SB-R6-82M	BRIDGE RAILING- HD STEEL BEAM - (TYPE A, B, C, D)	07/10/97

PROPOSED IMPROVEMENT TOWNS OF DOVER & STRATTON COUNTY OF WINDHAM VT ROUTE 100

BEGINNING IN DOVER ON VT ROUTE 100 AT THE WILMINGTON/DOVER TOWN LINE, STATION 0+000.00 (MM 0.000) AND EXTENDING NORTHERLY ALONG VT ROUTE 100 A DISTANCE OF 11078.57 METERS (6.884 MILES) TO THE STRATTON/WARDSBORO TOWNLINE, STATION 2+151.89 (MM 1.337).

PROJECT DATA

FROM STA 0+000.00 (MM 0.000) TO STA 2+151.89 (MM 1.337)
LENGTH OF ROADWAY = 11 078.57 METERS (6.884 MILES)
LENGTH OF PROJECT = 11 078.57 METERS (6.884 MILES)

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES RECLAIMING AND RESURFACING OF THE EXISTING HIGHWAY WITH A BASE COURSE, WEARING COURSE, NEW PAVEMENT MARKINGS, GUARDRAIL, SIGNS AND INCIDENTAL ITEMS AS SHOWN IN THE PROJECT QUANTITIES.

TRAFFIC DATA

DOVER MM 0.00 TO MM 2.16			
2002	ADT	=	7000
2002	DHV	=	1300
2012	ADT	=	8200
2012	DHV	=	1500
2002-2012 ESALS = 700 000			
DOVER MM 2.16 TO MM 3.59			
2002	ADT	=	5900
2002	DHV	=	1100
2012	ADT	=	6900
2012	DHV	=	1300
2002-2012 ESALS = 561 000			
DOVER MM 3.59 TO MM 1.34 STRATTON			
2002	ADT	=	1600
2002	DHV	=	300
2012	ADT	=	1800
2012	DHV	=	340
2002-2012 ESALS = 254 000			

BITUMINOUS CONCRETE PAVEMENT SUPERPAVE MIXTURE DESIGN CRITERIA	
DESIGN LANE/DESIGN LIFE ESAL	820,500
DESIGN NUMBER OF GYRATIONS	75
PERFORMANCE GRADED ASPHALT BINDER	58-34

RECORD PLANS

CONTRACTOR: FW WHITCOMB CONSTRUCTION CORP. - WALPOLE, NH
RESIDENT ENGINEER: MARK HAUGHWOUT
CONSTRUCTION BEGAN: JULY 12, 2004
CONSTRUCTION COMPLETE: NOVEMBER 5, 2005
RECORD PLANS BY: MARK HAUGHWOUT & JUDY GILMORE

I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.
BY *Mark W. Haughwout* RESIDENT ENGINEER
DATE *3/23/07*

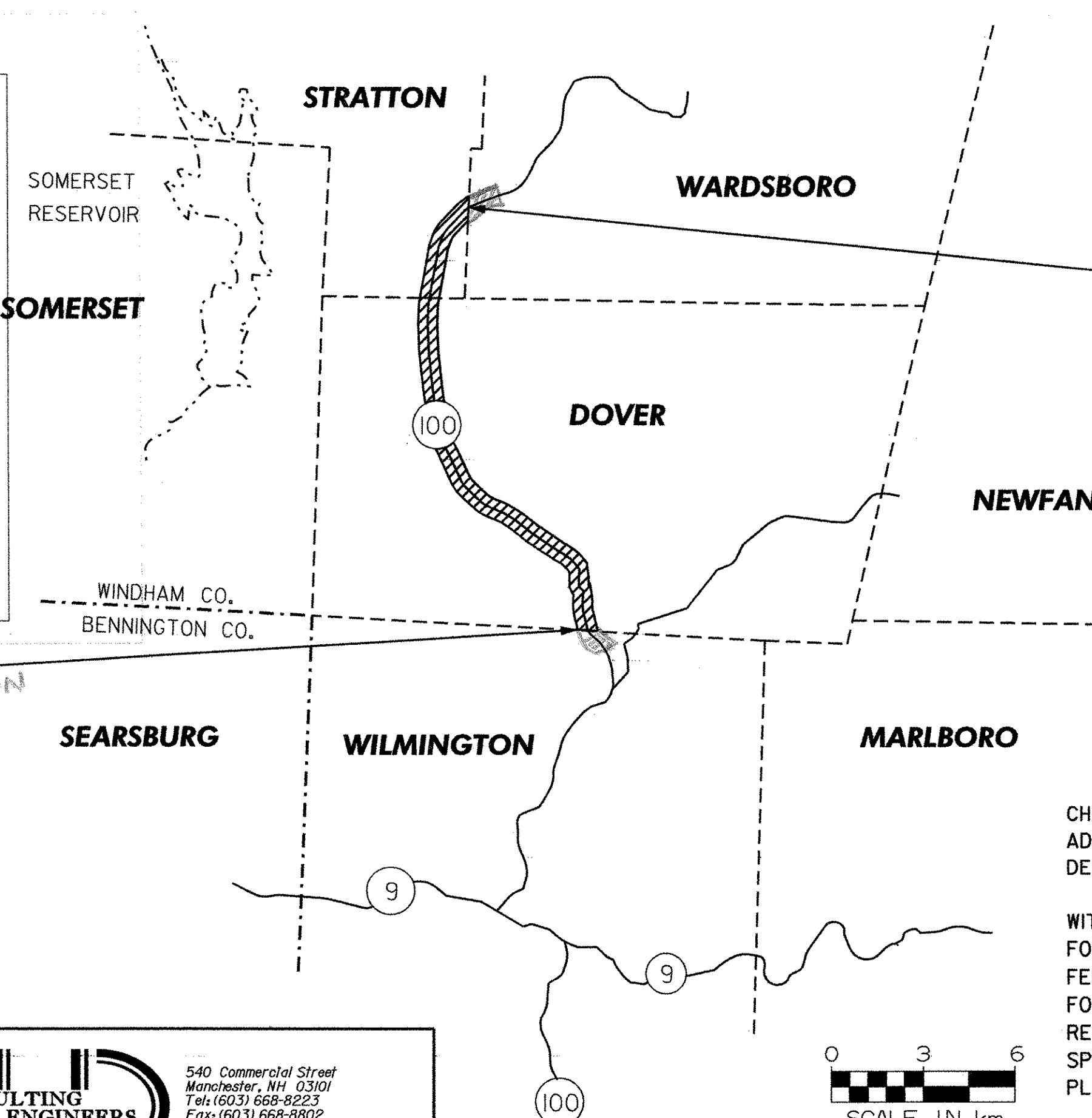
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.

BEGIN STP 2214(1)S
STA 0+000.00 11+000 WILMINGTON
(MM 0.000)

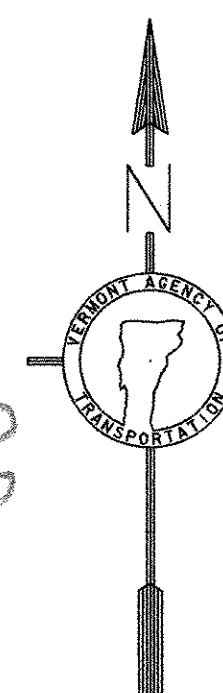
UNLESS OTHERWISE INDICATED, THE DRAWINGS AND DETAILS OF THESE PLANS ARE NOT TO SCALE.

SURVEYED BY :
SURVEYED DATE :
DATUM
VERTICAL N/A
HORIZONTAL N/A

METRIC PLANS
PREPARED BY:



END STP 2214(1)S
STA 2+151.89
(MM 1.337)
0+104.00
WARDSBORO



RIGHT-OF-WAY LIMITS, IF APPLICABLE, ARE PROVIDED SOLELY FOR THE CONVENIENCE OF THE STATE AND ITS CONTRACTOR DURING THE COURSE OF THIS PAVING PROJECT. ANY REFERENCES TO OFFSETS ON THESE PLANS ARE APPROXIMATE AND SHOULD NOT BE RELIED UPON FOR ANY OTHER PURPOSES.

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROJECT DEVELOPMENT.

CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2001, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JANUARY 4, 2001 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

/pave/99b180/pb180.dgn
pb180ts.1



UNLESS NOTED OTHERWISE
STATIONS ARE IN KILOMETERS
ELEVATIONS ARE IN METERS
DIMENSIONS ARE IN MILLIMETERS

DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATOR	
APPROVED _____	DATE _____
DIRECTOR OF PROJECT DEVELOPMENT	
APPROVED <i>[Signature]</i>	DATE <i>3/14/04</i>
PROJECT MANAGER :	
PROJECT NAME : DOVER - STRATTON	
PROJECT NUMBER : AC STP 2214(1)S	
SHEET 1 OF 49 SHEETS	

CONVENTIONAL SYMBOLS

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	