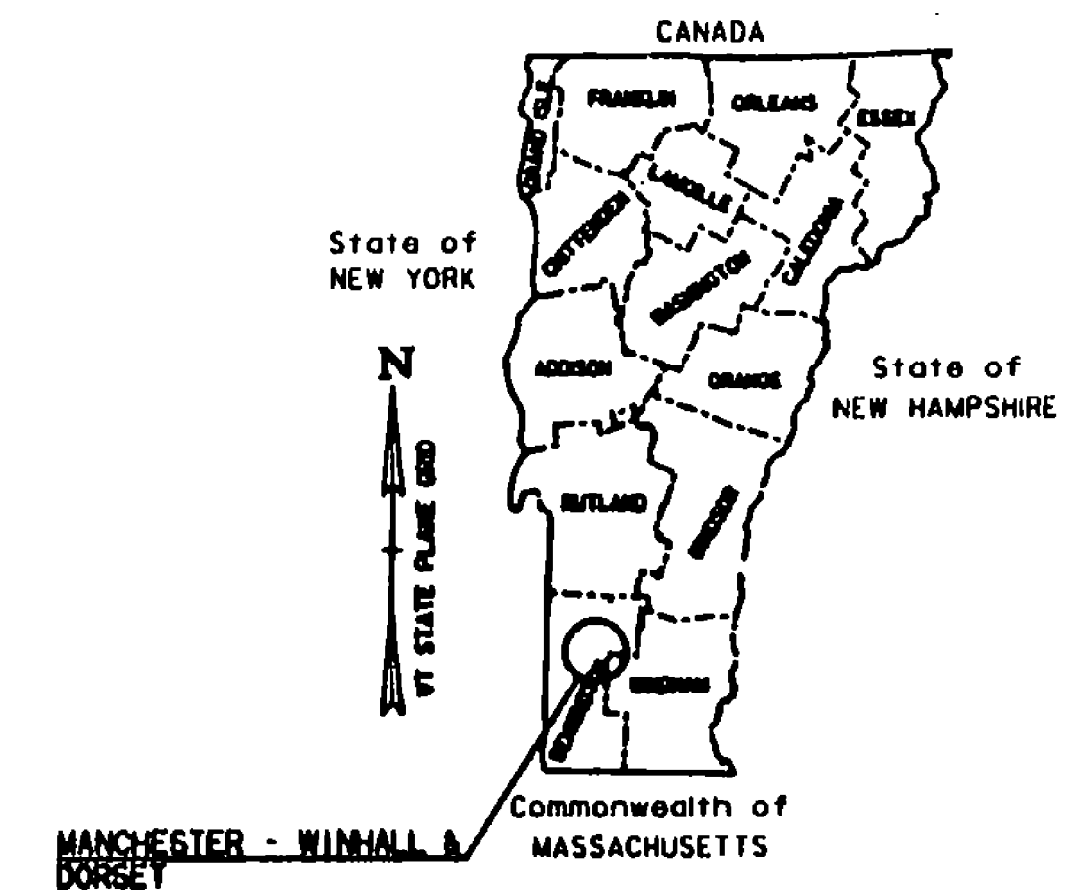
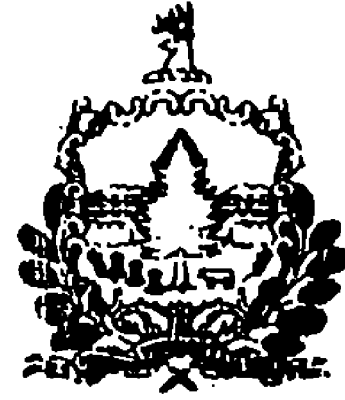


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STATE OF VERMONT AGENCY OF TRANSPORTATION



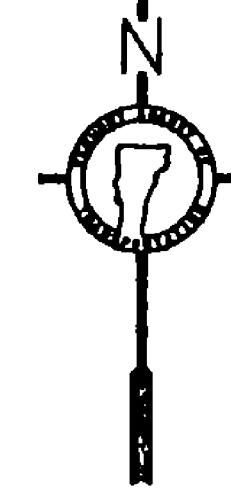
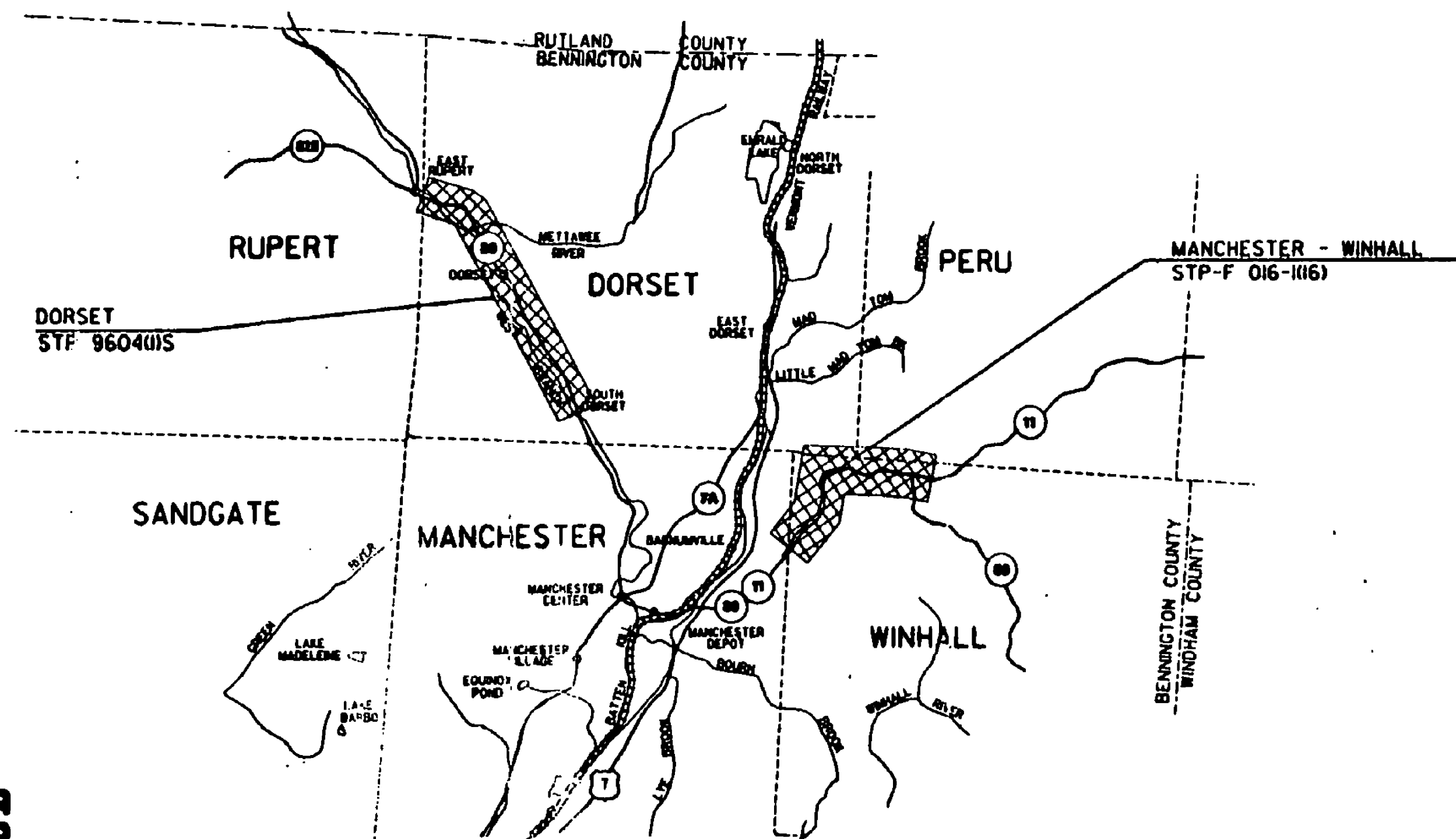
PROPOSED IMPROVEMENT TOWNS OF MANCHESTER, WINHALL AND DORSET COUNTY OF BENNINGTON VT. ROUTE 11 / VT. ROUTE 30

Date MAR 11 1998
 Contractor Pike Industries, Inc.
 Signature [Signature]
 Contracts Administrator [Signature]
 Director of Administration and Duty
 Authorized Agent

CONTRACT PLANS
 THESE PLANS DO NOT REFLECT
 CHANGES MADE ON THE PROJECT.

MANCHESTER - WINHALL
 STP-F 016-1(16)
 VT. ROUTE 11
 SEE SHEET 6 OF 43

DORSET
 STP 9604(1)S
 VT. ROUTE 30
 SEE SHEET 26 OF 43



UNLESS OTHERWISE NOTED, ALL DRAWINGS AND DETAILS OF THE PROJECT PLANS ARE NOT TO SCALE
 RIGHT OF WAY LIMITS, IF APPLICABLE, ARE PROVIDED SOLELY FOR THE CONVENIENCE OF THE STATE AND ITS CONTRACTOR DURING THE COURSE OF THIS PAVING PROJECT. ANY REFERENCES TO OFFSETS ON THESE PLANS ARE APPROXIMATE AND SHOULD NOT BE RELIED UPON FOR ANY OTHER PURPOSES

CONVENTIONAL SIGNS	
COUNTY LINE	---
TOWN LINE	- - - - -
LIMITS OF ACCESS	—○—○—○—
POINT OF ACCESS	X
FENCE LINE	—x—x—x—
STONE WALL	—x—x—x—
TRAVELED WAY	—x—x—x—
GUARD RAIL	—x—x—x—
RAILROAD	—x—x—x—
SURVEY LINE	—x—x—x—
CULVERT	—x—x—x—
POWER POLE	○
TELEPHONE POLE	○
TREES	●
CONTROL OF ACCESS	///
PROPERTY LINE	—x—x—x—
R.O.W. TAKING LINE	—x—x—x—
SLOPE RIGHTS	—x—x—x—
TOP OF CUT	—x—x—x—
TOE OF SLOPE	—x—x—x—

PLANS PREPARED BY
CHA CLOUGH, HARBOUR & ASSOCIATES LLP
 ENGINEERS, SURVEYORS, PLANNERS & LANDSCAPE ARCHITECTS
 111 WINNERS CIRCLE ALBANY, NEW YORK, 12208

BY Thomas P. Kain
 DATUM
 VERTICAL N/A
 HORIZONTAL N/A

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF CONSTRUCTION AND MAINTENANCE.
 CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1995, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON AUGUST 21, 1995 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

Metric

APPROVED [Signature] DATE 12/19/97
 DIRECTOR OF CONSTRUCTION AND MAINTENANCE

DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION

APPROVED _____ DATE _____
 DIVISION ADMINISTRATOR

PROJECT **MANCHESTER - WINHALL & DORSET**
 SHEET 1 OF 43 SHEETS

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2	COMPOSITE INDEX OF SHEETS
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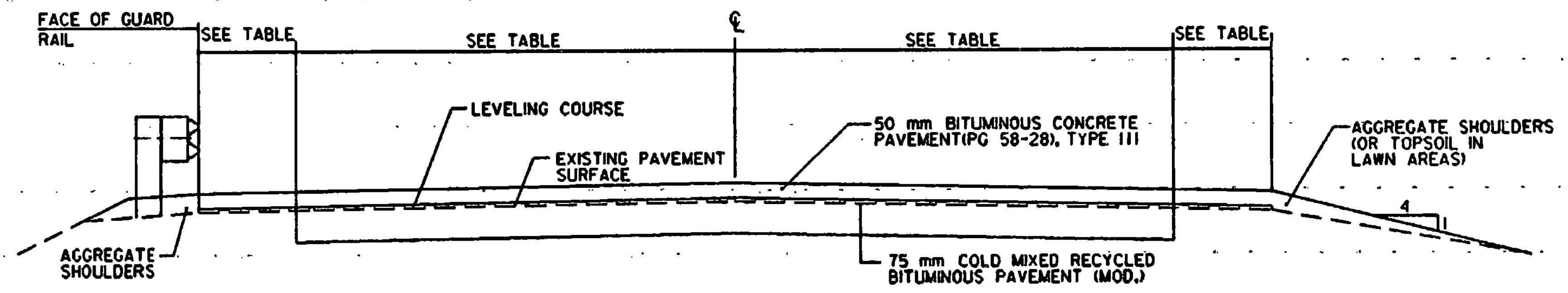
DA-TUM	
VERTICAL	N/A
HORIZONTAL	N/A

INDEX OF SHEETS	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	C.M.C.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/89b016/pb016.dgn		
	IPARM FILE	pb016n1	DATE PLOTTED	20-JAN-1998
	PROJ. NAME	MANCHESTER - WINHALL & DORSET		
PROJ. NO.	STP-F 016-11(16) & STP 9604(15)			
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NOTES

1. THE WEARING COURSE SHALL BE TYPE III BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IV UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. ALL ASPHALT CEMENT USED IN THE BITUMINOUS CONCRETE PAVEMENT SHALL BE PG 58-28.
2. EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 75 mm OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT, AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATION WILL BE PAID FOR AS ALL-PURPOSE EXCAVATOR OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH SUBBASE OF CRUSHED GRAVEL (FINE GRADED).
3. 1.0 m OF BACKING IS REQUIRED BEHIND THE FACE OF GUARD RAIL WITH 1.8 m POSTS. IF THIS CANNOT BE OBTAINED, THEN 2.4 m POSTS SHALL BE USED.
4. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, ON ALL COLD PLANED SURFACES, ON ALL RECYCLED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT AT THE RATE OF 0.07 L/m² OR AS DIRECTED BY THE RESIDENT ENGINEER.
5. ALL LIQUID BINDER USED IN COLD MIXED RECYCLED BITUMINOUS PAVEMENT, AS DIRECTED BY THE RESIDENT ENGINEER, SHALL BE PAID FOR AS EMULSIFIED ASPHALT (MOD.).
EMULSIFIED ASPHALT (MOD.) HAS BEEN ESTIMATED AT THE RATE OF 3.6 L/m² TO BE USED IN 75 mm COLD MIXED RECYCLED BITUMINOUS PAVEMENT.
6. BITUMINOUS CONCRETE PAVEMENT TOLERANCE = ±5 mm (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
7. ALL DRIVEWAYS, AND MAILBOX-PULL-OUTS SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER.
8. ITEM 604.40, 604.42 & 604.45 IS AN ESTIMATED QUANTITY AND SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER.
9. ALL TREATED TIMBER CURB SHALL BE BACKED UP FULL HEIGHT WITH ITEM 402.12, AGGREGATE SHOULDERS, AS DIRECTED BY THE RESIDENT ENGINEER.
10. AN ESTIMATED QUANTITY OF EARTH BORROW HAS BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING MELT FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 75 mm DEPTH OF AGGREGATE SHOULDER MATERIAL UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 20 m³ OF EARTH BORROW AND 5 TONS OF AGGREGATE SHOULDER MATERIAL FOR EACH GUARDRAIL TERMINAL.



COLD MIXED RECYCLED BITUMINOUS PAVEMENT

VT. ROUTE II MANCHESTER STA. 4+910.109 TO MANCHESTER STA. 5+030.409
 VT. ROUTE II MANCHESTER STA. 5+051.709 TO MANCHESTER STA. 5+222.321
 VT. ROUTE II WINHALL STA. 0+000.000 TO WINHALL STA. 5+399.349

CONSERVATION SEED MIX

RURAL AREA - SEED MIXTURE				
Z WT.	kg/ha.	NAME	PUR. %	GERM. %
37.14	26.0	CREeping RED FESCUE	98	85
37.14	26.0	TALL FESCUE	95	90
5.71	4.0	RED TOP	95	90
14.30	10.0	BIRDSFOOT TREFOIL	98	85
5.71	4.0	ANNUAL RYEGRASS	95	85
100.0	70.0			

SEED MIXTURE:
SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.

SEED:
TO BE APPLIED PER SEEDING FORMULA DIRECTED BY THE RESIDENT ENGINEER.

FERTILIZER:
FORMULA 10-20-10 TO BE USED WITH SEED, APPLIED AT THE RATE OF 560 kg/ha (HYDRO SEEDERS MAY USE 19-19-19 FORMULA)

AGRICULTURAL LIMESTONE:
TO BE APPLIED AT THE RATE OF 4500 kg/ha OR AS DIRECTED BY THE RESIDENT ENGINEER.

HAY MULCH:
TO BE APPLIED ON EARTH SLOPES AT THE RATE OF 4500 kg/ha, OR AS DIRECTED BY THE RESIDENT ENGINEER.

TOPSOIL:
TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.

MARKER POSTS:
TO BE PLACED AS DIRECTED BY THE RESIDENT ENGINEER.

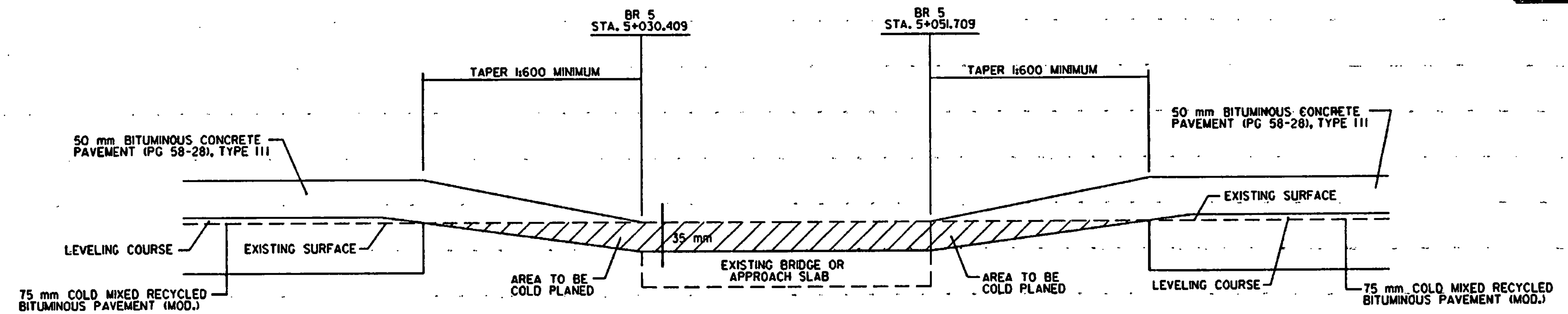
PROJECT PAVING LIMITS

TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING †	NOTES
MANCHESTER VT. ROUTE II	4+910.109	5+030.409	VARIES - SEE LAYOUT SHEETS	50 mm	35	
MANCHESTER VT. ROUTE II	5+030.409	5+051.709	2.8 m-3.6 m-3.6 m-2.7 m	35 mm	-	BR 5 - COLD PLANE 35 mm AND PAVE 35 mm TYPE III
MANCHESTER VT. ROUTE II	5+051.709	5+222.321	VARIES - SEE LAYOUT SHEETS	50 mm	52	
WINHALL VT. ROUTE II	0+000.000	0+975.000	0.6 m-3.6 m-3.6 m-0.6 m	50 mm	292	
WINHALL VT. ROUTE II	0+975.000	1+303.000	VARIES - SEE LAYOUT SHEETS	50 mm	170	
WINHALL VT. ROUTE II	1+303.000	2+713.000	VARIES -3.6 m-3.6 m-3.6 m-VARIES	50 mm	631	
WINHALL VT. ROUTE II	2+713.000	4+340.000	1.0 m-3.6 m-3.6 m-3.6 m-1.0 m	50 mm	741	
WINHALL VT. ROUTE II	4+340.000	5+011.600	0.6 m-3.6 m-3.6 m-3.6 m-0.6 m	50 mm	287	
WINHALL VT. ROUTE II	5+011.600	5+399.349	VARIES - SEE LAYOUT SHEETS	50 mm	196	

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

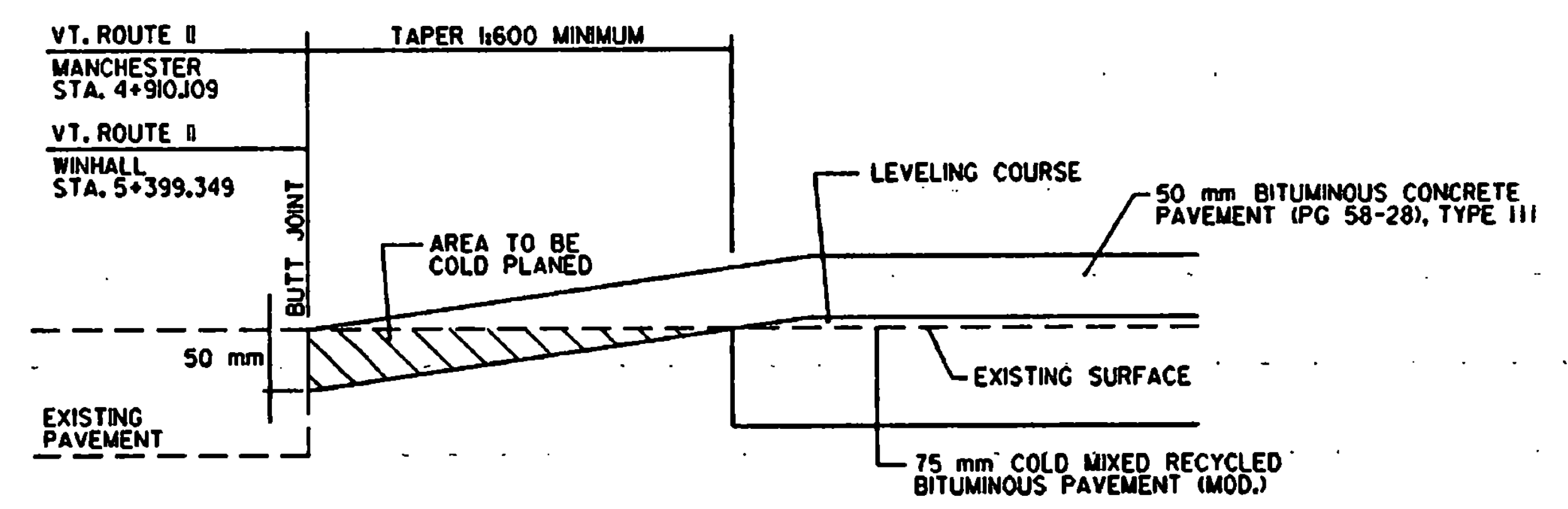
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	DRAWN BY C.M.C. DATE 11/97
	SQUAD LEADER T.P.K.
	DESIGN FILE NO. /pave/89b016/pb016.dgn
	IPARM FILE pb016.tbl DATE PLOTTED 20-JAN-1998
PROJ. NAME MANCHESTER - WINHALL	
PROJ. NO. STP-F 016-K16	
SHEET 7 OF 43 SHEETS	

: Sheet Number: 7

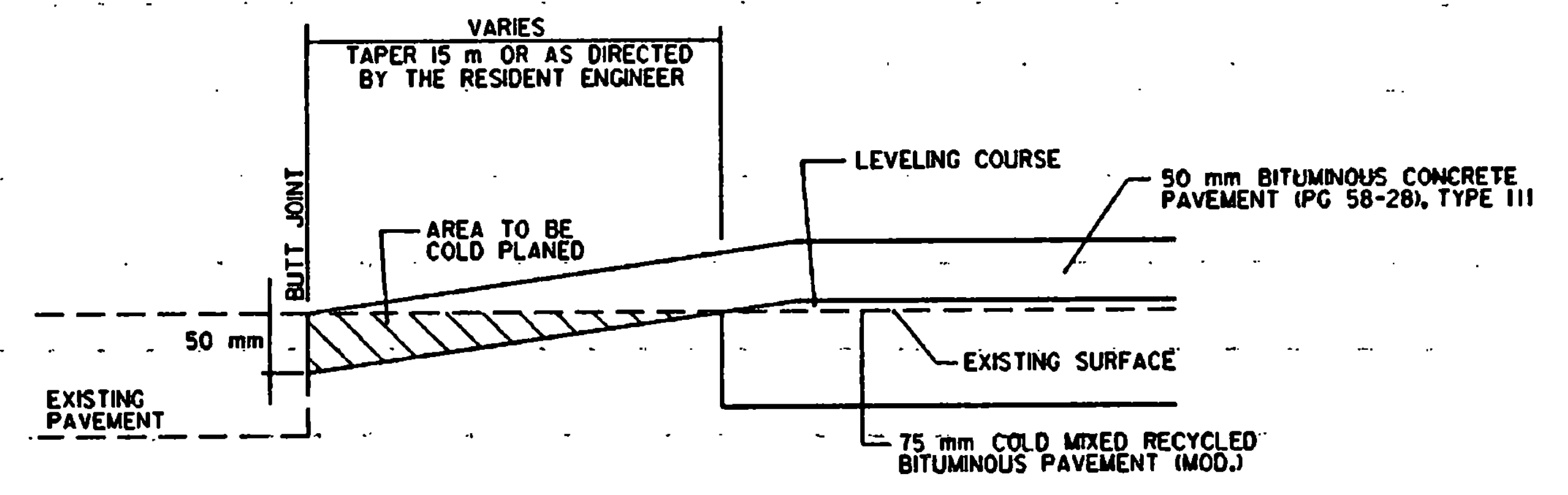


TRANSITION AREA DETAIL

BR 5
VT. ROUTE II MANCHESTER STA. 5+030.409 TO MANCHESTER STA. 5+051.709



APPROACH AREA DETAIL
VT. ROUTE II MANCHESTER STA. 4+910.109 - BEGIN OVERLAY
VT. ROUTE II WINHALL STA. 5+399.349 - END OVERLAY



APPROACH AREA DETAIL

WINHALL STATE HIGHWAY
VT. ROUTE 30

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVING DETAILS SHEET	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	C.M.C.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/89b016/pb016.dgn		
	IPBM FILE	pb016.dtl	DATE PLOTTED	20-JAN-1998
	PROJ. NAME	MANCHESTER - WINHALL		
PROJ. NO.	STP-F 018-K16			
SHEET	8 OF 43	SHEETS		

ITEM DETAIL SUMMARY SHEET



LOCATION			GUARD RAIL											DRAINAGE				REMARKS	
			616.35 TREATED TIMBER CURB m	616.41 REMOVE EXISTING CURB m	621.21 STEEL POST G.R. m	621.20 (TYPE IV) WOOD POST G.R. m		621.20 (TYPE IV) 2.4 m WOOD POST G.R. (MOD.) m		621.54 WOOD POST M.E.L.T. EA	621.54 (TYPE IV) WOOD POST M.E.L.T. EA		621.60 AND/OR FOR G.R. EA	621.77 REPLACE G.R. BEAM UNIT EA	621.80 REMOVE & DISP. G.R. m	621.81 REMOVE & DISP. G.P. EA	NEW PIPE		
STATION	STATION	POS.												DIA.	LENGTH	TH/CL OF PIPE			
MANCHESTER																			
5+025.0	5+047.8	RT			22.8				2					23.5					
			BR 5 MOUNT PROPOSED GUARDRAIL AND OFFSET BLOCK TO EXISTING SURFACE MOUNTED STEEL BRIDGE POSTS INSTALL NEW MELT AT STA. 5+025.0 & STA. 5+047.8																
5+034.8	5+057.6	LT			22.8				2					21.9					
			BR 5 MOUNT PROPOSED GUARDRAIL AND OFFSET BLOCK TO EXISTING SURFACE MOUNTED STEEL BRIDGE POSTS INSTALL NEW MELT AT STA. 5+034.8 & STA. 5+057.6																
WINHALL																			
0+162.2	0+401.6	LT						239.4			2			261					
			INSTALL NEW MELT AT STA. 0+162.2 & STA. 0+401.6																
0+555.9	0+669.5	LT						113.6			2			67.7	3				
			INSTALL NEW MELT AT STA. 0+555.9 & STA. 0+669.5																
0+671.9		LT																	
0+785.8		LT																	
0+787.5	0+806.5	LT						19			2			20.5					
			INSTALL NEW MELT AT STA. 0+787.5 & STA. 0+806.5																
0+810.5		LT																	
0+814.4	0+856.0	LT													9				
			INSTALL NEW MELT AT STA. 0+814.4 & STA. 0+856.0																
0+858.1	0+869.5	LT						11.4			2			22.9	1				
			INSTALL NEW MELT AT STA. 0+858.1 & STA. 0+869.5																
0+883.1		LT																	
0+891.4		LT																	
0+891.4		RT																	
0+904.7	0+969.3	LT							64.6		2			77.4					
			BR 6 SEE SHEET 19 FOR LARGE CULVERTS DETAIL. INSTALL NEW MELT AT STA. 0+904.7 & STA. 0+969.3																
0+982.3	0+986.5	RT						34.2			1	1		18.4	2	300	10	1.7	
			BR 6 SEE SHEET 19 FOR LARGE CULVERTS DETAIL. INSTALL NEW MELT AT STA. 0+982.3 SEE SHEET 19 FOR BURIED GUARDRAIL ENDS INTO BACKSLOPES DETAIL. PROVIDE ANCHOR AT STA. 0+984.6 REMOVE TWO CONCRETE POSTS, EXISTING REINFORCING STEEL TO BE CUT AND THE EXISTING CONCRETE TO BE PATCHED TO THE SATISFACTION OF THE RESIDENT ENGINEER.																
0+972.3		LT																	
1+176.1	1+274.9	LT	96.0					98.8			1	1		84.3	1	300	10	1.7	
			SEE SHEET 19 FOR BURIED GUARDRAIL ENDS INTO BACKSLOPES DETAIL. PROVIDE ANCHOR AT STA. 1+178.0 INSTALL NEW MELT AT STA. 1+274.9																
1+188.1		RT																	
1+188.1	1+275.6	LT		87.5															
1+195.6	1+313.4	RT						117.8			1	1		103.2	1	300	10	1.7	
			SEE SHEET 19 FOR BURIED GUARDRAIL ENDS INTO BACKSLOPES DETAIL. PROVIDE ANCHOR AT STA. 1+197.5 INSTALL NEW MELT AT STA. 1+313.4																
1+278.2		LT																	
1+316.3		RT																	
1+643.7	1+681.7	RT						38			2			39.1					
			INSTALL NEW MELT AT STA. 1+643.7 & STA. 1+681.7																
2+232.4	2+361.6	LT						129.2			2			149.0					
			INSTALL NEW MELT AT STA. 2+232.4 & STA. 2+361.6																
2+395.0	2+657.2	RT	259.2					262.2			2			267.8					
			INSTALL NEW MELT AT STA. 2+395.0 & STA. 2+657.2																
2+395.7	2+528.7	LT						133			2			152.2					
			INSTALL NEW MELT AT STA. 2+395.7 & STA. 2+528.7																
2+400.0	2+657.2	RT		267.8															
2+557.9	2+645.3	LT						87.4			2			107.9					
			INSTALL NEW MELT AT STA. 2+557.9 & STA. 2+645.3																
SHEET SUBTOTAL			355.2	355.3	45.6		1284.0	64.6	4	23	3		1416.8	27		30			

DA-FUM
VERTICAL N/A
HORIZONTAL N/A

**ITEM
DETAIL
SHEET 1**

SURVEYED BY N/A DATE N/A
 DRAWN BY E.C.D. DATE 11/97
 SQUAD LEADER T.P.K.
 DESIGN FILE NO/pgve/89b016/pb016.dgn
 I-PARM FILE pb016tbl1 DATE PLOTTED 20-JAN-1998
 PROJ. NAME MANCHESTER - WINHALL
 PROJ. NO. STP-F 016(05)
 SHEET 10 OF 43 SHEETS

: Sheet Number: 10

ITEM DETAIL SUMMARY SHEET



LOCATION			GUARD RAIL											DRAINAGE				REMARKS		
STATION	STATION	POS.	616.35	616.41	621.21	621.20 (TYPE IV)		621.54	621.54 (TYPE IV)	621.60	621.77	621.80	621.81	NEW PIPE			STONE FILL/TYPE			
			TREATED TIMBER CURB m	REMOVE EXISTING CURB m	STEEL POST G.R. m	WOOD POST G.R. m	STEEL POST G.R. m	2.4m WOOD POST G.R. (MOD.) m	WOOD POST M.E.L.T. EA	WOOD POST M.E.L.T. EA	ANCHOR FOR G.R. EA	REPLACE G.R. BEAM UNIT EA	REMOVE & DISP. G.R. m	REMOVE & DISP. G.P. EA	DIA. mm	LENGTH m			TH/CL OF PIPE	
WINHALL																				
3+365.6	4+004.0	RT					646.0					2			573.2					INSTALL NEW MELT AT STA. 3+365.6 & STA. 4+004.0
4+035.4	4+073.4	RT					38					2			49.4					BR 11 SEE SHEET 19 FOR SMALL CULVERTS DETAIL INSTALL NEW MELT AT STA. 4+035.4 & STA. 4+073.4
4+056.8	4+220.2	LT										2			177.2					BR 11 SEE SHEET 19 FOR SMALL CULVERTS DETAIL INSTALL NEW MELT AT STA. 4+056.8 & STA. 4+220.2
4+387.0	4+434.0	LT													10					
4+536.0		LT													1					
4+537.0	4+753.6	LT					216.6					2			217.3					INSTALL NEW MELT AT STA. 4+537.0 & STA. 4+753.6
4+756.6	4+843.0	LT													20					
4+842.5	4+880.5	LT						38				2			37.3					BR 12 SEE SHEET 19 FOR LARGE CULVERTS DETAIL. REMOVE CONCRETE POST, EXISTING REINFORCING STEEL TO BE CUT AND THE EXISTING CONCRETE TO BE PATCHED TO THE SATISFACTION OF THE RESIDENT ENGINEER. INSTALL NEW MELT AT STA. 4+842.5 & STA. 4+880.5
4+844.4	4+901.4	RT					57					1	1		34.9	300	10	1.7		BR 12 SEE SHEET 19 FOR LARGE CULVERTS DETAIL. SEE SHEET 19 FOR BURIED GUARDRAIL ENDS INTO BACKSLOPES DETAIL. PROVIDE ANCHOR AT STA. 4+846.3 INSTALL NEW MELT AT STA. 4+901.4. REMOVE CONC. POST, EXISTING REINFORCING STEEL TO BE CUT AND THE EXISTING CONCRETE TO BE PATCHED TO THE SATISFACTION OF THE RESIDENT ENGINEER.
5+039.0	5+105.5	LT													14					
5+118.6	5+179.4	LT					60.8					2			71.7					INSTALL NEW MELT AT STA. 5+118.6 & STA. 5+179.4
5+181.0		LT													1					
SHEET SUBTOTAL							1056.4	163.4			13	1		1161	46		10			
SHEET 10 SUBTOTAL			355.2	355.3	45.6		1284	64.6	4	23	3			1416.8	27		30			
ROUNDING			14.8	4.7	4.4		59.6	22						22.2						
TOTAL			370	360	50		2400	250	4	36	4			2600	73		40			

DA-FUM
 VERTICAL N/A
 HORIZONTAL N/A

ITEM DETAIL SHEET 2

SURVEYED BY N/A DATE N/A
 DRAWN BY E.C.D. DATE 11/97
 SQUAD LEADER T.P.K.
 DESIGN FILE NO. /pave/89b016/pb016.dgn
 IPARM. FILE d50161d21 DATE PLOTTED 20-JAN-1998
 PROJ. NAME MANCHESTER - WINHALL
 PROJ. NO. STP-F 061167
 SHEET 11 OF 43 SHEETS

: Sheet Number: 11

646.41 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MANCHESTER:
 STA. 4+910.109 - STA. 5+014.000 S = S
 STA. 5+014.000 - STA. 5+134.000 S = D
 STA. 5+134.000 - STA. 5+222.321 D = -

WINHALL:
 STA. 0+000.000 - STA. 0+209.000 - D = -
 STA. 0+209.000 - STA. 0+451.000 D = S
 STA. 0+451.000 - STA. 0+580.000 S = S

646.61 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

MANCHESTER:
 STA. 4+910.109 - STA. 5+014.000 S = S
 STA. 5+014.000 - STA. 5+134.000 S = D
 STA. 5+134.000 - STA. 5+222.321 D = -

WINHALL:
 STA. 0+000.000 - STA. 0+209.000 - D = -
 STA. 0+209.000 - STA. 0+451.000 D = S
 STA. 0+451.000 - STA. 0+580.000 S = S

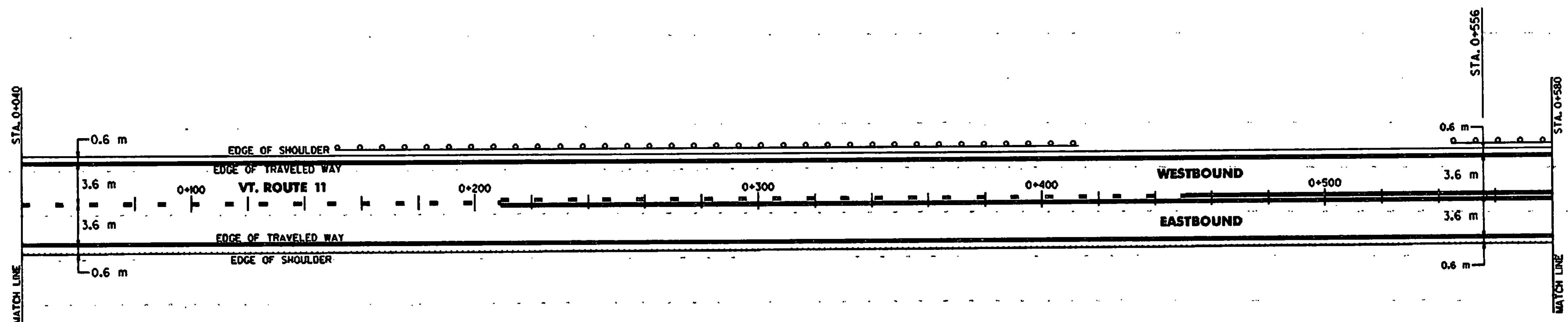
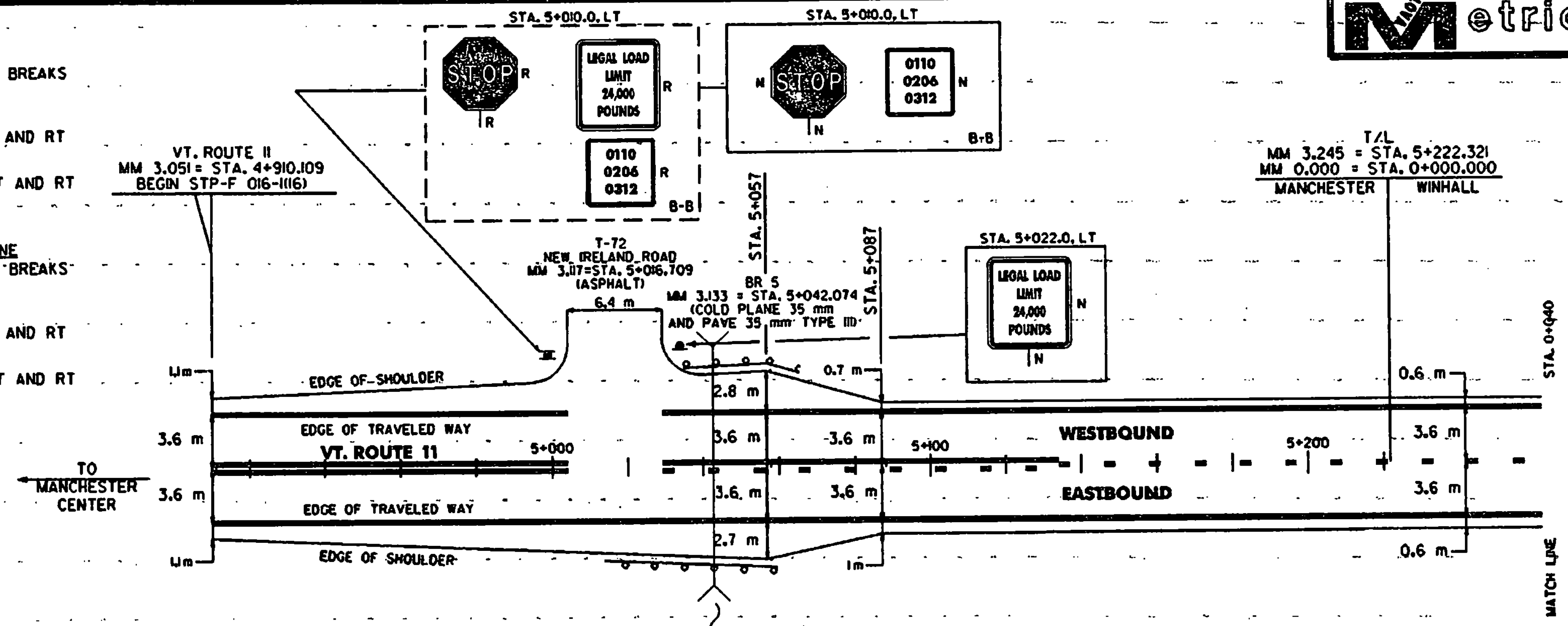
675.50 REMOVING SIGNS
 AS SHOWN - 3

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MANCHESTER:
 STA. 4+910.109 - STA. 5+222.321 LT AND RT
 WINHALL:
 STA. 0+000.000 - STA. 0+580.000 LT AND RT

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

MANCHESTER:
 STA. 4+910.109 - STA. 5+222.321 LT AND RT
 WINHALL:
 STA. 0+000.000 - STA. 0+580.000 LT AND RT



621.21 HEAVY DUTY STEEL BEAM GUARDRAIL

MANCHESTER:
 STA. 5+025.0 - STA. 5+047.8 RT (22.8 m)
 STA. 5+034.8 - STA. 5+057.6 LT (22.8 m)

621.20 STEEL BEAM GUARDRAIL (TYPE IV)

WINHALL:
 STA. 0+162.2 - STA. 0+401.6 LT (239.4 m)
 STA. 0+555.9 - STA. 0+580.0 LT (24.1 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL

MANCHESTER:
 STA. 5+013.6 - STA. 5+025.0 RT
 STA. 5+023.4 - STA. 5+034.8 LT
 STA. 5+047.8 - STA. 5+059.2 RT
 STA. 5+057.6 - STA. 5+069.0 LT

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)

WINHALL:
 STA. 0+150.8 - STA. 0+162.2 LT
 STA. 0+401.6 - STA. 0+413.0 LT
 STA. 0+544.5 - STA. 0+555.9 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

MANCHESTER:
 STA. 5+025.0 - STA. 5+048.5 RT (23.5 m)
 STA. 5+034.8 - STA. 5+056.7 LT (21.9 m)

WINHALL:
 STA. 0+150.8 - STA. 0+411.8 LT (261 m)

621.81 REMOVAL AND DISPOSAL OF GUIDEPOSTS

WINHALL:
 STA. 0+555.9 LT (1EA)
 STA. 0+562.0 LT (1EA)

NOTE:

1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

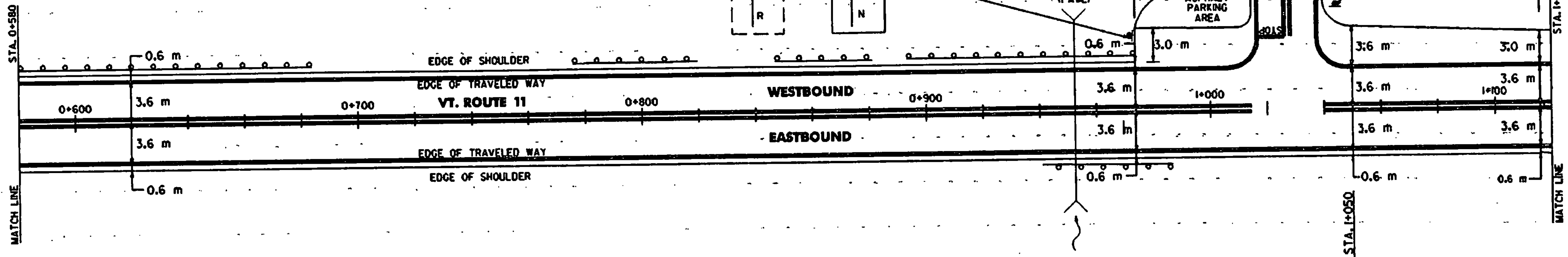
SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO./PAVE	89b016/pb016.dgn		
	IPARM FILE	pb016.dgn	DATE PLOTTED	20-JAN-1998
PROJ. NAME	MANCHESTER - WINHALL			
PROJ. NO.	STP-F 016-116			
SHEET	12 OF 43	SHEETS		

646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C/L RT
 WINHALL:
 STA. 0+580.000 - STA. 1+660.000 S - S
 STA. 1+027.200, DOUBLE SOLID, TOLLGATE ROAD
646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C/L RT
 WINHALL:
 STA. 0+580.000 - STA. 1+660.000 S - S
 STA. 1+027.200, DOUBLE SOLID, TOLLGATE ROAD

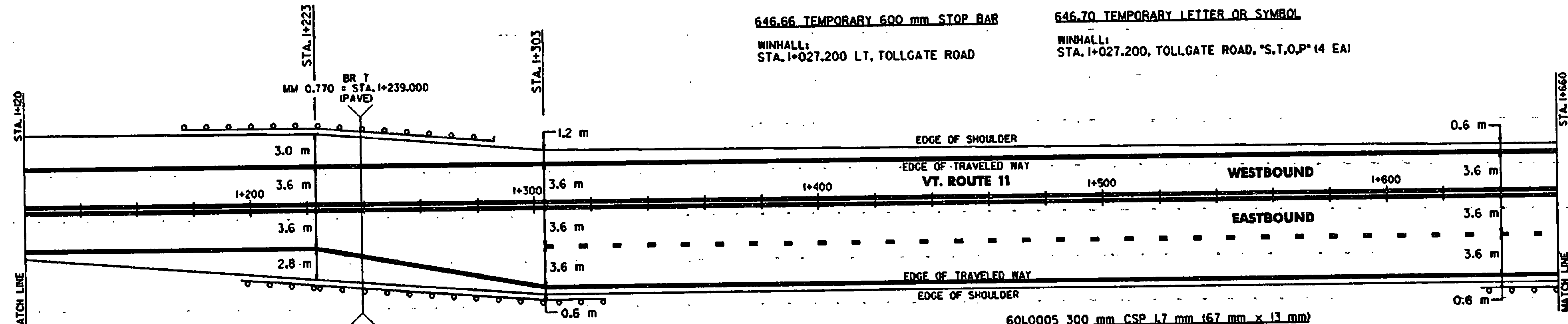
646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 WINHALL:
 STA. 0+580.000 - STA. 1+660.000 LT AND RT
 STA. 1+303.000 - STA. 1+660.000 DASHED 3.6 m RT
646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 WINHALL:
 STA. 0+580.000 - STA. 1+660.000 LT AND RT
 STA. 1+303.000 - STA. 1+660.000 DASHED 3.6 m RT

675.50 REMOVING SIGNS
 AS SHOWN - 2



646.46 DURABLE 600 mm STOP BAR
 WINHALL:
 STA. 1+027.200 LT, TOLLGATE ROAD
646.66 TEMPORARY 600 mm STOP BAR
 WINHALL:
 STA. 1+027.200 LT, TOLLGATE ROAD

646.50 DURABLE LETTER OR SYMBOL
 WINHALL:
 STA. 1+027.200, TOLLGATE ROAD, 'S.T.O.P.' (4 EA)
646.70 TEMPORARY LETTER OR SYMBOL
 WINHALL:
 STA. 1+027.200, TOLLGATE ROAD, 'S.T.O.P.' (4 EA)



501.22 CONCRETE, CLASS A

WINHALL:
 STA. 0+957.9 RT (0.25 m³)
 STA. 0+959.9 RT (0.25 m³)
529.20 PARTIAL REMOVAL OF STRUCTURE
 WINHALL:
 STA. 0+957.9 RT (1EA)
 STA. 0+959.9 RT (1EA)
616.35 TREATED TIMBER CURB
 WINHALL:
 STA. 1+178.9 - STA. 1+274.9 LT (96.0 m)
616.4 REMOVAL OF EXISTING CURB
 WINHALL:
 STA. 1+188J - STA. 1+275.6 LT (87.5 m)

621.20 STEEL BEAM GUARDRAIL (TYPE IV)
 WINHALL:
 STA. 0+580.0 - STA. 0+669.5 LT (89.5 m)
 STA. 0+787.5 - STA. 0+806.5 LT (19 m)
 STA. 0+858J - STA. 0+869.5 LT (11.4 m)
 STA. 0+952.3 - STA. 0+986.5 RT (34.2 m)
 STA. 1+176.1 - STA. 1+274.9 LT (98.8 m)
 STA. 1+195.6 - STA. 1+313.4 RT (117.8 m)
 STA. 1+643.7 - STA. 1+660.0 RT (16.3 m)
621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)
 WINHALL:
 STA. 0+669.5 - STA. 0+680.9 LT
 STA. 0+776J - STA. 0+787.5 LT
 STA. 0+806.5 - STA. 0+817.9 LT
 STA. 0+846.7 - STA. 0+858J LT
 STA. 0+869.5 - STA. 0+880.9 LT
 STA. 0+893J - STA. 0+904.7 LT
 STA. 0+940.9 - STA. 0+952.3 RT
 STA. 0+969.3 - STA. 0+980.7 LT
 STA. 1+274.9 - STA. 1+286.3 LT
 STA. 1+313.4 - STA. 1+324.8 RT
 STA. 1+632.3 - STA. 1+643.7 RT

621.20 STEEL BEAM GUARDRAIL W/ 2.4 m POSTS (TYPE IV) (MOD.)
 WINHALL:
 STA. 0+904.7 - STA. 0+969.3 LT (64.6 m)
621.60- ANCHOR FOR STEEL BEAM RAIL
 WINHALL:
 STA. 0+984.6 RT
 STA. 1+178.0 LT
 STA. 1+197.9 RT
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 WINHALL:
 STA. 0+601.8 - STA. 0+669.5 LT (67.7 m)
 STA. 0+787.5 - STA. 0+808.0 LT (20.5 m)
 STA. 0+858J - STA. 0+881.0 LT (22.9 m)
 STA. 0+893J - STA. 0+970.7 LT (77.4 m)
 STA. 0+952.3 - STA. 0+970.7 RT (18.4 m)
 STA. 1+191.3 - STA. 1+275.6 LT (84.3 m)
 STA. 1+210.8 - STA. 1+314.0 RT (103.2 m)
 STA. 1+643.7 - STA. 1+660.0 RT (16.3 m)

601.0005 300 mm CSP 1.7 mm (67 mm x 13 mm)
 WINHALL:
 STA. 0+984.6 RT (10 m)
 STA. 1+178.0 LT (10 m)
 STA. 1+197.5 RT (10 m)

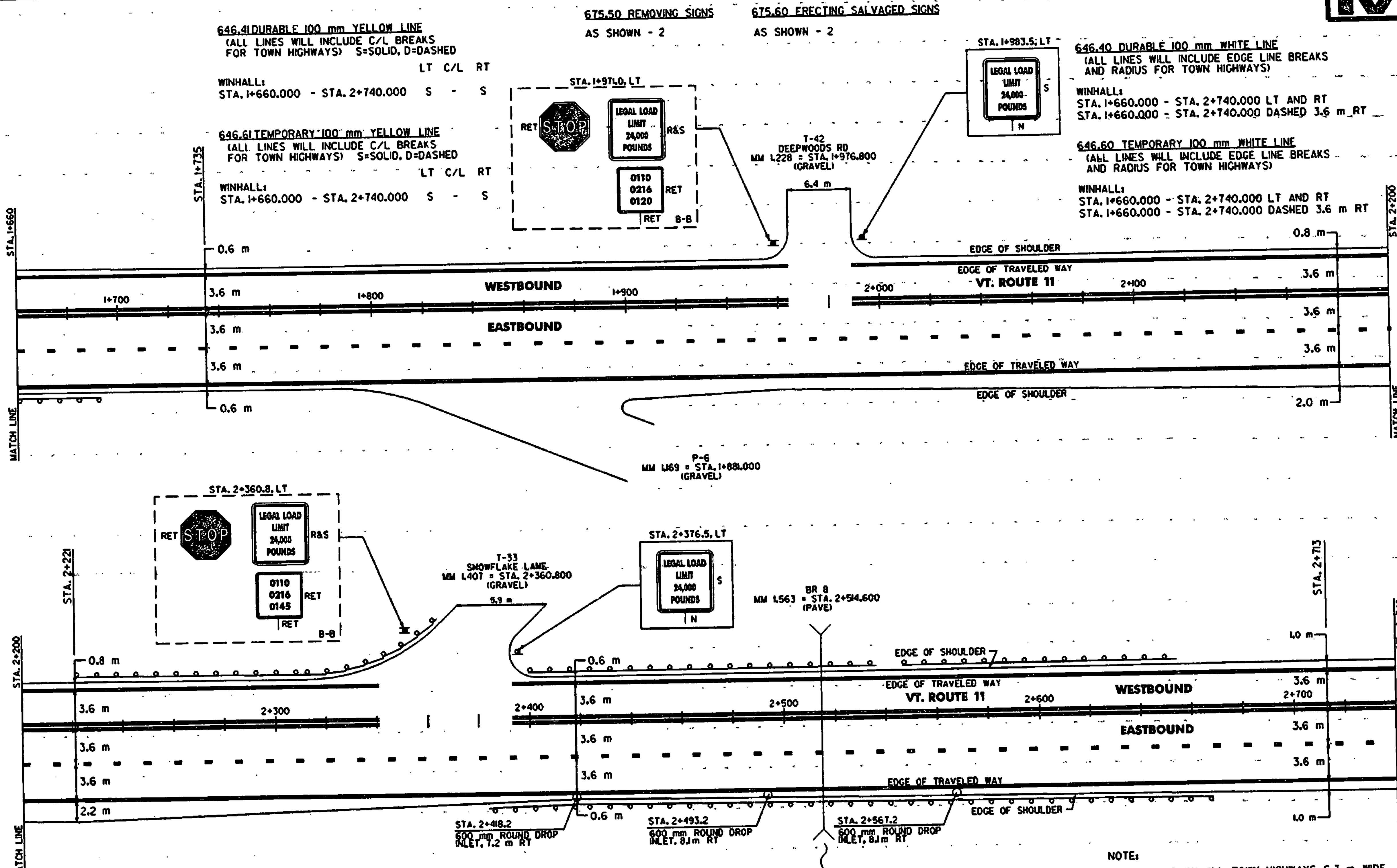
621.8 REMOVAL AND DISPOSAL OF GUIDEPPOSTS
 WINHALL:
 STA. 0+599.9 LT (1EA)
 STA. 0+671.9 LT (1EA)
 STA. 0+785.8 LT (1EA)
 STA. 0+810.5 LT (1EA)
 STA. 0+814.4 - STA. 0+856.0 LT (9 EA)
 STA. 0+856.0 LT (1EA)
 STA. 0+883J LT (1EA)
 STA. 0+891.4 RT (1EA)
 STA. 0+891.4 RT (1EA)
 STA. 0+950.3 RT (1EA)
 STA. 0+972.3 LT (1EA)
 STA. 0+972.3 RT (1EA)
 STA. 1+188J LT (1EA)
 STA. 1+188J RT (1EA)
 STA. 1+208.5 RT (1EA)
 STA. 1+278.2 LT (1EA)
 STA. 1+316.3 RT (1EA)

SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

PAVING PROJECT LAYOUT

NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/89b016/pb016.dgn		
IPARM. FILE	pb016i02.1	DATE PLOTTED	20-JAN-1998
PROJ. NAME	MANCHESTER - WINHALL		
PROJ. NO.	STP-P 016-N16		
SHEET	13 OF 43	SHEETS	



646.4 DURABLE 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
LT C/L RT

675.50 REMOVING SIGNS
AS SHOWN - 2

675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 2

646.40 DURABLE 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

646.6 TEMPORARY 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
LT C/L RT

646.60 TEMPORARY 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

WINHALL:
STA. 1+660.000 - STA. 2+740.000 S - S

WINHALL:
STA. 1+660.000 - STA. 2+740.000 LT AND RT
STA. 1+660.000 - STA. 2+740.000 DASHED 3.6 m RT

WINHALL:
STA. 1+660.000 - STA. 2+740.000 LT AND RT
STA. 1+660.000 - STA. 2+740.000 DASHED 3.6 m RT

621.20 STEEL BEAM GUARDRAIL (TYPE IV)

WINHALL:
STA. 1+660.0 - STA. 1+681.7 RT (21.7 m)
STA. 2+232.4 - STA. 2+361.6 LT (129.2 m)
STA. 2+395.0 - STA. 2+657.2 RT (262.2 m)
STA. 2+395.7 - STA. 2+528.7 LT (133.0 m)
STA. 2+557.9 - STA. 2+645.3 LT (87.4 m)

CHANGE ELEVATION OF DI, CB, OR MH
REHABILITATION OF DI, CB, OR MH
SEE NOTE 8, SHEET 7 OF 43

STA. 2+418.2 RT
STA. 2+493.2 RT
STA. 2+567.2 RT

NOTE:
1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

616.35 TREATED TIMBER CURB
WINHALL:
STA. 2+398.0 - STA. 2+657.2 RT (259.2 m)

616.4 REMOVAL OF EXISTING CURB
WINHALL:
STA. 2+400.0 - STA. 2+667.8 RT (267.8 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)

WINHALL:
STA. 1+681.7 - STA. 1+693.1 RT
STA. 2+221.0 - STA. 2+232.4 LT
STA. 2+361.6 - STA. 2+373.0 LT
STA. 2+383.6 - STA. 2+395.0 RT
STA. 2+384.3 - STA. 2+395.7 LT
STA. 2+528.7 - STA. 2+540.1 LT
STA. 2+546.5 - STA. 2+557.9 LT
STA. 2+645.3 - STA. 2+656.7 LT
STA. 2+657.2 - STA. 2+668.6 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

WINHALL:
STA. 1+660.0 - STA. 1+682.8 RT (22.8 m)
STA. 2+221.0 - STA. 2+370.0 LT (149.0 m)
STA. 2+384.3 - STA. 2+536.5 LT (152.2 m)
STA. 2+400.0 - STA. 2+667.8 RT (267.8 m)
STA. 2+546.5 - STA. 2+654.4 LT (107.9 m)

SIGN LEGEND
R= REMOVE
S= SALVAGE
N= NEW
RET= RETAIN
B-B= BACK TO BACK
EXISTING= _____
NEW= _____

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/89b016/pb016.dgn		
	IPARM FILE	bb0161031	DATE PLOTTED	25-MAR-1998
PROJ. NAME	MANCHESTER - WINHALL			
PROJ. NO.	STP-F 016-116			
SHEET	14 OF 43	SHEETS		

DAFUM	
VERTICAL	N/A
HORIZONTAL	N/A

646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

WINHALL: LT C/L RT
 STA. 2+740.000 - STA. 3+820.000 S - S

646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

WINHALL: LT C/L RT
 STA. 2+740.000 - STA. 3+820.000 S - S

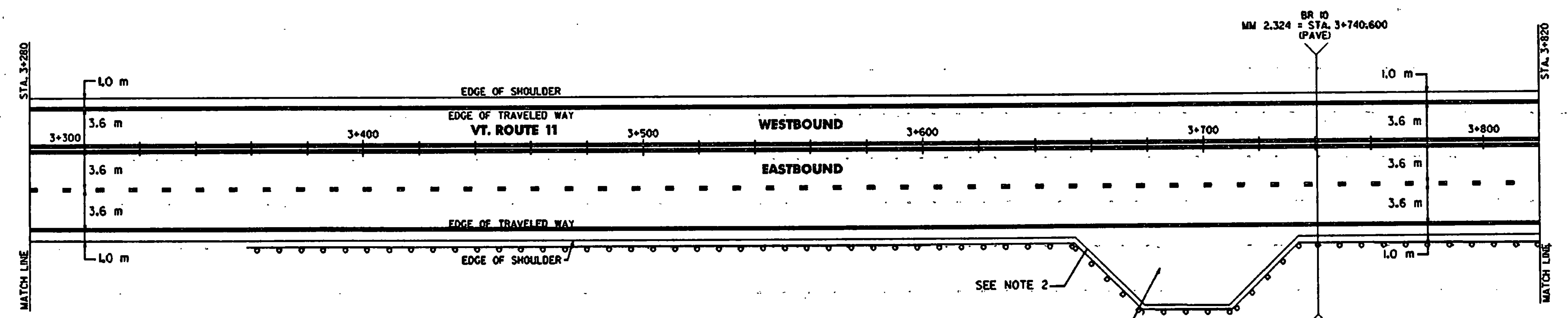
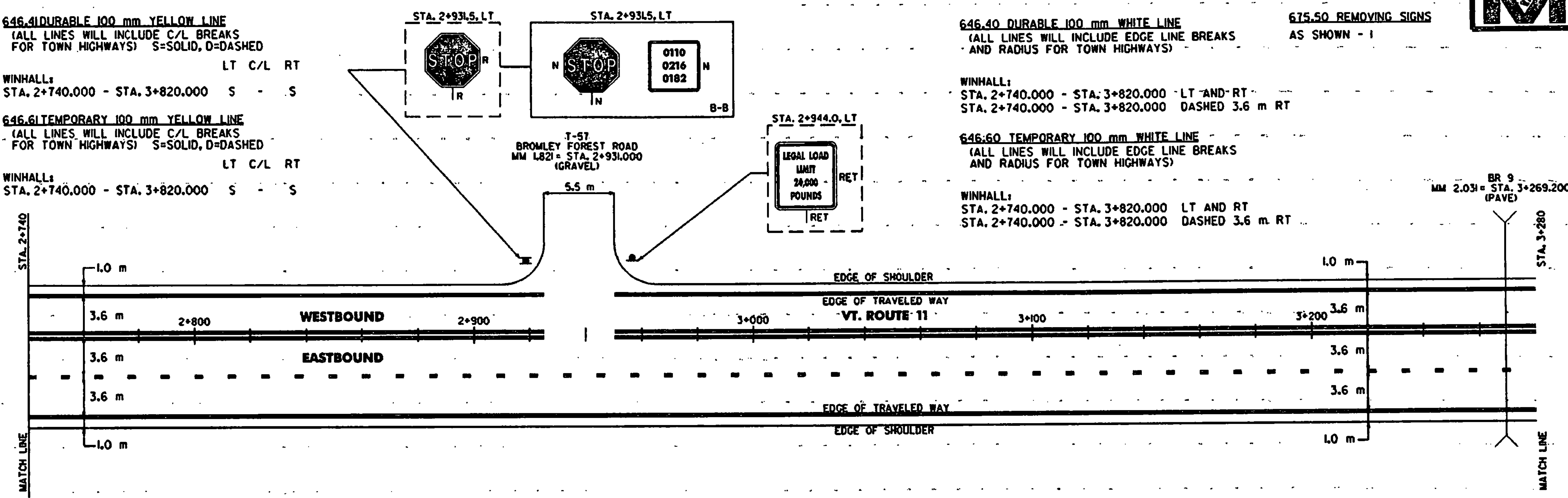
646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

WINHALL: LT AND RT
 STA. 2+740.000 - STA. 3+820.000 DASHED 3.6 m RT

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

WINHALL: LT AND RT
 STA. 2+740.000 - STA. 3+820.000 DASHED 3.6 m RT

675.50 REMOVING SIGNS
 AS SHOWN - I



621.20 STEEL BEAM GUARDRAIL (TYPE IV)
 WINHALL: STA. 3+365.6 - STA. 3+820.0 RT (462.0 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)
 WINHALL: STA. 3+355.8 - STA. 3+367.2 RT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 WINHALL: STA. 3+367.2 - STA. 3+656.2 RT (289 m)
 STA. 3+731.2 - STA. 3+820.0 RT (88.8 m)

PAVE PARKING AREA WITH 15 mm TYPE IV AND 50 mm OF TYPE III

SIGN LEGEND

R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

NOTES:

- PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- EIGHTEEN 3 m LONG CONCRETE BARRIERS ARE TO BE SAVED FOR THE VAOT.

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/89b016/pb016.dgn		
	IPARM. FILE	pb016041	DATE PLOTTED	20-11-1998
PROJ. NAME	MANCHESTER - WINHALL			
PROJ. NO.	STP-F 016-N16			
SHEET	15 OF 43	SHEETS		

DATUM

VERTICAL N/A

HORIZONTAL N/A

646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

WINHALL:
 STA. 3+820.000 - STA. 4+910.000 S - S

646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

WINHALL:
 STA. 3+820.000 - STA. 4+910.000 S - S

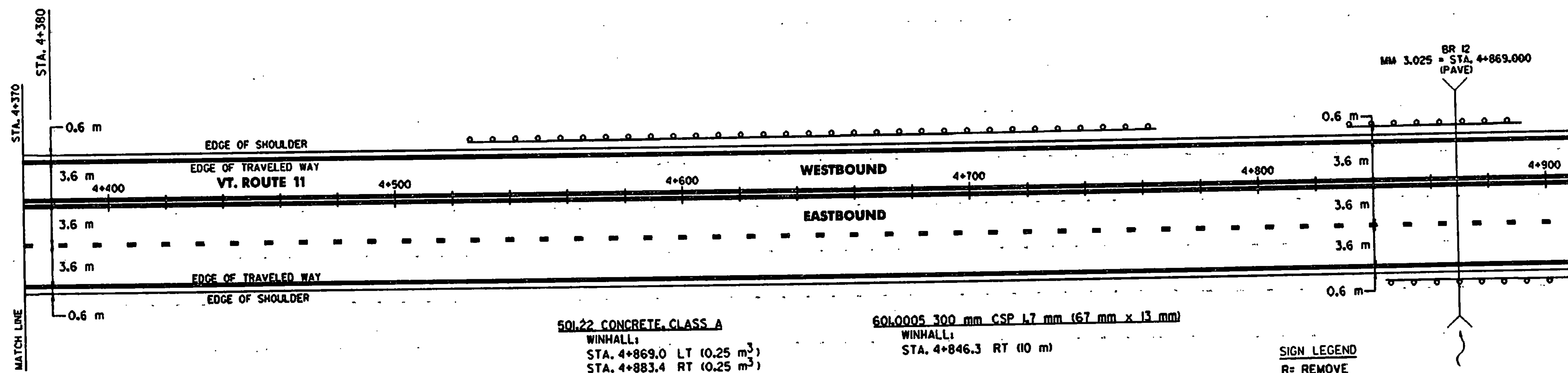
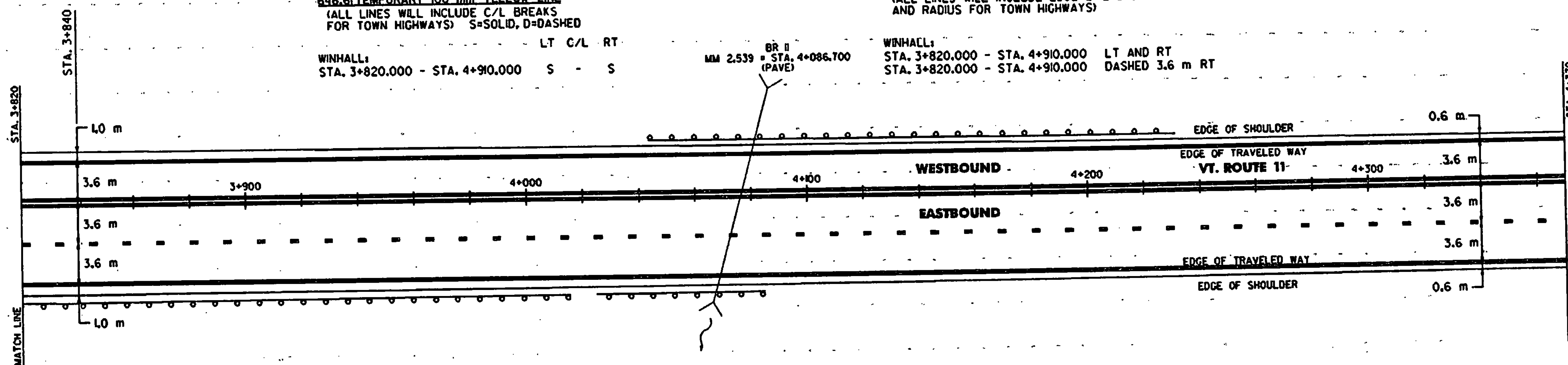
646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

WINHALL:
 STA. 3+820.000 - STA. 4+910.000 LT AND RT
 STA. 3+820.000 - STA. 4+910.000 DASHED 3.6 m RT

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

WINHALL:
 STA. 3+820.000 - STA. 4+910.000 LT AND RT
 STA. 3+820.000 - STA. 4+910.000 DASHED 3.6 m RT

BR 11
 MM 2.539 = STA. 4+086.700 (PAVE)



501.22 CONCRETE CLASS A

WINHALL:
 STA. 4+869.0 LT (0.25 m³)
 STA. 4+883.4 RT (0.25 m³)

601.0005 300 mm CSP L7 mm (67 mm x 13 mm)

WINHALL:
 STA. 4+846.3 RT (10 m)

SIGN LEGEND

- R= REMOVE
- S= SALVAGE
- N= NEW
- RET= RETAIN
- B-B= BACK TO BACK
- EXISTING=
- NEW=

621.20 STEEL BEAM GUARDRAIL (TYPE IV)

WINHALL:
 STA. 3+820.0 - STA. 4+004.0 RT (184 m)
 STA. 4+035.4 - STA. 4+073.4 RT (38 m)
 STA. 4+537.0 - STA. 4+753.6 LT (216.6 m)
 STA. 4+842.5 - STA. 4+880.5 LT (38 m)
 STA. 4+844.4 - STA. 4+901.4 RT (57.0 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)

WINHALL:
 STA. 4+004.0 - STA. 4+015.4 RT
 STA. 4+024.0 - STA. 4+035.4 RT
 STA. 4+045.4 - STA. 4+056.8 LT
 STA. 4+073.4 - STA. 4+084.8 RT
 STA. 4+220.2 - STA. 4+231.6 LT
 STA. 4+525.6 - STA. 4+537.0 LT
 STA. 4+753.6 - STA. 4+765.0 LT
 STA. 4+831.1 - STA. 4+842.5 LT
 STA. 4+880.5 - STA. 4+891.9 LT
 STA. 4+901.4 - STA. 4+912.8 RT

621.60 ANCHOR FOR STEEL BEAM RAIL

WINHALL:
 STA. 4+846.3 RT

529.20 PARTIAL REMOVAL OF STRUCTURE

WINHALL:
 STA. 4+869.0 LT (1EA)
 STA. 4+883.4 RT (1EA)

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL

WINHALL:
 STA. 3+820.0 - STA. 4+015.4 RT (195.4 m)
 STA. 4+035.4 - STA. 4+084.8 RT (49.4 m)
 STA. 4+056.8 - STA. 4+234.0 LT (177.2 m)
 STA. 4+537.0 - STA. 4+754.3 LT (217.3 m)
 STA. 4+843.0 - STA. 4+880.3 LT (37.3 m)
 STA. 4+863.8 - STA. 4+898.7 RT (34.9 m)

621.81 REMOVAL AND DISPOSAL OF GUIDEPOSTS

WINHALL:
 STA. 4+387.0 - STA. 4+434.0 LT (40 EA)
 STA. 4+536.0 LT (1EA)
 STA. 4+756.6 - STA. 4+843.0 LT (20 EA)

621.20 STEEL BEAM GUARDRAIL W/ 2.4 m STEEL POSTS (TYPE IV) (MOD.)

WINHALL:
 STA. 4+056.8 - STA. 4+220.2 LT (163.4 m)

DA-FUM	
VERTICAL	N/A
HORIZONTAL	N/A

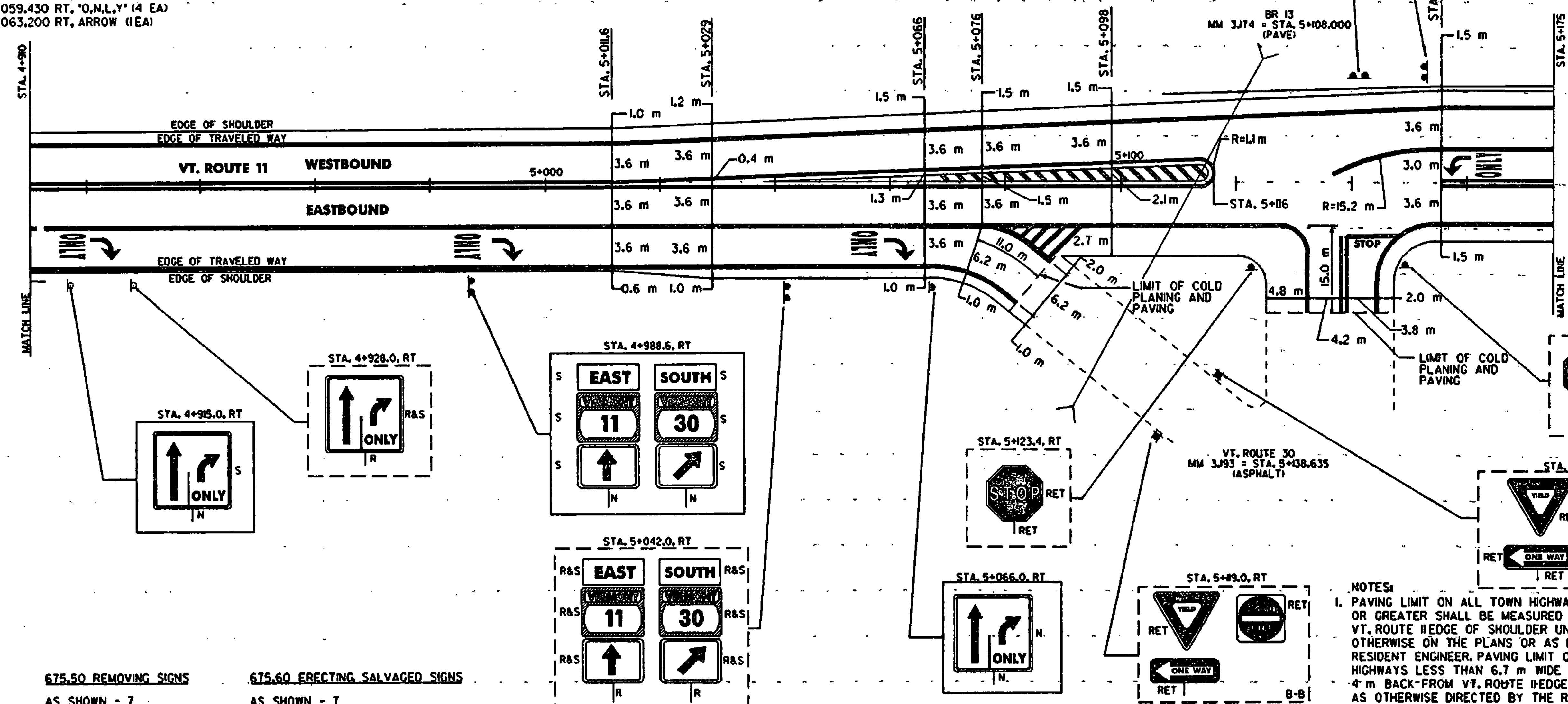
NOTE:

PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE II EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE II EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

PAVING PROJECT LAYOUT

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	K.H.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/89b016/pb016.dgn		
IPARM FILE	pb016105.1	DATE PLOTTED	20 JAN 1998
PROJ. NAME	MANCHESTER - WINHALL		
PROJ. NO.	STP-F 016-1167		
SHEET	16 OF 43	SHEETS	

- 646.4 DURABLE 100 mm YELLOW LINE**
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
WINHALL: LT C/L RT
STA. 4+910.000 - STA. 5+175.000 S - S
STA. 5+011.600 - STA. 5+116.000 LT (GORE EDGE LINES)
STA. 5+138.635 RT, DOUBLE SOLID, VT. ROUTE 30
- 646.61 TEMPORARY 100 mm YELLOW LINE**
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
WINHALL: LT C/L RT
STA. 4+910.000 - STA. 5+175.000 S - S
STA. 5+011.600 - STA. 5+116.000 LT (GORE EDGE LINES)
STA. 5+138.635 RT, DOUBLE SOLID, VT. ROUTE 30
- 646.50 DURABLE LETTER OR SYMBOL**
WINHALL:
STA. 4+916.270 RT, "O,N,L,Y" (4 EA) STA. 5+138.635 RT, VT. ROUTE 30, "S,T,O,P" (4 EA)
STA. 4+920.040 RT, ARROW (1EA) STA. 5+158.400 LT, ARROW (1EA)
STA. 4+988.630 RT, "O,N,L,Y" (4 EA) STA. 5+162.170 LT, "O,N,L,Y" (4 EA)
STA. 4+992.400 RT, ARROW (1EA)
STA. 5+059.430 RT, "O,N,L,Y" (4 EA)
STA. 5+063.200 RT, ARROW (1EA)
- 646.40 DURABLE 100 mm WHITE LINE**
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
WINHALL:
STA. 4+910.000 - STA. 5+175.000 LT AND RT
STA. 4+910.000 - STA. 4+915.000 DASHED 3.6 m RT
STA. 4+915.000 - STA. 5+060.760 SOLID 3.6 m RT
STA. 5+140.200 - STA. 5+175.000 SOLID 3.0 m LT
- 646.60 TEMPORARY 100 mm WHITE LINE**
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
WINHALL:
STA. 4+910.000 - STA. 5+175.000 LT AND RT
STA. 4+910.000 - STA. 4+915.000 DASHED 3.6 m RT
STA. 4+915.000 - STA. 5+060.760 SOLID 3.6 m RT
STA. 5+140.200 - STA. 5+175.000 SOLID 3.0 m LT
- 646.42 DURABLE 200 mm WHITE LINE**
WINHALL:
STA. 5+060.760 - STA. 5+095.000 3.6 m RT
STA. 5+076.000 - STA. 5+095.000 RT (DIAGONAL LINES)
- 646.43 DURABLE 200 mm YELLOW LINE**
WINHALL:
STA. 5+011.600 - STA. 5+116.000 LT (DIAGONAL LINES)
- 646.46 DURABLE 600 mm STOP BAR**
WINHALL:
STA. 5+135.635 RT, VT. ROUTE 30
- 646.62 TEMPORARY 200 mm WHITE LINE**
WINHALL:
STA. 5+060.760 - STA. 5+095.000 3.6 m RT
STA. 5+076.000 - STA. 5+095.000 RT (DIAGONAL LINES)
- 646.66 TEMPORARY 600 mm STOP BAR**
WINHALL:
STA. 5+135.635 RT, VT. ROUTE 30
- 646.70 TEMPORARY LETTER OR SYMBOL**
WINHALL:
STA. 4+920.040 RT, ARROW (1EA)
STA. 5+063.200 RT, ARROW (1EA)
STA. 5+138.635 RT, VT. ROUTE 30, "S,T,O,P" (4 EA)
STA. 5+158.400 LT, ARROW (1EA)



675.50 REMOVING SIGNS AS SHOWN - 7
675.60 ERECTING SALVAGED SIGNS AS SHOWN - 7

- NOTES:**
- PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE II EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE II EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
 - ALL DESTINATION SIGNS ASSOCIATED WITH THE VT. ROUTE II INTERSECTION WITH VT. ROUTE 30 SHALL BE RETAINED IN THE FIELD BUT WERE OMITTED ON THESE PLANS FOR CLARITY.

621.20 STEEL BEAM GUARDRAIL (TYPE IV)
WINHALL:
STA. 5+118.6 - STA. 5+175.0 LT (56.4 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)
WINHALL:
STA. 5+107.2 - STA. 5+118.6 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
WINHALL:
STA. 5+107.2 - STA. 5+175.0 LT (67.8 m)

621.8 REMOVAL AND DISPOSAL OF GUIDEPOSTS
WINHALL:
STA. 5+039.0 - STA. 5+105.5 LT (11 EA)

SIGN LEGEND
R= REMOVE
S= SALVAGE
N= NEW
RET= RETAIN
B-B= BACK TO BACK
EXISTING=

PAVING PROJECT LAYOUT

SURVEYED BY: N/A DATE: N/A
 DRAWN BY: K.H.D. DATE: 11/97
 SQUAD LEADER: T.P.K.
 DESIGN FILE NO.: /pave/89b016/pb016.dgn
 IPARM FILE: pb01606.r DATE PLOTTED: 20-JAN-1998
 PROJ. NAME: MANCHESTER - WINHALL
 PROJ. NO.: STP-F 016-1167
 SHEET 17 OF 43 SHEETS

DATUM

VERTICAL	N/A
HORIZONTAL	N/A



646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 WINHALL: LT C/L RT
 STA. 5+175.000 - STA. 5+399.349 S - S
 STA. 5+187.700 - STA. 5+392.000 LT (GORE EDGE LINES)

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 WINHALL:
 STA. 5+175.000 - STA. 5+399.349 LT AND RT
 STA. 5+175.000 - STA. 5+187.700 SOLID 3.0 m LT
 STA. 5+187.700 - STA. 5+248.000 DOTTED 3.0 m LT

646.43 DURABLE 200 mm YELLOW LINE
 WINHALL:
 STA. 5+187.700 - STA. 5+392.000 LT (DIAGONAL LINES)
646.50 DURABLE LETTER OR SYMBOL
 WINHALL:
 STA. 5+182.660 LT, ARROW (1EA)
 STA. 5+186.430 LT, "O.N.L.Y." (4 EA)

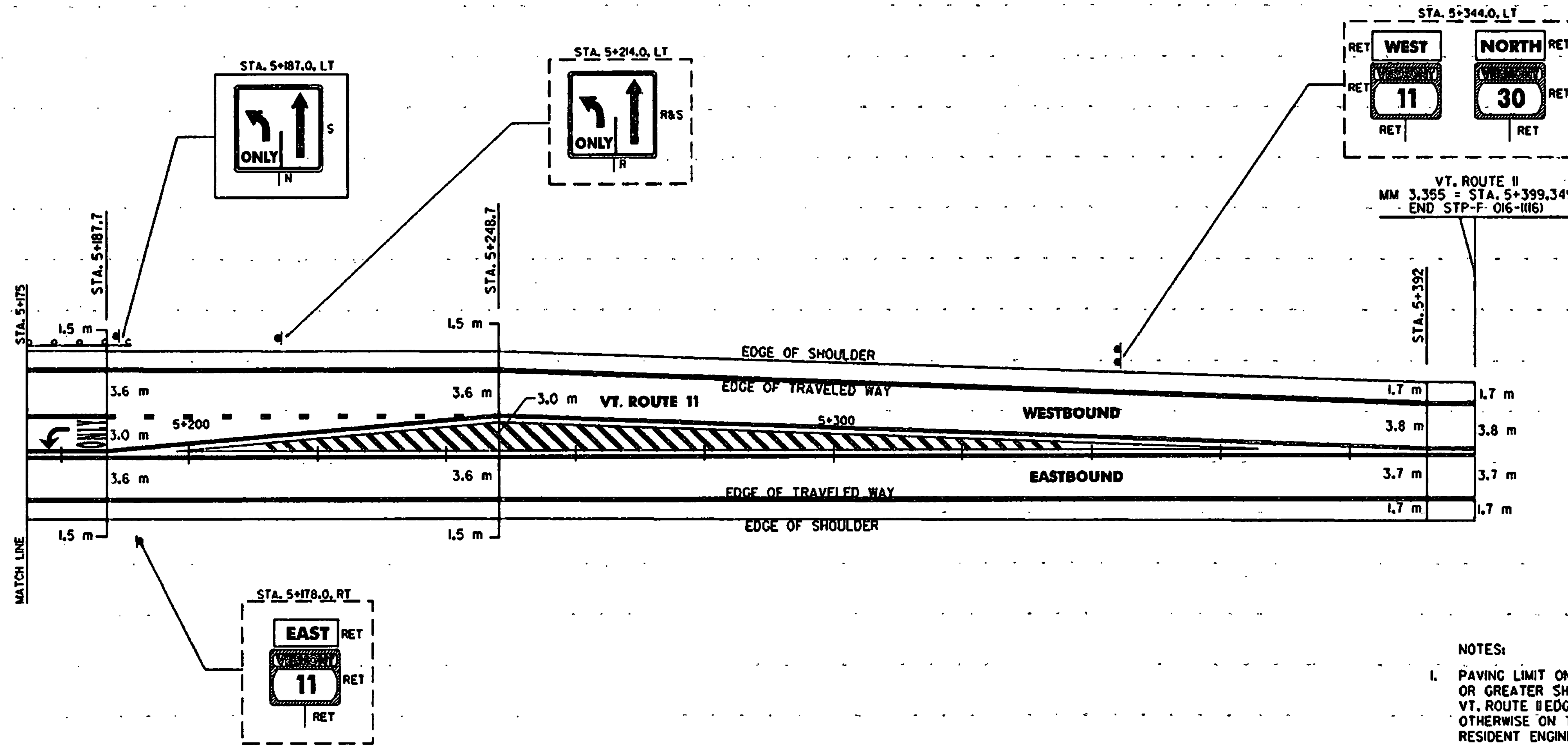
675.50 REMOVING SIGNS
 AS SHOWN - 1

675.60. ERECTING SALVAGED SIGNS
 AS SHOWN - 1

646.61 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 WINHALL: LT C/L RT
 STA. 5+175.000 - STA. 5+399.349 S - S
 STA. 5+187.700 - STA. 5+392.000 LT (GORE EDGE LINES)

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 WINHALL:
 STA. 5+175.000 - STA. 5+399.349 LT AND RT
 STA. 5+175.000 - STA. 5+187.700 SOLID 3.0 m LT
 STA. 5+187.700 - STA. 5+248.000 DOTTED 3.0 m LT

646.70 TEMPORARY LETTER OR SYMBOL
 WINHALL:
 STA. 5+182.660 LT, ARROW (1EA)



VT. ROUTE 11
 MM 3.355 = STA. 5+399.349
 END STP-F-016-1116

NOTES:

- PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 15 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 4 m BACK FROM VT. ROUTE 11 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
- ALL DESTINATION SIGNS ASSOCIATED WITH THE VT. ROUTE 11 INTERSECTION WITH VT. ROUTE 30 SHALL BE RETAINED IN THE FIELD BUT WERE OMITTED ON THESE PLANS FOR CLARITY.

621.20 STEEL BEAM GUARDRAIL (TYPE IV)
 WINHALL:
 STA. 5+175.0 - STA. 5+179.4 LT (4.4 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL (TYPE IV)
 WINHALL:
 STA. 5+179.4 - STA. 5+190.8 LT

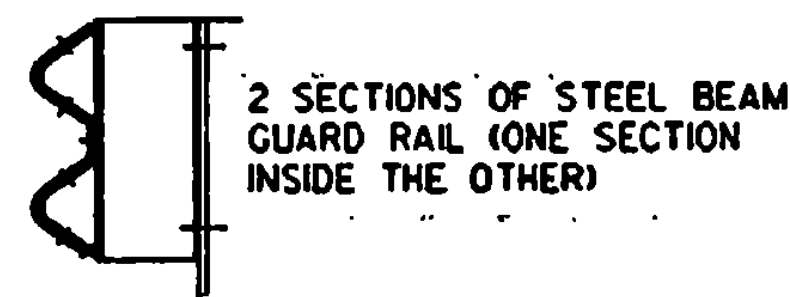
621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 WINHALL:
 STA. 5+175.0 - STA. 5+178.9 LT (3.9 m)

621.81 REMOVAL AND DISPOSAL OF GUIDEPOSTS
 WINHALL:
 STA. 5+180.0 LT (1EA)

SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	K.H.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	pava/89b016/pb016.dgn		
	IPARM FILE	05016107	DATE PLOTTED	20-JAN-1998
PROJ. NAME	MANCHESTER - WINHALL			
PROJ. NO.	STP-F-016-1116			
SHEET	18 OF 43	SHEETS		

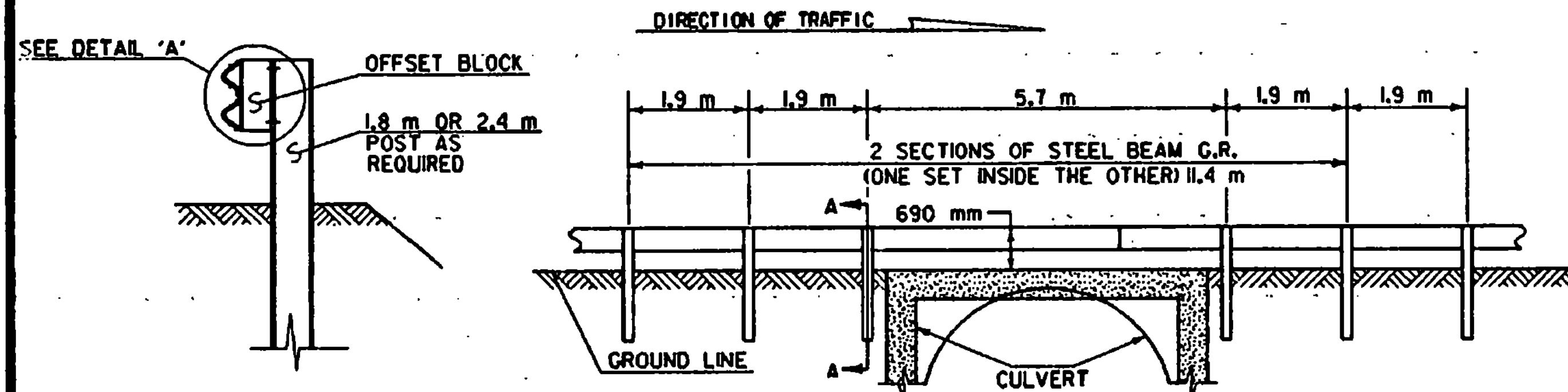
DATUM
 VERTICAL: N/A
 HORIZONTAL: N/A



DETAIL A
NOT TO SCALE

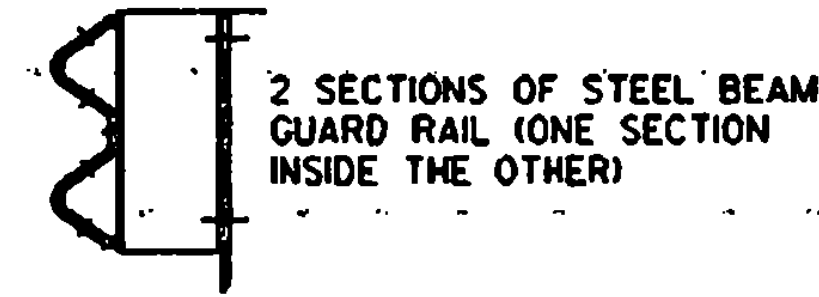
NOTES

1. SEE STANDARD G-1 & G-1D FOR STEEL BEAM GUARD RAIL DETAILS.
2. THIS WORK SHALL BE PAID UNDER ITEM 621.20, STEEL BEAM GUARD RAIL (TYPE IV) (MOD.), AT A PAY FACTOR OF 1.0.
3. THIS DETAIL TO BE USED AS INDICATED ON THE ITEM DETAIL SUMMARY SHEET OR AS DIRECTED BY THE ENGINEER.



SECTION A-A
NOT TO SCALE

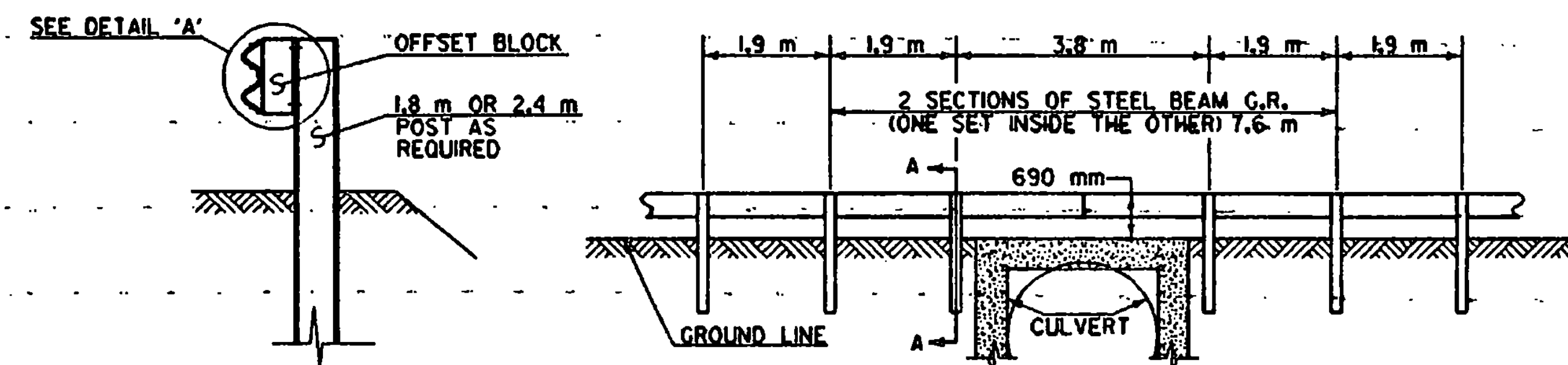
DETAIL OF STEEL BEAM GUARD RAIL AT LARGE CULVERTS
BR 6
BR 12
NOT TO SCALE



DETAIL A
NOT TO SCALE

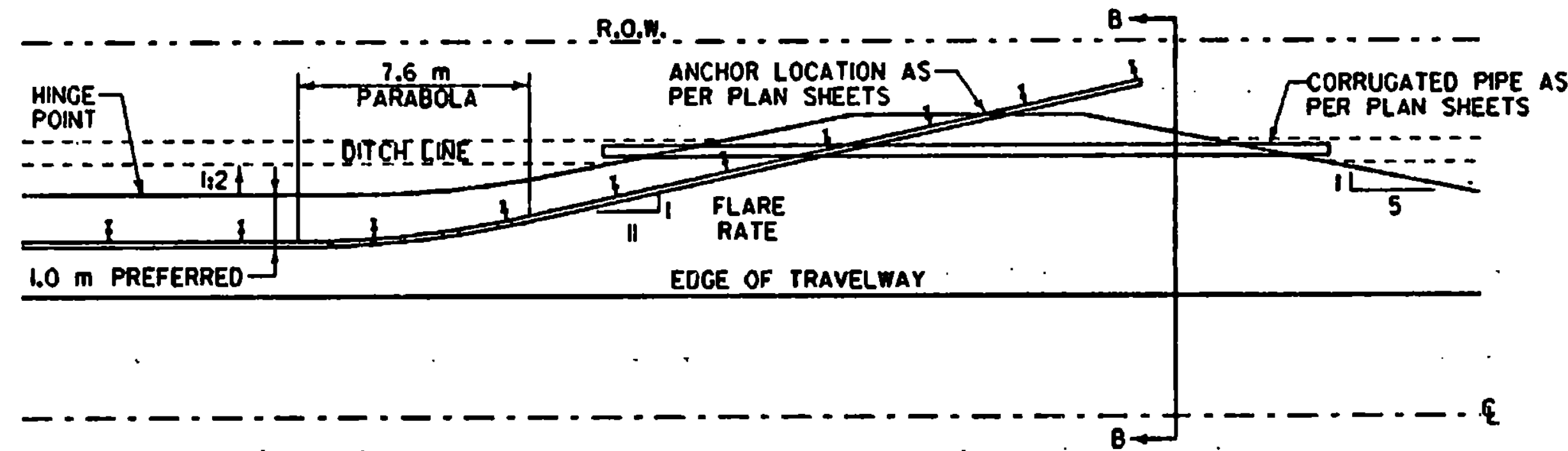
NOTES

1. SEE STANDARD G-1 & G-1D FOR STEEL BEAM GUARD RAIL DETAILS.
2. THIS WORK SHALL BE PAID UNDER ITEM 621.20, STEEL BEAM GUARD RAIL (TYPE IV) (MOD.), AT A PAY FACTOR OF 1.0.
3. THIS DETAIL TO BE USED AS INDICATED ON THE ITEM DETAIL SUMMARY SHEET OR AS DIRECTED BY THE ENGINEER.

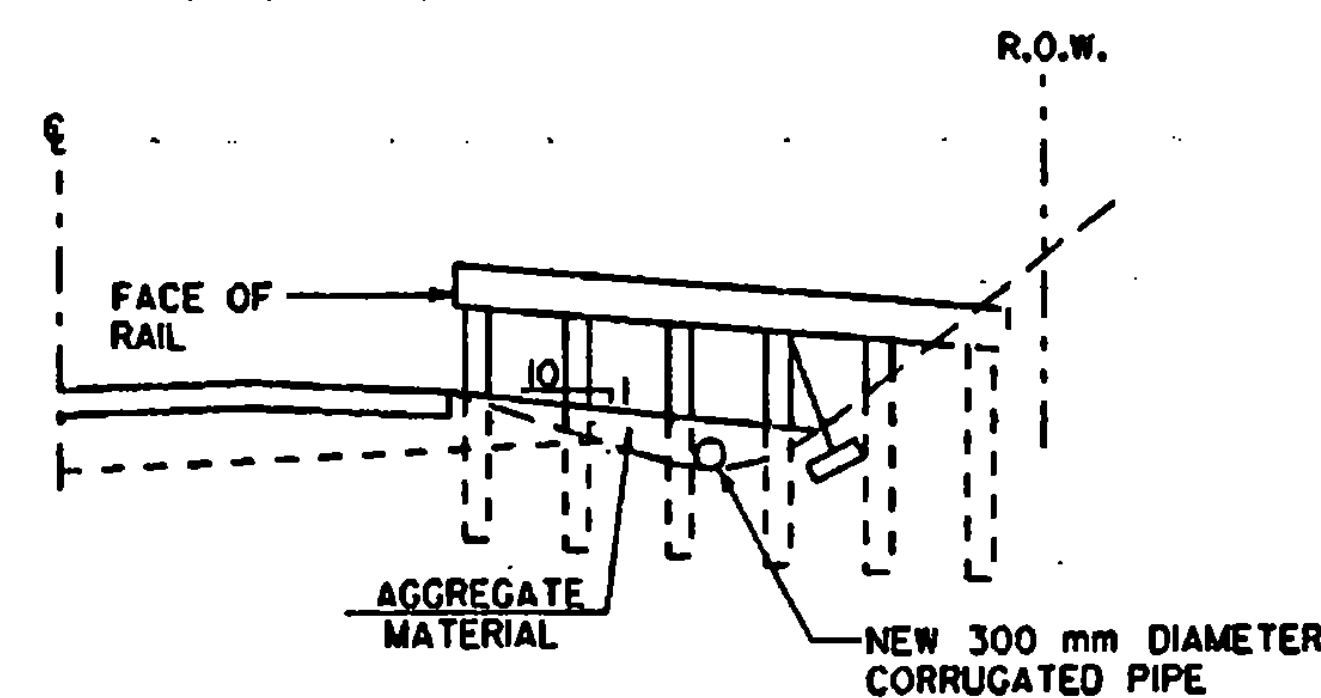


SECTION A-A
NOT TO SCALE

DETAIL OF STEEL BEAM GUARD RAIL AT SMALL CULVERTS
BR 11
NOT TO SCALE



DETAIL FOR BURIED GUARD RAIL ENDS INTO BACKSLOPES
NOT TO SCALE



SECTION B-B
NOT TO SCALE

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

**BRIDGE
DETAIL
SHEET**

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	T.J.B.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/89b016/pb16.dwg		
IPARM FILE	pb016dt2.i	DATE PLOTTED	20-08-1998
PROJ. NAME	MANCHESTER - WINHALL		
PROJ. NO.	SPP-F-016-M66		
SHEET 19 OF 43		SHEETS	

TRAFFIC SIGN SUMMARY SHEET

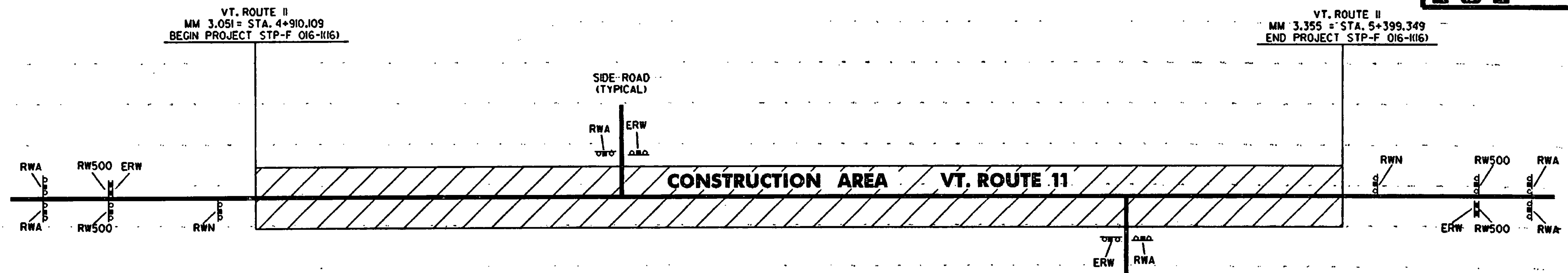


KILOMETER MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS		NEW & SALVAGED SIGNS				EXISTING POSTS	NO. OF POSTS	NEW SIGN POSTS												REMARKS	SIGN DETAIL							
		E	A	WIDTH (mm)	HEIGHT (mm)	"A"	"B"			SALV SIGN	SALV TIS	FLANGED CHANNEL			SQUARE STEEL (mm)			TUBULAR ALUMINUM (mm)			TUBULAR STEEL (mm)			W-SHAPE STEEL		DETAIL ON SHEET NUMBER	STD. SHEET NUMBER			
												.17	3.0	4.5	44	50	63	75	100	100 MOD	75		89	100	125			600	750	WEIGHT
OPTION ITEMS																														
MANCHESTER: 5+010.0, LT		1	150	750	200	0.56	0.03		1																				BACK-TO-BACK	E-143 E-138
5+022.0, LT		1	600	750		0.45			1																					E-141
WINHALL: 0+974.5, LT		1	300	900		0.27			1																				22	
1+037.2, LT		1	600	750		0.45			1																					E-141
1+983.5, LT									1																				SALVAGED SIGN TO BE MOUNTED ON NEW POST	
2+376.5, LT									1																				SALVAGED SIGN TO BE MOUNTED ON NEW POST	
2+931.5, LT		1	150	750	200	0.56	0.03		1																				BACK-TO-BACK	E-143 E-138
4+915.0, RT									1																				SALVAGED SIGN TO BE MOUNTED ON NEW POST	

FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE TRAFFIC & SAFETY DIVISION'S "SIGN POST DESIGN GUIDELINE."

TOTALS	2.35		3			36.8	36.8																							
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PROJECT: MANCHESTER - WINHALL	PROJECT NO. # STP-F 016-1(16)
DESIGN FILE NAME: /pave/89b016/pb016.dgn	PLOT DATE: 20-JAN-1998
IPARM FILE NAME: pb016t.sl	SURVEY DATE: N/A
SURVEYED BY: N/A	DRAWN BY: E.C.D.
SQUAD LEADER: T.P.K.	SHEET: 20 OF 43

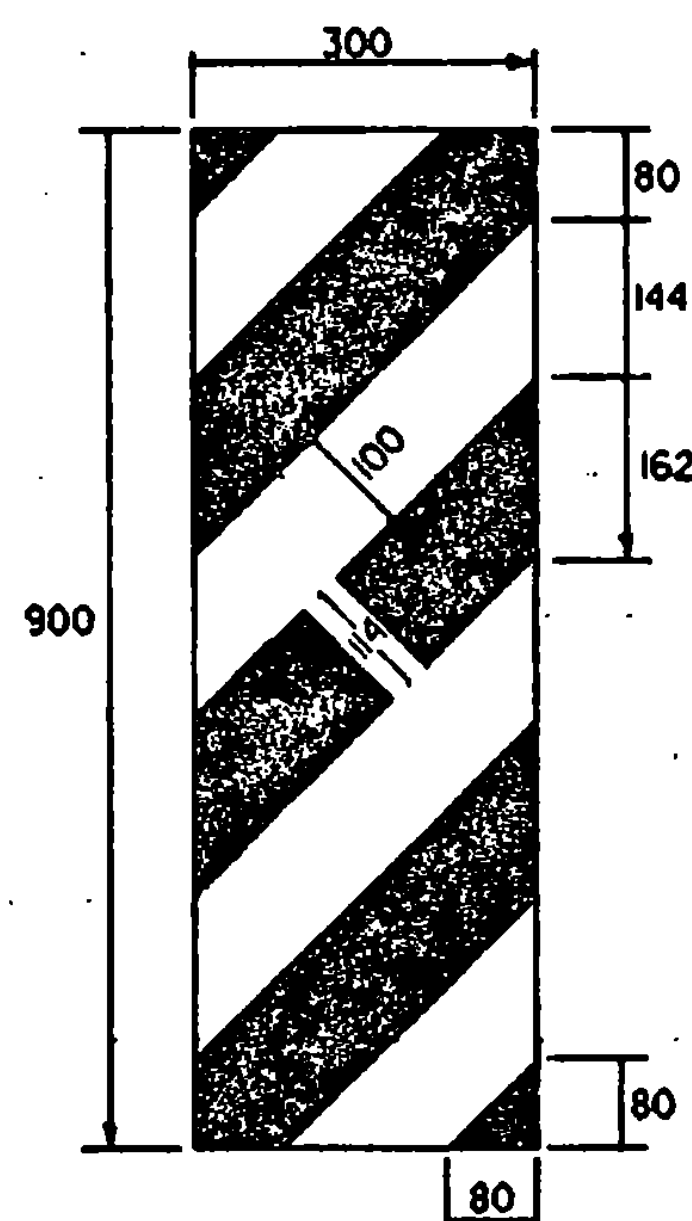


LIST OF TOWN HIGHWAYS FOR CONSTRUCTION SIGNS			
TOWN HIGHWAY NUMBER	ROAD WORK AHEAD	END ROAD WORK	OTHER
BEGIN PROJECT	2	1	2 - RW500 1 - RWN
T-72	1	1	
WINHALL STATE HIGHWAY	1	1	
T-42	1	1	
T-33	1	1	
T-57	1	1	
VT. ROUTE 30	1	2	
END PROJECT	2	1	2 - RW500 1 - RWN
TOTAL	10	9	6

SIGN LEGEND

- RWA = ROAD WORK AHEAD
- RW500 = ROAD WORK 500 FEET
- ERW = END ROAD WORK
- SRWA = SIDE ROAD WORK AHEAD
- SRW500 = SIDE ROAD WORK 500 FEET
- RWN = ROAD WORK NEXT 3 1/2 MILES

CONSTRUCTION APPROACH SIGNING
SEE STD. E-100 AND E-100A FOR ADDITIONAL SIGN PLACEMENT



COLOR: BLACK BORDER & TEXT
YELLOW BACKGROUND
MATERIAL: PER VAOT STANDARD E-150
SIGN - VT. ROUTE 11
STA. 0+974.5, LT

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

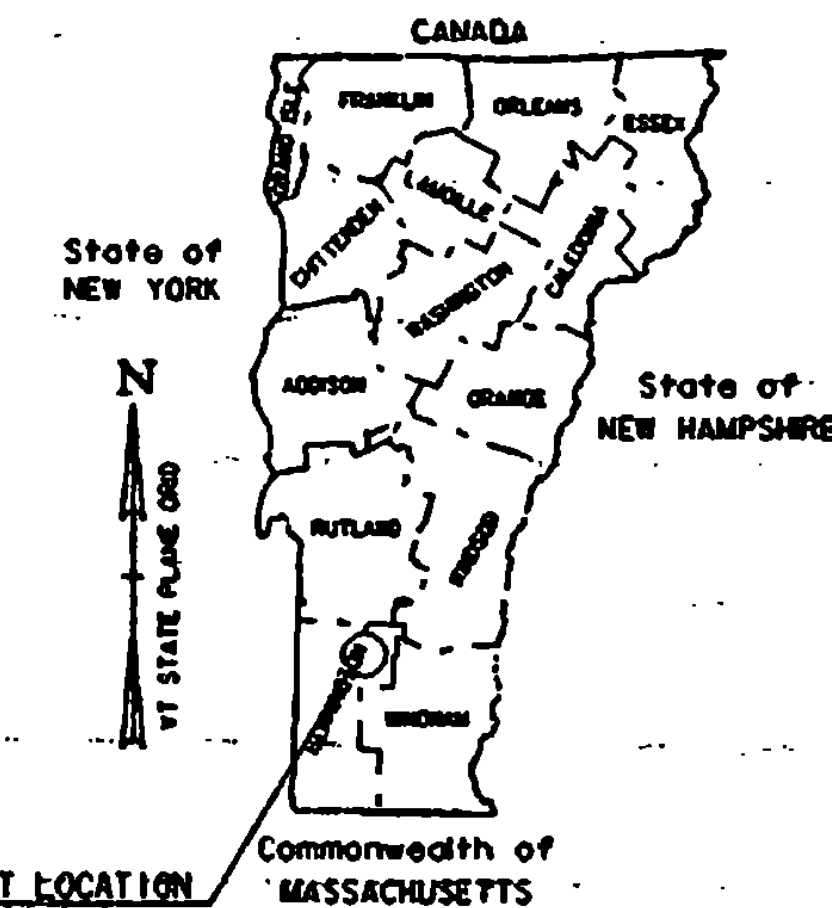
ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

CONSTRUCTION APPROACH SIGNING & TRAFFIC SIGN DETAIL SHEET	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pava/89b016/pb016.dgn		
	IPARM FILE	pb016cs1	DATE PLOTTED	20-JAN-1998
PROJ. NAME	MANCHESTER - WINHALL			
PROJ. NO.	STP-F 016-1116			
SHEET 22	OF 43	SHEETS		

STATE OF VERMONT
AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT
TOWN OF DORSET
COUNTY OF BENNINGTON
VT. ROUTE 30



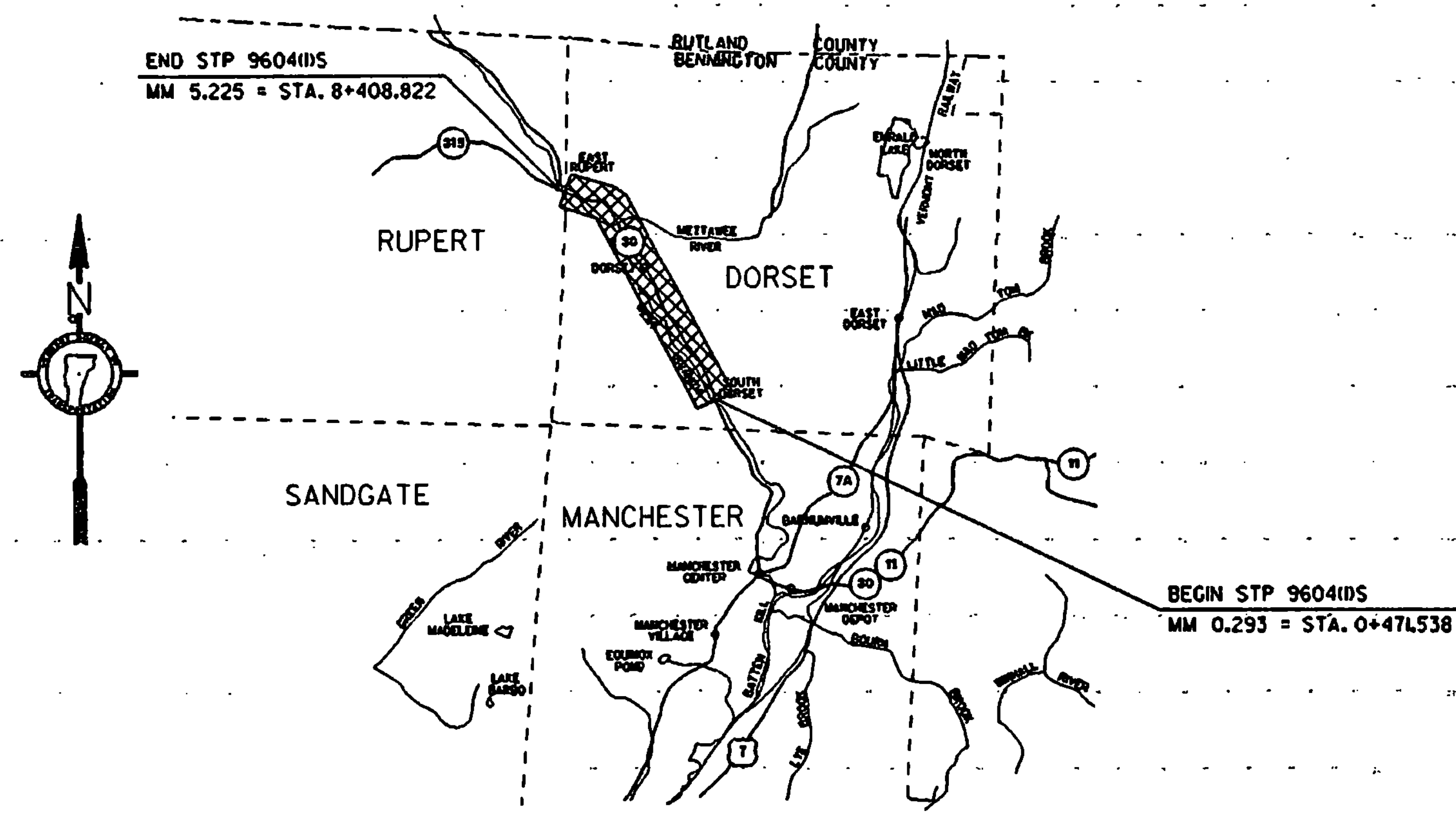
BEGINNING IN DORSET AT A POINT 471.538 m NORTH OF THE MANCHESTER / DORSET TOWN LINE AT MILE MARKER 0.293 = STA. 0+471.538 AND EXTENDING NORTHERLY ALONG VT. ROUTE 30 FOR A DISTANCE OF 7937.284 m (4.932 MILES) TO THE DORSET/RUPERT TOWN LINE AT MILE MARKER 5.225 = STA. 8+408.822.

LENGTH OF ROADWAY 7937.284 m (4.932 MILES)
LENGTH OF PROJECT 7937.284 m (4.932 MILES)

WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES COLD PLANING, RESURFACING OF THE EXISTING HIGHWAY WITH A SHIM / LEVELING COURSE AND WEARING COURSE, NEW PAVEMENT MARKINGS, DRAINAGE IMPROVEMENTS, GUARDRAIL IMPROVEMENTS AND INCIDENTAL ITEMS.

TRAFFIC DATA

VT. ROUTE 30 (MANCHESTER/DORSET T/L - TH-3)	
1998 ADT	= 6240
1998 DHV	= 810
2008 ADT	= 7360
2008 DHV	= 915
1998 ~ 2008 CUM. ESALS	= 1,066,000
VT. ROUTE 30 (TH-3 - TH-2/TH-36)	
1998 ADT	= 5810
1998 DHV	= 760
2008 ADT	= 6860
2008 DHV	= 870
1998 ~ 2008 CUM. ESALS	= 993,000
VT. ROUTE 30 (TH-36 - DORSET/RUPERT T/L)	
1998 ADT	= 3580
1998 DHV	= 500
2008 ADT	= 4230
2008 DHV	= 580
1998 ~ 2008 CUM. ESALS	= 683,000



UNLESS OTHERWISE NOTED, ALL DRAWINGS AND DETAILS OF THE PROJECT PLANS ARE NOT TO SCALE
RIGHT OF WAY LIMITS, IF APPLICABLE, ARE PROVIDED SOLELY FOR THE CONVENIENCE OF THE STATE AND ITS CONTRACTOR DURING THE COURSE OF THIS PAVING PROJECT. ANY REFERENCES TO OFFSETS ON THESE PLANS ARE APPROXIMATE AND SHOULD NOT BE RELIED UPON FOR ANY OTHER PURPOSES

CONVENTIONAL SIGNS

COUNTY LINE	---
TOWN LINE	- - - -
LIMITS OF ACCESS	—○—
POINT OF ACCESS	—○— X
FENCE LINE	—x—x—
STONE WALL	—o—o—o—o—
TRAVELED WAY	— — —
GUARD RAIL	—o—o—o—o—
RAILROAD	—+—+—+—+—
SURVEY LINE	—+—+—+—+—
CULVERT	—x—x—x—x—
POWER POLE	—○—
TELEPHONE POLE	—◇—
TREES	—●—
CONTROL OF ACCESS	—//—
PROPERTY LINE	—- - - -
R.O.W. TAKING LINE	—SR—
SLOPE RIGHTS	—○—
TOP OF CUT	—△—
TOE OF SLOPE	—○—

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

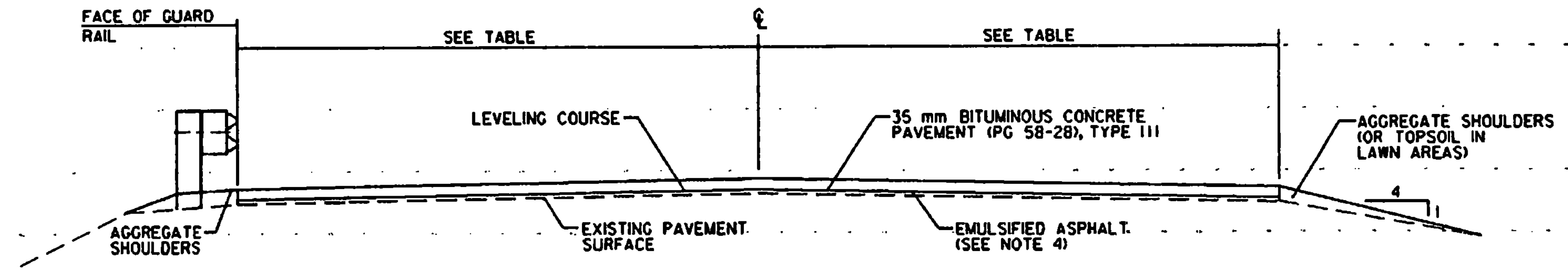
THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF CONSTRUCTION AND MAINTENANCE.
CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1995, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON AUGUST 24, 1995 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

Metric

APPROVED	DATE
DIRECTOR OF CONSTRUCTION AND MAINTENANCE	
DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED	DATE
DIVISION ADMINISTRATOR	
PROJECT DORSET STP 9604(i)s	
SHEET 26 OF 43 SHEETS	

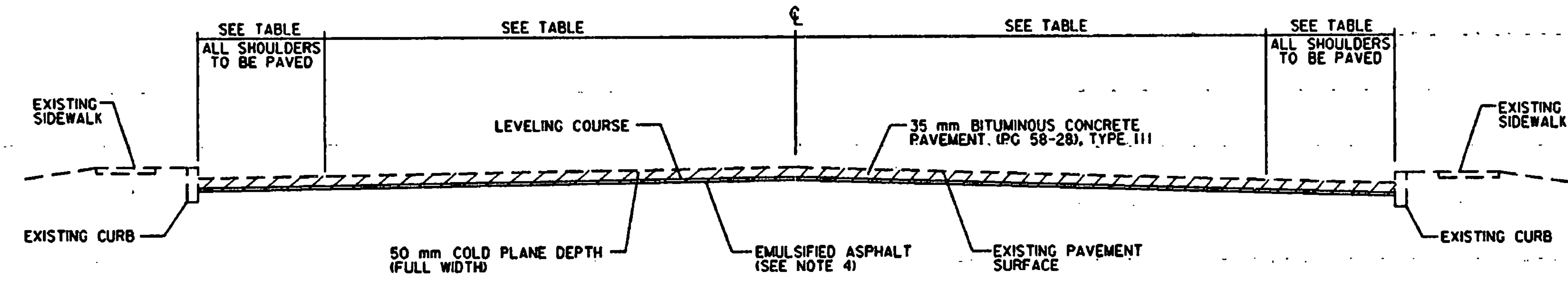
NOTES

1. THE WEARING COURSE SHALL BE TYPE III BITUMINOUS CONCRETE PAVEMENT. THE LEVELING COURSE SHALL BE TYPE IV UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. ALL ASPHALT CEMENT USED IN THE BITUMINOUS PAVEMENT SHALL BE PG 58-28.
2. EXISTING SHOULDER MATERIAL DEEMED UNSUITABLE BY THE RESIDENT ENGINEER SHALL BE EXCAVATED TO A DEPTH OF 75 mm OR AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATED MATERIAL SHALL BE SPREAD ON THE ADJACENT SLOPES OR REMOVED FROM THE PROJECT, AS DIRECTED BY THE RESIDENT ENGINEER. EXCAVATION WILL BE PAID FOR AS ALL-PURPOSE EXCAVATOR OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH SUBBASE OF CRUSHED GRAVEL (FINE GRADED).
3. 1.0 m OF BACKING IS REQUIRED BEHIND THE FACE OF GUARDRAIL WITH 1.8 m POSTS. IF THIS CANNOT BE OBTAINED THEN 2.4 m POSTS SHALL BE USED.
4. EMULSIFIED ASPHALT SHALL BE APPLIED ON ALL EXISTING PAVEMENT SURFACES, ON ALL COLD PLANED SURFACES AND BETWEEN ALL COURSES OF PAVEMENT AT THE RATE OF 0.07 L/m² OR AS DIRECTED BY THE RESIDENT ENGINEER.
5. BITUMINOUS CONCRETE PAVEMENT TOLERANCE = ±5 mm (TOTAL PAVEMENT THICKNESS EXCLUDING LEVELING).
6. ITEM 604.40, 604.412, 604.415 & 604.418 ARE ESTIMATED QUANTITIES AND SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER.
7. ALL DRIVEWAYS AND MAILBOX TURNOUTS SHALL RECEIVE A PAVED APRON AS DIRECTED BY THE RESIDENT ENGINEER.
8. ITEM 203.99, SHOULDER BERM REMOVAL, HAS BEEN ADDED TO REMOVE EXCESS GRANULAR MATERIAL LOCATED NEAR EXISTING GUARDRAIL TO REMAIN TO ALLOW THE SHOULDER TO DRAIN.
9. AN ESTIMATED QUANTITY OF EARTH BORROW HAS BEEN INCLUDED FOR THE PROVISION OF CONSTRUCTING MELT FLARES WHICH SHALL BE CAPPED WITH AN ESTIMATED 75 mm DEPTH OF AGGREGATE SHOULDER MATERIAL, UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER. THE QUANTITIES INCLUDED REFLECT 20 m³ OF EARTH BORROW AND 5 TONS OF AGGREGATE SHOULDER MATERIAL FOR EACH GUARDRAIL TERMINAL.



OVERLAY TYPICAL SECTION

VT. ROUTE 30 STA. 0+471.538 TO STA. 5+260.000
 VT. ROUTE 30 STA. 5+460.000 TO STA. 6+518.000
 VT. ROUTE 30 STA. 6+535.300 TO STA. 8+145.800
 VT. ROUTE 30 STA. 8+155.800 TO STA. 8+408.822



COLD PLANE TYPICAL SECTION - CURBED

VT. ROUTE 30 STA. 5+260.000 TO STA. 5+460.000

CONSERVATION SEED MIX

RURAL AREA - SEED MIXTURE				
% WT.	kg/ha.	NAME	PUR.%	GERM.%
37.4	26.0	CREeping RED FESCUE	98	85
37.4	26.0	TALL FESCUE	95	90
5.7	4.0	RED TOP	95	90
14.30	10.0	BIRDFOOT TREFOIL	98	85
5.7	4.0	ANNUAL RYEGRASS	95	85
100.0	70.0			

SEED MIXTURE:
 SHALL NOT HAVE A WEED CONTENT EXCEEDING 0.40% BY WEIGHT AND SHALL BE FREE OF ALL NOXIOUS WEED SEED.

SEED:
 TO BE APPLIED PER SEEDING FORMULA DIRECTED BY THE RESIDENT ENGINEER.

FERTILIZER:
 FORMULA 10-20-10 TO BE USED WITH SEED, APPLIED AT THE RATE OF 560 kg/ha (HYDRO SEEDERS MAY USE 19-19-19 FORMULA)

AGRICULTURAL LIMESTONE:
 TO BE APPLIED AT THE RATE OF 4500 kg/ha OR AS DIRECTED BY THE RESIDENT ENGINEER.

HAY MULCH:
 TO BE APPLIED ON EARTH SLOPES AT THE RATE OF 4500 kg/ha, OR AS DIRECTED BY THE RESIDENT ENGINEER.

TOPSOIL:
 TO BE USED WITH SEED AS INDICATED ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER.

MARKER POSTS:
 TO BE PLACED AS DIRECTED BY THE RESIDENT ENGINEER.

PROJECT PAVING LIMITS

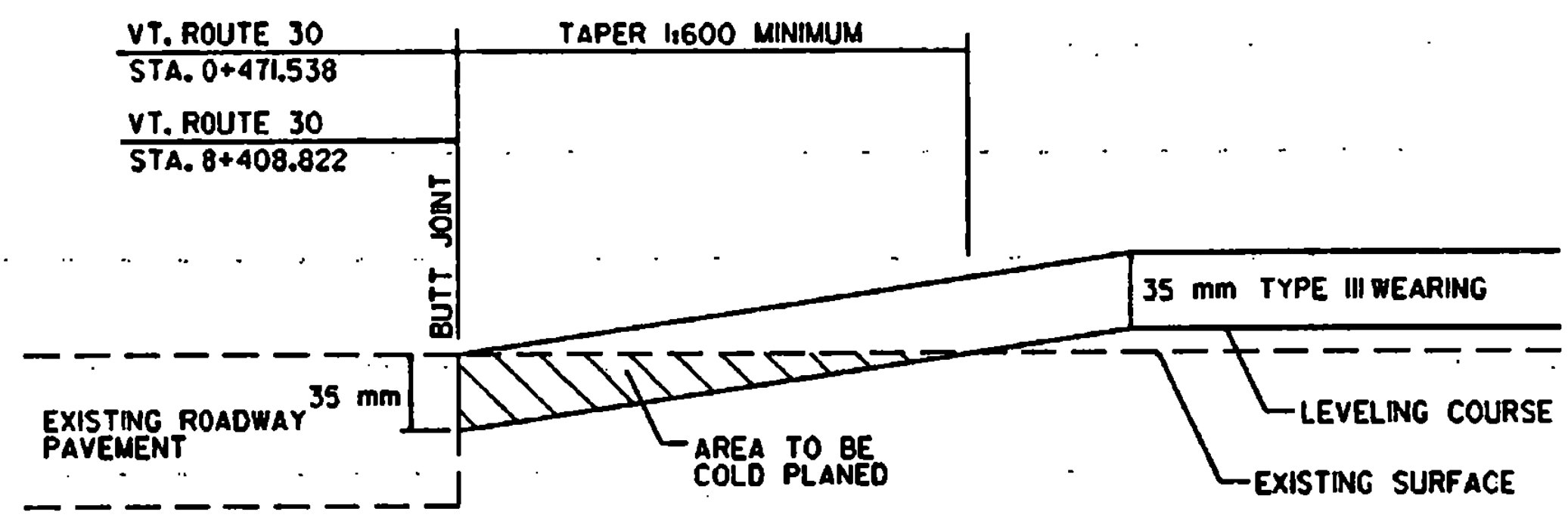
TOWN & ROUTE	BEGIN STATION	END STATION	LANE TYPICAL	WEARING DEPTH	LEVELING	NOTES
DORSET VT. ROUTE 30	0+471.538	0+960.000	2.4 m - 3.6 m - 3.6 m - 2.4 m	35 mm	219	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	0+960.000	1+080.000	2.1 m - 3.6 m - 3.6 m - 2.1 m	35 mm	49	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	1+080.000	5+260.000	0.9 m - 3.3 m - 3.3 m - 0.9 m	35 mm	1250	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	5+260.000	5+460.000	VARIES- SEE LAYOUT SHEETS	35 mm	60	COLD PLANE 50 mm, LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	5+460.000	5+530.000	0.9 m - 3.3 m - 3.3 m - 0.9 m	35 mm	22	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	5+530.000	5+700.000	0.3 m - 3.3 m - 3.3 m - 0.3 m	35 mm	44	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	5+700.000	6+000.000	0.0 m - 3.3 m - 3.3 m - 0.0 m	35 mm	70	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	6+000.000	6+518.000	0.3 m - 3.3 m - 3.3 m - 0.3 m	35 mm	133	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	6+518.000	6+535.300	1.1 m - 3.3 m - 3.3 m - 1.2 m	-	-	BR 58 - DO NOT PAVE
DORSET VT. ROUTE 30	6+535.300	8+145.800	0.9 m - 3.3 m - 3.3 m - 0.9 m	35 mm	482	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III
DORSET VT. ROUTE 30	8+145.800	8+155.800	0.9 m - 3.3 m - 3.3 m - 0.9 m	-	-	BR 60 - DO NOT PAVE
DORSET VT. ROUTE 30	8+155.800	8+408.822	0.9 m - 3.3 m - 3.3 m - 0.9 m	35 mm	76	LEVEL WITH 15 mm TYPE IV & PAVE WITH 35 mm TYPE III

DATUM

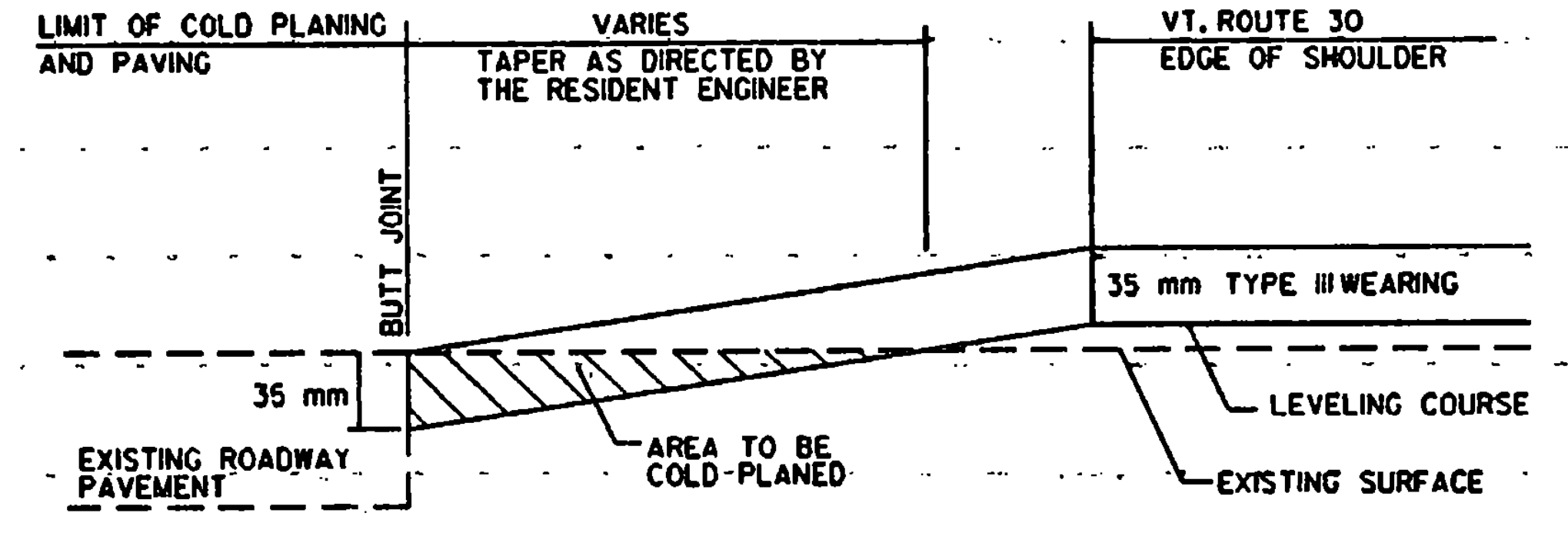
VERTICAL	N/A
HORIZONTAL	N/A

PROJECT TYPICAL SHEET

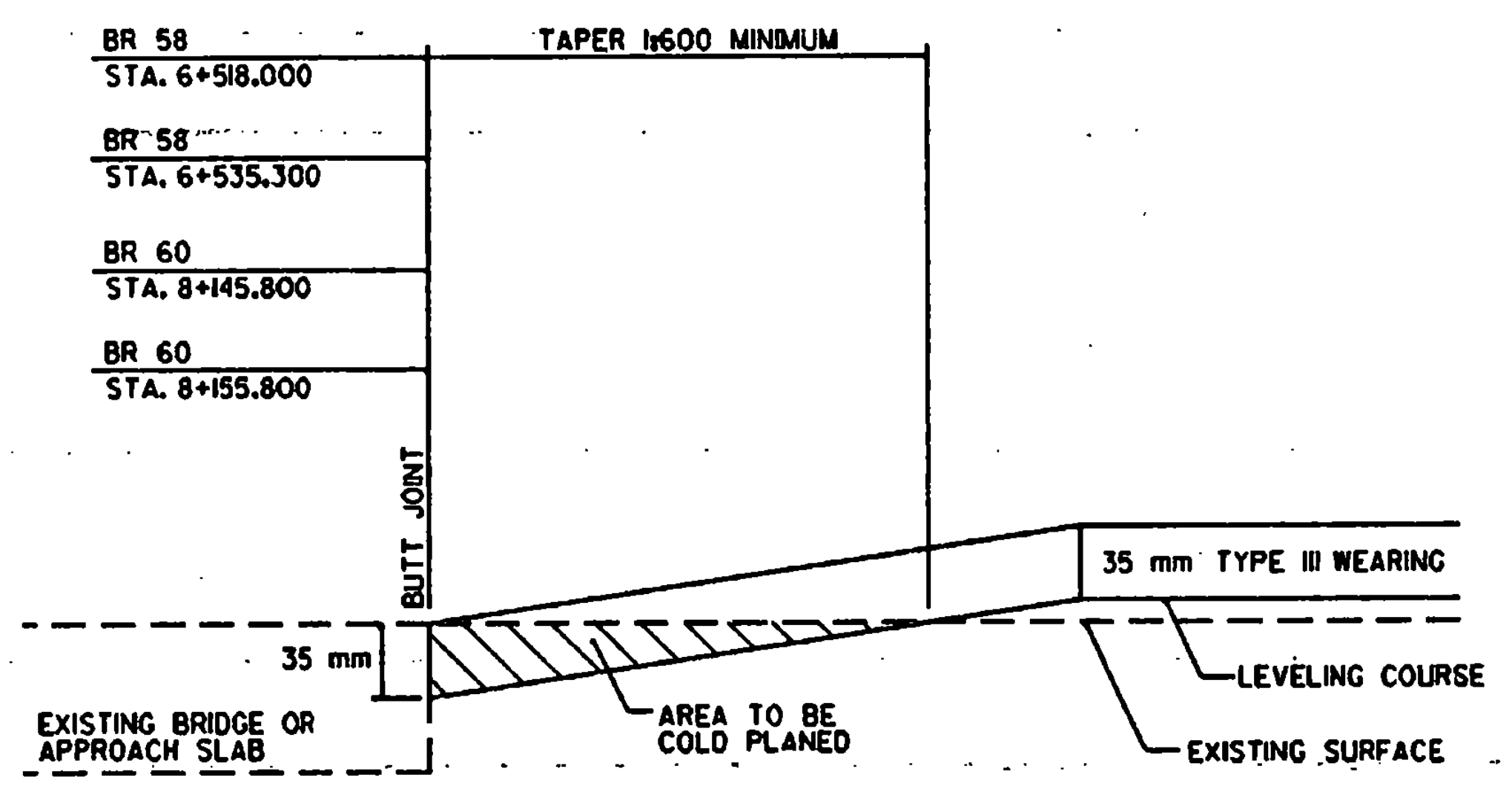
SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	pave/95b022/pb022.dgn		
IPARM FILE	p022+y.r	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET		
PROJ. NO.	STP 980411S		
SHEET	27 OF 43	SHEETS	



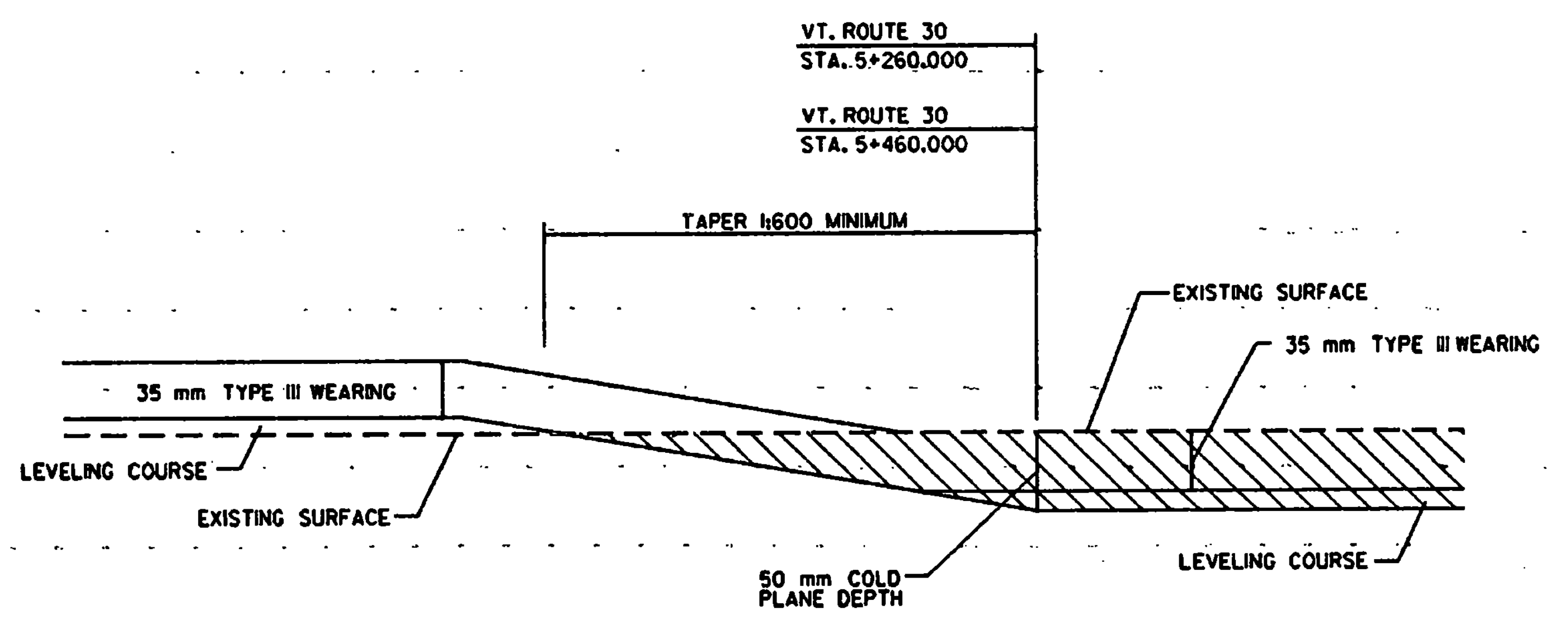
APPROACH AREA DETAIL
 VT. ROUTE 30 STA. 0+471.538 - BEGIN OVERLAY
 VT. ROUTE 30 STA. 8+408.822 - END OVERLAY



APPROACH AREA DETAIL
 T-1 T-13
 T-3 T-36
 T-26 T-2
 T-25 T-6
 T-10 T-8



TRANSITION AREA DETAIL
 VT. ROUTE 30 BR 58 STA. 6+518.000
 VT. ROUTE 30 BR 58 STA. 6+535.300
 VT. ROUTE 30 BR 60 STA. 8+145.800
 VT. ROUTE 30 BR 60 STA. 8+155.800



TRANSITION AREA DETAIL
 VT. ROUTE 30 STA. 5+260.000 END OVERLAY / BEGIN COLD PLANE
 VT. ROUTE 30 STA. 5+460.000 END COLD PLANE / BEGIN OVERLAY

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

PAVING DETAILS SHEET	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/95b022/pb022.dgn		
	IPARM FILE	pb022d1.r	DATE PLOTTED	20-JAN-1998
	PROJ. NAME	DORSET		
PROJ. NO.	STP 9504115			
SHEET	28 OF 43	SHEETS		

ITEM DETAIL SUMMARY SHEET



LOCATION			GUARD RAIL										DROP INLETS			DRAINAGE				MISCELLANEOUS				REMARKS
			621.20	621.20	621.54	621.60	621.75	621.76	621.77	621.80	621.81				NEW PIPE						203.15	203.16	301.28	
STATION	STATION	POS.	WOOD POST G.R.	STEEL POST G.R.	2.4 m STEEL POST G.R.	WOOD POST M.E.L.T.	ANCHOR FOR G.R.	REMOVE & RESET G.R.	REPLACE G.R. POST ASSEMBLY	REPLACE BEAM UNIT	REMOVE & DISP. G.R.	REMOVE & DISP. G.P.	CHANGE ELEV. D.I.	REHAB. D.I.	GRATE/TYPE	DIA.	LENGTH	THICK. OF PIPE	STONE FILL/TYPE	COMMON EXCAVATION	SOLID ROCK EXCAVATION	SUBBASE OF CRUSHED GRAVEL	P.C.C. SIDEWALK 125 mm	
			m.	m.	m.	EA	EA	m.	EA	EA	m	EA	EA	EA		mm.	m.			m ³	m ³	t.	m ²	
7+086.0	7+189.0	RT				1			12	6	12.0													REMOVE EXISTING 4.8 m RADIUS AND ANCHOR AND INSTALL NEW MELT AT STA. 7+086.0
7+124.4	7+192.8	LT				2		68.4			12.6													INSTALL NEW MELT AT STA. 7+124.4 & STA. 7+192.8
7+748.0		RT										1												
7+391.0	7+840.0	LT							18	6														
7+926.0	7+975.0	RT							4	2														
8+046.3	8+104.5	LT			58.2		2				57.5													SEE VAOT STANDARD SHEET G-1D. PROVIDE ANCHOR AT STA. 8+048.8 & STA. 8+102.0
8+125.1	8+129.5	LT		4.4			1				4.4													SEE VAOT STANDARD SHEET G-1D. PROVIDE ANCHOR AT STA. 8+127.6
8+131.6	8+136.0	RT		4.4			1				4.4													SEE VAOT STANDARD SHEET G-1D. PROVIDE ANCHOR AT STA. 8+134.1
8+157.0	8+170.0	RT							2	1														
8+248.0		LT										1												
SHEET SUBTOTAL			8.8	58.2		3	4	68.4	36	15	90.9	2												
SHEET 30 SUBTOTAL			238.0	95.0		9	4	7.6	-	-	404.3	12												
ROUNDING			13.2	6.8		-	-	4.0	-	-	24.8	-												
TOTAL			260.0	160.0		12	8	80.0	36	15	520.0	14												

DATUM
 VERTICAL N/A
 HORIZONTAL N/A

ITEM DETAIL SHEET 2

SURVEYED BY N/A DATE N/A
 DRAWN BY E.C.D. DATE 11/97
 SQUAD LEADER T.P.K.
 DESIGN FILE NO. /pave/95b022/pb022.dgn
 IPARM FILE pb0221021 DATE PLOTTED 20-JAN-1998
 PROJ. NAME DORSET
 PROJ. NO. STP 9604(IIS)
 SHEET 31 OF 43 SHEETS

646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT & RT
 STA. 0+471.538 - STA. 1+540.000 S - S
 STA. 0+808.000 DOUBLE SOLID LT, DORSET WEST ROAD
 STA. 1+211.000 DOUBLE SOLID RT, MORSE HILL ROAD

646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT & RT
 STA. 0+471.538 - STA. 1+540.000 S - S
 STA. 0+808.000 DOUBLE SOLID LT, DORSET WEST ROAD
 STA. 1+211.000 DOUBLE SOLID RT, MORSE HILL ROAD

675.50- REMOVING SIGNS
 AS SHOWN - 8

646.46 DURABLE 600 mm STOP BAR
 STA. 0+808.000 LT, DORSET WEST ROAD
 STA. 1+211.000 RT, MORSE HILL ROAD

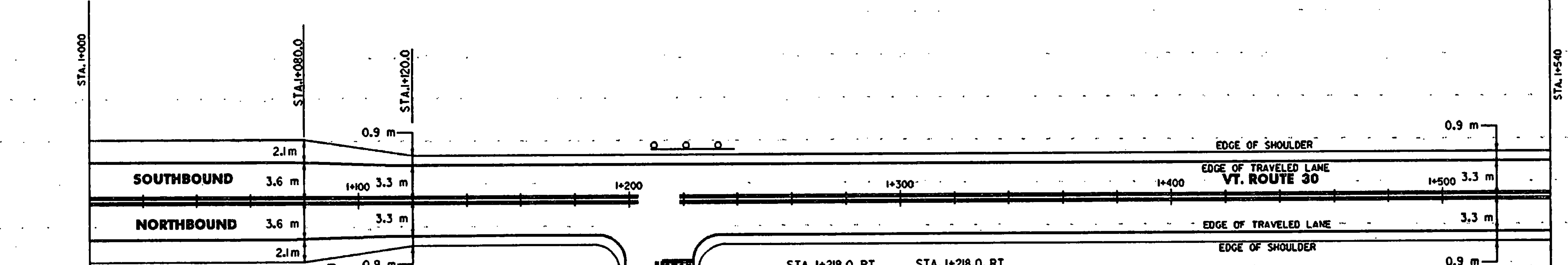
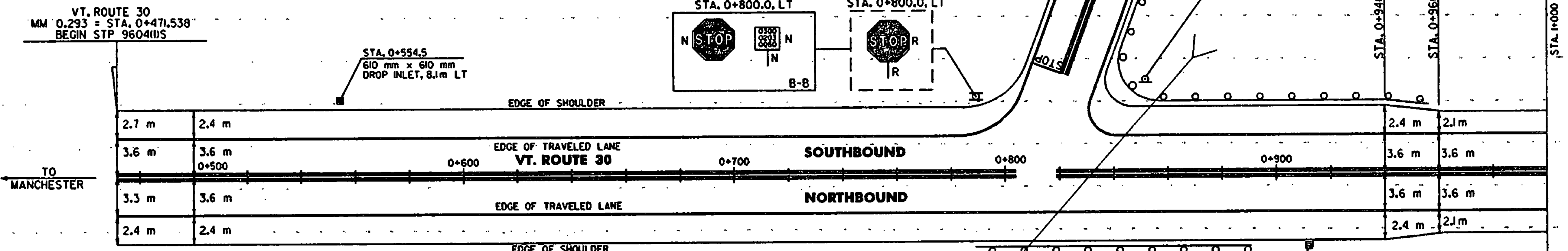
646.66 TEMPORARY 600 mm STOP BAR
 STA. 0+808.000 LT, DORSET WEST ROAD
 STA. 1+211.000 RT, MORSE HILL ROAD

646.50 DURABLE LETTER OR SYMBOL
 STA. 0+808.000 LT, DORSET WEST ROAD, "S,T,O,P" (4 EA)
 STA. 1+211.000 RT, MORSE HILL ROAD, "S,T,O,P" (4 EA)

646.70 TEMPORARY LETTER OR SYMBOL
 STA. 0+808.000 LT, DORSET WEST ROAD, "S,T,O,P" (4 EA)
 STA. 1+211.000 RT, MORSE HILL ROAD, "S,T,O,P" (4 EA)

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 0+471.538 - STA. 1+540.000 LT & RT.

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 0+471.538 - STA. 1+540.000 LT & RT.



621.20 STEEL BEAM GUARDRAIL W/2.4 m POSTS (MOD.)
 STA. 0+845.0 - STA. 0+940.0 LT (95.0 m)

621.60 ANCHOR FOR STEEL BEAM RAIL
 STA. 1+210.5 LT

621.20 STEEL BEAM GUARDRAIL
 STA. 0+799.4 - STA. 0+879.2 RT (79.8 m)
 STA. 0+812.0 - STA. 0+845.0 LT (33.5 m)
 STA. 1+208.0 - STA. 1+227.6 LT (19.6 m)

621.54 MODIFIED ECCENTRIC LOADER TERMINAL
 STA. 0+788.0 - STA. 0+799.4 RT
 STA. 0+812.0 - STA. 0+890.6 LT
 STA. 0+879.2 - STA. 0+890.6 RT
 STA. 0+940.0 - STA. 0+951.4 LT
 STA. 1+227.6 - STA. 1+239.0 LT

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 0+789.0 - STA. 0+890.6 RT (101.6 m)
 STA. 0+812.0 - STA. 0+946.0 LT (134.0 m)
 STA. 1+208.0 - STA. 1+238.5 LT (30.5 m)

621.8 REMOVAL AND DISPOSAL OF GUIDEPOSTS
 STA. 0+554.5 RT & LT (2 EA)
 STA. 1+018.0 RT (1 EA)

SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

PAVING PROJECT LAYOUT

SURVEYED BY N/A DATE N/A
 DRAWN BY E.C.D. DATE 11/97
 SQUAD LEADER T.P.K.
 DESIGN FILE NO. /pave/95b022/pb022.dgn
 IPARM FILE db022011 DATE PLOTTED 20-JAN-1998
 PROJ. NAME DORSET
 PROJ. NO. STP 9604(IIS)
 SHEET 32 OF 43 SHEETS



646.4 DURABLE 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

STA. 1+540.000 - STA. 2+620.000 S - S
STA. 1+841.000 DOUBLE SOLID LT, CROSS ROAD

646.6 TEMPORARY 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

STA. 1+540.000 - STA. 2+620.000 S - S
STA. 1+841.000 DOUBLE SOLID LT, CROSS ROAD

646.50 DURABLE LETTER OR SYMBOL

STA. 1+841.000 LT, CROSS ROAD, "S,T,O,P" (4 EA)

646.70 TEMPORARY LETTER OR SYMBOL

STA. 1+841.000 LT, CROSS ROAD, "S,T,O,P" (4 EA)

646.46 DURABLE 600 mm STOP BAR

STA. 1+841.000 LT, CROSS ROAD

646.66 TEMPORARY 600 mm STOP BAR

STA. 1+841.000 LT, CROSS ROAD

675.50 REMOVING SIGNS

AS SHOWN - 4

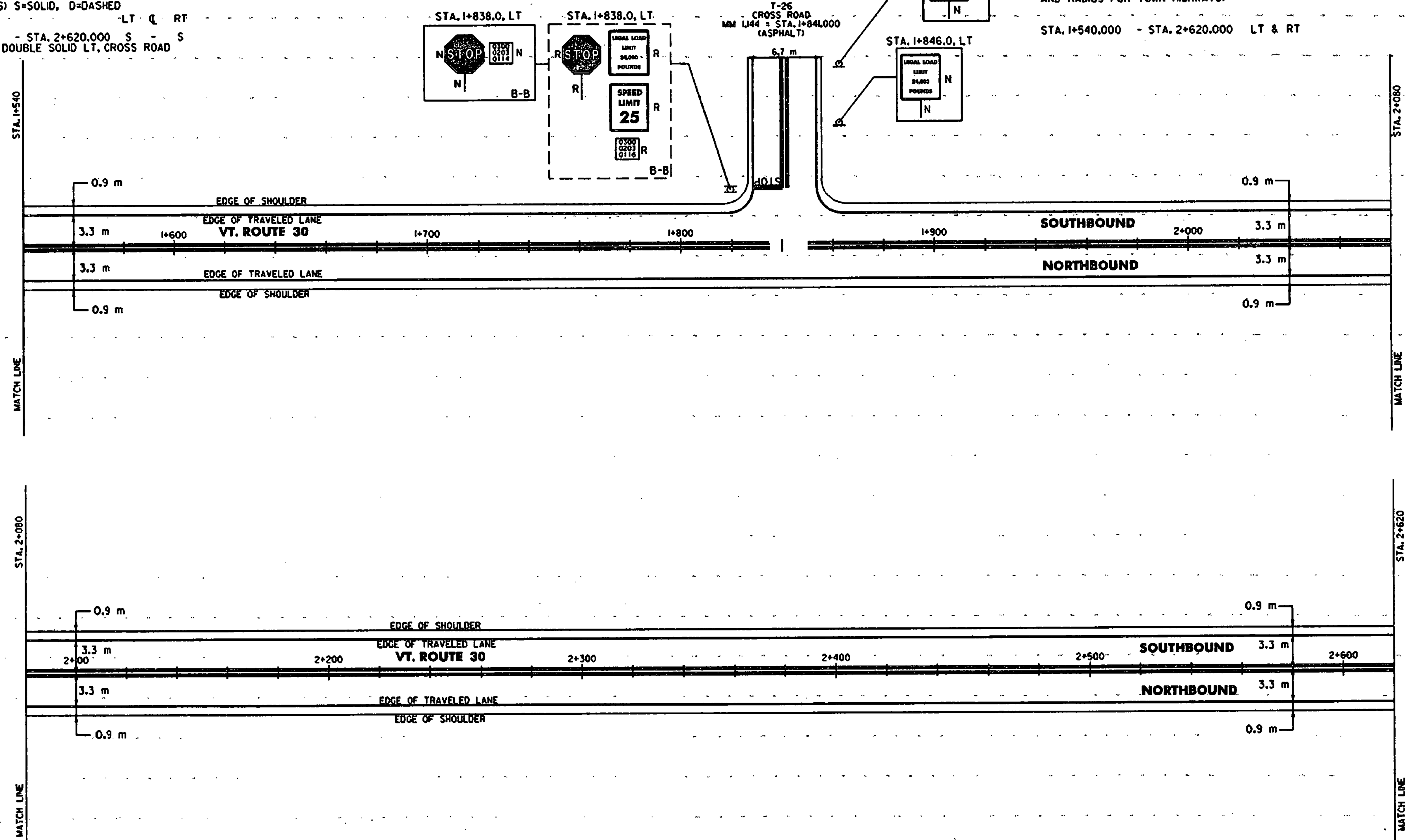
646.40 DURABLE 100 mm WHITE LINE

(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

STA. 1+540.000 - STA. 2+620.000 LT & RT

646.60 TEMPORARY 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

STA. 1+540.000 - STA. 2+620.000 LT & RT



621.8 REMOVAL AND DISPOSAL OF GUIDEPOSTS
STA. 2+139.0 RT (1EA)
STA. 2+273.0 LT (1EA)
STA. 2+594.0 RT (1EA)

NOTE:
1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
R= REMOVE
S= SALVAGE
N= NEW
RET= RETAIN
B-B= BACK TO BACK
EXISTING= _____
NEW= _____

PAVING PROJECT LAYOUT

SURVEYED BY N/A DATE N/A
DRAWN BY E.C.D. DATE 11/97
SQUAD LEADER T.P.K.
DESIGN FILE NO. /pave/95b022/pb022.dgn
IPARM FILE pb02202.r DATE PLOTTED 20-JAN-1998
PROJ. NAME DORSET
PROJ. NO. STP 960411S
SHEET 33 OF 43 SHEETS

DATUM
VERTICAL N/A
HORIZONTAL N/A



646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 2+620.000 - STA. 3+700.000 S - S
 STA. 2+960.000 DOUBLE SOLID RT (KELLEY ROAD)

646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 2+620.000 - STA. 3+700.000 S - S

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 2+620.000 - STA. 3+700.000 LT & RT

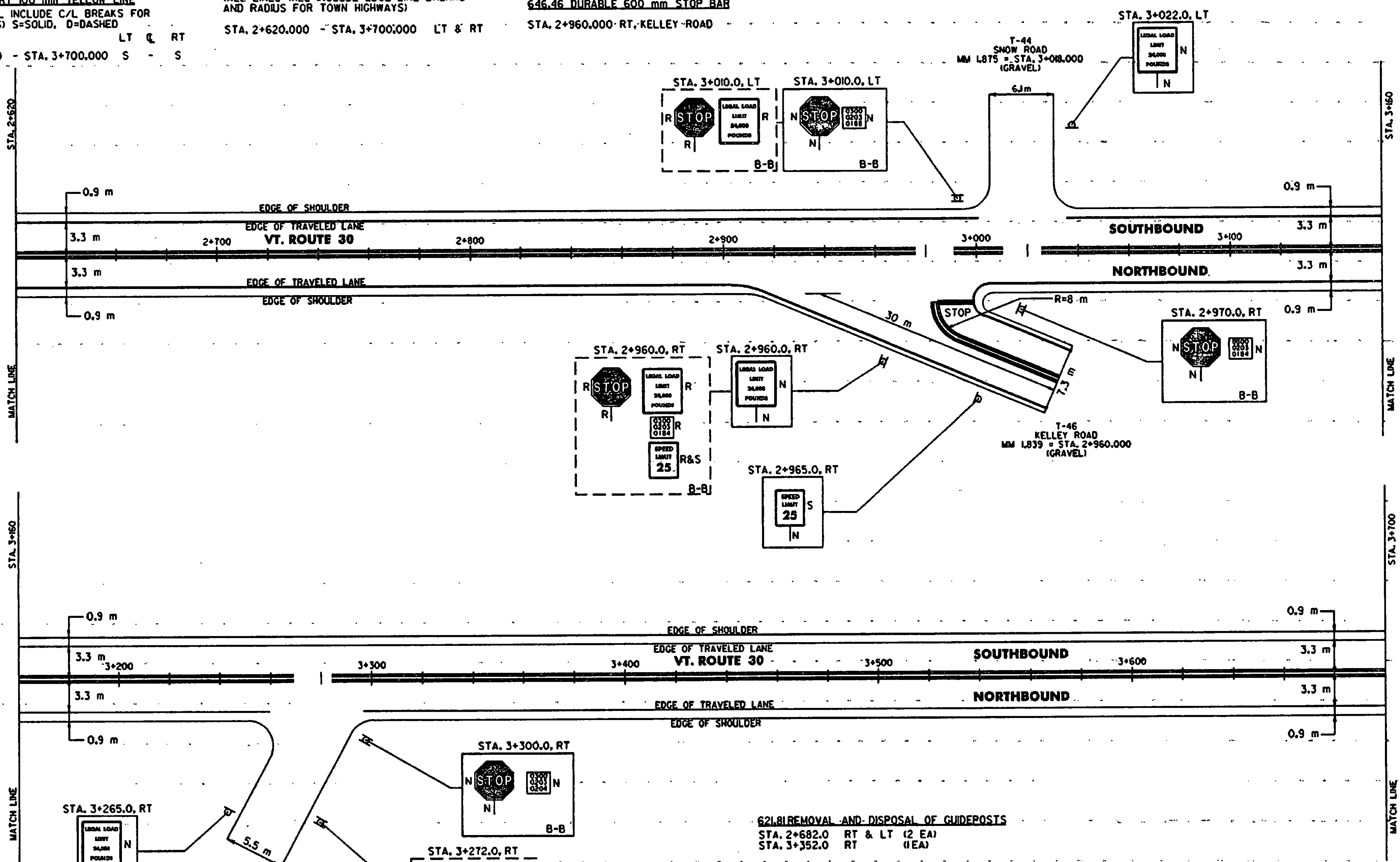
646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 2+620.000 - STA. 3+700.000 LT & RT

646.50 DURABLE LETTER OR SYMBOL
 STA. 2+960.000 RT, KELLEY ROAD, 'S,T,O,P' (4 EA)
 AS SHOWN - 1

646.46 DURABLE 600 mm STOP BAR
 STA. 2+960.000 RT, KELLEY ROAD
 AS SHOWN - 9

675.60-ERECTING SALVAGED SIGNS
 AS SHOWN - 1

675.50 REMOVING SIGNS
 AS SHOWN - 9



621.8 REMOVAL AND DISPOSAL OF GUIDEPPOSTS
 STA. 2+682.0 RT & LT (2 EA)
 STA. 3+352.0 RT (1EA)

NOTE:

1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING= _____
 NEW= _____

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/95b022/pb022.dgn		
	IPARM FILE	pb02203.r	DATE PLOTTED	20 JAN 1998
PROJ. NAME	DORSET			
PROJ. NO.	STP 9604HS			
SHEET	34	OF	43	SHEETS

DATUM
 VERTICAL N/A
 HORIZONTAL N/A



646.4 DURABLE 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

646.40 DURABLE 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

STA. 3+700.000 - STA. 4+780.000 S - S

STA. 3+700.000 - STA. 4+780.000 LT & RT

675.50 REMOVING SIGNS

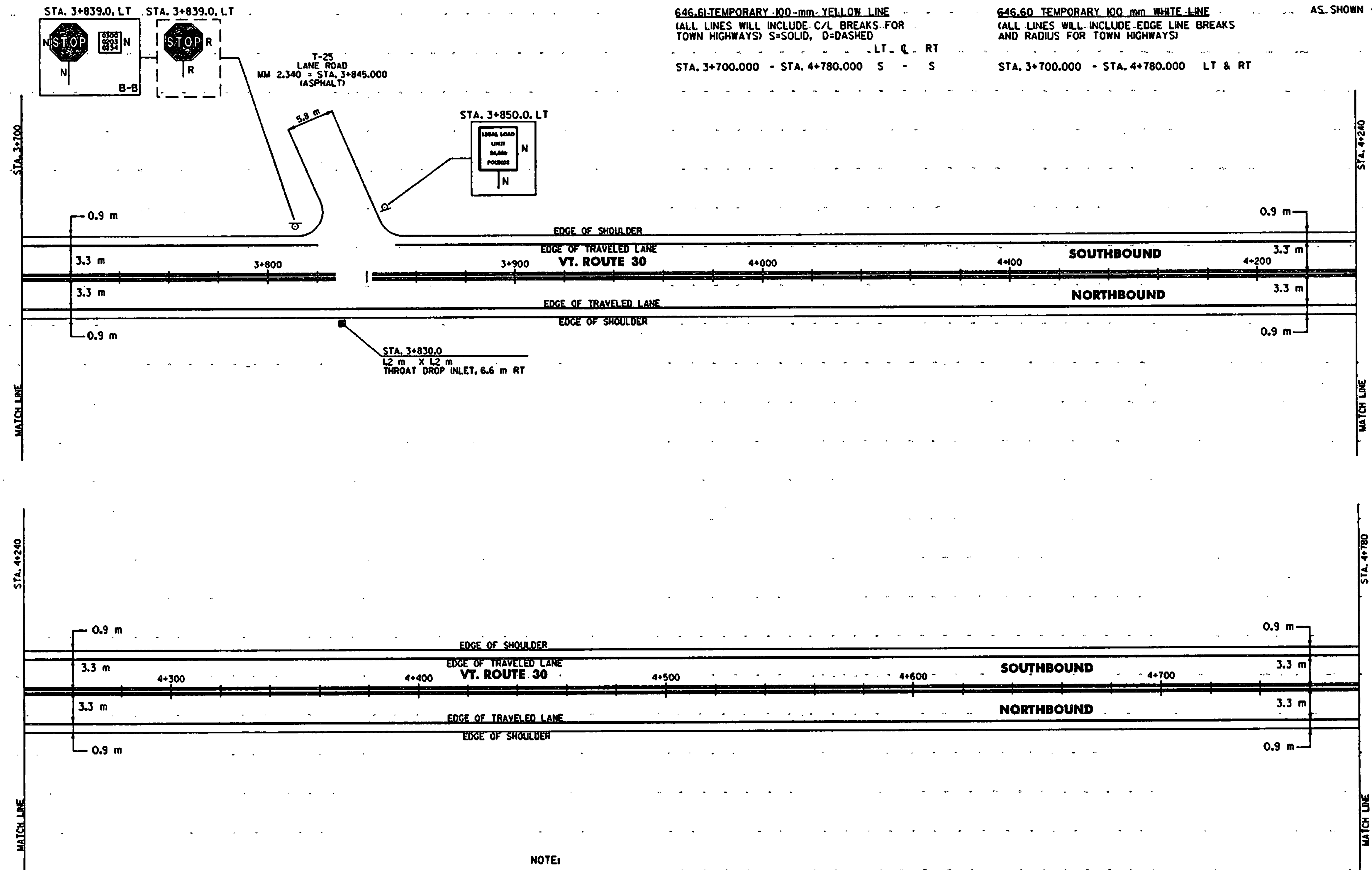
646.6 TEMPORARY 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED

646.60 TEMPORARY 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)

STA. 3+700.000 - STA. 4+780.000 S - S

STA. 3+700.000 - STA. 4+780.000 LT & RT

AS SHOWN - I



CHANGE ELEVATION OF DI, CB, OR MH
REHABILITATION OF DI, CB, OR MH
SEE NOTE 6, SHEET 27 OF 43
STA. 3+830.0 RT

NOTE:
1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND
R= REMOVE
S= SALVAGE
N= NEW
RET= RETAIN
B-B= BACK TO BACK
EXISTING= _____
NEW= _____

PAVING PROJECT LAYOUT

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/95b022/pb022.dgn		
IPARM FILE	pb022i04j	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET		
PROJ. NO.	STP 960411S		
SHEET	35 OF 43	SHEETS	

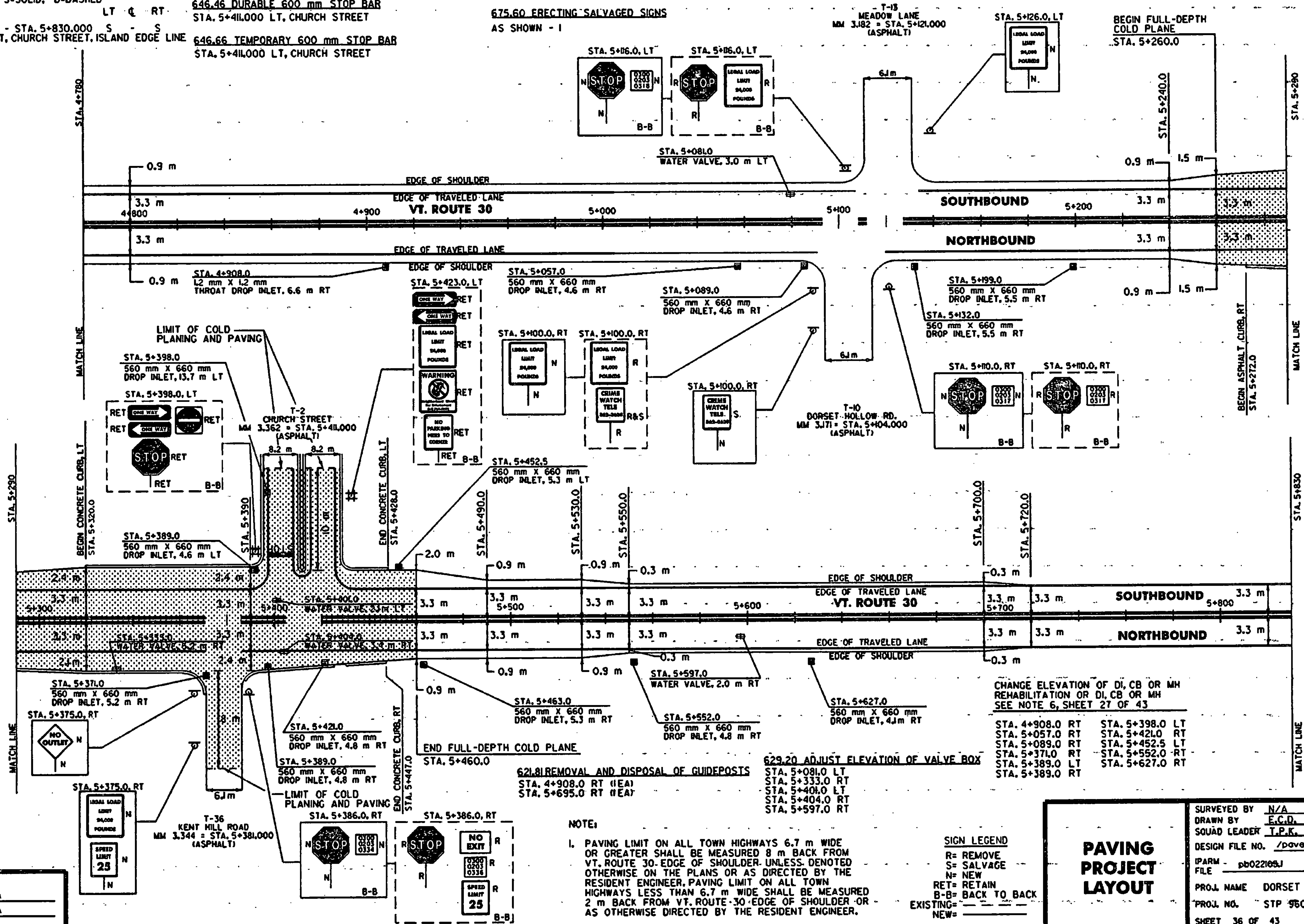
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 4+780.000 - STA. 5+830.000 S - S
 STA. 5+411.000 LT, CHURCH STREET, ISLAND EDGE LINE
646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 4+780.000 - STA. 5+830.000 S - S
 STA. 5+411.000 LT, CHURCH STREET, ISLAND EDGE LINE

675.50 REMOVING SIGNS
 AS SHOWN - 10
646.50 DURABLE LETTER OR SYMBOL
 STA. 5+411.000 LT, CHURCH STREET, "S,T,O,P" (4 EA)
646.70 TEMPORARY LETTER OR SYMBOL
 STA. 5+411.000 LT, CHURCH STREET, "S,T,O,P" (4 EA)
646.46 DURABLE 600 mm STOP BAR
 STA. 5+411.000 LT, CHURCH STREET
646.66 TEMPORARY 600 mm STOP BAR
 STA. 5+411.000 LT, CHURCH STREET

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 4+780.000 - STA. 5+830.000 LT & RT
646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 4+780.000 - STA. 5+830.000 LT & RT
675.60 ERECTING SALVAGED SIGNS
 AS SHOWN - 1

646.8 PAINTED CURB
 STA. 5+411.000 LT, CHURCH STREET, 25.0 m



DATUM

VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

SIGN LEGEND

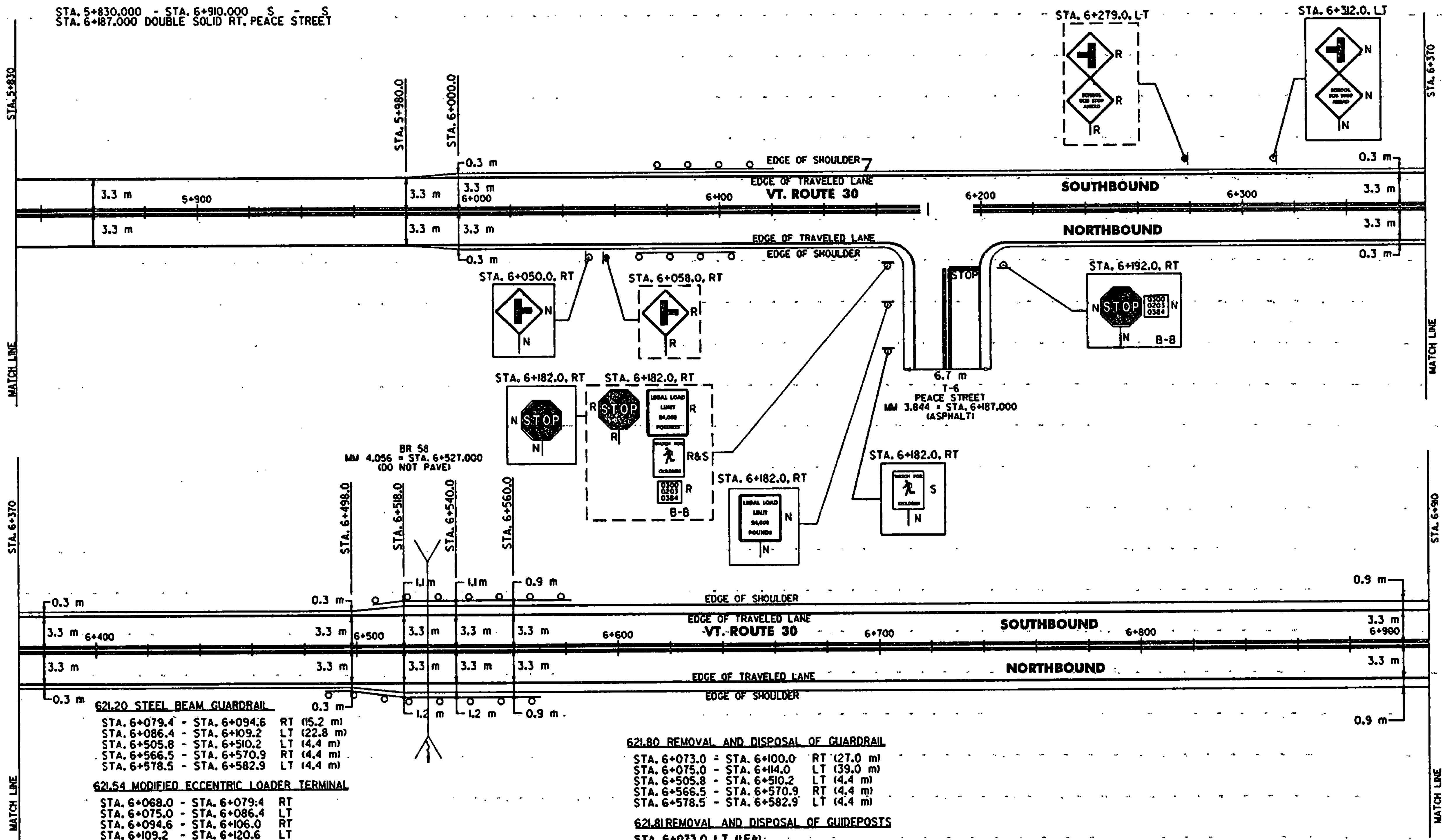
R= REMOVE
S= SALVAGE
N= NEW
RET= RETAIN
B-B= BACK TO BACK
EXISTING=
NEW=

PAVING PROJECT LAYOUT

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/95b022/pb022.dgn		
IPARM	pb02205J	DATE	PLOTTED 20-JAN-1998
FILE			
PROJ. NAME	DORSET		
PROJ. NO.	STP 9604M5		
SHEET	36 OF 43	SHEETS	



- 646.4 DURABLE 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
LT & RT
STA. 5+830.000 - STA. 6+910.000 S - S
STA. 6+187.000 DOUBLE SOLID RT, PEACE STREET
- 646.6 TEMPORARY 100 mm YELLOW LINE
(ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
LT & RT
STA. 5+830.000 - STA. 6+910.000 S - S
STA. 6+187.000 DOUBLE SOLID RT, PEACE STREET
- 646.40 DURABLE 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
STA. 5+830.000 - STA. 6+910.000 LT & RT
- 646.60 TEMPORARY 100 mm WHITE LINE
(ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
STA. 5+830.000 - STA. 6+910.000 LT & RT
- 646.46 DURABLE 600 mm STOP BAR
STA. 6+187.000 RT, PEACE STREET
- 646.66 TEMPORARY 600 mm STOP BAR
STA. 6+187.000 RT, PEACE STREET
- 646.50 DURABLE LETTER OR SYMBOL
STA. 6+187.000 RT, PEACE STREET, "STOP" (4 EA)
- 646.70 TEMPORARY LETTER OR SYMBOL
STA. 6+187.000 RT, PEACE STREET, "STOP" (4 EA)
- 675.50 REMOVING SIGNS
AS SHOWN - 7
- 675.60 ERECTING SALVAGED SIGNS
AS SHOWN - 1



- 621.20 STEEL BEAM GUARDRAIL
STA. 6+079.4 - STA. 6+094.6 RT (15.2 m)
STA. 6+086.4 - STA. 6+109.2 LT (22.8 m)
STA. 6+505.8 - STA. 6+510.2 RT (4.4 m)
STA. 6+566.5 - STA. 6+570.9 RT (4.4 m)
STA. 6+578.5 - STA. 6+582.9 LT (4.4 m)
- 621.54 MODIFIED ECCENTRIC LOADER TERMINAL
STA. 6+068.0 - STA. 6+079.4 RT
STA. 6+075.0 - STA. 6+086.4 LT
STA. 6+094.6 - STA. 6+106.0 RT
STA. 6+109.2 - STA. 6+120.6 LT

- 621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
STA. 6+073.0 - STA. 6+100.0 RT (27.0 m)
STA. 6+075.0 - STA. 6+114.0 LT (39.0 m)
STA. 6+505.8 - STA. 6+510.2 LT (4.4 m)
STA. 6+566.5 - STA. 6+570.9 RT (4.4 m)
STA. 6+578.5 - STA. 6+582.9 LT (4.4 m)
- 621.8 REMOVAL AND DISPOSAL OF GUIDEPOSTS
STA. 6+073.0 LT (1EA)

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOTE:
 1. PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.

- SIGN LEGEND
- R= REMOVE
 - S= SALVAGE
 - N= NEW
 - RET= RETAIN
 - B-B= BACK TO BACK
 - EXISTING=
 - NEW=

PAVING PROJECT LAYOUT

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/95b022/pb022.dgn		
IPARM FILE	pb022866.1	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET		
PROJ. NO.	"STP" 9604m5		
SHEET	37 OF 43	SHEETS	



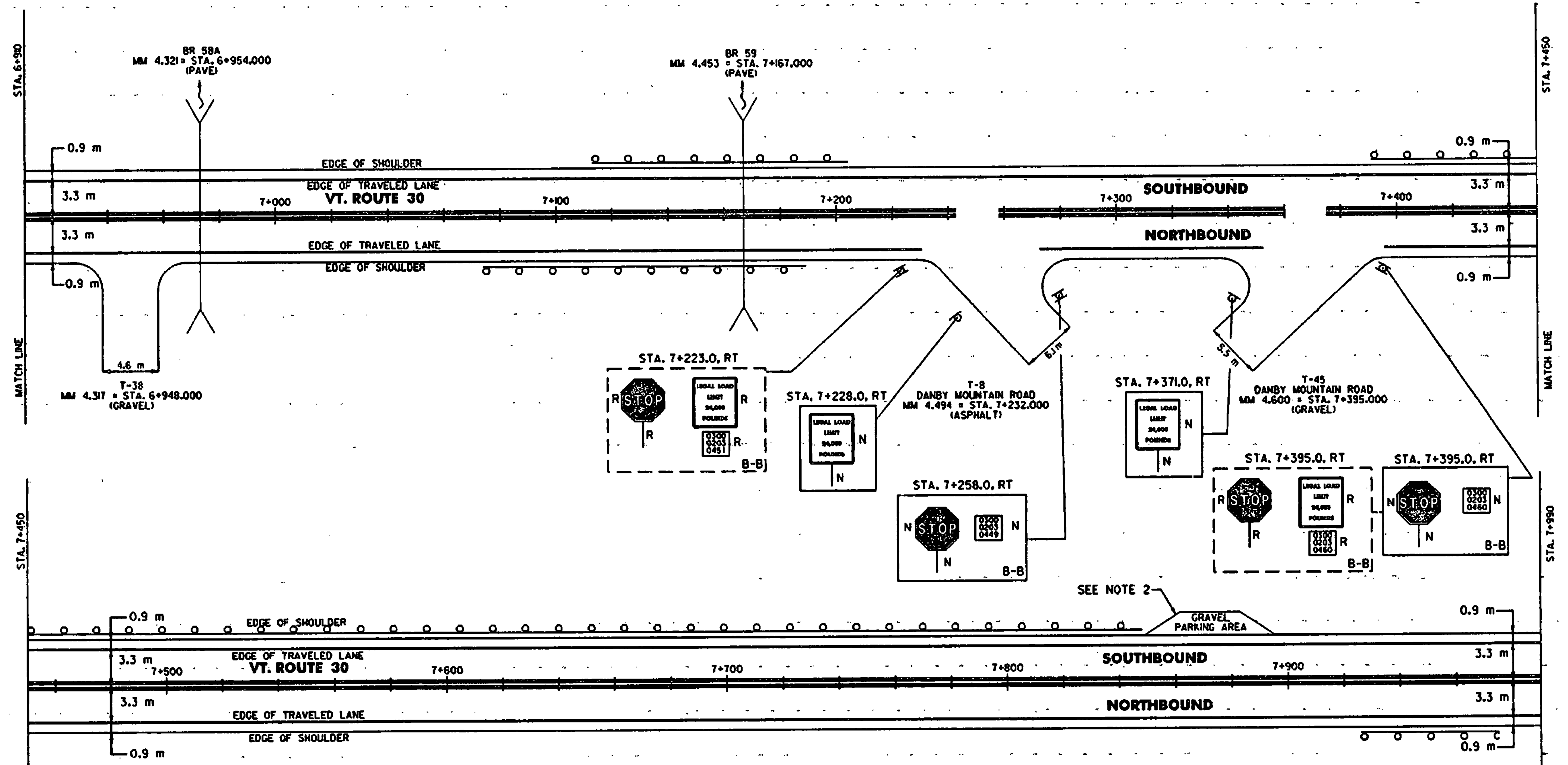
646.41 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT & RT
 STA. 6+910.000 - STA. 7+990.000 S - S

646.61 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT & RT
 STA. 6+910.000 - STA. 7+990.000 S - S

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 6+910.000 - STA. 7+990.000 LT & RT

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 6+910.000 - STA. 7+990.000 LT & RT

675.50 REMOVING SIGNS
 AS SHOWN - 6



621.54 MODIFIED ECCENTRIC LOADER TERMINAL
 STA. 7+074.6 - STA. 7+086.0 RT
 STA. 7+113.0 - STA. 7+124.4 LT
 STA. 7+192.8 - STA. 7+204.2 LT

621.75 REMOVING AND RESET GUARDRAIL
 STA. 7+124.4 - STA. 7+192.8 LT (68.4 m)

621.76 REPLACE GUARDRAIL POST ASSEMBLY
 STA. 7+086.0 - STA. 7+189.0 RT (12 EA)
 STA. 7+391.0 - STA. 7+840.0 LT (18 EA)
 STA. 7+926.0 - STA. 7+975.0 RT (4 EA)

621.77 REPLACE GUARDRAIL BEAM UNIT
 STA. 7+086.0 - STA. 7+189.0 RT (6 EA)
 STA. 7+391.0 - STA. 7+840.0 LT (6 EA)
 STA. 7+926.0 - STA. 7+975.0 RT (2 EA)

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 7+074.0 - STA. 7+086.0 RT (12.0 m)
 STA. 7+120.0 - STA. 7+124.4 LT (4.4 m)
 STA. 7+192.8 - STA. 7+204.0 LT (8.2 m)

621.81 REMOVAL AND DISPOSAL OF GUIDEPOSTS
 STA. 7+748.0 RT (1EA)

- NOTE:
- PAVING LIMIT ON ALL TOWN HIGHWAYS 6.7 m WIDE OR GREATER SHALL BE MEASURED 8 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER UNLESS DENOTED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. PAVING LIMIT ON ALL TOWN HIGHWAYS LESS THAN 6.7 m WIDE SHALL BE MEASURED 2 m BACK FROM VT. ROUTE 30 EDGE OF SHOULDER OR AS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
 - PROVIDE A 35 mm TYPE III 1.5 m WIDE APRON FOR THE EXISTING PARKING AREA. ANY REQUIRED EXCAVATION SHALL BE PERFORMED AS DIRECTED BY THE RESIDENT ENGINEER AND PAID UNDER ITEM 608.25.

SIGN LEGEND
 R= REMOVE
 S= SALVAGE
 N= NEW
 RET= RETAIN
 B-B= BACK TO BACK
 EXISTING=

PAVING PROJECT LAYOUT

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/95b022/pb022.dgn		
IPARL FILE	pb022107.r	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET		
PROJ. NO.	STP 9504(1)S		
SHEET	38	OF	43 SHEETS

D.A.T.U.M.	
VERTICAL	N/A
HORIZONTAL	N/A

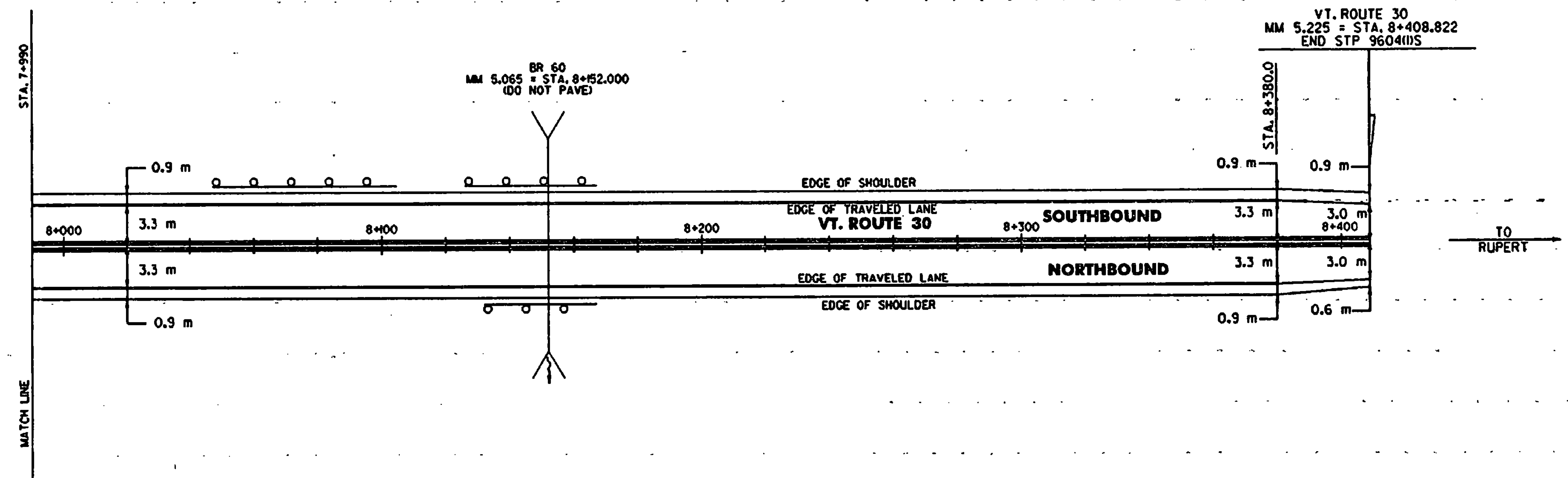


646.4 DURABLE 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 7+990.000 - STA. 8+408.822 S - S

646.6 TEMPORARY 100 mm YELLOW LINE
 (ALL LINES WILL INCLUDE C/L BREAKS FOR TOWN HIGHWAYS) S=SOLID, D=DASHED
 LT C RT
 STA. 7+990.000 - STA. 8+408.822 S - S

646.40 DURABLE 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 7+990.000 - STA. 8+408.822 LT & RT

646.60 TEMPORARY 100 mm WHITE LINE
 (ALL LINES WILL INCLUDE EDGE LINE BREAKS AND RADIUS FOR TOWN HIGHWAYS)
 STA. 7+990.000 - STA. 8+408.822 LT & RT



VT. ROUTE 30
 MM 5.225 = STA. 8+408.822
 END STP 960411S

621.20 STEEL BEAM GUARDRAIL
 STA. 8+125.1 - STA. 8+129.5 LT (4.4 m)
 STA. 8+131.6 - STA. 8+136.0 RT (4.4 m)

621.20 STEEL BEAM GUARDRAIL W/2.4' m POSTS (MOD.)
 STA. 8+046.3 - STA. 8+104.5 LT (58.2 m)

621.60 ANCHOR FOR STEEL BEAM-RAIL
 STA. 8+048.8 LT
 STA. 8+102.0 LT
 STA. 8+127.6 LT
 STA. 8+134J RT

621.76 REPLACE GUARDRAIL POST ASSEMBLY
 STA. 8+157.0 - STA. 8+170.0 RT (2 EA)

621.77 REPLACE GUARDRAIL BEAM UNIT
 STA. 8+157.0 - STA. 8+170.0 RT (1EA)

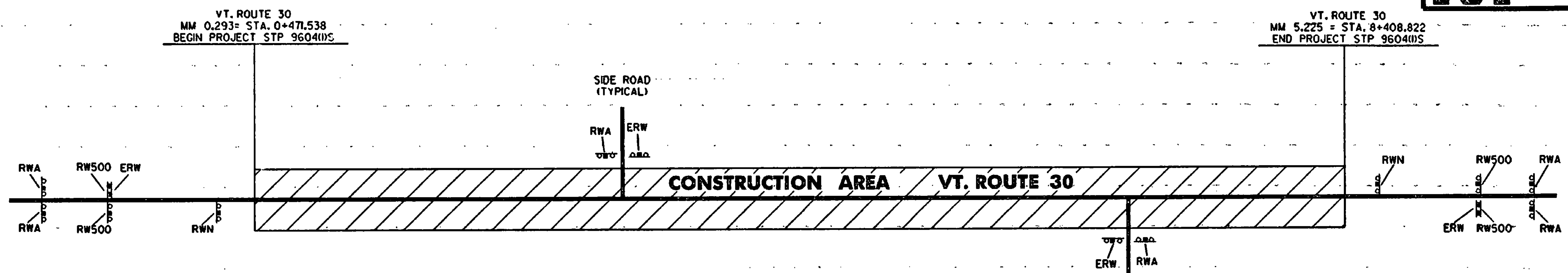
621.8 REMOVAL AND DISPOSAL OF GUIDEPOSTS
 STA. 8+248.0 LT (1EA)

621.80 REMOVAL AND DISPOSAL OF GUARDRAIL
 STA. 8+047.0 - STA. 8+104.5 LT (57.5 m)
 STA. 8+125.1 - STA. 8+129.5 LT (4.4 m)
 STA. 8+131.6 - STA. 8+136.0 RT (4.4 m)

DATUM
 VERTICAL N/A
 HORIZONTAL N/A

PAVING PROJECT LAYOUT	SURVEYED BY	N/A	DATE	N/A
	DRAWN BY	E.C.D.	DATE	11/97
	SQUAD LEADER	T.P.K.		
	DESIGN FILE NO.	/pave/95b022/pb022.dgn		
	TEAM FILE	pb02208.1	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET			
PROJ. NO.	STP 960411S			
SHEET	39 OF 43	SHEETS		

: Sheet Number: 39



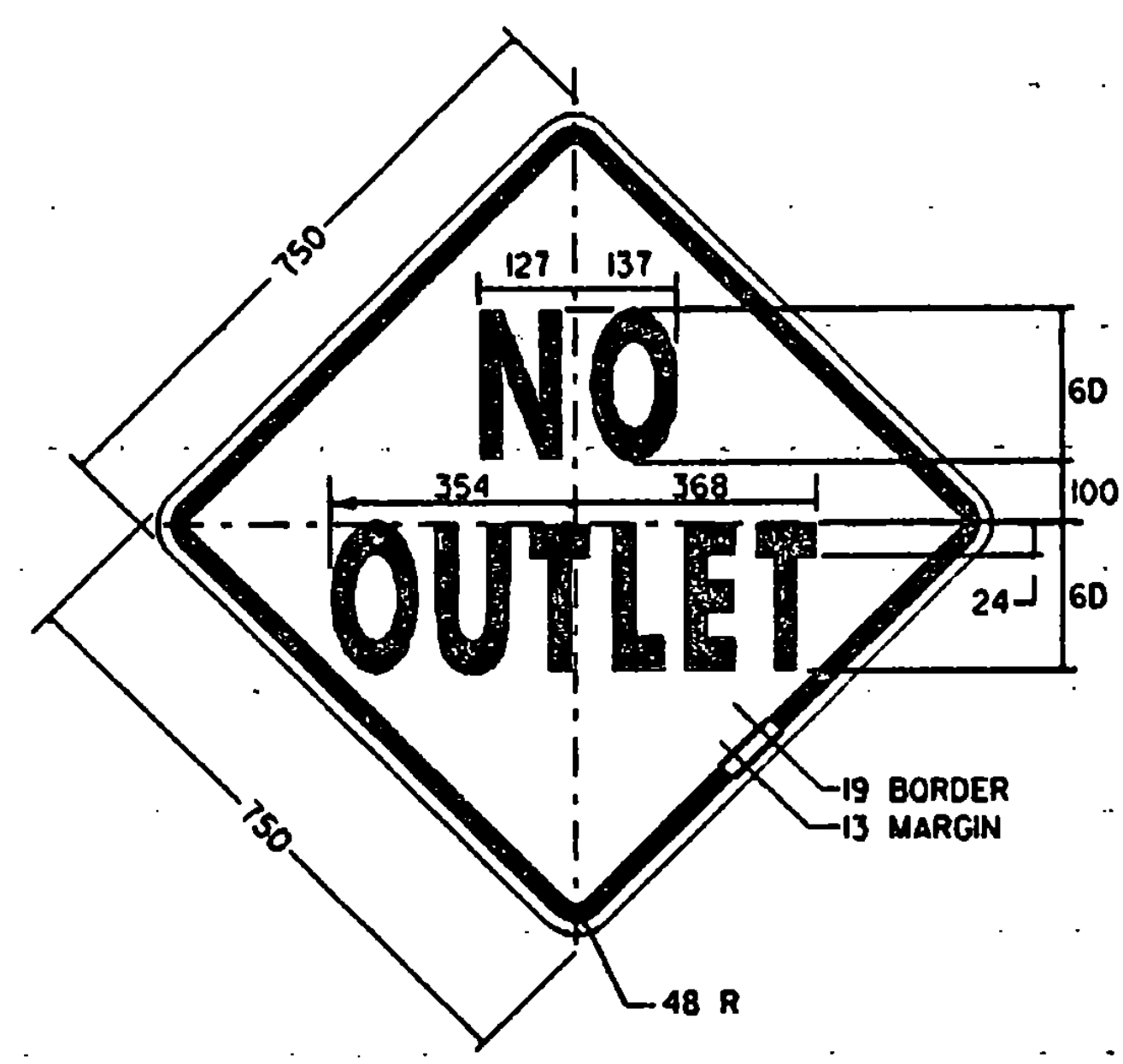
TOWN HIGHWAY NUMBER	ROAD WORK AHEAD	END ROAD WORK	OTHER
BEGIN PROJECT	2	1	2 - RW500 1 - RWN
T-1	1	1	
T-3	1	1	
T-26	1	1	
T-46	1	1	
T-44	1	1	
T-46	1	1	
T-25	1	1	
T-10	1	1	

TOWN HIGHWAY NUMBER	ROAD WORK AHEAD	END ROAD WORK	OTHER
T-13	1	1	
T-36	1	1	
T-2	1	1	
T-6	1	1	
T-38	-	-	
T-8	1	1	
T-45	1	1	
END PROJECT	2	1	2 - RW500 1 - RWN
TOTAL	18	16	6

SIGN LEGEND

RWA = ROAD WORK AHEAD
RW500 = ROAD WORK 500 FEET
ERW = END ROAD WORK
SRWA = SIDE ROAD WORK AHEAD
SRW500 = SIDE ROAD WORK 500 FEET
RWN = ROAD WORK NEXT 5 MILES

CONSTRUCTION APPROACH SIGNING
SEE STD. E-100 FOR ADDITIONAL SIGN PLACEMENT



COLOR: BLACK BORDER & TEXT
YELLOW BACKGROUND
MATERIAL: PER VAOT STANDARD E-153
SIGN - VT. ROUTE 30
STA. 5+375.0, RT

ALL DIMENSIONS IN MILLIMETERS EXCEPT WHERE OTHERWISE INDICATED.

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

CONSTRUCTION APPROACH SIGNING & TRAFFIC SIGN DETAIL SHEET

SURVEYED BY	N/A	DATE	N/A
DRAWN BY	E.C.D.	DATE	11/97
SQUAD LEADER	T.P.K.		
DESIGN FILE NO.	/pave/95b022/pb022.dgn		
IPARM FILE	pb022ca.f	DATE PLOTTED	20-JAN-1998
PROJ. NAME	DORSET		
PROJ. NO.	STP 9604(I)S		
SHEET	43 OF 43	SHEETS	