

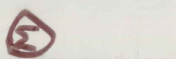
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RECORD PLANS & MATERIAL SUPPLIERS
CONTRACTOR - PIKE INDUSTRIES, CONTRACT DATE - JUNE 6, 1989
CONST. BEGAN - JUNE 1989, COMPLETED & ACCEPTED SEPT. 12, 1989
RES. ENGR. - J. LAVALLEE RECORD PLANS - J. MARSCHELL
BIT.CONC. PVMT. PIKES BERLIN & W. LEBANON PLANTS



CONVENTIONAL SIGNS

COUNTY LINE	---
TOWN LINE	----
LIMITS OF ACCESS	○-○-○
POINT OF ACCESS	X
FENCE LINE	x-x-x
STONE WALL	o-o-o-o-o
TRAVELED WAY	-----
GUARD RAIL	o-o-o-o-o
RAILROAD	
SURVEY LINE	+
CLVERT	+
POWER POLE	□
TELEPHONE POLE	□
TREES	⊗
CONTROL OF ACCESS	///
PROPERTY LINE	---
R.O.W. TAKING LINE	---
SLOPE RIGHTS	○ SR
TOP OF CUT	△
TOE OF SLOPE	○

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

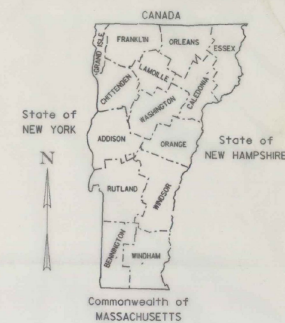
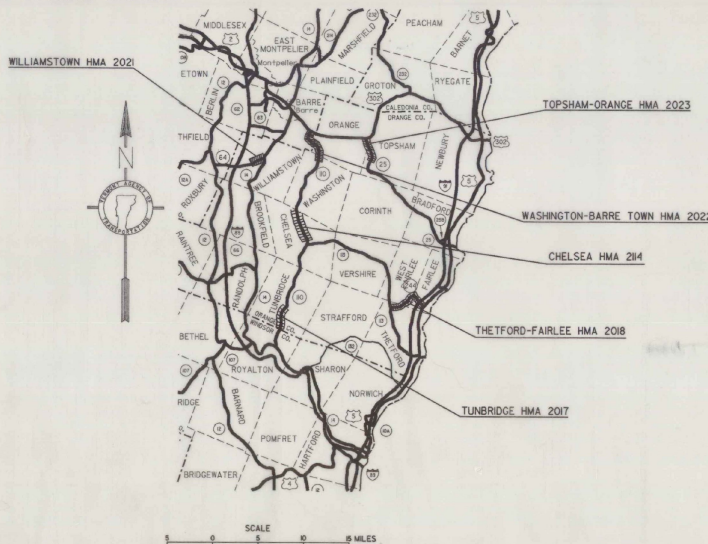
STATE OF VERMONT
 AGENCY OF TRANSPORTATION



PROPOSED IMPROVEMENT
 TOWNS OF WILLIAMSTOWN, BARRE TOWN, ORANGE
 WASHINGTON, TOPSHAM, TUNBRIDGE, CHELSEA
 THETFORD, WEST FAIRLEE & FAIRLEE
 COUNTIES OF WASHINGTON AND ORANGE
 VT. ROUTES 64, 110, 244 (F.A.S.) & 25 (F.A.P.)

WILLIAMSTOWN: VT. 64 MM 3.071 ~ MM 4.242 = 6182.880 FT. = 1.171 MILES
 TOPSHAM-ORANGE: VT. 25 TOPSHAM MM 4.424 ~ ORANGE MM 0.306 = 12249.920 FT. = 2.320 MILES
 WASHINGTON - BARRE TOWN: VT. 110 WASHINGTON MM 5.724 ~ BARRE TOWN MM 1.124 = 22255.200 FT. = 4.215 MILES
 THETFORD - FAIRLEE: VT. 244 THETFORD MM 0.000 ~ FAIRLEE MM 2.646 = 29779.200 FT. = 5.640 MILES
 TUNBRIDGE: VT. 110 MM 0.259 ~ MM 1.591 = 7032.960 FT. = 1.332 MILES
 CHELSEA: VT. 110 MM 3.332 ~ MM 6.960 = 19155.840 FT. = 3.628 MILES
 TOTAL CONTRACT LENGTH = 96656.00 FT. = 18.306 MILES

PROJECT CONSISTS OF LEVELING AND OVERLAYING THE ABOVE MENTIONED ROUTES WITH BITUMINOUS CONCRETE PAVEMENT.



TRAFFIC DATA
 1989 ADT

TUNBRIDGE: I290
THETFORD-FAIRLEE: I340
WILLIAMSTOWN: I190
WASHINGTON-BARRE TOWN: 1650
TOPSHAM-ORANGE: 1100
CHELSEA: 1230

V = 50 MPH (UNLESS OTHERWISE NOTED)

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE CHIEF ENGINEER. CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 1986, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON NOVEMBER 21, 1985 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

SUBMITTED BY ORDER OF THE STATE TRANSPORTATION BOARD

APPROVED *David D. Hickey* DATE *2/16/89*
 CHIEF ENGINEER

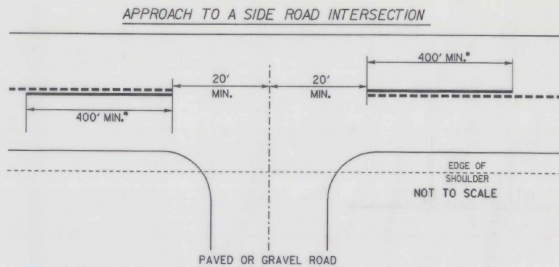
DEPARTMENT OF TRANSPORTATION
 FEDERAL HIGHWAY ADMINISTRATION

APPROVED _____ DATE _____
 DIVISION ADMINISTRATOR

HMA
 PROJECT 2017, 2018, 2021, 2022, 2023, 2114
 SHEET 1 OF 25 SHEETS

PAVEMENT MARKING DETAILS

NOT TO SCALE

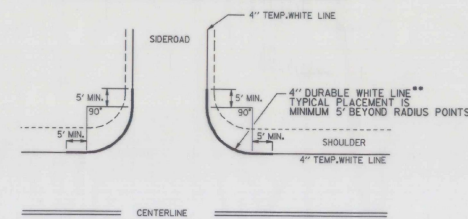


A SOLID LINE IN THE DIRECTION OF TRAVEL IS BEGUN AT A LOCATION 400 FEET IN ADVANCE OF THE INTERSECTION. THE DISTANCE IS MEASURED FROM A POINT 20 FEET IN ADVANCE OF THE CENTERLINE OF THE INTERSECTING ROADWAY. ADJUSTMENTS TO THE 20 FOOT OPENING MAY BE MADE TO ACCOMMODATE SKEWED INTERSECTIONS.

* CENTERLINE TREATMENT SHALL CONSIST OF A MINIMUM OF 400 FEET OF SOLID LINE IN ADVANCE OF THE INTERSECTION AND SHALL BE PAIRED WITH EITHER A SOLID OR DASHED LINE DEPENDING ON SIGHT DISTANCE AVAILABILITY IN THE OPPOSING LANE.

BUILT AS DESIGNED

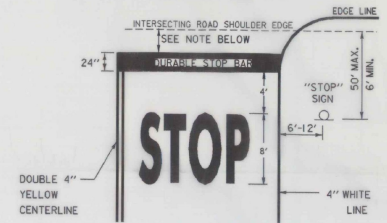
DURABLE EDGELINE RADIUS LAYOUT TYPICAL



** FOR ALL PAVED TOWN HIGHWAYS AND FOR CLASS 3 TOWN HIGHWAYS, 22'+ WIDE.

PAVEMENT MARKING DETAILS

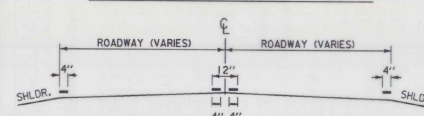
(SEE TRAFFIC NOTE "F")



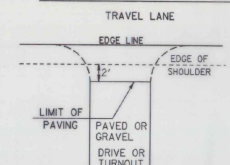
THE "DESIRED STOPPING POINT" IS THE LOCATION BASED ON SITE CONDITIONS THAT BEST ALLOWS THE STOPPED VEHICLE TO VIEW THE APPROACHING TRAFFIC. IN NO CASE MORE THAN 30 FEET OR LESS THAN 4 FEET FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY.

TOPSHAM - ORANGE HMA 2023		TRAFFIC SIGN SUMMARY SHEET							PLAN SHEET NUMBER	STD. SHEET NUMBER	
MILE MARKER, STATION, OR SIGN NUMBER	SIGN LEGEND	SIGN DIMENSIONS	NEW SIGNS "A"	NEW SIGN POSTS					REMARKS		
				NUMBER OF POSTS	FLANGED 2.0	CHANNEL 2.5	TUBULAR ALUMINUM 3.0	3.0" x 3.0" 4.0" x 4.0" 4.0" x 4.0"			
TOWN OF ORANGE											
U.S. 302 / VT. 25 INTERSECT.											
ASSEMBLY #1		36" x 30"	3.90 6.25	1		X			BACK TO BACK	9	E-143
ASSEMBLY #2		30" x 30"	6.25	1		X				9	E-143
FINAL POST LENGTHS ARE TO BE DETERMINED IN THE FIELD. POST SIZES ARE COMPUTED BASED ON INFORMATION FURNISHED ON THE STANDARD SHEETS AND THE DESIGN DIVISION'S "SIGN POST DESIGN MANUAL".			SF.								
TOTALS			16.40			63					

PAVEMENT MARKING PLACEMENT DETAIL

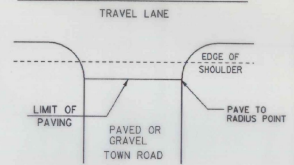


PAVING LIMITS FOR DRIVES



NOTE: PAVING LIMITS FOR ALL GRAVEL TURNOUTS SHALL BE THE SAME AS FOR DRIVES (2')

PAVING LIMITS FOR TOWN ROADS



TRAFFIC NOTES

GUIDELINES FOR MINIMUM INTERIM PAVEMENT MARKINGS FOR CONSTRUCTION ZONES

- DEFINITION - INTERIM MARKINGS ARE MARKINGS THAT ARE PLACED PRIOR TO THE APPLICATION OF TEMPORARY PAVEMENT MARKINGS AS SPECIFIED IN THE PROJECT PLANS. THE COST ASSOCIATED WITH THE INTERIM MARKINGS INCLUDING MATERIAL COST, PLACEMENT COSTS, AND REMOVAL COSTS SHALL BE SUBSIDIARY TO THE SPECIFIED PAVEMENT MARKINGS.
- INTERIM MARKINGS SHALL CONSIST OF TAPE OR TEMPORARY RAISED PAVEMENT MARKERS (RPM'S)
 - THE TAPE SHALL BE THE TYPE THAT CAN BE PAVED OVER OR OF A TYPE EASILY REMOVED IF PLACED ON THE FINAL PAVEMENT SURFACE. INTERIM TAPE MARKINGS OR RPM'S WILL BE OFFSET AND REMOVED PRIOR TO THE APPLICATION OF THE SPECIFIC MARKINGS AND SHALL BE APPLIED AS DESCRIBED BELOW.
- CENTERLINE AND CORE AREA MARKINGS SHALL BE APPLIED AT THE END OF EACH WORKING DAY. THE FOLLOWING LAYOUT REQUIREMENTS SHALL BE MET:
 - NO PASSING ZONE
 - SOLID STRIPES - DOUBLE YELLOW REMOVABLE PAVEMENT MARKING TAPE LINES SPACED 4" APART.
 - RAISED PAVEMENT MARKERS - IN NO CASE SHALL TEMPORARY RAISED PAVEMENT MARKERS BE USED FOR MORE THAN THREE CALENDAR DAYS BEFORE THE APPLICATION OF THE SPECIFIED MARKINGS.

RAISED PAVEMENT MARKERS (REMOVABLE) WHEN USED, SHALL BE PLACED ON 5' CENTERS WITH EVERY 4TH MARKER BEING REFLECTORIZED. THE RAISED PAVEMENT MARKINGS SHALL BE SUPPLEMENTED WITH BLACK ON ORANGE "NO PASSING" SIGNS PLACED AT THE BEGINNING OF THE NO PASSING ZONE AND EVERY 100' THEREAFTER THROUGHOUT THE NO PASSING ZONE. ALL TEMPORARY RAISED PAVEMENT MARKERS SHALL BE REMOVED IMMEDIATELY UPON PLACEMENT OF THE SPECIFIED MARKINGS. ALL COSTS ASSOCIATED WITH "NO PASSING" SIGNS AND THEIR INSTALLATION SHALL BE SUBSIDIARY TO THE SPECIFIED PAVEMENT MARKINGS.

DASHED LINE
-10-FOOT SOLID LINE WITH 30-FOOT GAP.

OR
FOR MARKINGS TO BE USED FOR THREE DAYS OR LESS
-4 FOOT SOLID LINE WITH 36 FOOT GAP TO BE REMOVED IMMEDIATELY UPON THE APPLICATION OF THE SPECIFIED MARKINGS.

OR
-4 RETRO-REFLECTIVE RPM'S ON THREE FOOT CENTERS WITH A 30 FOOT GAP TO BE REMOVED IMMEDIATELY UPON THE APPLICATION OF THE SPECIFIED MARKINGS.

CORE AREAS
CORE AREAS TO INCLUDE 8 INCH CHANNELIZING LINE AND 4" DASHED LINE. SEE STANDARD E-19L.

D. EDGE LINES
WHEN SPECIFIED, EDGE LINES ARE NOT REQUIRED UNTIL COMPLETION OF THE PROJECT. EDGE LINES SHOULD BE APPLIED WHERE TRAFFIC VOLUMES ARE HIGH AND/OR A DELAY OF SEVERAL DAYS IS ANTICIPATED. IN THESE INSTANCES, PAINT OR RPM'S CAN BE USED AS THE INTERIM MARKING MATERIAL TO BE PAID UNDER THE APPROPRIATE BID ITEM. RPM'S SHOULD BE RETRO-REFLECTIVE TEMPORARY MARKERS ON A 5' SPACING PATTERN AND THE SAME COLOR AS THE LINE THEY ARE SUBSTITUTING FOR.

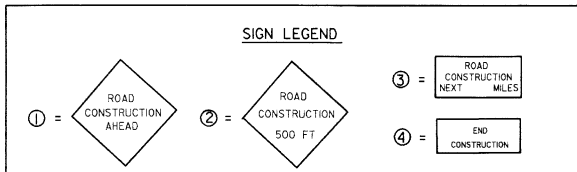
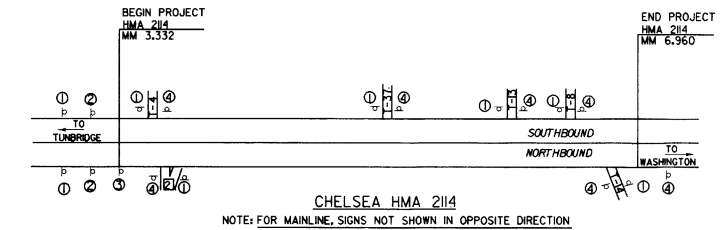
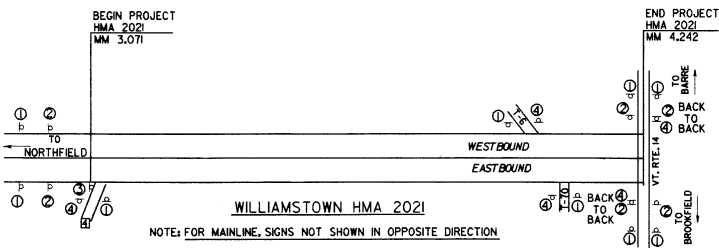
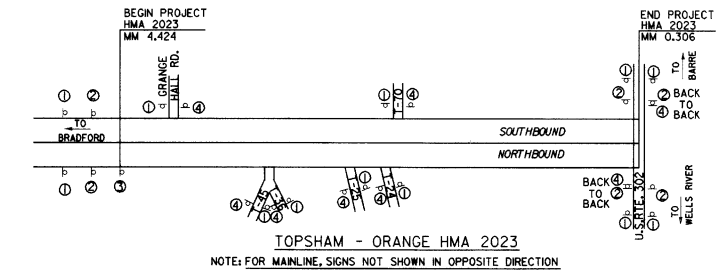
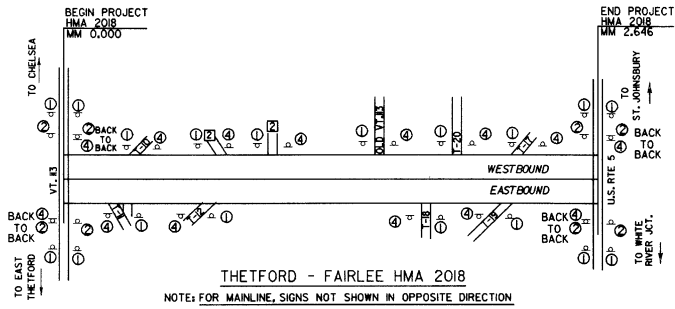
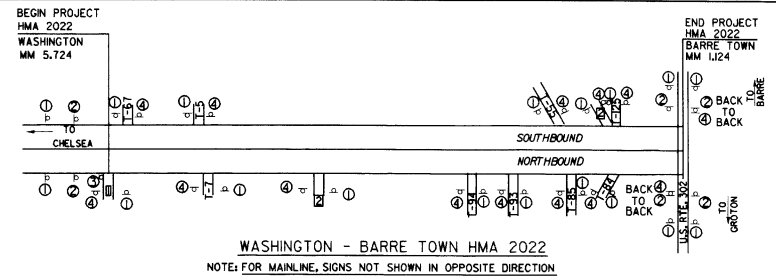
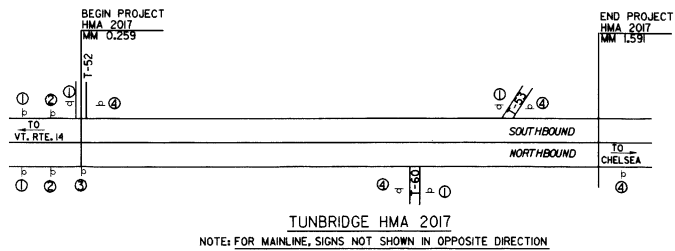
E. PRIOR TO ACCEPTANCE, THE PAVEMENT MARKINGS SHALL BE COMPLETED FOR THE ENTIRE PROJECT BY THE CONTRACTOR AS DETAILED ON THE PLANS OR DIRECTED BY THE RESIDENT ENGINEER.

F. THE DURABLE STOP BAR SHALL BE APPLIED AT ALL SIDEROAD LOCATIONS, BUT THE DURABLE "STOP" MARKINGS ARE TO BE APPLIED ONLY ON PAVED SIDE ROADS OR AS DIRECTED BY THE RESIDENT ENGINEER.

G. IT IS NOT NECESSARY TO REFLECTORIZE CONES AND BARRELS FOR DAYTIME USE.

NOT TO SCALE HMA2904MK

SURVEYED BY DATE
DRAWN BY MCCLURE DATE 11/88
SQUAD LEADER BDD
DESIGN FILE NO. HMA2904.DGN DATE 2/89
PROJ. NAME HMA PLOTTED
PROJ. NO. 2017, 2018, 2021, 2022, 2023, 2114
SHEET 3 OF 25 SHEETS



CONSTRUCTION APPROACH SIGNING
 FOR SIGN PLACEMENT LOCATION, SEE STANDARD E-100

NOT TO SCALE DATE PLOTTED 2/89

DRAWN BY MCCLURE DATE 2/89
 SQUAD LEADER BRUCE E. BOYD
 DESIGN FILE NO. ZFA2150.54PVA1NG08.DGN

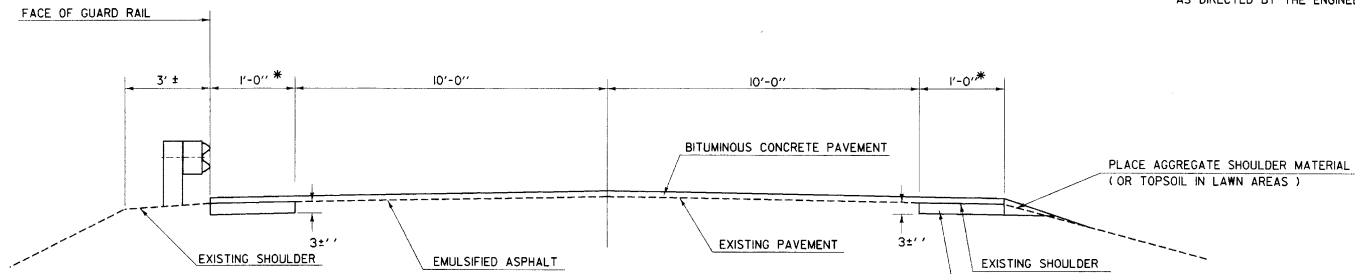
PROJ. NAME HMA
 PROJ. NO. 2017, 2018, 2021, 2022, 2023, 214
 SHEET 4 OF 25 SHEETS

TYPICAL SECTION AND PAVEMENT MARKINGS

BUILT AS DESIGNED

1/4" BITUMINOUS CONCRETE PAVEMENT (± 1/4")
 325 AVERAGE TONS, PER/MILE LEVELING COURSE OF BITUMINOUS CONCRETE PAVEMENT
 EMULSIFIED ASPHALT, TO BE APPLIED AT THE RATE OF .015 GAL./SQ. YD.
 OR AS DIRECTED BY THE ENGINEER.

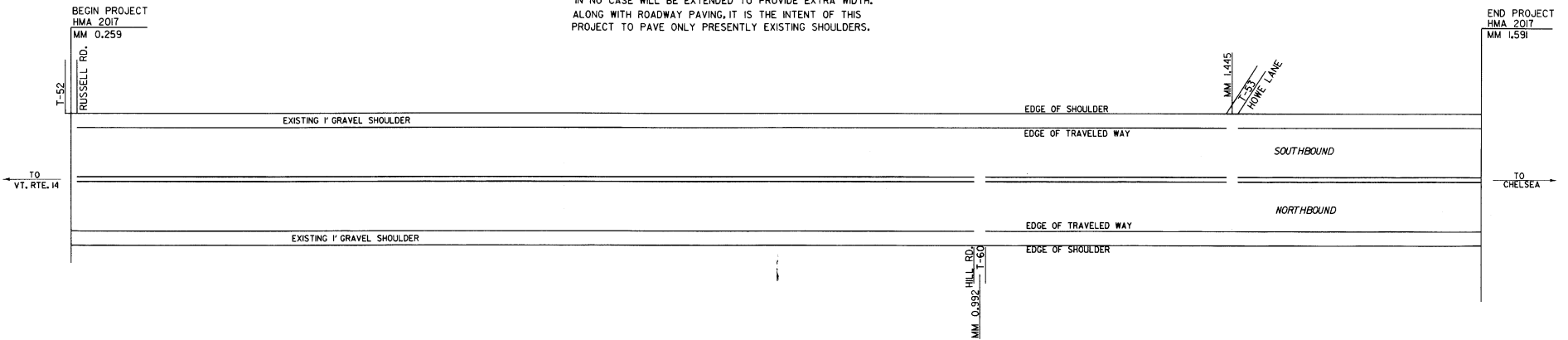
NOTE: THE WEARING COURSE WILL BE TYPE III.
 THE LEVELING COURSE MAY BE TYPE III OR TYPE IV,
 AS DIRECTED BY THE ENGINEER.



VT. 110 MILE 0.259 TO MILE 1.591
NEW PAVED SHOULDERS OVER EXISTING GRAVEL SHOULDERS

* A UNIFORM PAVED WIDTH WILL BE PROVIDED AS SHOWN TO THE EXTENT FEASIBLE. HOWEVER, EXISTING SHOULDERS IN NO CASE WILL BE EXTENDED TO PROVIDE EXTRA WIDTH. ALONG WITH ROADWAY PAVING, IT IS THE INTENT OF THIS PROJECT TO PAVE ONLY PRESENTLY EXISTING SHOULDERS.

IF EXISTING SHOULDER MATERIAL IS DEEMED UNSUITABLE BY THE RESIDENT ENGINEER, IT SHALL BE EXCAVATED TO A DEPTH OF 3+ ". EXCAVATION SHALL BE PAID AS ALL PURPOSE EXCAVATOR RENTAL OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH AGGREGATE SHOULDER MATERIAL IMMEDIATELY.



DURABLE 4" WHITE LINE
 FOR CLASS 3 TOWN HIGHWAYS, 22'+ WIDE

DURABLE 24" STOP BAR
 FOR CLASS 3 TOWN HIGHWAYS, 22'+ WIDE

TEMPORARY 4" YELLOW LINE
 MM 0.263 ~ MM 0.988, CL (DOUBLE)
 MM 0.996 ~ MM 1.441, CL (DOUBLE)
 MM 1.449 ~ MM 1.591, CL (DOUBLE)

TEMPORARY 4" WHITE LINE
 MM 0.263 ~ MM 1.441 LT.
 MM 1.449 ~ MM 1.591 LT.
 MM 0.259 ~ MM 0.988 RT.
 MM 0.996 ~ MM 1.591 RT.

DUMFRIES	
DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

NOT TO SCALE DATE PLOTTED 10/88

DRAWN BY	MCCLURE	DATE	10/88
SQUAD LEADER	BRUCE E. BOYD		
DESIGN FILE NO.	ZFA260.54PAVING88.DGN		
PROJ. NAME	TUNBRIDGE		
PROJ. NO.	HMA 2017		
SHEET	5	OF	25 SHEETS

TYPICAL SECTION AND PAVEMENT MARKINGS

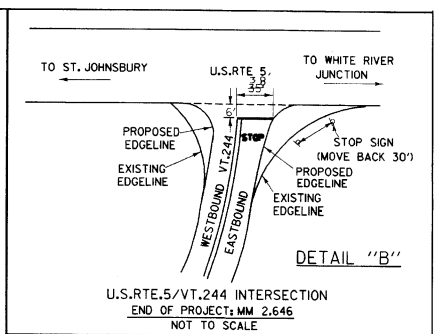
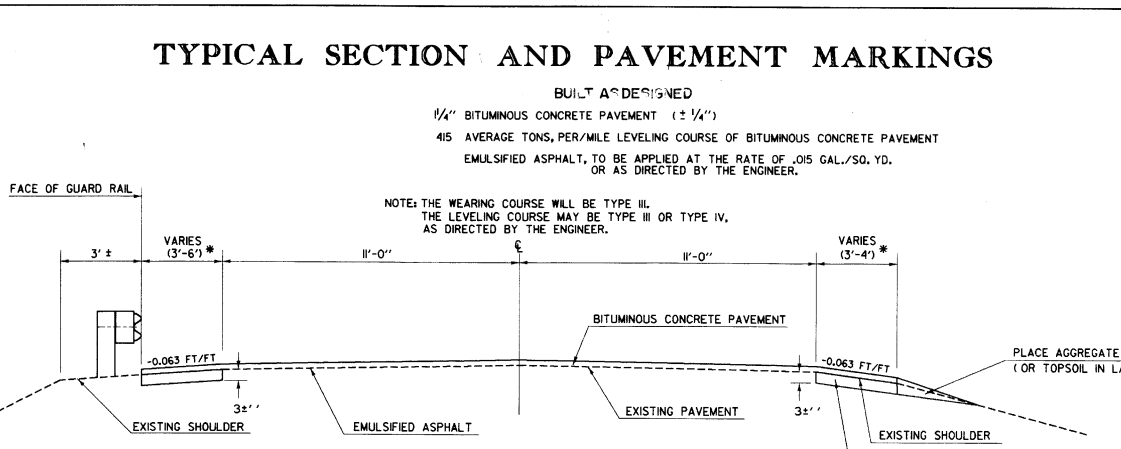
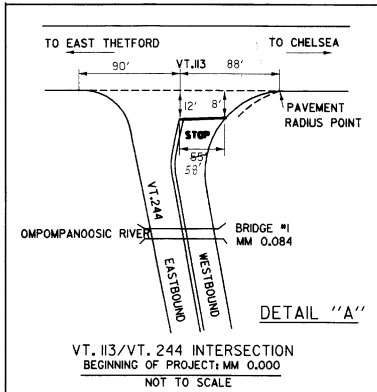
BUILT AS DESIGNED

1/4" BITUMINOUS CONCRETE PAVEMENT (± 1/4")

415 AVERAGE TONS, PER MILE LEVELING COURSE OF BITUMINOUS CONCRETE PAVEMENT

EMULSIFIED ASPHALT, TO BE APPLIED AT THE RATE OF .015 GAL./SQ. YD. OR AS DIRECTED BY THE ENGINEER.

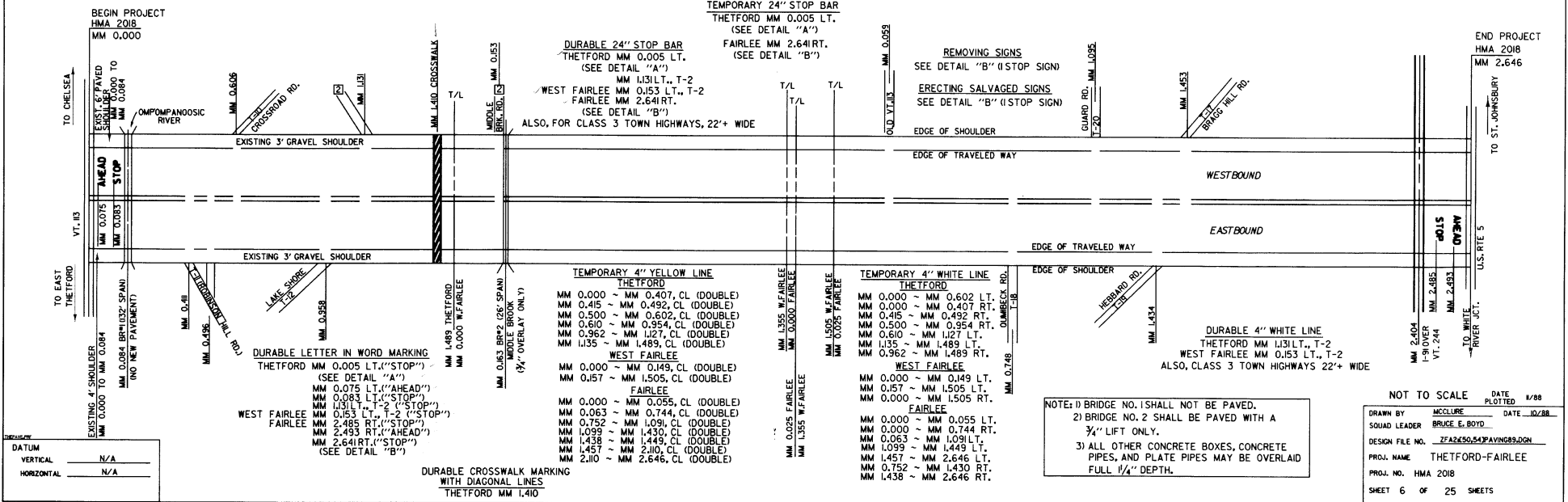
NOTE: THE WEARING COURSE WILL BE TYPE III, THE LEVELING COURSE MAY BE TYPE III OR TYPE IV, AS DIRECTED BY THE ENGINEER.



* A UNIFORM PAVED WIDTH WILL BE PROVIDED AS SHOWN TO THE EXTENT FEASIBLE. HOWEVER, EXISTING SHOULDERS IN NO CASE WILL BE EXTENDED TO PROVIDE EXTRA WIDTH. ALONG WITH ROADWAY PAVING, IT IS THE INTENT OF THIS PROJECT TO PAVE ONLY PRESENTLY EXISTING SHOULDERS.

VT. 244 THETFORD MILE 0.000 TO FAIRLEE MILE 2.646
NEW PAVED SHOULDERS OVER EXISTING GRAVEL SHOULDERS

IF EXISTING SHOULDER MATERIAL IS DEEMED UNSUITABLE BY THE RESIDENT ENGINEER, IT SHALL BE EXCAVATED TO A DEPTH OF 3'-4". EXCAVATION SHALL BE PAID AS ALL PURPOSE EXCAVATOR RENTAL OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH AGGREGATE SHOULDER MATERIAL IMMEDIATELY.



NOTE: 1) BRIDGE NO. 1 SHALL NOT BE PAVED.
2) BRIDGE NO. 2 SHALL BE PAVED WITH A 3/4" LIFT ONLY.
3) ALL OTHER CONCRETE BOXES, CONCRETE PIPES, AND PLATE PIPES MAY BE OVERLAID FULL 1/4" DEPTH.

NOT TO SCALE DATE PLOTTED 1/88
DRAWN BY MCCLURE DATE 10/88
SQUAD LEADER BRUCE E. BOYD
DESIGN FILE NO. ZFA245054PAVING88.DGN
PROJ. NAME THETFORD-FAIRLEE
PROJ. NO. HMA 2018
SHEET 6 OF 25 SHEETS

TYPICAL SECTION AND PAVEMENT MARKINGS

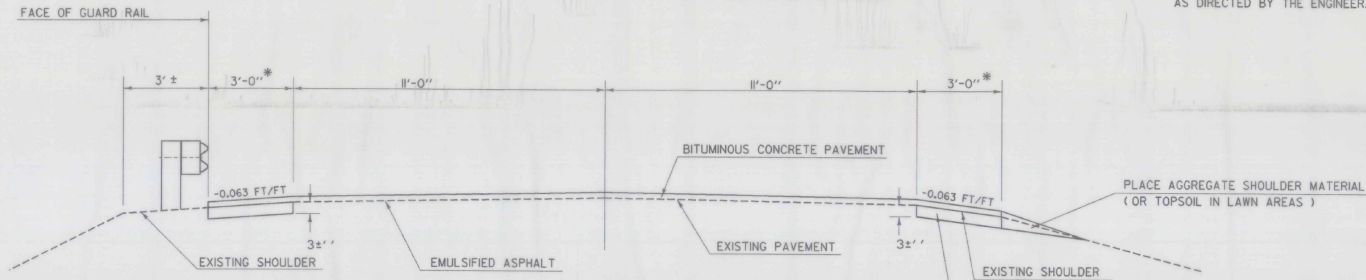
BUILT AS DESIGNED

1/4" BITUMINOUS CONCRETE PAVEMENT (± 1/4")

745 AVERAGE TONS, PER/MILE LEVELING COURSE OF BITUMINOUS CONCRETE PAVEMENT

EMULSIFIED ASPHALT, TO BE APPLIED AT THE RATE OF .015 GAL./SQ. YD. OR AS DIRECTED BY THE ENGINEER.

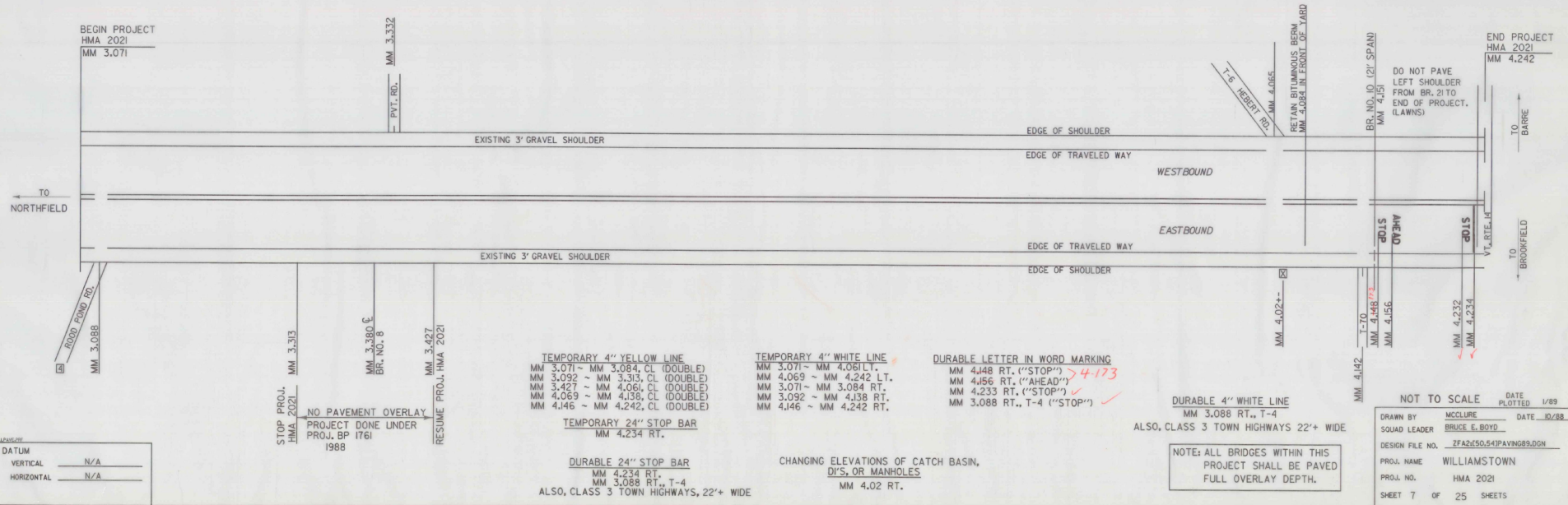
NOTE: THE WEARING COURSE WILL BE TYPE III. THE LEVELING COURSE MAY BE TYPE III OR TYPE IV, AS DIRECTED BY THE ENGINEER.



* A UNIFORM PAVED WIDTH WILL BE PROVIDED AS SHOWN TO THE EXTENT FEASIBLE. HOWEVER, EXISTING SHOULDERS IN NO CASE WILL BE EXTENDED TO PROVIDE EXTRA WIDTH. ALONG WITH ROADWAY PAVING, IT IS THE INTENT OF THIS PROJECT TO PAVE ONLY PRESENTLY EXISTING SHOULDERS.

VT. 64 MILE 3.071 TO MILE 4.242
NEW PAVED SHOULDERS OVER EXISTING GRAVEL SHOULDERS

IF EXISTING SHOULDER MATERIAL IS DEEMED UNSUITABLE BY THE RESIDENT ENGINEER, IT SHALL BE EXCAVATED TO A DEPTH OF 3+-", EXCAVATION SHALL BE PAID AS ALL PURPOSE EXCAVATOR RENTAL OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH AGGREGATE SHOULDER MATERIAL IMMEDIATELY.



DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

STOP PROJ. HMA 2021 MM 3.313
NO PAVEMENT OVERLAY PROJECT DONE UNDER PROJ. BP 1761 1988
RESUME PROJ. HMA 2021 MM 3.427

TEMPORARY 4" YELLOW LINE
MM 3.071 ~ MM 3.084, CL (DOUBLE)
MM 3.092 ~ MM 3.313, CL (DOUBLE)
MM 3.427 ~ MM 4.061, CL (DOUBLE)
MM 4.063 ~ MM 4.136, CL (DOUBLE)
MM 4.146 ~ MM 4.242, CL (DOUBLE)

TEMPORARY 24" STOP BAR
MM 4.234 RT.

DURABLE 24" STOP BAR
MM 4.234 RT.
MM 3.088 RT., T-4
ALSO, CLASS 3 TOWN HIGHWAYS, 22'+ WIDE

TEMPORARY 4" WHITE LINE
MM 3.071 ~ MM 4.061 LT.
MM 4.069 ~ MM 4.242 LT.
MM 3.071 ~ MM 3.084 RT.
MM 3.092 ~ MM 4.136 RT.
MM 4.146 ~ MM 4.242 RT.

CHANGING ELEVATIONS OF CATCH BASIN, D/S, OR MANHOLES
MM 4.02 RT.

DURABLE LETTER IN WORD MARKING
MM 4.148 RT. ("STOP")
MM 4.156 RT. ("AHEAD")
MM 4.233 RT. ("STOP")
MM 3.088 RT., T-4 ("STOP")

DURABLE 4" WHITE LINE
MM 3.088 RT., T-4
ALSO, CLASS 3 TOWN HIGHWAYS 22'+ WIDE

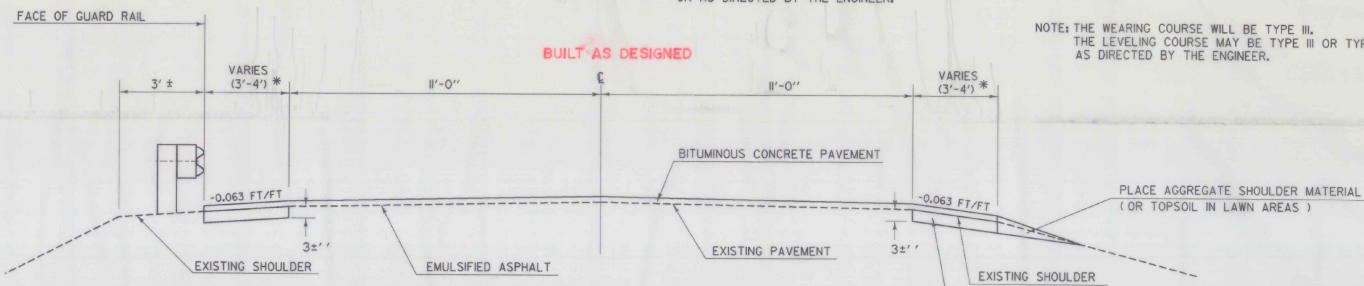
NOTE: ALL BRIDGES WITHIN THIS PROJECT SHALL BE PAVED FULL OVERLAY DEPTH.

NOT TO SCALE DATE PLOTTED 1/89
DRAWN BY MCCLURE DATE 10/88
SQUAD LEADER BRUCE E. BOYD
DESIGN FILE NO. 2FA2150.54PAVING89.DGN
PROJ. NAME WILLIAMSTOWN
PROJ. NO. HMA 2021
SHEET 7 OF 25 SHEETS

TYPICAL SECTION AND PAVEMENT MARKINGS

1/4" BITUMINOUS CONCRETE PAVEMENT (± 1/4")
 450 AVERAGE TONS, PER/MILE LEVELING COURSE OF BITUMINOUS CONCRETE PAVEMENT
 EMULSIFIED ASPHALT, TO BE APPLIED AT THE RATE OF .015 GAL./SQ. YD.
 OR AS DIRECTED BY THE ENGINEER.

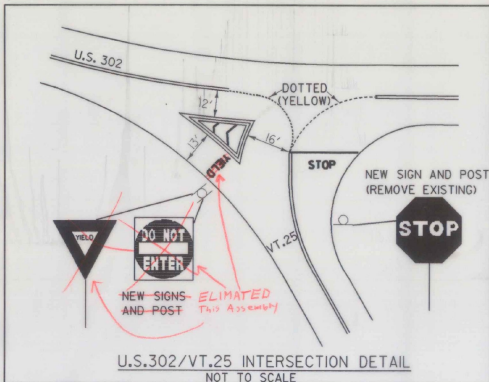
NOTE: THE WEARING COURSE WILL BE TYPE III, THE LEVELING COURSE MAY BE TYPE III OR TYPE IV, AS DIRECTED BY THE ENGINEER.



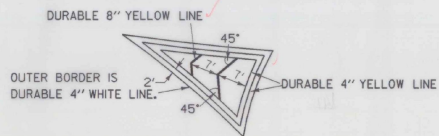
VT. 25 TOPSHAM MILE 4.424 TO ORANGE MILE 0.306
 NEW PAVED SHOULDERS OVER EXISTING GRAVEL SHOULDERS

* A UNIFORM PAVED WIDTH WILL BE PROVIDED AS SHOWN TO THE EXTENT FEASIBLE. HOWEVER, EXISTING SHOULDERS IN NO CASE WILL BE EXTENDED TO PROVIDE EXTRA WIDTH. ALONG WITH ROADWAY PAVING, IT IS THE INTENT OF THIS PROJECT TO PAVE ONLY PRESENTLY EXISTING SHOULDERS.

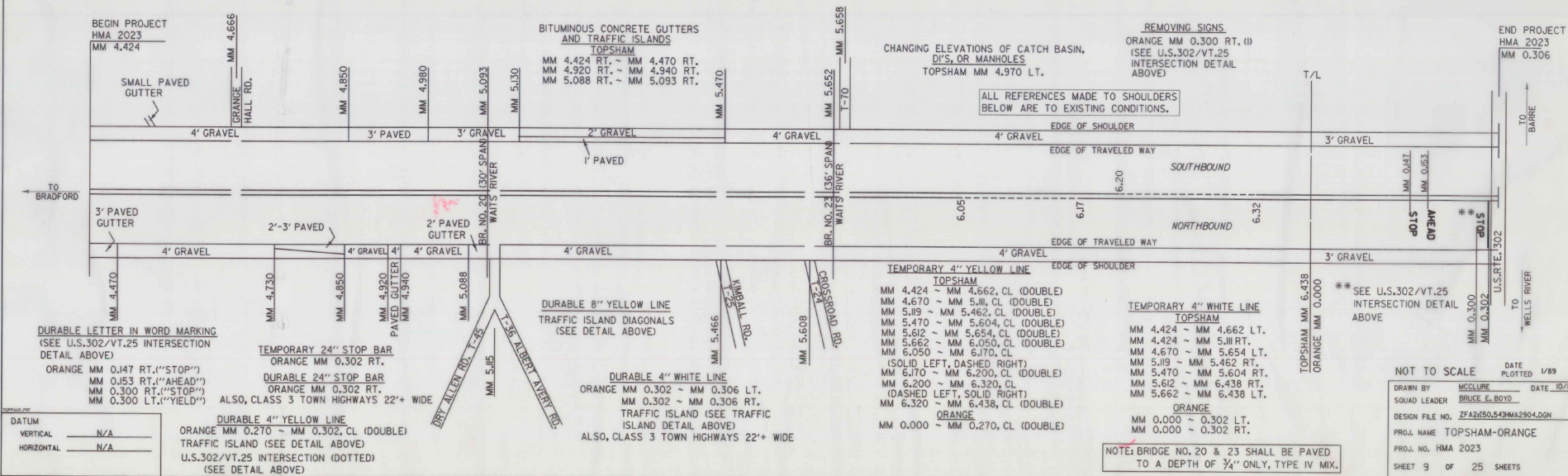
IF EXISTING SHOULDER MATERIAL IS DEEMED UNSUITABLE BY THE RESIDENT ENGINEER, IT SHALL BE EXCAVATED TO A DEPTH OF 3'-". EXCAVATION SHALL BE PAID AS ALL PURPOSE EXCAVATOR RENTAL OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH AGGREGATE SHOULDER MATERIAL IMMEDIATELY.



U.S. 302/VT. 25 INTERSECTION DETAIL
 NOT TO SCALE



TRAFFIC ISLAND DETAIL
 NOT TO SCALE



TOPSHAM, VT.

DATUM	
VERTICAL	N/A
HORIZONTAL	N/A

DURABLE 4" YELLOW LINE
 ORANGE MM 0.270 ~ MM 0.302, CL (DOUBLE)
 TRAFFIC ISLAND (SEE DETAIL ABOVE)
 U.S. 302/VT. 25 INTERSECTION (DOTTED)
 (SEE DETAIL ABOVE)

DURABLE 4" WHITE LINE
 ORANGE MM 0.302 ~ MM 0.306 LT.
 MM 0.302 ~ MM 0.306 RT.
 TRAFFIC ISLAND (SEE TRAFFIC ISLAND DETAIL ABOVE)
 ALSO, CLASS 3 TOWN HIGHWAYS 22'+ WIDE

TEMPORARY 4" YELLOW LINE
 TOPSHAM
 MM 4.424 ~ MM 4.662, CL (DOUBLE)
 MM 4.670 ~ MM 5.111, CL (DOUBLE)
 MM 5.119 ~ MM 5.462, CL (DOUBLE)
 MM 5.470 ~ MM 5.604, CL (DOUBLE)
 MM 5.612 ~ MM 5.654, CL (DOUBLE)
 MM 5.662 ~ MM 6.050, CL (DOUBLE)
 MM 6.050 ~ MM 6.170, CL (DOUBLE)
 (SOLID LEFT, DASHED RIGHT)
 MM 6.170 ~ MM 6.200, CL (DOUBLE)
 MM 6.200 ~ MM 6.320, CL (DOUBLE)
 (DASHED LEFT, SOLID RIGHT)
 MM 6.320 ~ MM 6.438, CL (DOUBLE)
 ORANGE
 MM 0.000 ~ MM 0.270, CL (DOUBLE)

TEMPORARY 4" WHITE LINE
 TOPSHAM
 MM 4.424 ~ MM 4.662 LT.
 MM 4.424 ~ MM 5.111 RT.
 MM 4.670 ~ MM 5.654 LT.
 MM 5.119 ~ MM 5.462 RT.
 MM 5.470 ~ MM 5.604 RT.
 MM 5.612 ~ MM 6.438 RT.
 ORANGE
 MM 0.000 ~ 0.302 LT.
 MM 0.000 ~ 0.302 RT.

NOTE: BRIDGE NO. 20 & 23 SHALL BE PAVED TO A DEPTH OF 74" ONLY, TYPE IV MIX.

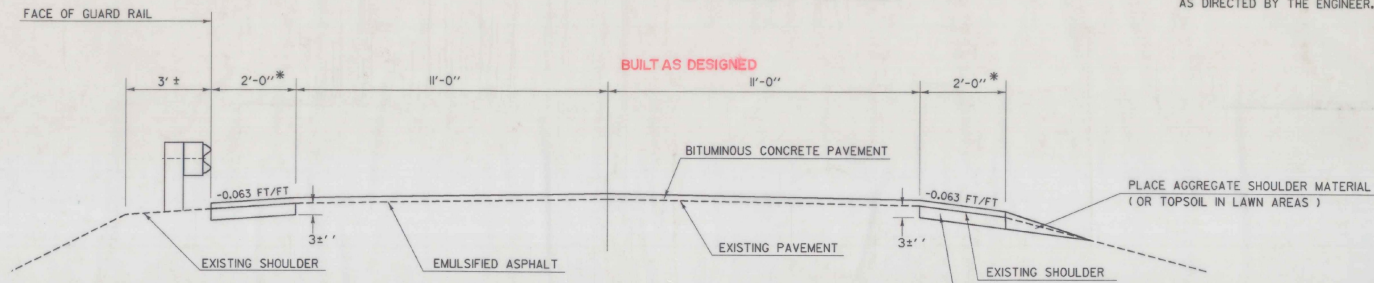
NOT TO SCALE DATE PLOTTED 1/89

DRAWN BY	MCCLURE	DATE	10/88
SQUAD LEADER	BRUCE E. BODI		
DESIGN FILE NO.	ZF22050.543RMA2804.DGN		
PROJ. NAME	TOPSHAM-ORANGE		
PROJ. NO.	HMA 2023		
SHEET	9	OF	25 SHEETS

TYPICAL SECTION AND PAVEMENT MARKINGS

1/4" BITUMINOUS CONCRETE PAVEMENT (± 1/4")
 385 AVERAGE TONS, PER/MILE LEVELING COURSE OF BITUMINOUS CONCRETE PAVEMENT
 EMULSIFIED ASPHALT, TO BE APPLIED AT THE RATE OF .015 GAL./SQ. YD.
 OR AS DIRECTED BY THE ENGINEER.

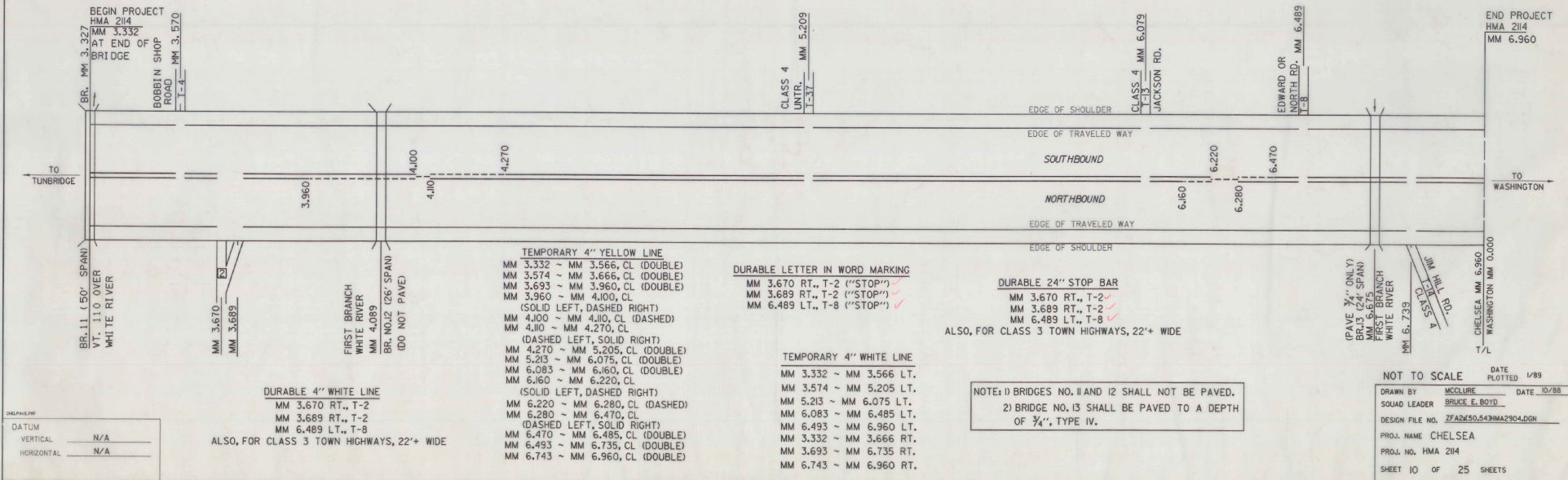
NOTE: THE WEARING COURSE WILL BE TYPE III.
 THE LEVELING COURSE MAY BE TYPE III OR TYPE IV,
 AS DIRECTED BY THE ENGINEER.



* A UNIFORM PAVED WIDTH WILL BE PROVIDED AS SHOWN TO THE EXTENT FEASIBLE. HOWEVER, EXISTING SHOULDERS IN NO CASE WILL BE EXTENDED TO PROVIDE EXTRA WIDTH. ALONG WITH ROADWAY PAVING, IT IS THE INTENT OF THIS PROJECT TO PAVE ONLY PRESENTLY EXISTING SHOULDERS.

VT. 110 MILE 3,332 TO MILE 6,960
 NEW PAVED SHOULDERS OVER EXISTING GRAVEL SHOULDERS

IF EXISTING SHOULDER MATERIAL IS DEEMED UNSUITABLE BY THE RESIDENT ENGINEER, IT SHALL BE EXCAVATED TO A DEPTH OF 3'-0". EXCAVATION SHALL BE PAID AS ALL PURPOSE EXCAVATOR RENTAL OR GRADER RENTAL. MATERIAL REMOVED SHALL BE REPLACED WITH AGGREGATE SHOULDER MATERIAL IMMEDIATELY.



DRAWN BY: _____
 DATUM: _____
 VERTICAL: N/A
 HORIZONTAL: N/A

DURABLE 4" WHITE LINE
 MM 3,670 RT., T-2
 MM 3,689 RT., T-2
 MM 6,489 LT., T-8
 ALSO, FOR CLASS 3 TOWN HIGHWAYS, 22'+ WIDE

TEMPORARY 4" YELLOW LINE
 MM 3,332 ~ MM 3,566, CL (DOUBLE)
 MM 3,574 ~ MM 3,666, CL (DOUBLE)
 MM 3,693 ~ MM 3,960, CL (DOUBLE)
 MM 3,960 ~ MM 4,100, CL (SOLID LEFT, DASHED RIGHT)
 MM 4,100 ~ MM 4,110, CL (DASHED)
 MM 4,110 ~ MM 4,270, CL (DASHED LEFT, SOLID RIGHT)
 MM 4,270 ~ MM 5,205, CL (DOUBLE)
 MM 5,213 ~ MM 6,075, CL (DOUBLE)
 MM 6,083 ~ MM 6,160, CL (DOUBLE)
 MM 6,160 ~ MM 6,220, CL (SOLID LEFT, DASHED RIGHT)
 MM 6,220 ~ MM 6,280, CL (DASHED)
 MM 6,280 ~ MM 6,470, CL (DASHED LEFT, SOLID RIGHT)
 MM 6,470 ~ MM 6,485, CL (DOUBLE)
 MM 6,493 ~ MM 6,735, CL (DOUBLE)
 MM 6,743 ~ MM 6,960, CL (DOUBLE)

DURABLE LETTER IN WORD MARKING
 MM 3,670 RT., T-2 ("STOP")
 MM 3,689 RT., T-2 ("STOP")
 MM 6,489 LT., T-8 ("STOP")

TEMPORARY 4" WHITE LINE
 MM 3,332 ~ MM 3,566 LT.
 MM 3,574 ~ MM 5,205 LT.
 MM 5,213 ~ MM 6,075 LT.
 MM 6,083 ~ MM 6,485 LT.
 MM 6,493 ~ MM 6,960 LT.
 MM 3,332 ~ MM 3,666 RT.
 MM 3,693 ~ MM 6,735 RT.
 MM 6,743 ~ MM 6,960 RT.

DURABLE 24" STOP BAR
 MM 3,670 RT., T-2
 MM 3,689 RT., T-2
 MM 6,489 LT., T-8

ALSO, FOR CLASS 3 TOWN HIGHWAYS, 22'+ WIDE

NOTE: 1) BRIDGES NO. 11 AND 12 SHALL NOT BE PAVED.
 2) BRIDGE NO. 13 SHALL BE PAVED TO A DEPTH OF 3/4", TYPE IV.

NOT TO SCALE DATE PLOTTED 1/89
 DRAWN BY: MCCLURE BRUCE E. BOYD DATE: 10/88
 SQUAD LEADER: BRUCE E. BOYD
 DESIGN FILE NO.: ZFA2E50.549MA2904.DGN
 PROJ. NAME: CHELSEA
 PROJ. NO.: HMA 2114
 SHEET 10 OF 25 SHEETS

89

(W)

THETFORD-FAIRLEE HMA 2018
 TOPSHAM-ORANGE HMA 2023
 CHELSEA HMA 2114

Williamstown, Barre Town, orange
 Washington, Topsham, Tunbridge,
 Chelsea,

	2017	2114
	2018	
H.M.A.	2021	
	2022	
	2023	