

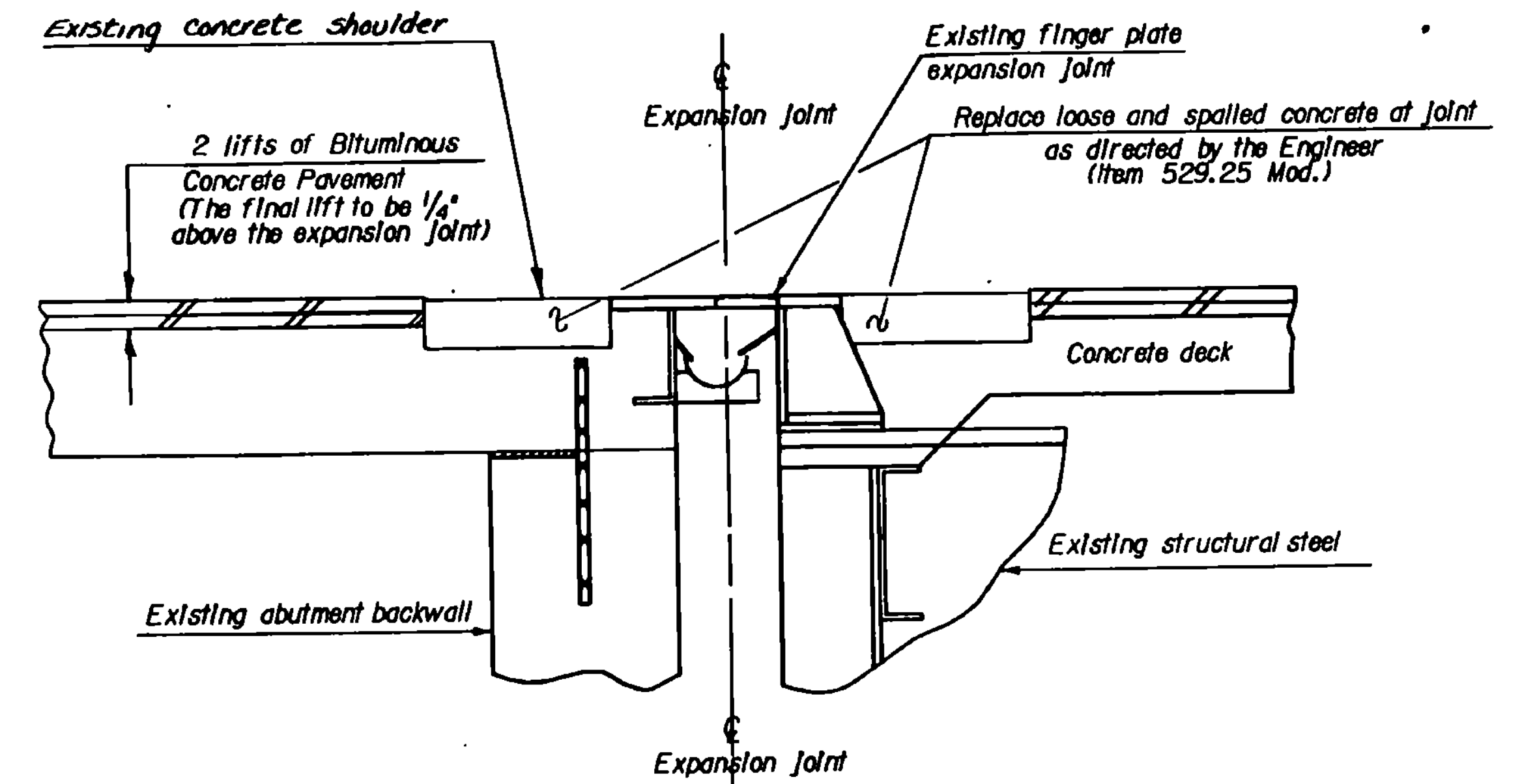
SPECIFIC NOTES

BR 58A US 5 OVER I 91

1. THERE ARE NO SCUPPERS ON THIS BRIDGE.
2. ALL WEEP PIPES SHALL BE SEALED OVER WITH SHEET MEMBRANE WATERPROOFING.
3. REPAIR COMPRESSION SEALS AT THE SOUTH END OF THE BRIDGE AND IN THE CURBS AT THE PIERS, AS DETERMINED BY THE ENGINEER, IN ACCORDANCE WITH DETAILS 4 AND 5 ON SHEET 11. THE WIDTH OF THE REPLACEMENT COMPRESSION SEAL SHALL BE 1 3/4 TIMES THE WIDTH OF THE OPENING AS MEASURED IN THE FIELD. THE CONTRACTOR MAY USE OTHER CONFIGURATIONS FOR THE NEOPRENE COMPRESSION SEALS PROVIDED THEY MEET THE SPECIFICATIONS FOR ITEM 524.15.
4. THE CONCRETE SHOULDERS AT THE CURBS ARE APPROXIMATELY 2 FEET WIDE AND ARE IN GOOD CONDITION. THEY WILL BE REPAIRED, IF REQUIRED, AS DETERMINED BY THE ENGINEER. THE SHEET MEMBRANE SHALL BE PLACED AGAINST THESE SHOULDERS AS DIRECTED BY THE ENGINEER.
5. THE CONCRETE SHOULDERS AT THE EXPANSION DAM SHALL BE REPAIRED AS DIRECTED BY THE ENGINEER. THIS WORK WILL BE PAID UNDER THE ITEM "REPAIR OF CONCRETE OR MASONRY (MOD.)". (See Detail 5, This Sheet.)
6. THE FINAL LIFT OF PAVEMENT SHALL BE 1/4 INCH HIGHER THAN THE TOP OF THE CONCRETE SHOULDERS AT THE FINGER PLATE JOINTS. CARE SHALL BE USED DURING PAVING OPERATIONS TO MAKE SURE NO BITUMINOUS MATERIAL BECOMES LODGED IN THE EXPANSION JOINTS NOR ENTERS THE DRAIN TROUGHS.
7. THIS BRIDGE DECK IS TO BE PAVED CONCRETE SHOULDER TO CONCRETE SHOULDER WITH 1 3/4 INCH +/- BITUMINOUS CONCRETE PAVEMENT IN TWO COURSES. THE BOTTOM COURSE SHALL BE 3/4 INCH +/- OF TYPE IV MIX (2) COURSES. THE TOP COURSE SHALL BE 1 INCH +/- OF TYPE IV MIX. THE TOTAL THICKNESS AND THICKNESS OF EACH OF THE COURSES SHALL BE DETERMINED BY THE ENGINEER.
8. DETAIL 2, SHEET 11, DOES NOT APPLY TO THIS STRUCTURE.
9. THE FOLLOWING GENERAL NOTES DO NOT APPLY TO THIS STRUCTURE: NOS. 28, 29 AND 30.

BR 63A - US 4 OVER I 89

1. THERE ARE NO SCUPPERS ON THIS BRIDGE.
2. ALL WEEP PIPES SHALL BE SEALED OVER WITH SHEET MEMBRANE WATERPROOFING.
3. REPAIR COMPRESSION SEALS AT THE ENDS OF THE BRIDGE AND IN THE CURBS AT THE PIERS, AS DETERMINED BY THE ENGINEER, IN ACCORDANCE WITH DETAILS 4 AND 5 ON SHEET 11. THE WIDTH OF THE REPLACEMENT COMPRESSION SEALS SHALL BE 1 3/4 TIMES THE WIDTH OF THE OPENING AS MEASURED IN THE FIELD. THE CONTRACTOR MAY USE OTHER CONFIGURATIONS FOR THE NEOPRENE COMPRESSION SEALS PROVIDED THEY MEET THE SPECIFICATIONS FOR ITEM 524.15.
4. THE EXISTING METAL PLATES IN THE EXPANSION JOINTS AT THE PIERS SHALL BE STRAIGHTENED AS REQUIRED AND NEW CONCRETE SHOULDERS ADDED AS SHOWN IN DETAIL 4 ON SHEET 12. NO WORK OTHER THAN THAT SHOWN SHALL BE DONE ON THE METAL PLATE EXPANSION JOINTS UNLESS THE ENGINEER CONTACTS THE STRUCTURES DIVISION.
5. THE FINAL LIFT OF PAVEMENT SHALL BE 1/4 INCH HIGHER THAN THE TOP OF THE CONCRETE SHOULDERS. CARE SHALL BE USED DURING PAVING OPERATIONS TO MAKE SURE NO BITUMINOUS MATERIAL BECOMES LODGED IN THE EXPANSION JOINTS NOR ENTERS THE DRAIN TROUGHS.
6. THIS BRIDGE DECK IS TO BE PAVED CURB TO CURB WITH 2 INCHES +/- OF BITUMINOUS CONCRETE PAVEMENT IN TWO COURSES. THE FIRST COURSE SHALL BE 1 INCH +/- OF TYPE IV MIX AND THE TOP COURSE SHALL BE 1 INCH +/- OF TYPE IV MIX. THE TOTAL THICKNESS AND THE THICKNESS OF EACH COURSE SHALL BE DETERMINED BY THE ENGINEER.
7. DETAIL 2, SHEET 11, DOES NOT APPLY TO THIS STRUCTURE.
8. THE FOLLOWING GENERAL NOTES DO NOT APPLY TO THIS STRUCTURE: NOS. 28, 29 AND 30.



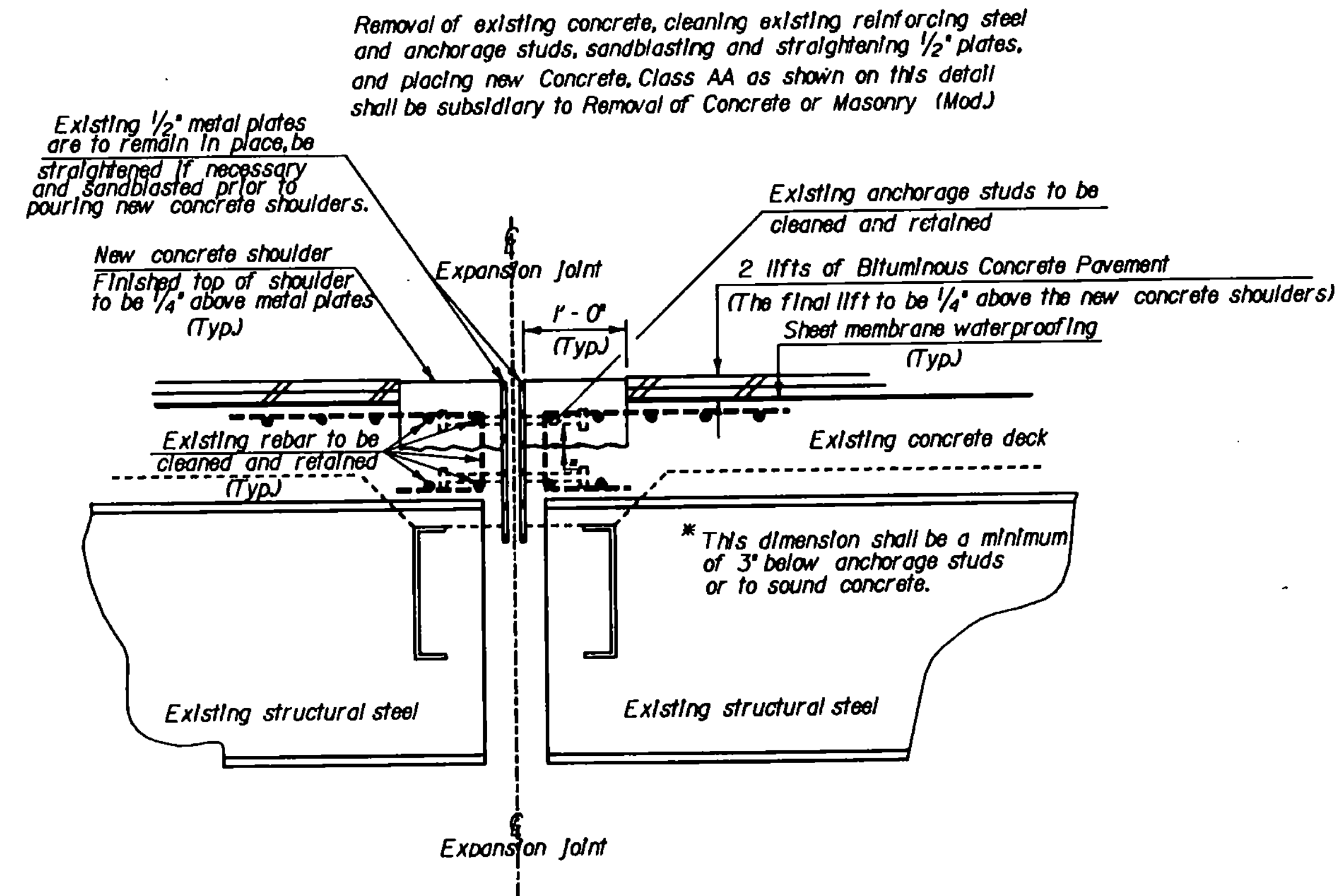
FINGER PLATE JOINT DETAILS AT ABUTMENTS

NTS

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BR. 15 H & B I 89 OVER I 28

1. THERE ARE NO SCUPPERS ON THESE BRIDGES.
2. ALL WEEP PIPES SHALL BE SEALED OVER WITH SHEET MEMBRANE WATERPROOFING.
3. REPAIR COMPRESSION SEALS AT THE ENDS OF THESE BRIDGES AND IN THE CURBS AT THE PIERS, AS DETERMINED BY THE ENGINEER, IN ACCORDANCE WITH DETAILS 4 AND 5 ON SHEET 11. THE WIDTH OF THE REPLACEMENT COMPRESSION SEALS SHALL BE 1 3/4 TIMES THE WIDTH OF THE OPENING AS MEASURED IN THE FIELD. THE CONTRACTOR MAY USE OTHER CONFIGURATIONS FOR THE NEOPRENE COMPRESSION SEALS PROVIDED THEY MEET THE SPECIFICATIONS FOR ITEM 524.15.
4. THE EXISTING METAL PLATES IN THE EXPANSION JOINTS AT THE PIERS SHALL BE STRAIGHTENED AS REQUIRED AND NEW CONCRETE SHOULDERS ADDED AS SHOWN IN DETAIL 4 ON SHEET 12. NO WORK OTHER THAN THAT SHOWN SHALL BE DONE ON THE METAL PLATE EXPANSION JOINTS UNLESS THE ENGINEER CONTACTS THE STRUCTURES DIVISION.
5. BRIDGE 15H CURRENTLY HAS CYBOND 2501 POLYESTER RESIN SYSTEM ON THE DECK WHICH WAS APPLIED IN AUGUST OF 1964 AND BRIDGE 15B HAS RS-1 ASPHALT EMULSION WHICH WAS APPLIED IN SEPTEMBER OF 1968. THESE SYSTEMS ARE PERFORMING WELL WITH AVERAGE READINGS OF 44 OF THE AREA INDICATING CORROSION READINGS OF 0.25 VOLTS OR GREATER. CARE SHALL BE USED WHEN REMOVING THE EXISTING PAVEMENT (WHICH IS IN POOR CONDITION) TO MINIMIZE DAMAGE TO THE PROTECTIVE COATING SYSTEM AND RETAIN IT AS INTACT AS POSSIBLE. THE NEW SHEET MEMBRANE WATERPROOFING WILL BE APPLIED DIRECTLY ON THE EXISTING SYSTEM AS DIRECTED BY THE ENGINEER.
6. BECAUSE OF LOW CORROSION READINGS, IT IS EXPECTED THAT THERE WILL BE LITTLE OR NO CONCRETE REPAIR REQUIRED. HOWEVER, 1 SQUARE YARD OF BOTH PREPARATION OF CONCRETE SURFACES, CLASS I AND CLASS II AND 100 LBS. OF REINFORCING STEEL HAVE BEEN ADDED TO THE QUANTITIES. THE ACTUAL AMOUNT REQUIRED WILL BE AS DETERMINED BY THE ENGINEER. THE QUANTITY SHOWN FOR REMOVAL OF CONCRETE OR MASONRY IS BASED ON ESTIMATED WORK REQUIRED AT THE METAL PLATE JOINTS.
7. THE FINAL LIFT OF PAVEMENT SHALL BE 1/4 INCH HIGHER THAN THE TOP OF THE NEW CONCRETE SHOULDERS AT THE JOINTS. CARE SHALL BE USED DURING PAVING OPERATIONS TO MAKE SURE NO BITUMINOUS MATERIAL BECOMES LODGED IN THE EXPANSION JOINT NOR ENTERS THE DRAIN TROUGHS.
8. THESE BRIDGE DECKS ARE TO BE PAVED CURB TO CURB WITH 2 1/2 INCHES +/- OF BITUMINOUS CONCRETE PAVEMENT IN TWO COURSES. THE FIRST COURSE SHALL BE 1 1/4 INCH +/- OF TYPE IV MIX AND THE TOP COURSE SHALL BE 1 1/4 INCH +/- OF TYPE IV MIX. THE TOTAL THICKNESS AND THE THICKNESS OF EACH COURSE SHALL BE DETERMINED BY THE ENGINEER.
9. DETAIL 2, SHEET 11, DOES NOT APPLY TO THESE STRUCTURES.
10. THE FOLLOWING GENERAL NOTES DO NOT APPLY TO THESE STRUCTURES: NOS. 28, 29 AND 30.

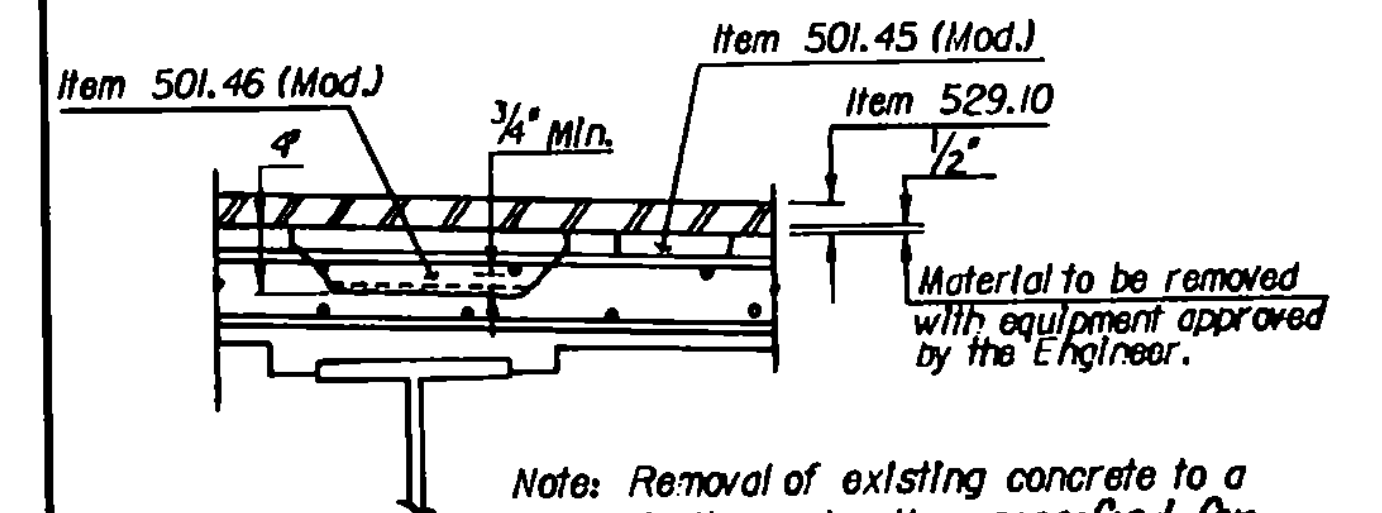


METAL PLATE JOINT DETAILS AT PIERS

NTS

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Note: All edges of repair areas are to be saw cut square and a minimum of one (1) inch deep.
 Note: Item 501.45 (Mod.) shall include removal of concrete to a maximum depth as determined by the top of the top bars of reinforcing steel.



Note: Removal of existing concrete to a depth greater than specified for Item 501.46 (Mod.) shall be paid under the Item 529.25 (Mod.)

TYPICAL LIMITS FOR REMOVAL ITEMS

NTS

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STATE OF VERMONT AGENCY OF TRANSPORTATION

Town of HARTLAND, HARTFORD, SHARON Bridge No. _____
 Highway No. _____ Leg. Sta. _____
 Surv. Sta. _____

TYPICAL DETAILS AND SPECIFIC NOTES

Designed By G.S. ROGERS Drawn By D.W. NEWTON
 Checked By G.S. ROGERS Date 7/86 Bridge Design Supervisor F.W. Bolkum Date 8/86
 PROJECT HARTLAND PROJECT NO. _____
HARTFORD, SHARON IR-DECK (15)
 I&C Info. 05A/30.32/SCUPREHAB/DGN
 Bridge Sheet No. _____ Sheet 12 of 39