

**STATE OF VERMONT  
AGENCY OF TRANSPORTATION**

Dated MAR 26 1984  
Cooley Asphalt Paving  
 Contractor  
Walter J. Lapinski  
 Signature  
President  
 Title



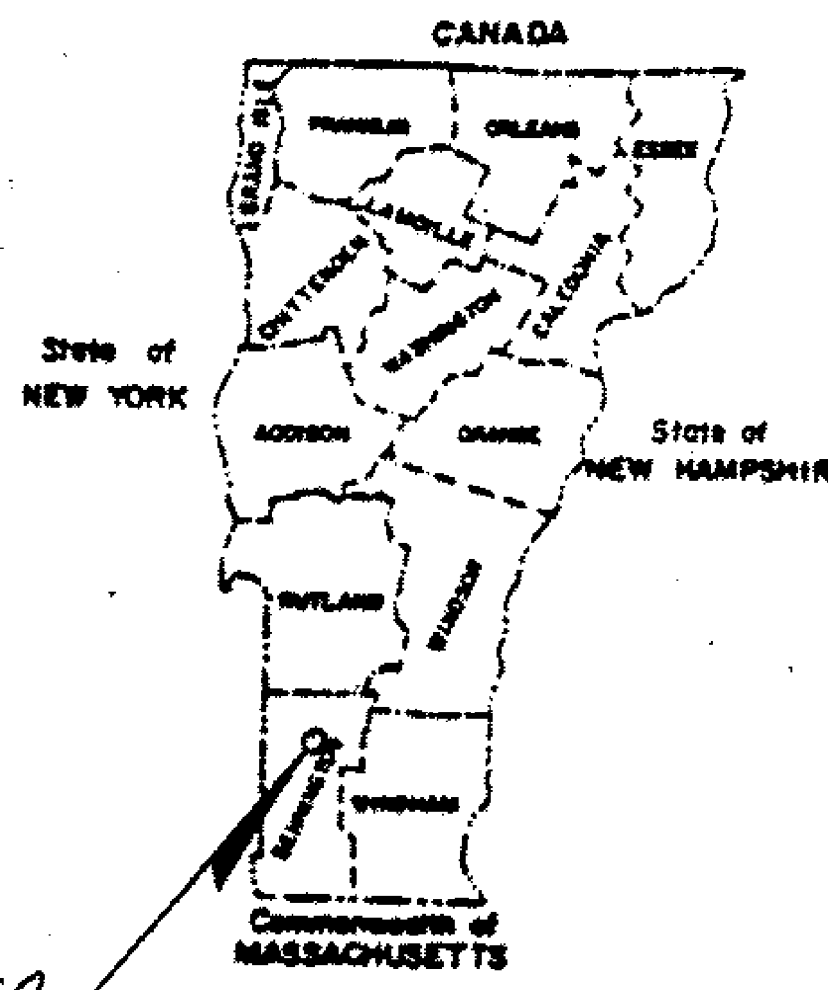
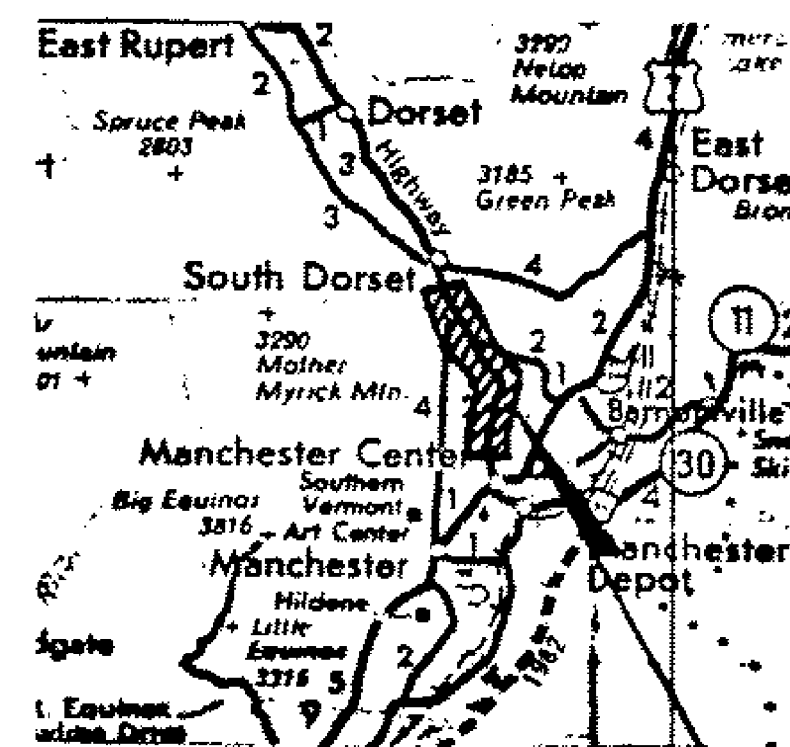
CONTRACT PLANS  
 THESE PLANS DO NOT REFLECT  
 CHANGES MADE ON THE PROJECT.

Transportation Secretary's Signature

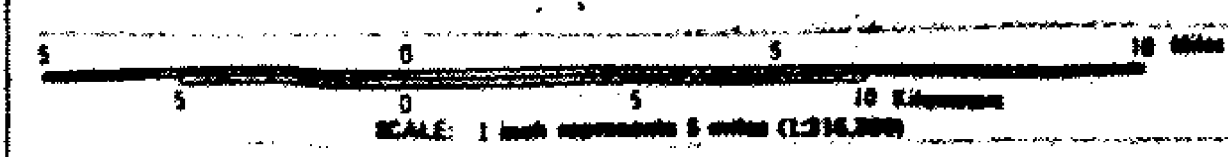
**PROPOSED IMPROVEMENT**

RESURFACING PROJECT

TOWNS OF: MANCHESTER - DORSET  
 COUNTY OF: BENNINGTON  
 ROUTE NO: V.T. 30  
 ROUTE CLASS: F.A.P.



PROJECT AREA



**PROJECT PROCESSED UNDER  
SECONDARY ROAD PLAN**

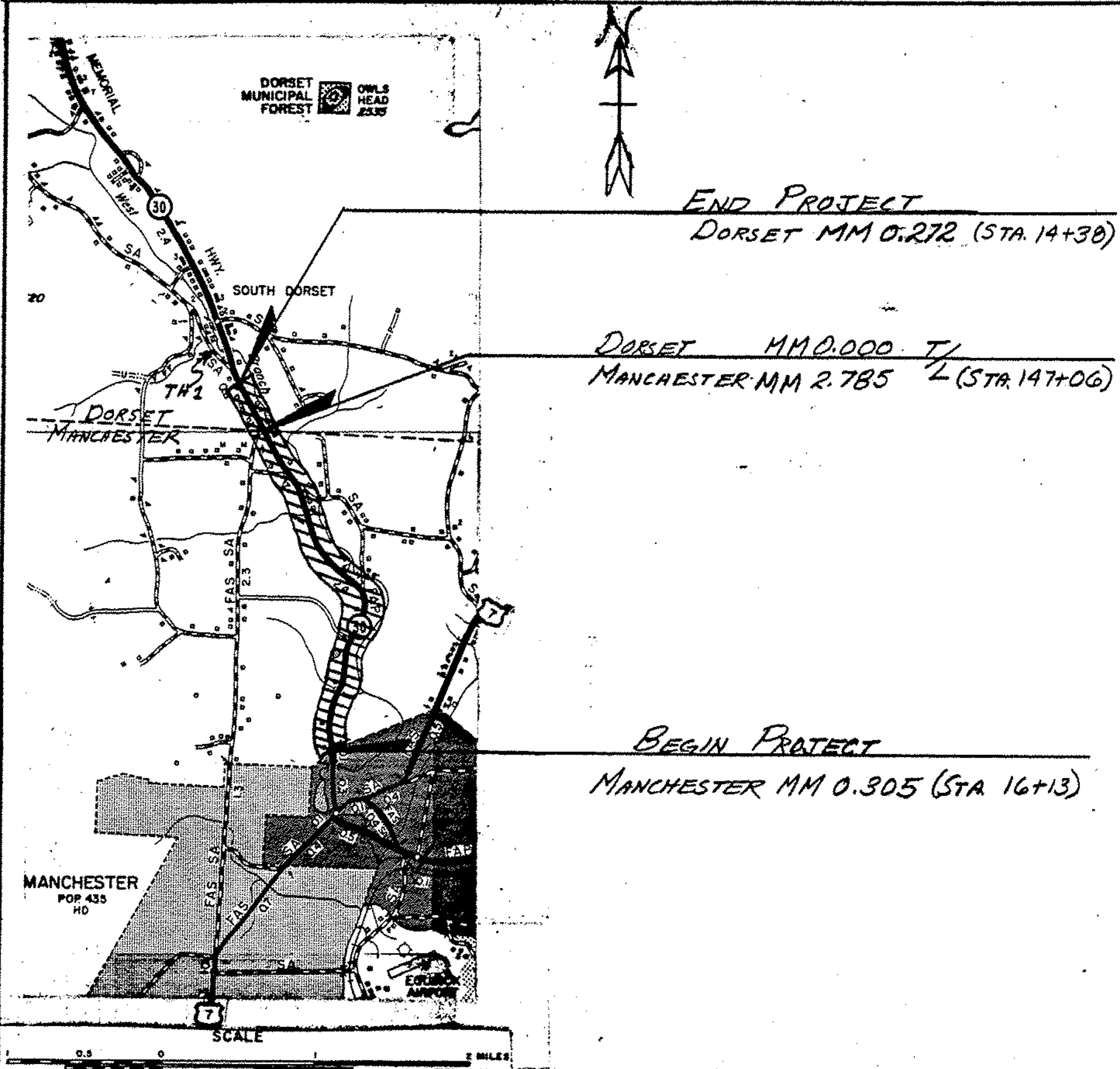
These plans are subject to such engineering changes as may be required by the Federal Highway Administration or the Director of Engineering and Construction.  
 Construction is to be carried on in accordance with these plans and the Standard Specifications for Highway and Bridge Construction dated March, 1978, as approved by the Federal Highway Administration on October 27, 1978 for use on this project, including all subsequent revisions and such revised specifications and special provisions as are incorporated in these plans.

**INDEX OF SHEETS**

1. TITLE SHEET
2. PROJECT DESCRIPTION AND LOCATION
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- 4-4a. PROJECT LENGTHS AND ITEM QUANTITIES
5. STANDARD SHEET E-4 ( 3- 4-81 R )
6. STANDARD SHEET E-6 ( 4- 1-80 R )
7. STANDARD SHEET E-8 ( 11-23-81 R )
- 8-10. CENTERLINE DETAILS
11. STANDARD SHEET E-50 ( 3-16-82 R )

SUBMITTED BY ORDER OF THE STATE TRANSPORTATION BOARD	
APPROVED <u>S. J. Gage P.E.</u>	DATE <u>1-23-84</u>
DIRECTOR OF ENGINEERING AND CONSTRUCTION	
DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED _____	DATE _____
DIVISION ADMINISTRATOR	
PROJECT NO. <u>F 015-2(2)S</u>	
SHEET 1 OF 11 SHEETS	

# PROJECT DESCRIPTION AND LOCATION



**PROJECT LOCATION**

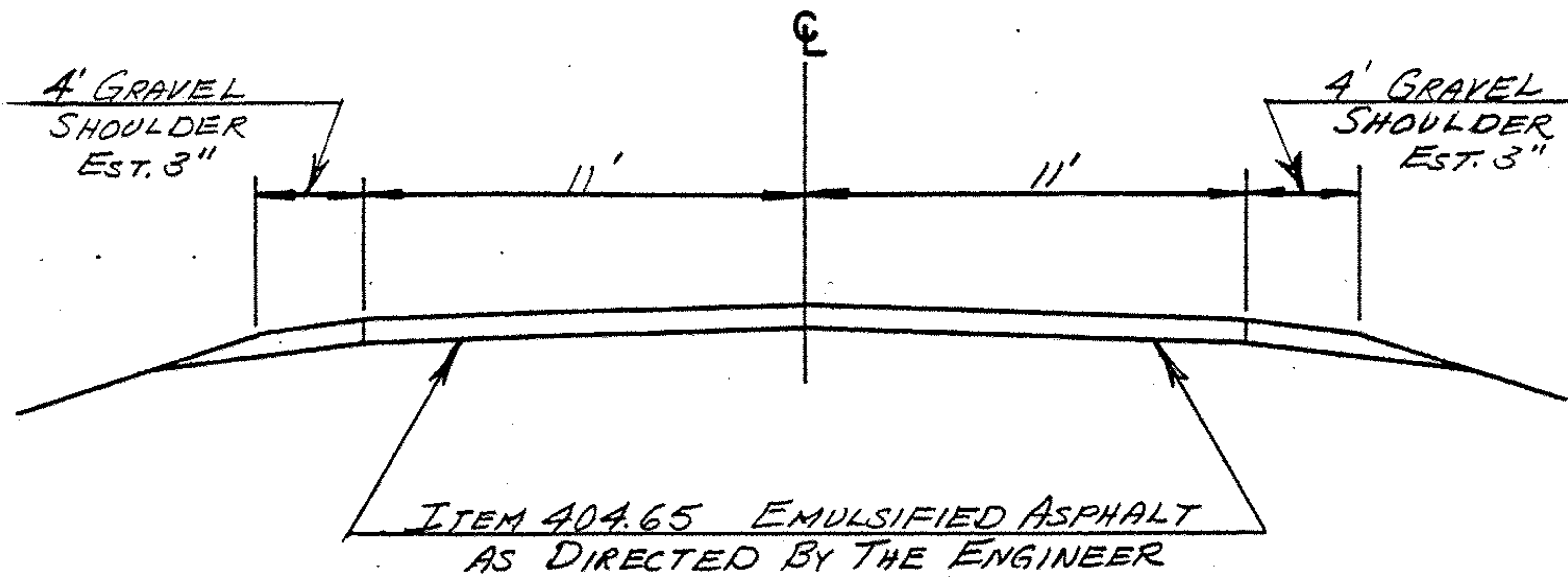
BEGINNING ON THE NORTH END OF BRIDGE #53 - THE MANCHESTER URBAN LIMITS - AT MM 0.305 ON VT. 30 IN MANCHESTER WHICH IS 1613' (0.305 MI) NORTHWEST OF THE INTERSECTION WITH US 7 AND PROCEEDING 2.752 MILES NORTHWEST TO MM 0.272 IN DORSET, 1212' (0.230 MI) SOUTHEAST OF TH 1  
 LENGTH OF PROJECT = 14,531' = 2.752 MI.

PROJECT MANCHESTER - DORSET

NO. F 015-2(2)S  
 SHEET 2 OF 11 SHEETS

# TYPICAL SECTIONS & DESIGN DATA

BITUMINOUS CONCRETE PAVEMENT, ITEM 406.25  
LEVELING COURSE TYPE III OR IV (22' WIDE), EST. 150 TONS/M.I.,  
1" WEARING COURSE ( $\pm \frac{1}{4}$ " TYPE IV (22' WIDE) \*SEE NOTE #1



MANCHESTER MM 0.305 TO DORSET MM 0.272

## TRAFFIC DATA

1982 AADT=2730  
V=50

NOTE #1: LEVELING COURSE BASED ON 22' WIDE ROADWAY,  
QUANTITY FACTORED FOR ACTUAL WIDTH.

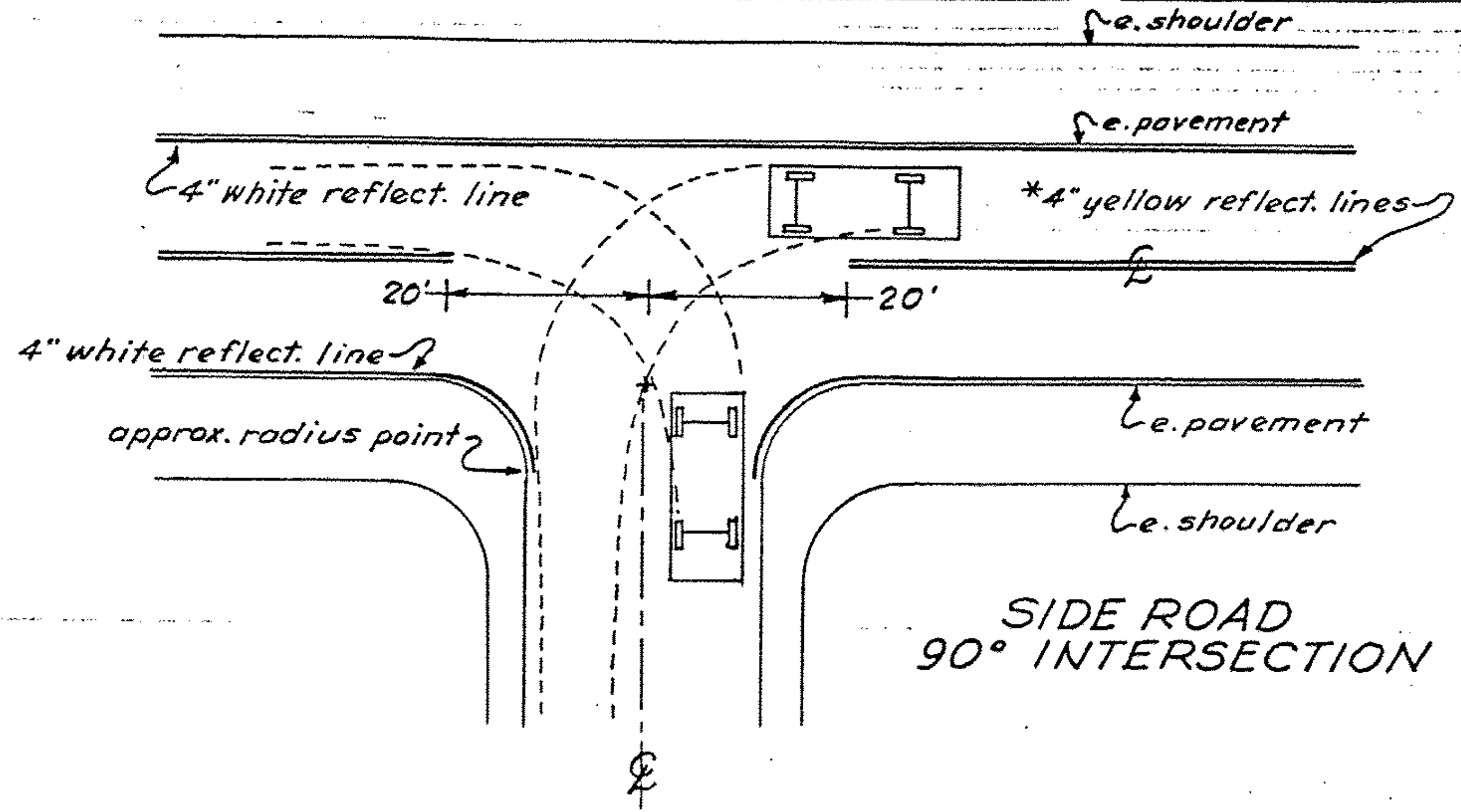
PROJECT MANCHESTER - DORSET

NO. F 015-2(2)S

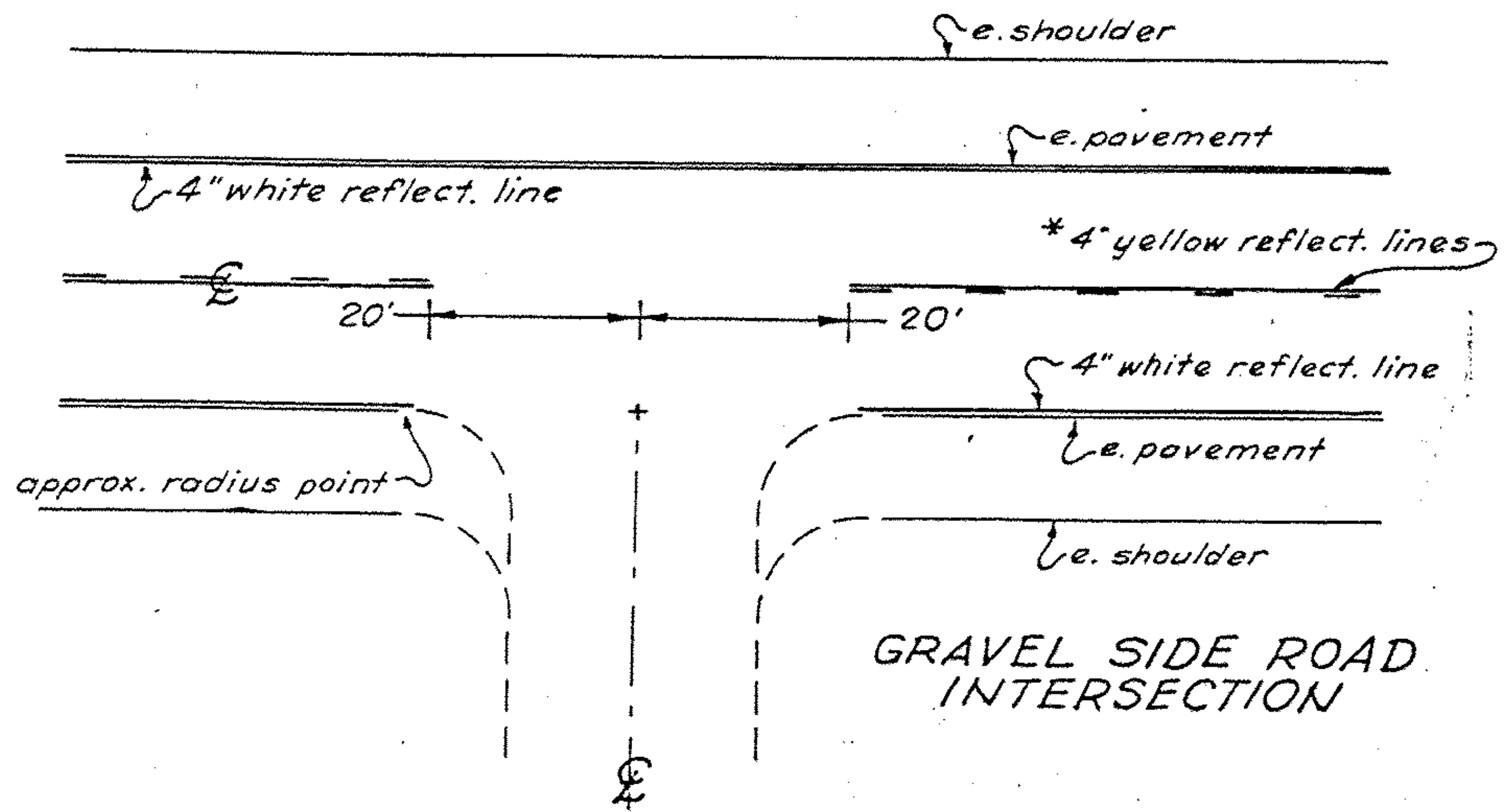
SHEET 3 OF 11 SHEETS







\* Centerline treatment shall consist of a minimum of 400 feet of solid line in advance of the intersection and shall be paired with either a solid or dashed line depending on sight distance availability in the opposing lane.



- A. CENTERLINE AND GORE AREA MARKINGS SHALL BE APPLIED AT THE END OF EACH WORKING DAY. THE FOLLOWING LAYOUT REQUIREMENTS SHALL BE MET:

NO PASSING BARRIER

SOLID STRIPES.

DASHED LINE

10-FOOT SOLID LINE WITH 30-FOOT GAP.

SOLID LINE - (GORE AREAS TO INCLUDE CHANNELIZING LINE AND DASHED LINE)

PER STANDARD SHEET E-50.

EDGE LINES

WHERE SPECIFIED EDGE LINES ARE NOT REQUIRED UNTIL COMPLETION OF THE PROJECT. ON INTERSTATE PROJECTS, TEMPORARY EDGE LINES SHOULD BE APPLIED WHERE TRAFFIC VOLUMES AND SPEEDS ARE HIGH AND DELAY OF SEVERAL DAYS IS ANTICIPATED.

- B. TEMPORARY MARKINGS MAY CONSIST OF PAINT, TAPE OR RAISED PAVEMENT MARKERS (RPM'S). THE TAPE SHALL BE A RETRO-REFLECTIVE FILM ON A CONFORMABLE METALIC BACKING THAT CAN BE PAVED OVER. TAPE MAY BE USED ON THE FINAL SURFACE COURSE IF IT WILL NOT INTERFERE WITH THE FINAL MARKING APPLICATION. THE RPM'S SHALL HAVE A SELF-ADHESIVE BACKING EASILY REMOVED BEFORE PAVING AND SHALL CONFORM TO THE FOLLOWING LAYOUT PATTERN: TEMPORARY TAPE MARKINGS WILL BE OFFSET AND REMOVED WHEN PROJECT IS FINISHED AND FINAL CENTERLINE PAINTED.

NO PASSING BARRIER

NO RPM'S ALLOWED.

DASHED LINE

FOUR RETRO-REFLECTIVE RPM'S ON 3 1/2 FOOT CENTERS WITH A 30 FOOT GAP.

SOLID LINE - EDGE LINES

INTERSTATE MEDIAN SIDE-RETRO-REFLECTIVE RPM'S ON 4 TO 5 FOOT CENTER. DRIVERS RIGHT SIDE-RPM'S NOT ALLOWED.

- C. WHEN PAINT IS USED FOR TEMPORARY MARKING, AN ALTERNATE MATERIAL SUCH AS TAPE OR RPM'S SHALL BE ON HAND IN THE EVENT RAIN PREVENTS THE PAINT APPLICATION FROM BEING COMPLETED. ALL PAINT SHALL BE REFLECTORIZED.
- D. PAYMENT FOR PAINT AND TAPE SHALL BE COMPUTED ON A LINEAR FOOT BASIS AS IF PAINT WAS USED. PAYMENT FOR THE RPM'S SHALL BE COMPUTED AS IF AN EQUIVALENT PAINT LINE WAS USED. (FOR EXAMPLE, DASHED LINE PAID AS 10 FEET OF PAINT, SOLID LINE PAID AS THE TOTAL DISTANCE COVERED WITH THE MARKERS).
- E. PRIOR TO ACCEPTANCE, THE FINAL PAVEMENT MARKINGS SHALL BE COMPLETED FOR THE ENTIRE PROJECT BY THE CONTRACTOR AS DETAILED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER

REVISED  
12/22/83

TEMPORARY 4" REFLECTORIZED YELLOW LINE

MILE	MILE	LT	RT	Lt	Rt	
0.305	0.48	Solid	Solid	924	924	1848
0.48	0.61	Dashed	Solid	172	686	858
0.61	1.88	Solid	Solid	6706	6706	13412
1.88	1.99	Solid	Dash	581	145	726
1.99	2.06	Dashed		92		92
2.06	2.22	Dash	Solid	211	845	1056
2.22	2.41	Solid	Solid	1003	1003	2006
2.41	2.57	Solid	Dash	845	211	1056
2.57	2.62	Dashed		66		66
2.62	2.76	Dash	Solid	185	739	924
T/L 2.76	2.785	Solid	Solid	132	132	264
0.00	0.272	Solid	Solid	1436	1436	2872
						25,180
5TH's @ 40' Each = 200' x 2 =						- 400
Assume 50% Lost During Leveling						Sub Total 24,780
.50 x 24,780 = 12,390						+ 12,390
						37,170
						Rounding + 30
						37,200

TEMPORARY 4" REFLECTORIZED WHITE LINE

MILE	MILE	LT	RT			
T/L 0.305	2.785	Solid		13,094	x 2 =	26,188
0.000	0.272	Solid		1436	x 2 =	2872
						Sub Total 29,060
						Rounding 40
						29,100

Although not indicated on this sheet the center and edge lines shall be broken at all intersecting roads as shown on the plan detail sheet. The Resident Engineer shall verify the accuracy of pavement markings in the field in accordance with the "Guidelines on Marking of Centerlines."