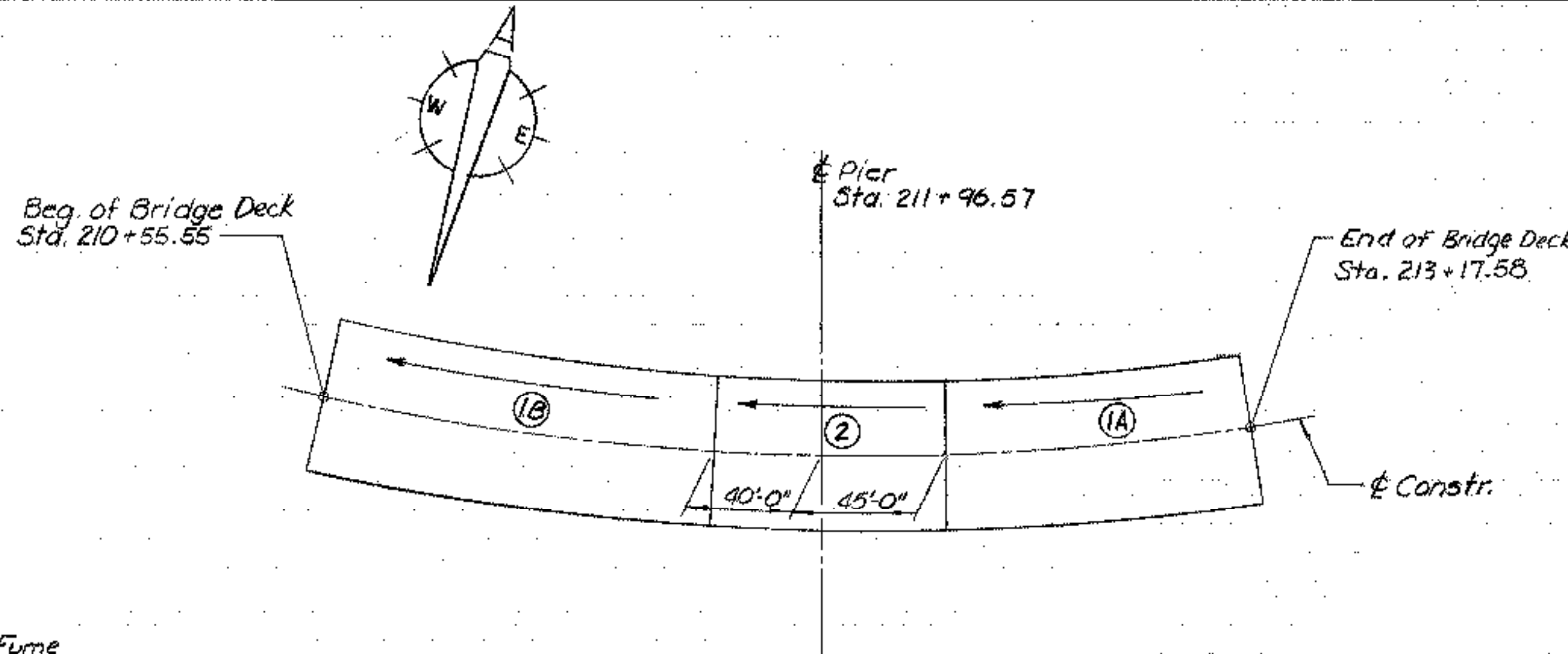


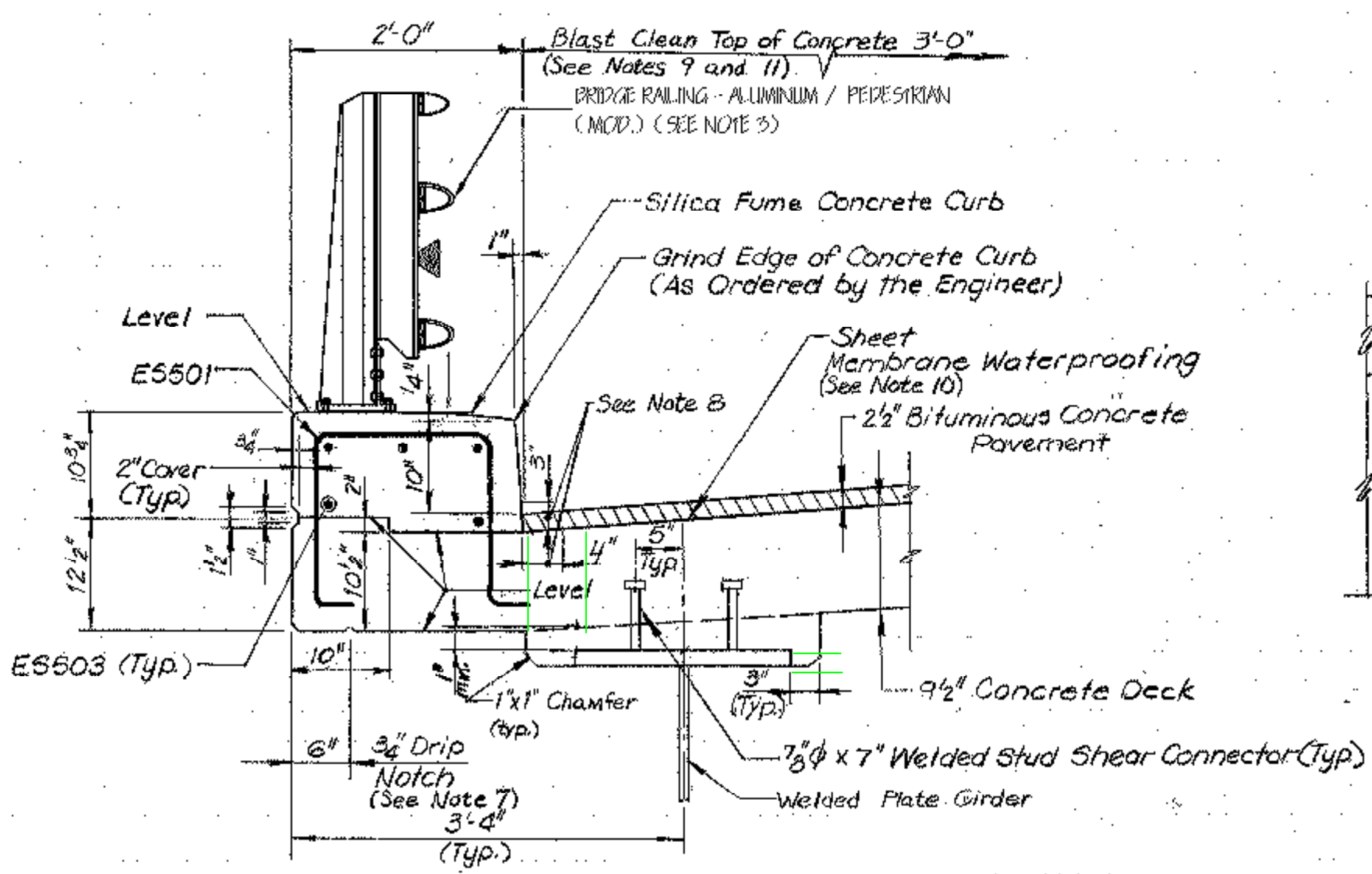
**TRANSVERSE SECTION**  
Scale: 3/8" = 1'-0"



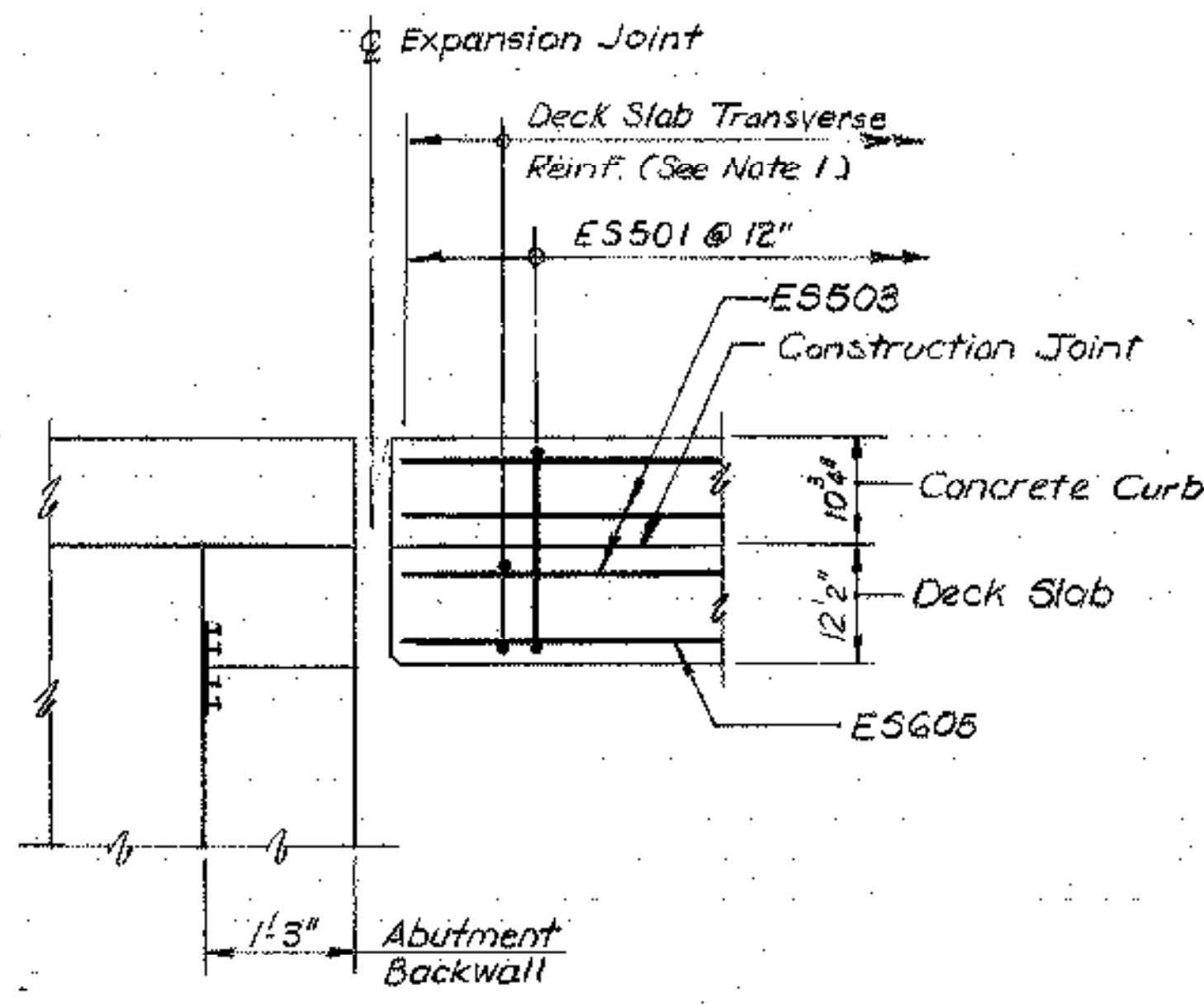
**DECK POUR SEQUENCE**  
Not To Scale

**NOTES:**

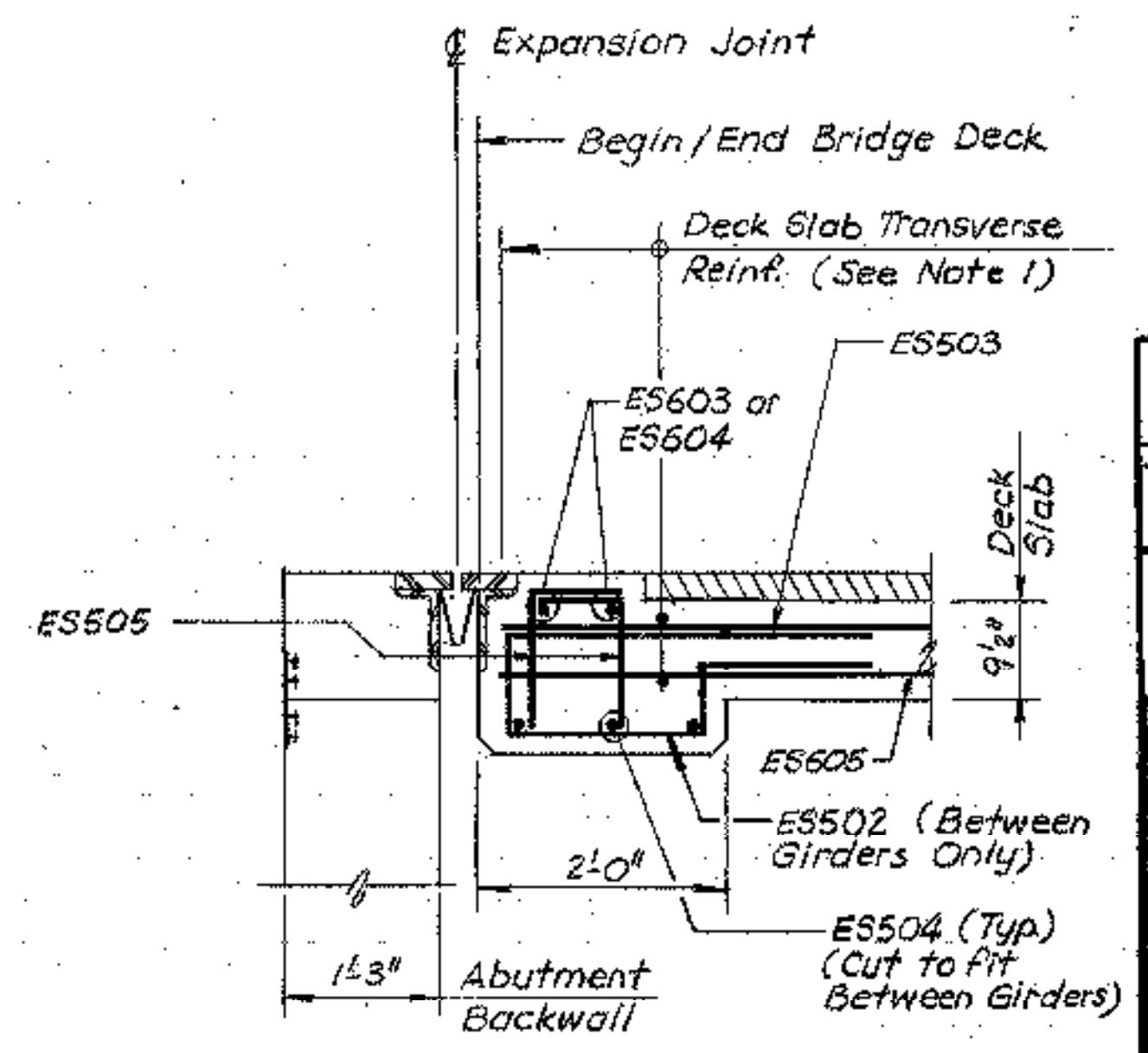
1. WORK THIS SHEET WITH DECK REINFORCEMENT PLAN, BRIDGE SHEET NO. BR106.
2. FOR CONCRETE CURB DETAILS, SEE BRIDGE SHEET NO. BR100.
3. FOR RAILING DETAILS, SEE BR122 & BR123 SHEETS.
4. POUR 1 (CONSISTING OF 1A AND 1B AS SHOWN) SHALL BE PLACED CONTINUOUSLY IN ONE 8 HOUR WORKING DAY. THERE SHALL BE A MINIMUM DELAY PERIOD OF 96 HOURS AFTER COMPLETION OF POUR 1 BEFORE BEGINNING POUR 2. POURS 1 AND 2 MAY BE COMBINED INTO A SINGLE POUR IF APPROVED BY THE ENGINEER. IN ANY CASE, THE PLACEMENT SEQUENCE SHALL BE IN THE ORDER 1A, 1B, 2.
5. THE DECK CONCRETE SHALL BE RETARDED SUFFICIENTLY TO REMAIN FLUID UNTIL EACH POUR IS COMPLETE. THE QUANTITY OF RETARDANT SHALL BE ESTABLISHED BY THE CONCRETE ENGINEER PRIOR TO PLACEMENT. ANY DEVIATIONS FROM THIS PROCEDURE MUST BE APPROVED BY THE STRUCTURES ENGINEER IN WRITING BEFORE THE POUR BEGINS.
6. FOR EXPANSION JOINT DETAILS AND TYPICAL SECTION AT END OF BRIDGE, SEE MISCELLANEOUS DETAILS, BRIDGE SHEET NOS. BR110, BR110A, AND BR110B.
7. STOP DRIP NOTCH 3" FROM FACE OF ABUTMENT AND OUTLET AT 45° TO FASCIA.
8. TWO COATS OF POLYURETHANE MEMBRANE SHALL BE PLACED 4" WIDE ALONG DECK, 3" UP FACE OF CURB AND ALONG CONCRETE SHOULDER AT EXPANSION JOINT. SEE SPEC. S1905(2).
9. BLAST CLEAN 3'-0" FROM FACE OF CURB AND 3" UP CURB FACE PRIOR TO PLACING MEMBRANCE.
10. SHEET MEMBRANE WATERPROOFING SHALL EXTEND TO FACE OF CURB.
11. POLYURETHANE MEMBRANE AND BLAST CLEANING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SHEET MEMBRANE WATERPROOFING.



**FASCIA DETAIL**  
Scale: 1" = 1'-0"



**END OF SLAB DETAIL**  
(Outside of Fascia Girders)  
Scale: 3/4" = 1'-0"



**END OF SLAB DETAIL**  
(Between Fascia Girders)  
Scale: 3/4" = 1'-0"

THIS SHEET HAS BEEN REVISED TO REFLECT 1993 AASHTO AND VACT SPECIFICATIONS AND STANDARDS  
 REVISED DESIGN BY: R. KELM DRAWN BY: G. SAUGER  
 CHECKED BY: J. PATUSKY DATE: 6/95

**BETTIGOLE ANDREWS & CLARK, INC.**  
Consulting Engineers

**STATE OF VERMONT**  
**AGENCY OF TRANSPORTATION**

Town Of MORETOWN / MIDDLESEX Bridge No. \_\_\_\_\_  
 Highway No. VT RTE 100B Log Sta. \_\_\_\_\_  
 Sur. Sta. 212+00

OVER THE WINOOSKI RIVER  
**TRANSVERSE SECTION**

Designed By JPH Drawn By NJC  
 Checked By DMN Date 12-85 Bridge Design Supervisor COB  
 Date 7-87

PROJECT MORETOWN / MIDDLESEX PROJECT NO. BRS 0167 (II)  
 I.G.C. info.

Bridge Sheet No. BR105 Sheet 92 of 92

**SEA** BEA Consultants Inc.  
Science/Engineering/Architecture  
 CAMBRIDGE, MASSACHUSETTS ROCKY HILL, CONNECTICUT  
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