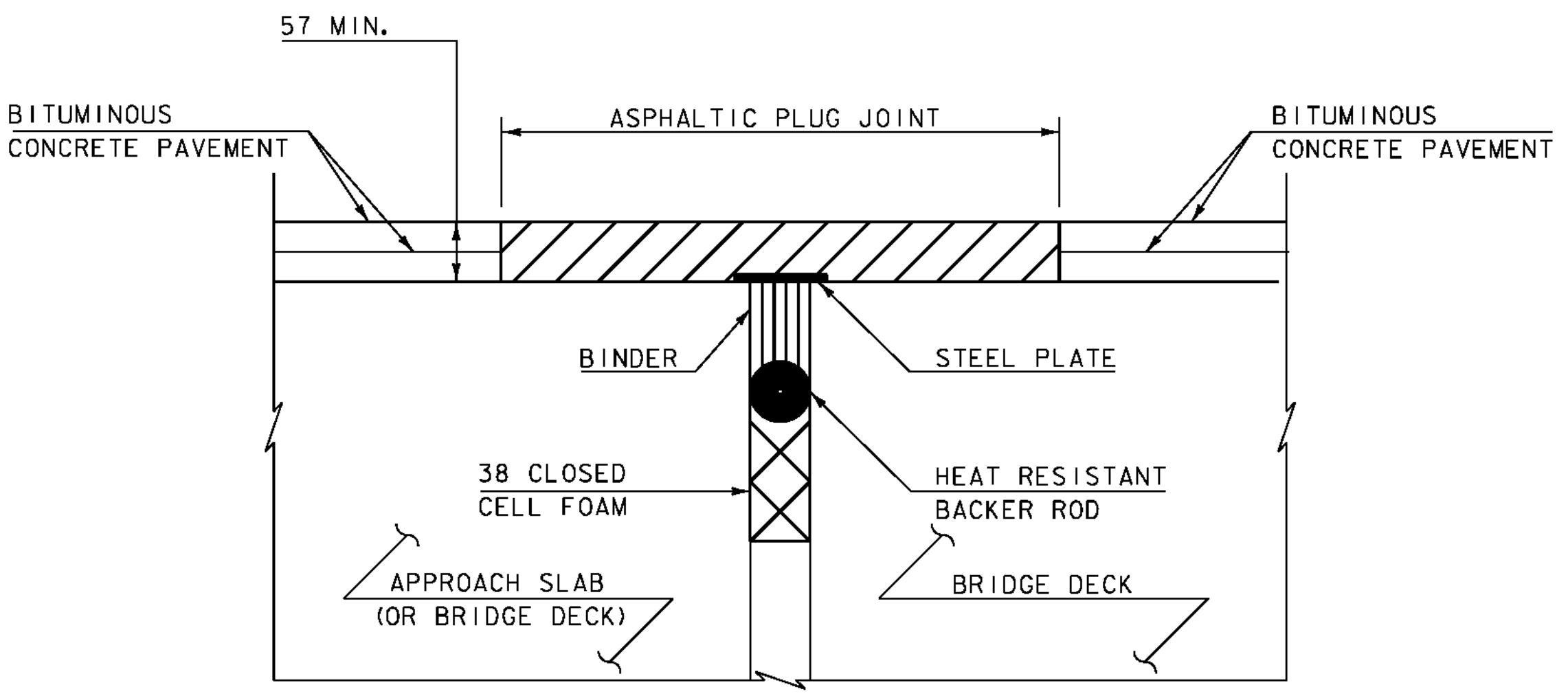
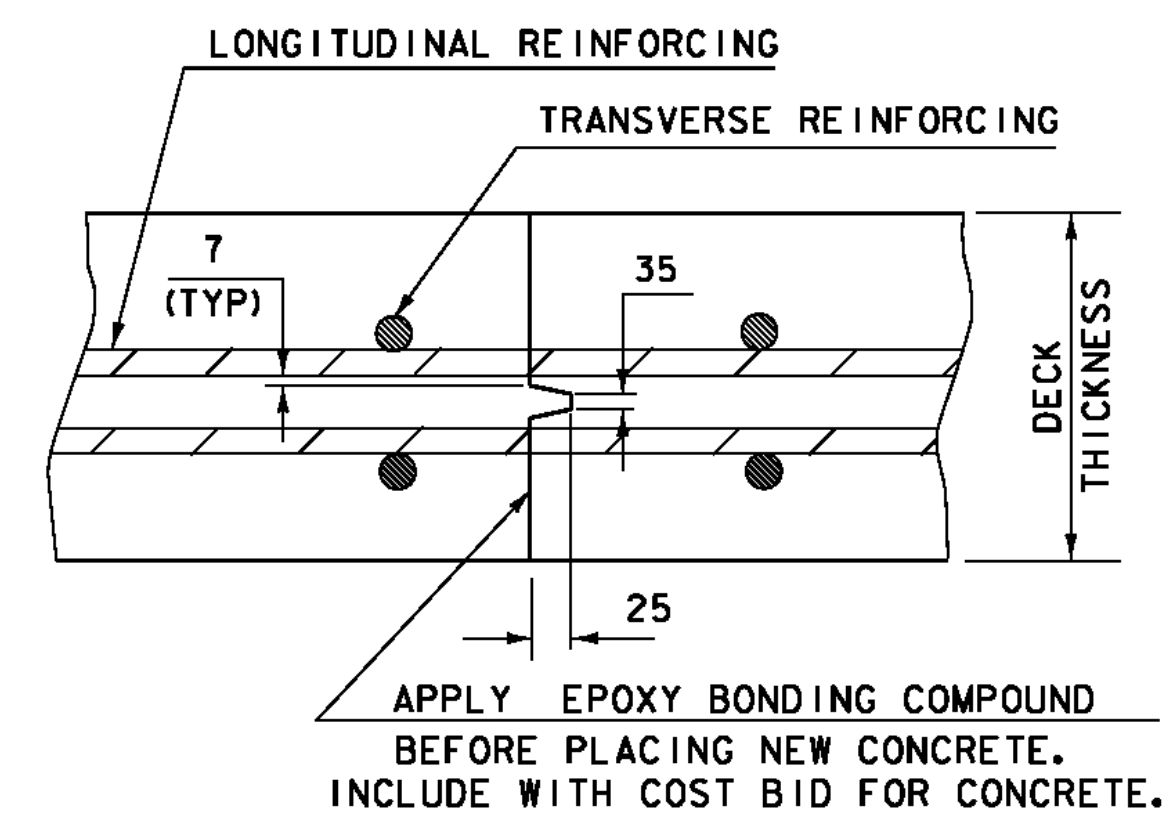


CONCRETE CURB JOINT NOTES

1. CONCRETE CURBS MAY BE PLACED IN ONE CONTINUOUS OPERATION. IF AN APPROVED SHRINKAGE REDUCING ADMIXTURE LISTED IN THE SPECIAL PROVISIONS IS USED WITH THE CONCRETE MIX DESIGN. PAYMENT FOR THE SHRINKAGE REDUCING ADMIXTURE WILL BE INCIDENTAL TO THE BRIDGE CURB CONCRETE ITEM.
2. IF THE CONTRACTOR CHOOSES NOT TO USE AN APPROVED SHRINKAGE REDUCING ADMIXTURE, THE CURBS SHALL BE CONSTRUCTED WITH CONSTRUCTION JOINTS SPACED AT A MAXIMUM OF 4500 MM CENTER TO CENTER AND 600 MM MINIMUM FROM THE CENTER OF NEAREST BRIDGE RAILING POST.
3. ON MULTI-SPAN CONTINUOUS SUPERSTRUCTURES, REGARDLESS OF WHETHER APPROVED SHRINKAGE REDUCING ADMIXTURE IS USED, CURB JOINTS SHALL BE LOCATED OVER THE CENTERLINE OF PIERS AND 2100 EACH SIDE OF THE CENTERLINE OF EACH PIER.
4. WHEN CURB JOINTS ARE USED THE CURBS SHALL BE PLACED IN ALTERNATE SECTIONS WITH A MINIMUM OF 48 HOUR DELAY BETWEEN ADJACENT PLACEMENTS.
5. LONGITUDINAL REINFORCING SHALL BE CONTINUOUS THROUGH CURB CONSTRUCTION JOINTS. CURB STIRRUP BARS SHALL BE TURNED AS NECESSARY TO MAINTAIN COVER IN THE FLARED CURB ENDS.
6. THE JOINT SPACING AND DETAILS SHOWN SHALL APPLY TO SIDEWALKS WHEN SHOWN IN THE PLANS.



ASPHALTIC PLUG-TYPE JOINT DETAIL
(NOT TO SCALE)



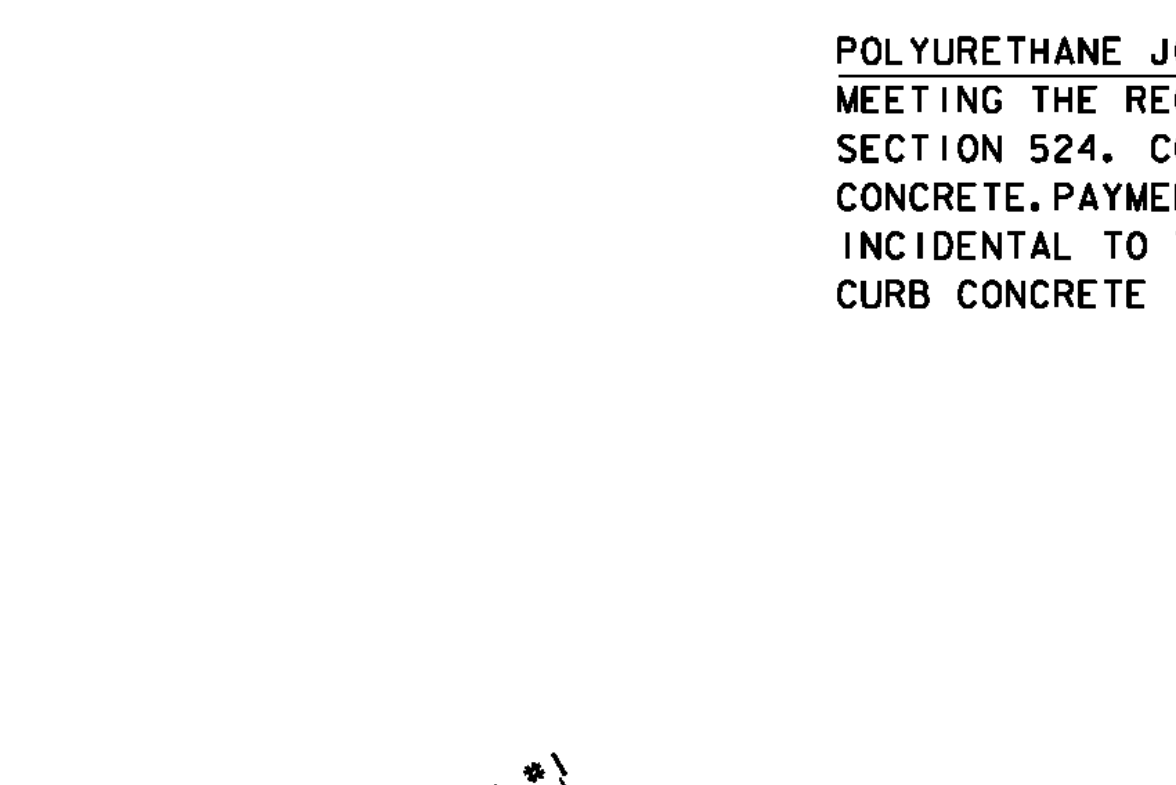
TRANSVERSE BRIDGE SLAB CONSTRUCTION JOINT DETAILS

ASPHALTIC PLUG JOINT NOTES

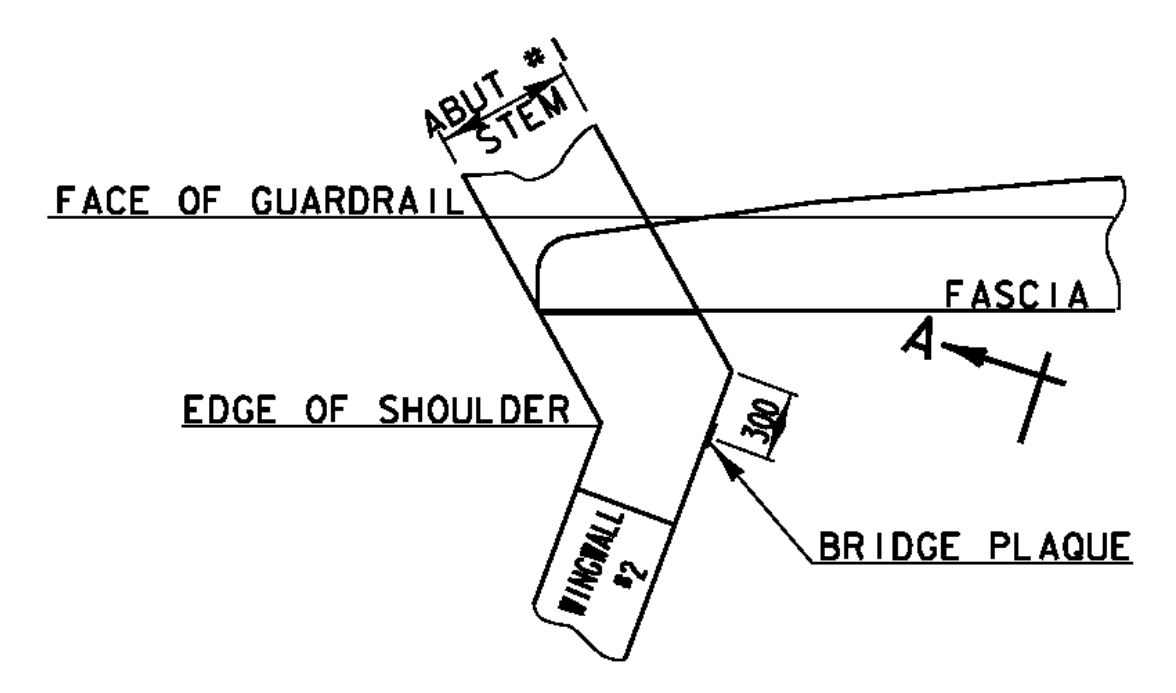
- INSTALLATION**
- LOCATE THE JOINT CENTRALLY OVER THE DECK OVERLAY EXPANSION GAP OR FIXED JOINT MARKED OUT TO THE MANUFACTURER'S RECOMMENDED WIDTH.
 - REMOVE THE BITUMINOUS CONCRETE PAVEMENT FULL DEPTH AS SHOWN ON THE PLANS. THE PAVEMENT SHALL BE DRY AND SAW CUT TO THE LIMITS REQUIRED TO PLACE THE JOINT. A PNEUMATIC HAMMER AND CHISEL MAY BE USED ADJACENT TO THE CURB ONLY WHEN SAW CUTTING IS NOT POSSIBLE.
 - BLAST CLEAN THE JOINT AREA OF DEBRIS, ASPHALT AND SHEET MEMBRANE. THOROUGHLY DRY THE JOINT AREA WITH COMPRESSED AIR PRIOR TO APPLYING BINDER MATERIAL.
 - REPAIR SPALLED AND DEFECTIVE CONCRETE WITH AN APPROVED MATERIAL AS AGREED UPON BY THE ENGINEER.
 - THE STEEL PLATE, BINDER, BACKER ROD, AND CLOSED CELL FOAM MAY BE ELIMINATED WHEN LOCATED AT A FIXED JOINT.
 - PLACE PROPERLY SIZED HEAT RESISTANT BACKER ROD IN THE MOVEMENT GAP ALLOWING FOR 1" (25) +/- OF BINDER ABOVE THE ROD.
 - HEAT AND PLACE THE BINDER MATERIAL AS RECOMMENDED BY THE MANUFACTURER.
 - PLACE 1/4" (6) THICK BY 8" (200) WIDE SECTIONS OF STEEL PLATE OVER THE CENTER OF THE MOVEMENT GAP. SECURE THE PLATES FROM MOVING BY INSERTING LOCATING PINS THROUGH THE PRE-STAMPED HOLES INTO BACKER ROD AND COVER WITH HOT BINDER. THE STEEL PLATES MAY BE OMITTED WHERE THE ENGINEER DETERMINES THAT THE APPROACH SLAB OR BRIDGE DECK WILL PROVIDE INADEQUATE SUPPORT AND WHERE VERTICAL MOVEMENT OF THE PLATES MIGHT OCCUR.
 - HEAT AND MIX THE BINDER MATERIAL AND AGGREGATE AS RECOMMENDED BY THE MANUFACTURER.
 - INSTALLATION OF MATERIAL, COMPACTION, AND TOP COATING SHALL BE AS RECOMMENDED BY THE MANUFACTURER.
 - IMMEDIATELY AFTER TOP COATING, CAST AN ANTI-SKID MATERIAL OVER THE JOINT TO REDUCE THE RISK OF TRACKING.
 - ONCE THE JOINT REACHES 82 DEG C (180 DEG F) +/-, WATER MAY BE USED TO EXPEDITE THE COOLING PROCESS.
 - PROTECT JOINT FROM TRAFFIC UNTIL THE MATERIAL HAS COOLED TO 51 DEG C (125 DEG F) +/-.

WEATHER LIMITATIONS. (APPLY BINDER MATERIAL ONLY WHEN THE FOLLOWING CONDITIONS PREVAIL OR AS RECOMMENDED BY THE MANUFACTURER):

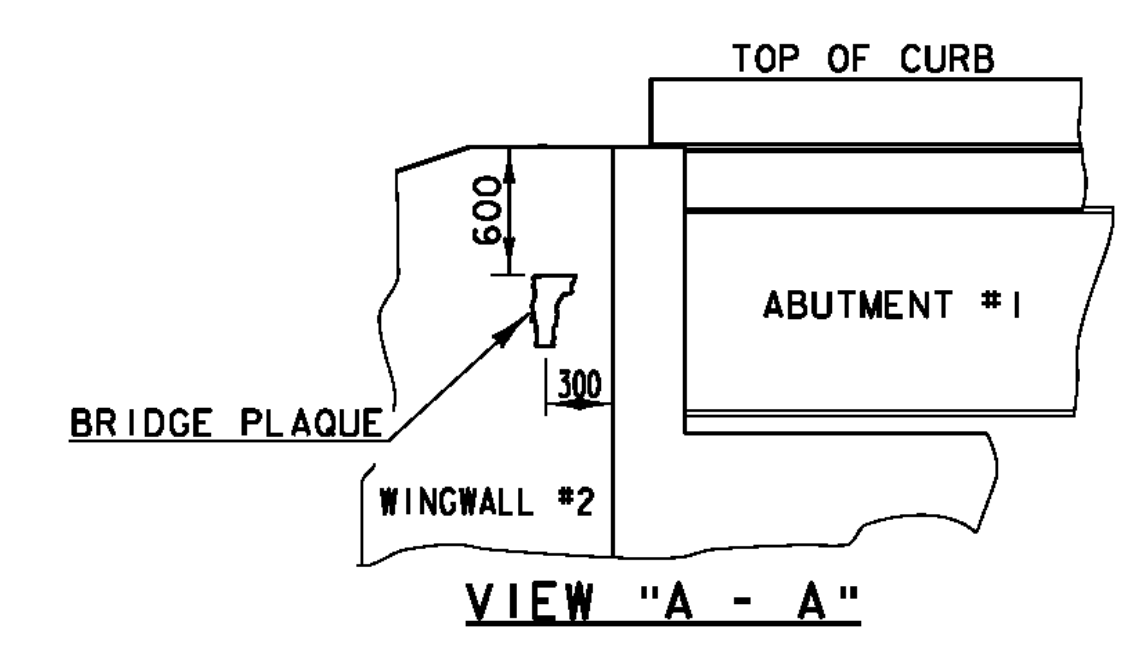
- THE AMBIENT AIR TEMPERATURE IS AT LEAST 10 DEG C (50 DEG F) AND RISING.
- THE ROAD SURFACE IS DRY.
- WEATHER CONDITIONS OR OTHER CONDITIONS ARE FAVORABLE AND ARE EXPECTED TO REMAIN SO FOR THE PERFORMANCE OF SATISFACTORY WORK.



DETAIL "B"
NOT TO SCALE

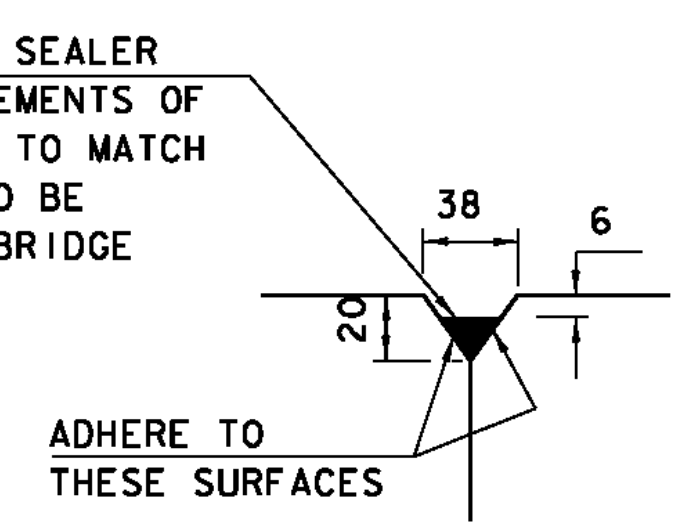


PLAN

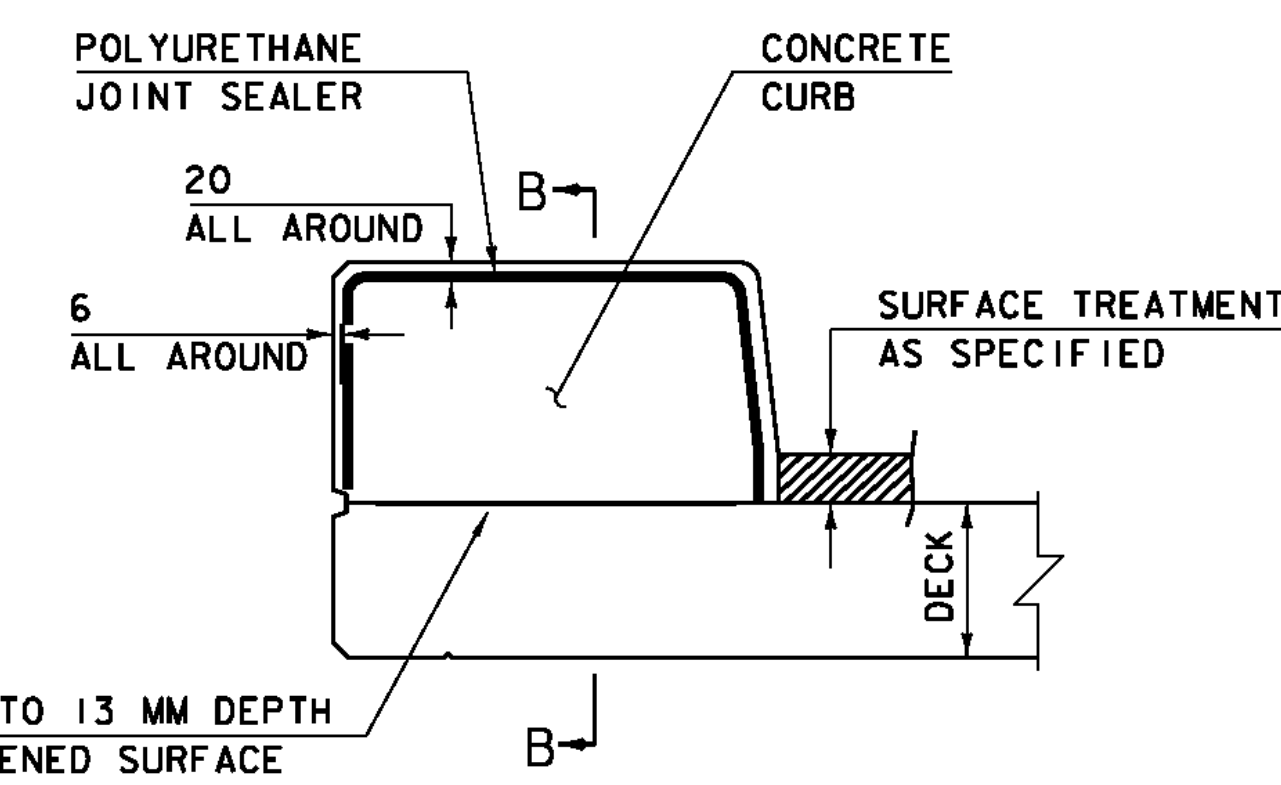


LOCATE BRIDGE PLAQUE
(NOT TO SCALE)

THE BRIDGE PLAQUE WILL BE SUPPLIED BY THE AGENCY OF TRANSPORTATION AND SHALL BE INSTALLED BY THE CONTRACTOR AT ABUTMENT #1 ON THE RIGHT SIDE AS SHOWN OR AS DIRECTED BY THE ENGINEER.

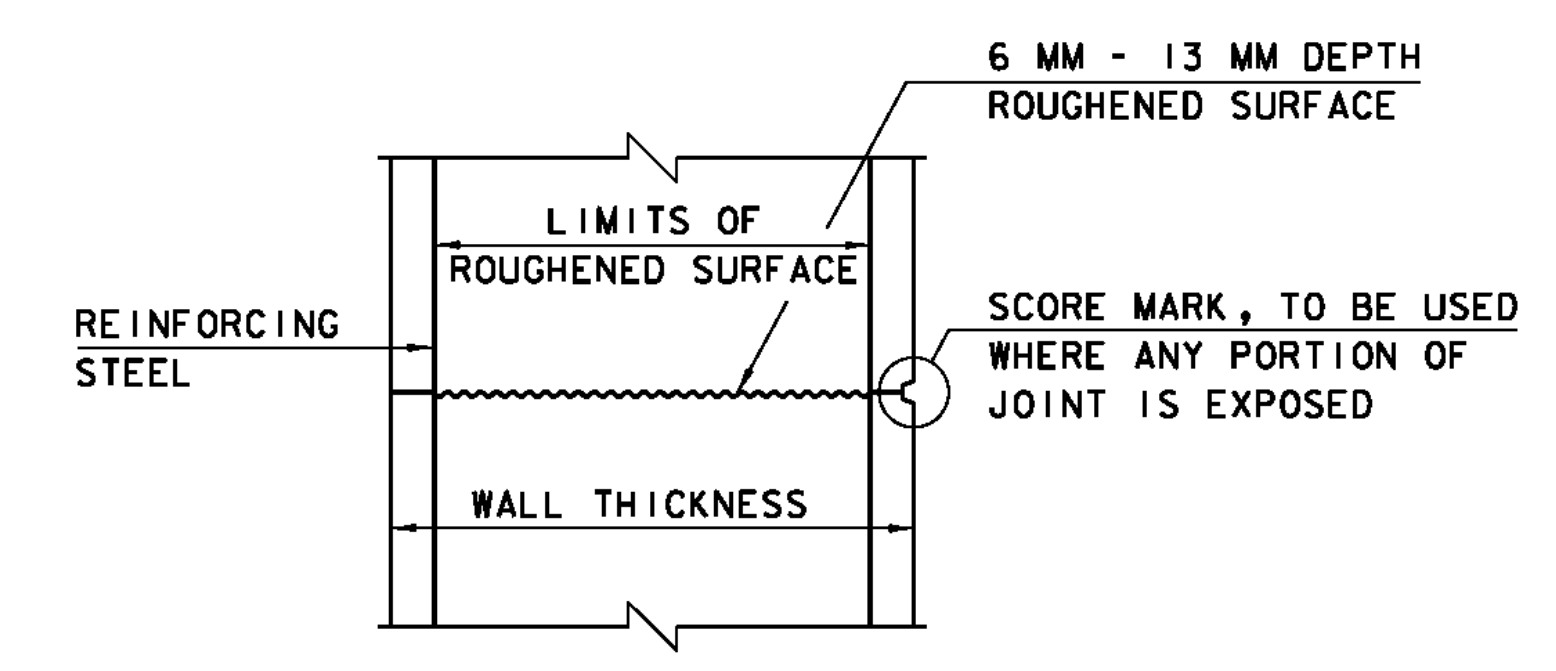


DETAIL "B"
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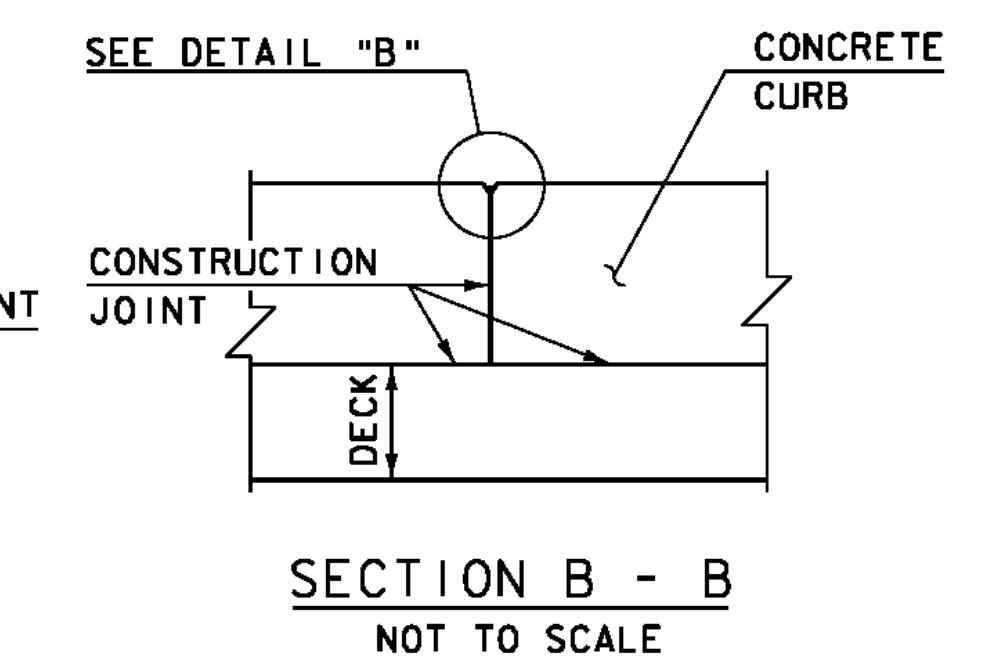
CONCRETE CURB JOINT SECTION
NOT TO SCALE

1. SEE TYPICAL HORIZONTAL CONSTRUCTION JOINT DETAIL FOR ADDITIONAL INFORMATION

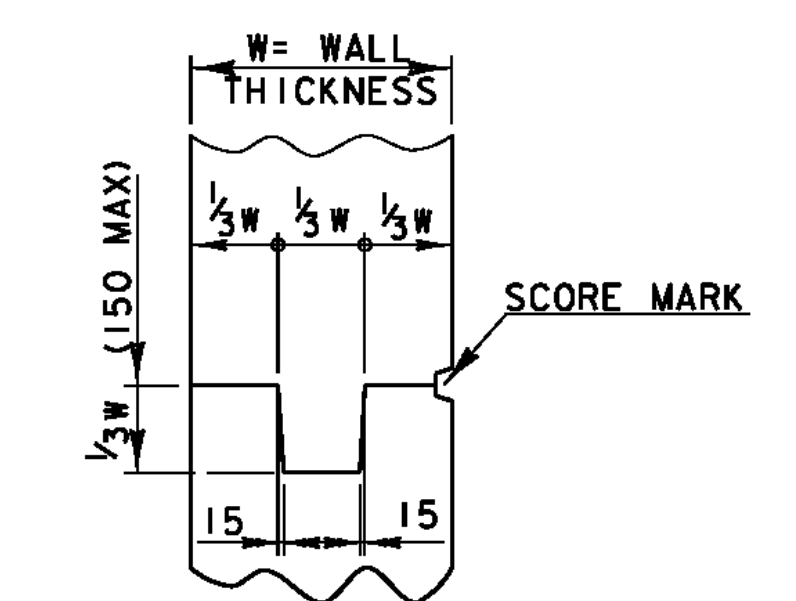


TYPICAL HORIZONTAL CONSTRUCTION JOINT
(NOT TO SCALE)

1. THE SURFACE OF THE CONCRETE CONSTRUCTION JOINTS SHALL BE CLEANED AND FREE OF LAITANCE.
2. IMMEDIATELY BEFORE NEW CONCRETE IS PLACED, ALL CONSTRUCTION JOINTS SHALL BE WETTED AND STANDING WATER REMOVED.



SECTION B - B
NOT TO SCALE



TYPICAL VERTICAL CONCRETE CONSTRUCTION JOINT
(NOT TO SCALE)

PROJECT: STOCKBRIDGE	PROJECT NO.: BRF 022-1 (20)
DESIGN FILE NAME: 85e039\Structures\de039joint.dgn	PLOT DATE: 08-APR-2010
IPARM FILE NAME: de039mscjointdet.i	DRAWN BY: D.D. BEARD
DESIGNED BY: R. YOUNG	CHECKED BY: R.S. YOUNG
SQUAD LEADER: C. P. WILLIAMS	SHEET: 68 OF 139
MISC. DETAIL SHEET	