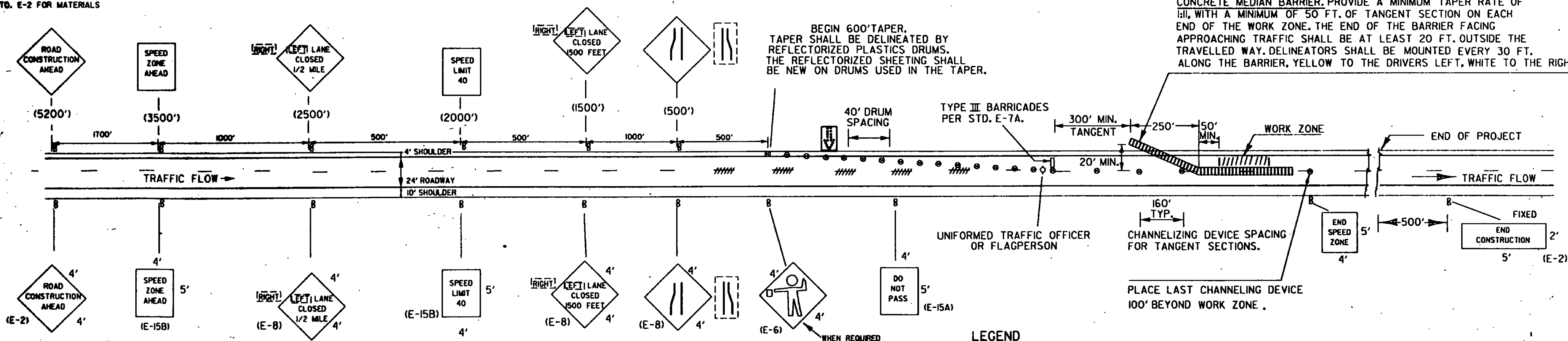


Ⓢ COLORS: LEGEND - BLACK (NON-REFLECTORIZED) BACKGROUND - WHITE (REFLECTORIZED) SEE STD. 158 FOR MATERIALS

Ⓢ COLORS: LEGEND - BLACK (NON-REFLECTORIZED) BACKGROUND - ORANGE (REFLECTORIZED HIGH INTENSITY) SEE STD. E-2 FOR MATERIALS

INSTALLATION OF SIGN WITH TWO 8" YELLOW FLASHING SIGNAL BEACONS. USE AS REQUIRED ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

1. WHEN WORKING AT OR NEAR THE EXIT OR ENTRANCE RAMP, FLAGPERSONS OR UNIFORMED TRAFFIC CONTROL OFFICERS SHALL BE USED TO ASSIST IN CONTROLLING TRAFFIC. LANE CLOSURE ADVANCE WARNING SIGNS SHALL BE INSTALLED ON ENTRANCE RAMP.
2. EXIT RAMP SHALL HAVE TRAFFIC LANES DELINEATED TO INDICATE DESIRED VEHICLE PATH. TAPER LENGTHS SHALL BE AT LEAST 320 FEET. SPACING FOR CHANNELIZING DEVICES SHALL BE 40 FEET. SEE THE 'TRAFFIC CONTROL SIGN SUMMARY SHEET FOR RAMP' FOR ADDITIONAL DETAILS.
3. CHANNELIZING DEVICES SHALL BE WEIGHTED AT THE BASE TO PREVENT OVERTURNING AND KEPT CLEAN.
4. ALL SIGNS WILL BE PLACED BEFORE ANY WORK IS BEGUN OR EQUIPMENT PUT ON ROADWAY. SIGNS SHALL BE COVERED WHEN NOT APPLICABLE.
5. CONTRACTOR SHALL HAVE CHANNELIZING DEVICES AND SIGNS FOR LEFT SIDE CLOSURE AND RIGHT SIDE CLOSURE ON PROJECT BEFORE STARTING PROJECT.
6. WORK CREW, PRIVATE VEHICLES SHALL NOT BE PARKED IN OR AROUND WORKING AREA. CONTRACTOR WILL PROVIDE PARKING FOR WORK CREW VEHICLES OFF PROJECT.
7. UNIFORMED TRAFFIC OFFICERS WILL PARK THEIR VEHICLES ON THE SHOULDER AWAY FROM TRAFFIC.
8. EXISTING SPEED LIMIT SIGNS IN REDUCED SPEED AREA WILL BE COVERED WHEN APPROPRIATE. APPLICATION OF TAPE TO SIGN FACES WILL NOT BE PERMITTED.
9. CHANNELIZING DEVICES OTHER THAN REFLECTORIZED PLASTIC DRUMS WILL BE ALLOWED ALONG TANGENT SECTIONS AS LONG AS THEY CONFORM TO THE M.U.T.C.D. AND ARE APPROVED FOR USE BY THE RESIDENT ENGINEER.
10. REFER TO PROJECT 'SPECIAL PROVISIONS' FOR SPEED ZONE ENACTMENT.
11. THE 'SPEED LIMIT 40', 'DO NOT PASS' AND 'END SPEED ZONE' SIGNS SHALL BE LOCATED TO THE LEFT HAND SIDE OF THE ROAD WHEN WORK IS BEING DONE ON THE RIGHT HAND LANE.
12. THE 'SPEED LIMIT 40' AND OTHER RELATED SIGNS SHALL BE REMOVED OR COVERED WHEN WORK IS NOT IN PROGRESS.
13. THE FLASHING ARROW SHALL BE MOUNTED ON A BREAKAWAY OR YIELDING SYSTEM. PLACEMENT AT THE START OF THE TAPER IS PREFERRED TO PLACEMENT IN THE MIDDLE OF THE TAPER. THE FLASHING ARROW BOARD SHALL BE EQUIPPED WITH A DIMMER FOR NIGHTTIME USE, AND SHALL BE DIMMED AT NIGHT.
14. 'REDUCED SPEED AHEAD' SIGNS MAY BE USED IN LIEU OF 'SPEED ZONE AHEAD'.
15. WHEN RIGHT HAND LANE IS CLOSED. THE 10' SHOULDER SHALL ALSO BE CLOSED.
16. THE SIGNS 'LOW SHOULDER', 'BUMP', 'MOTORCYCLE' WARNING, 'CAUTION PAVEMENT CHANGE' AND 'ROUGH ROAD' ARE TO BE USED AND PLACED AS DIRECTED BY THE RESIDENT ENGINEER.
17. ALL FIXED SIGNS SHALL BE MOUNTED ON YIELDING STEEL OR ALUMINUM SUPPORTS AS SHOWN ON STANDARD SHEETS E-24A AND E-25.
18. CENTERLINE PAVEMENT MARKINGS SHALL BE REMOVED THROUGH THE TAPER AND FOR 500 FEET IN ADVANCE OF THE TAPER.



CONCRETE MEDIAN BARRIER. PROVIDE A MINIMUM TAPER RATE OF 1:11, WITH A MINIMUM OF 50 FT. OF TANGENT SECTION ON EACH END OF THE WORK ZONE. THE END OF THE BARRIER FACING APPROACHING TRAFFIC SHALL BE AT LEAST 20 FT. OUTSIDE THE TRAVELLED WAY. DELINEATORS SHALL BE MOUNTED EVERY 30 FT. ALONG THE BARRIER, YELLOW TO THE DRIVERS LEFT, WHITE TO THE RIGHT.

BEGIN 600' TAPER. TAPER SHALL BE DELINEATED BY REFLECTORIZED PLASTICS DRUMS. THE REFLECTORIZED SHEETING SHALL BE NEW ON DRUMS USED IN THE TAPER.

40' DRUM SPACING
TYPE III BARRICADES PER STD. E-7A.
300' MIN. TANGENT
250'
50' MIN.
WORK ZONE
END OF PROJECT
TRAFFIC FLOW
160' TYP.
CHANNELIZING DEVICE SPACING FOR TANGENT SECTIONS.
PLACE LAST CHANNELING DEVICE 100' BEYOND WORK ZONE.
END SPEED ZONE 5'
FIXED CONSTRUCTION 2' (E-2)

ALL SIGNS SHALL BE REMOVED OR COVERED WHEN NOT NEEDED.

- LEGEND**
- (500) - DENOTES DISTANCE FROM BEGIN TAPER
 - (E-2) - DENOTES SIGN DETAIL STANDARD SHEET
 - ▶ - FLASHING ARROW PANEL
 - ▬▬▬ - CONCRETE MEDIAN BARRIER
 - Ⓢ - REFLECTORIZED PLASTIC DRUM
 - ▬▬▬ - PAVEMENT MARKING REMOVAL

NOT TO SCALE

MAINLINE TRAFFIC CONTROL PLAN DIVIDED HIGHWAY ONE LANE CLOSED (WITH POSITIVE BARRIER PROTECTION)	REVIEWED BY <u>DSP</u> DATE <u>12/86</u>
	TRAFFIC DESIGN SUPERVISOR _____ DATE _____
	PROJECT <u>HARTLAND, HARTFORD</u>
	<u>SHARON</u> IR-DECK (15)
	TRAFFIC SHEET NO. <u>501</u> OF <u>6</u>
	SHEET <u>5</u> OF <u>31</u>

PREPARED NOV. 1986	BY
REVISIONS	