

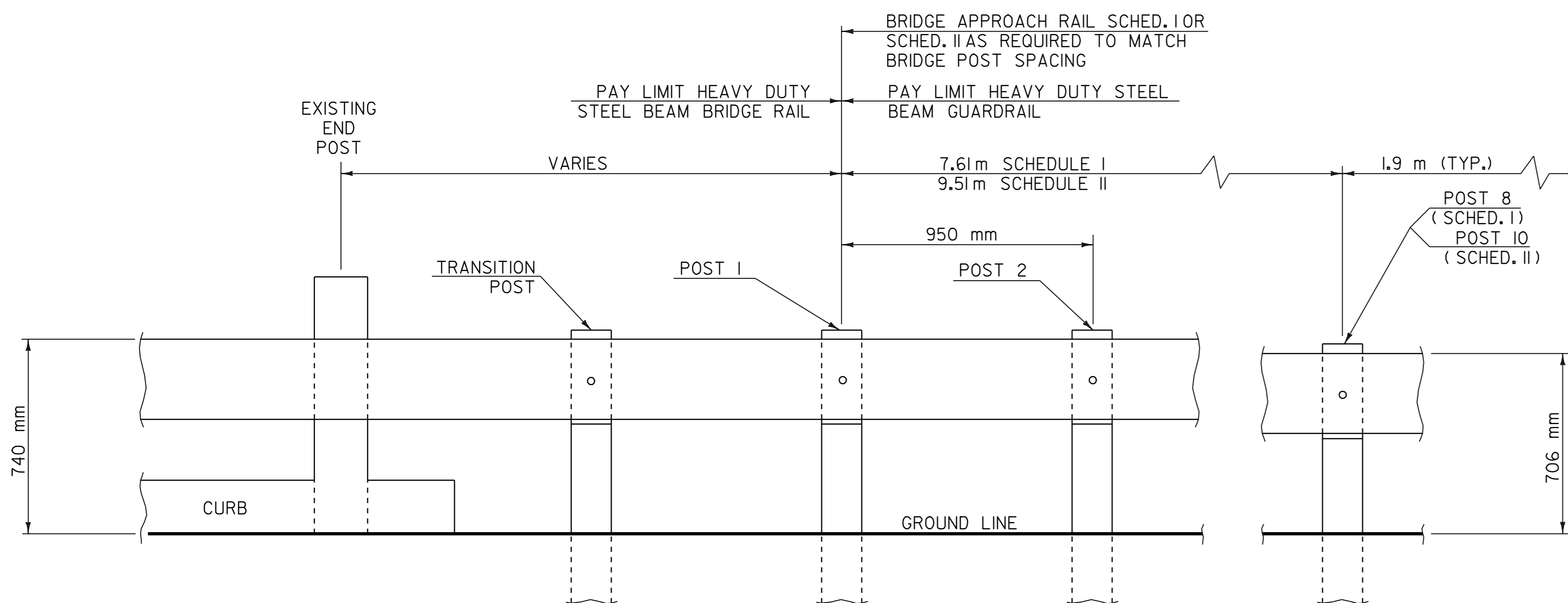
BRIDGE QUANTITY SHEET



STATION	POS.	BRIDGE NUMBER	OFFSET BLOCK	525.10	525.41 (MOD.3)	621.30 (MOD.1)	621.30 (MOD.2)	621.30 (MOD.3)	REMARKS
HANCOCK:									
I+903.3 - I+940.2	LT	I45	--	17.1		120'11" (36.86 m)			
I+903.3 - I+940.2	RT	I45	--	17.1		120'11" (36.86 m)			
GRANVILLE:									
I+529.0 - I+563.4	LT	I48	--	19			112'8" (34.34 m)		
I+531.0 - I+565.4	RT	I48	--	19			112'8" (34.34 m)		
2+819.1 - 2+822.9	LT	I49	200 mm	3.8	3.8				
2+819.1 - 2+822.9	RT	I49	200 mm	3.8	3.8				
3+146.4 - 3+154.0	LT	I50	200 mm	7.6	7.6				
3+147.4 - 3+155.0	RT	I50	200 mm	7.6	7.6				
3+322.2 - 3+343.1	RT	I52	200 mm	20.9	20.9				
3+329.9 - 3+350.8	LT	I52	200 mm	20.9	20.9				
4+386.6 - 4+423.3	LT	I53	--	20.9			120'8" (36.78 m)		
4+391.7 - 4+428.4	RT	I53	--	20.9			120'8" (36.78 m)		
ROUNDING				11.4	5.4	1.28 m	1.32 m	1.44 m	
TOTAL				190	70	75.0 m	70.0 m	75.0 m	

NOTES

- BRIDGE RAIL SHALL BE HEAVY DUTY STEEL BEAM RAIL.
- BRIDGE APPROACH RAIL HEIGHT SHALL BE TRANSITIONED TO NORMAL ROADWAY RAIL HEIGHT IN 7.61 m.
- APPROACH RAILING SHALL BE HEAVY DUTY STEEL BEAM FOR 7.61 m FROM THE ENDS OF THE BRIDGE UNLESS OTHERWISE NOTED IN THE PLANS OR DIRECTED BY THE RESIDENT ENGINEER.
- FOR BRIDGE RAILING, THE TRANSITION POST SHALL HAVE AN OFFSET BLOCK AND BE LOCATED AS CLOSE AS PRACTICABLE TO THE MIDPOINT BETWEEN THE BRIDGE END POST AND APPROACH RAIL POST 1.
- SPLICES SHALL LAP IN DIRECTION OF TRAFFIC FLOW.
- SEE STANDARD SHEET G-1M FOR DELINEATION DETAILS AND PLACEMENT.
- ERECT DELINEATORS ON EVERY FIFTH POST OR APPROXIMATELY 9 m APART. PAYMENT SHALL BE INCIDENTAL TO OTHER ITEMS.
- ALL BRIDGE POSTS, HEAVY DUTY STEEL BEAM BRIDGE RAIL, AND RELATED HARDWARE SHALL BE PAID FOR UNDER ITEM 525.40 BRIDGE RAILING - HD STEEL BEAM/CURB MOUNTED (MOD. 2) OR ITEM 525.41 BRIDGE RAILING - HD STEEL BEAM/FASCIA MOUNTED (MOD. 1), (MOD. 2) OR (MOD. 3) AS DENOTED IN THE PLANS.
- ALL STEEL POSTS, PLATES, OFFSET BLOCKS AND FIXTURES SHALL BE AASHTO M 183/M 183M, UNLESS OTHERWISE NOTED, AND SHALL BE GALVANIZED AFTER FABRICATION TO CONFORM TO AASHTO M III.
- SEE STANDARD SHEET G-1M FOR CONNECTION OF STEEL BEAM TO OFFSET BLOCK AND OFFSET BLOCK TO BRIDGE POST.



BRIDGE APPROACH RAILING

NOT TO SCALE

BRIDGE APPROACH RAILING

WHEN A RAIL PANEL SPLICE OCCURS AT POST NO. 1 USE SCHEDULE I FOR APPROACH RAILING. WHEN A RAIL PANEL SPLICE OCCURS AT THE BRIDGE END POST USE SCHEDULE II FOR APPROACH RAILING.

SCHEDULE I

POST NO.	SPACING	PAYMENT FACTOR
1	950 mm	1.4 x 3.8 m
2	950 mm	
3	950 mm	
4	950 mm	
5	1.27 m	1.2 x 3.8 m
6	1.27 m	
7	1.27 m	
8	1.9 m (TYP.)	1.0 (TYP.)

SCHEDULE II

POST NO.	SPACING	PAYMENT FACTOR
1	950 mm	1.4 x 5.7 m
2	950 mm	
3	950 mm	
4	950 mm	
5	950 mm	
6	1.27 m	1.2 x 3.8 m
7	1.27 m	
8	1.27 m	
9	1.27 m	
10	1.9 m (TYP.)	1.0 (TYP.)

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

BRIDGE DETAIL SHEET 1

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	C.A.K.	DATE	11/00
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/98b180/pbl80.dgn		
IPARM FILE	pbl80b01.i	DATE PLOTTED	21-DEC-2006
PROJ. NAME	ROCHESTER-GRANVILLE		
PROJ. NO.	AC_SIP_212411S		
SHEET	36	OF	49 SHEETS