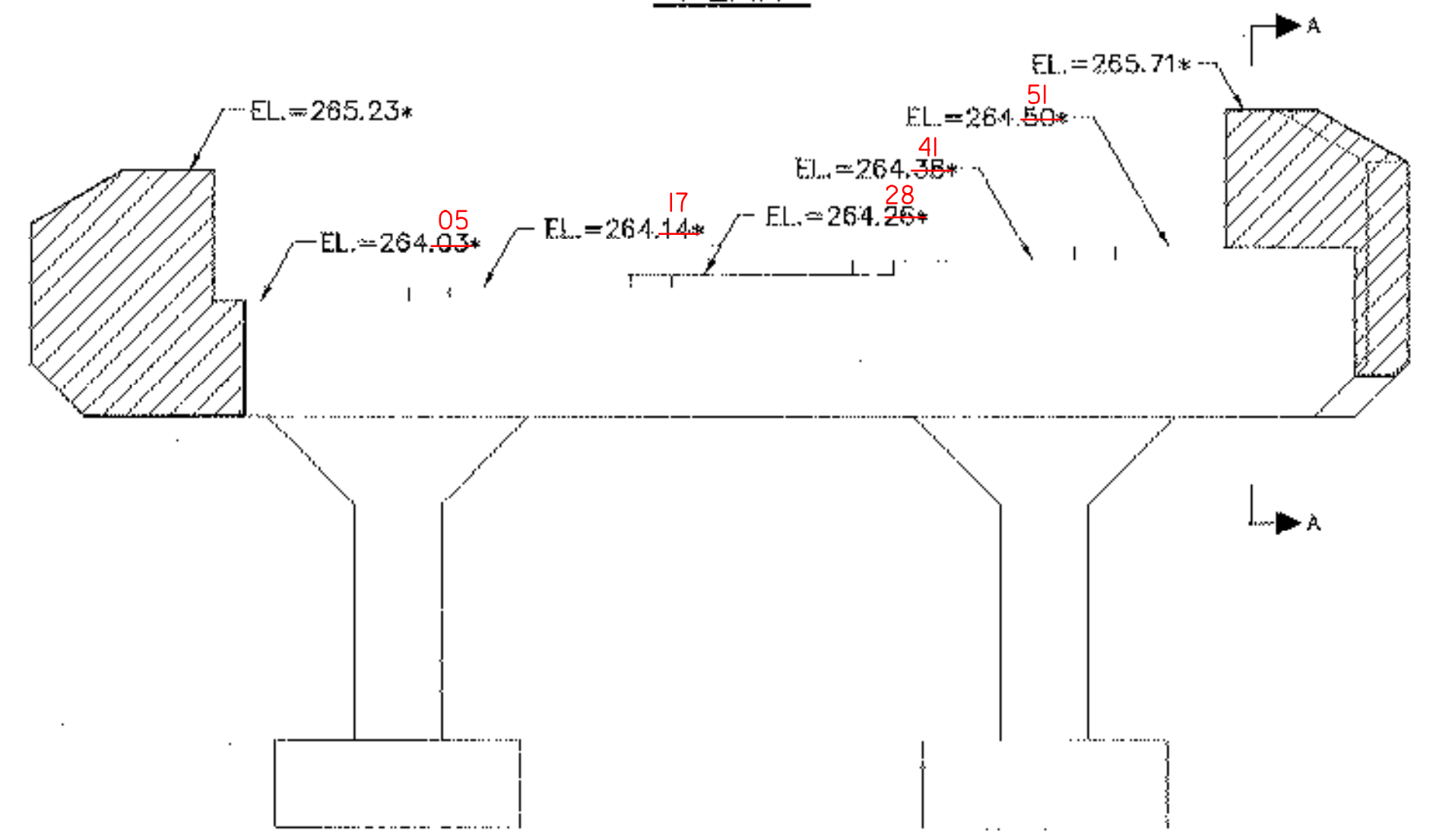
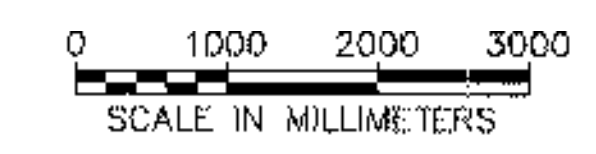


PLAN

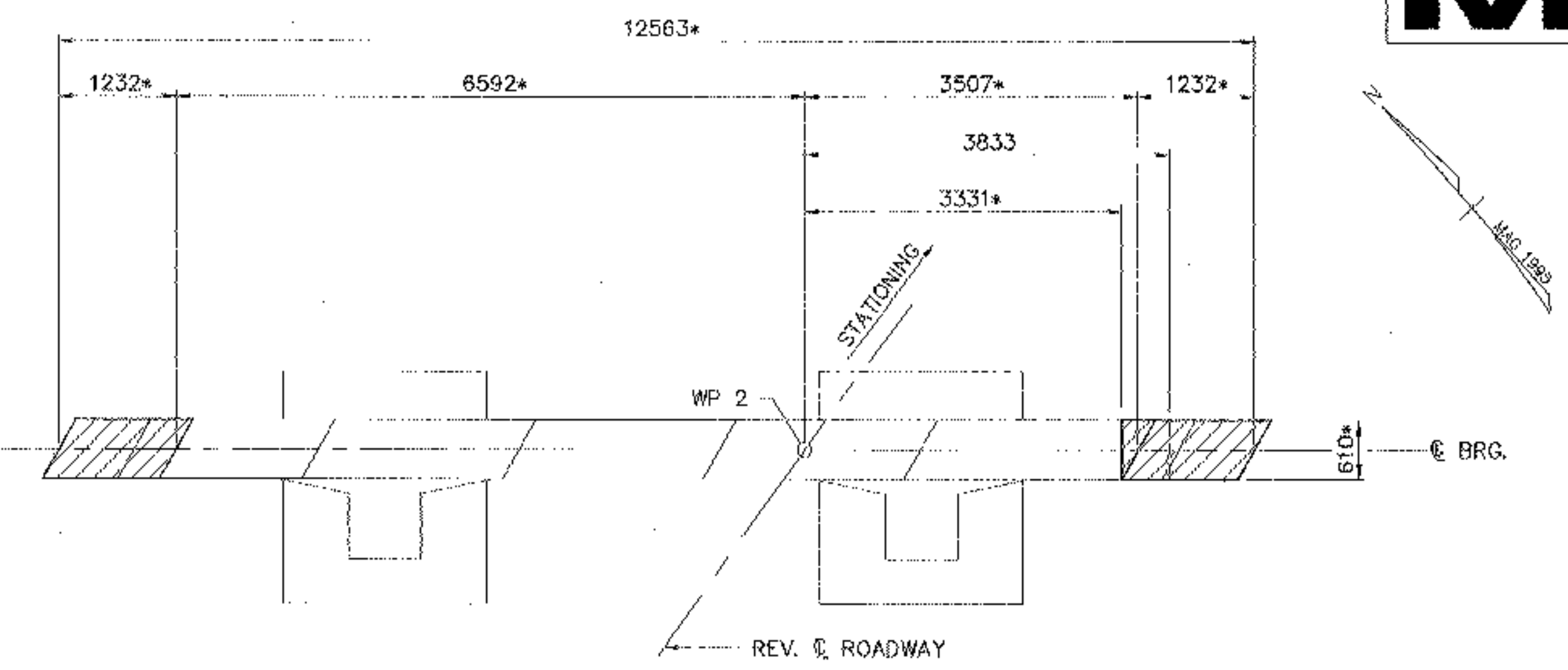


ELEVATION

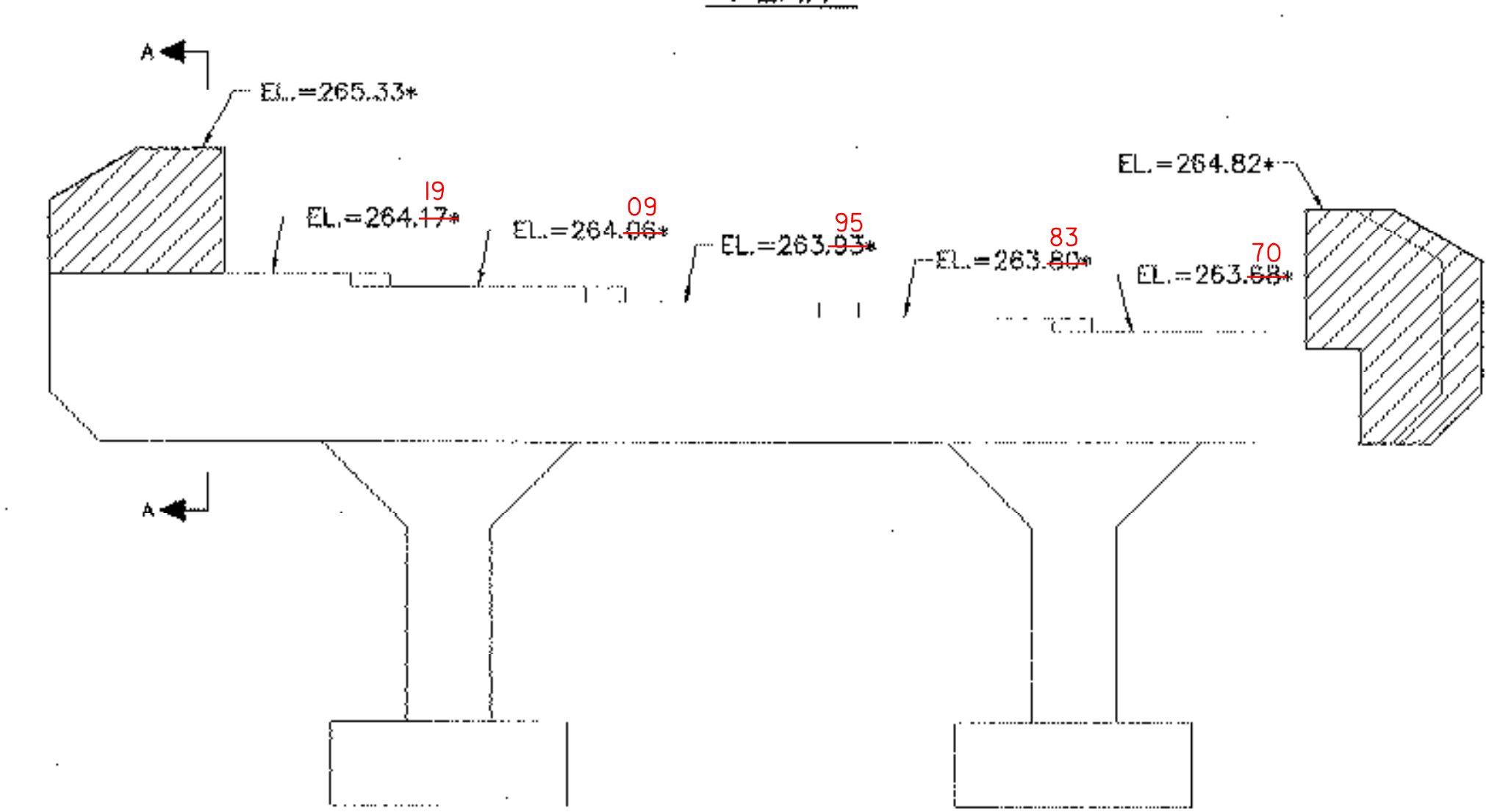
ABUTMENT NO. 1
SCALE: 1:50



DENOTES LIMIT OF REMOVAL
* APPROXIMATE EXISTING ELEVATION OR DIMENSION

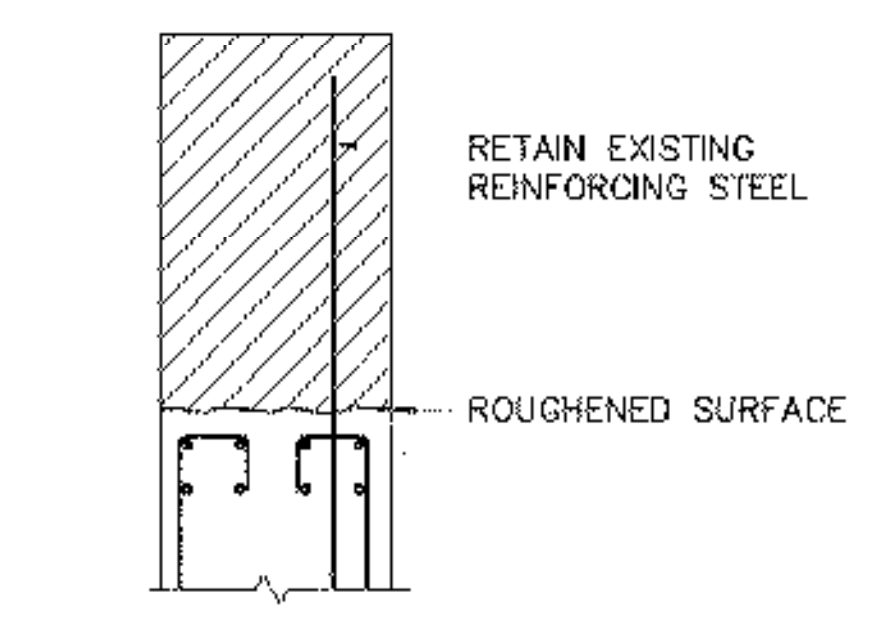


PLAN

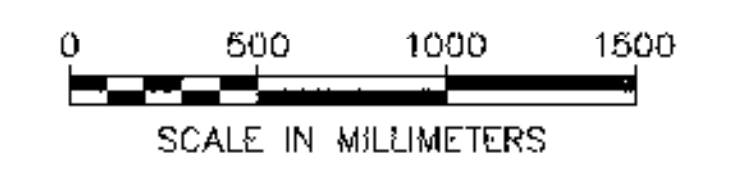


ELEVATION

ABUTMENT NO. 2
SCALE: 1:50

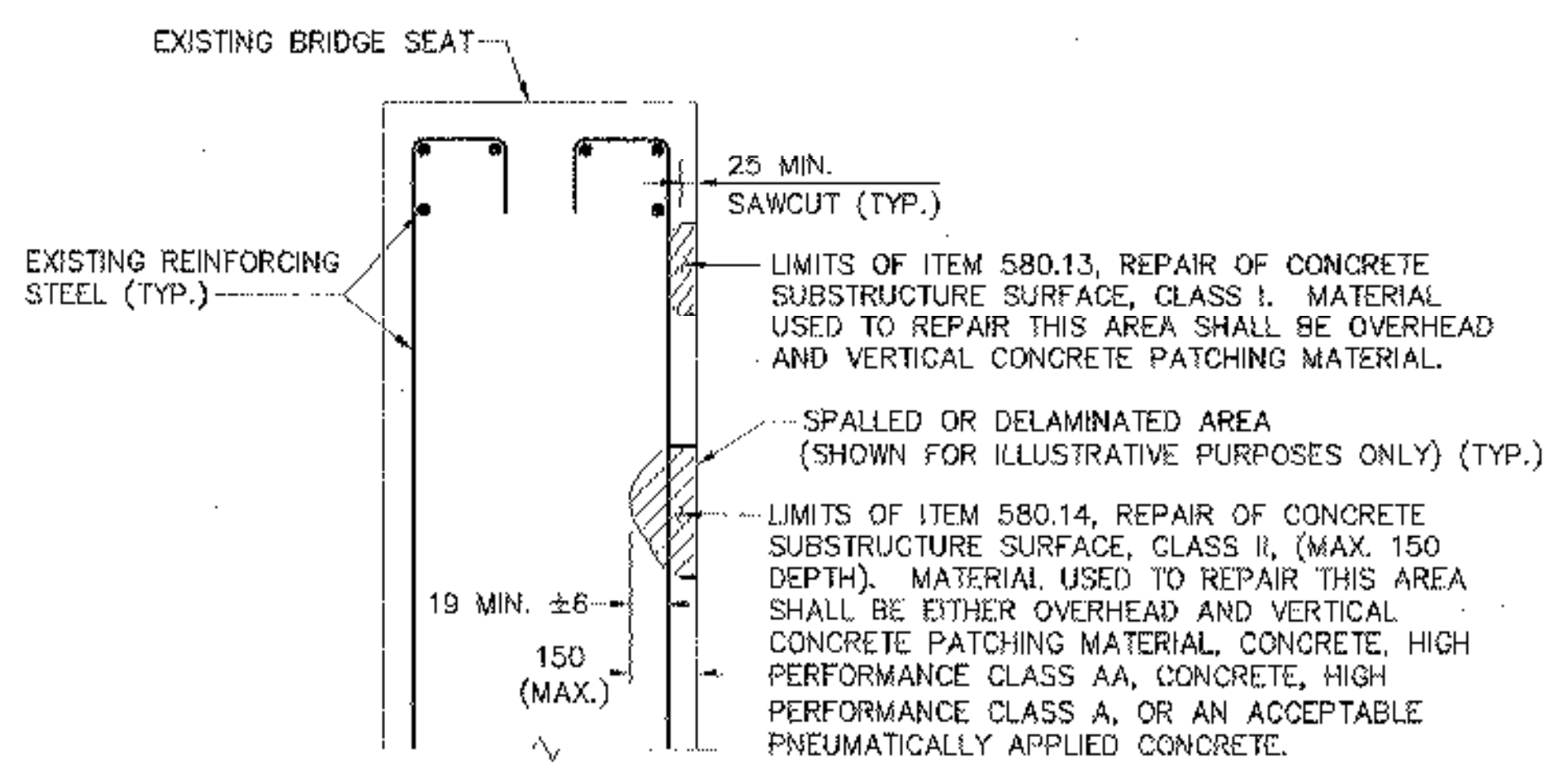


SECTION A-A
SCALE: 1:20



REMOVAL AND REPAIR NOTES:

1. SEE SHEETS 14 AND 15 FOR CONSTRUCTION PHASING.
2. REMOVAL OF EXISTING BRIDGE PAVEMENT SHALL BE PAID AS ITEM 529.10, REMOVAL OF BRIDGE PAVEMENT.
3. ITEM 529.20, PARTIAL REMOVAL OF STRUCTURE SHALL INCLUDE:
 - a. REMOVAL OF THE EXISTING BRIDGE RAILING, CONCRETE CURBS, CONCRETE BRIDGE DECK, AND CURTAIN WALLS.
 - b. REMOVAL OF THE EXISTING STRUCTURAL STEEL BEAMS AND DIAPHRAGMS. THE STRUCTURAL STEEL IS PAINTED WITH A MATERIAL THAT MAY CONTAIN LEAD PAINT. THE STRUCTURAL STEEL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND THE CONTRACTOR MAY DISPOSE OF IT OR RETAIN IT FOR FUTURE USE. THE CONTRACTOR SHALL SUBMIT A DISPOSAL OR SALVAGE PLAN TO THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO REMOVAL.
 - c. REMOVAL OF ALL THE BEARING DEVICES.
 - d. REMOVAL OF PORTIONS OF THE EXISTING ABUTMENTS AND WINGWALLS AS SHOWN ON THE PLANS AND DIRECTED BY THE RESIDENT ENGINEER.
 - e. ERECTION, MAINTENANCE, AND REMOVAL OF TEMPORARY STRUCTURES TO PREVENT DEBRIS FROM FALLING INTO MILL BROOK.
4. THE CONTRACTOR'S METHODS FOR PARTIAL REMOVAL OF THE EXISTING STRUCTURES SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY REMOVAL WORK.
5. LIMITS OF REMOVAL ARE APPROXIMATE. THE ENGINEER SHALL ESTABLISH ACTUAL LIMITS AFTER A COOPERATIVE INSPECTION BY THE CONTRACTOR AND THE RESIDENT ENGINEER. EXISTING ELEVATIONS SHALL BE FIELD VERIFIED TO ENSURE THE REMOVAL LIMITS ARE ADEQUATE TO OBTAIN THE REQUIRED DIMENSIONS AND ELEVATIONS OF THE NEW CONSTRUCTION.
6. SAWCUTS SHALL BE 25 MILLIMETERS DEEP ALONG ALL EXPOSED REMOVAL LINES WHERE NEW CONCRETE IS PLACED AGAINST EXISTING CONCRETE. ALL COSTS SHALL BE INCLUDED IN ITEM 529.20, PARTIAL REMOVAL OF STRUCTURE.
7. EXISTING REINFORCING STEEL EXPOSED DURING REMOVAL OPERATIONS, WITHIN THE LIMITS OF THE NEW MASONRY, SHALL BE RETAINED AND INCORPORATED INTO THE NEW MASONRY. EXISTING REINFORCING STEEL TO BE RETAINED SHALL BE CLEANED OF ALL CONCRETE, DIRT, SCALE, PAINT, OIL, AND OTHER FOREIGN SUBSTANCES. ALL COSTS SHALL BE INCLUDED IN ITEM 529.20, PARTIAL REMOVAL OF STRUCTURE.
8. EXISTING REINFORCING STEEL THAT WILL NOT BE INCORPORATED INTO THE NEW MASONRY SHALL BE REMOVED A MINIMUM OF 25 MILLIMETERS BEYOND THE MASONRY SURFACE. CAVITIES PRODUCED BY REMOVAL SHALL BE REPAIRED IN ACCORDANCE WITH REPAIR OF CONCRETE SUBSTRUCTURE SURFACE, CLASS I. ALL COSTS SHALL BE INCLUDED IN ITEM 529.20, PARTIAL REMOVAL OF STRUCTURE.
9. EXISTING ANCHOR BOLTS SHALL BE PARTIALLY REMOVED OR INCORPORATED INTO THE NEW WORK AS DIRECTED BY THE RESIDENT ENGINEER. PARTIAL REMOVAL DETAILS FOR OR INCORPORATION INTO THE NEW WORK SHALL BE SIMILAR AS THAT REQUIRED FOR REINFORCING STEEL.



REPAIR OF CONCRETE SUBSTRUCTURE SURFACE
CLASS I OR CLASS II
N.T.S.

STATE OF VERMONT AGENCY OF TRANSPORTATION			
Town Of	FAYSTON	Bridge No.	36
Highway No.	VT 17	Log Sta.	
		Surv. Sta.	
VT 17 OVER MILL BROOK			
ABUTMENT REMOVAL AND REPAIR			
Designed By	C.W. COMIELLO	Drawn By	B.J. MASSE
Checked By	S.M. SAREAULT	Bridge Design Supervisor	M.A. COLGAN
Date	1/06	Date	1/06
PROJECT	FAYSTON	PROJECT NO.	BHF 0200(9)
I.G.C. Info.			
Bridge Sheet No.	50543AR	Sheet	27 of 70