

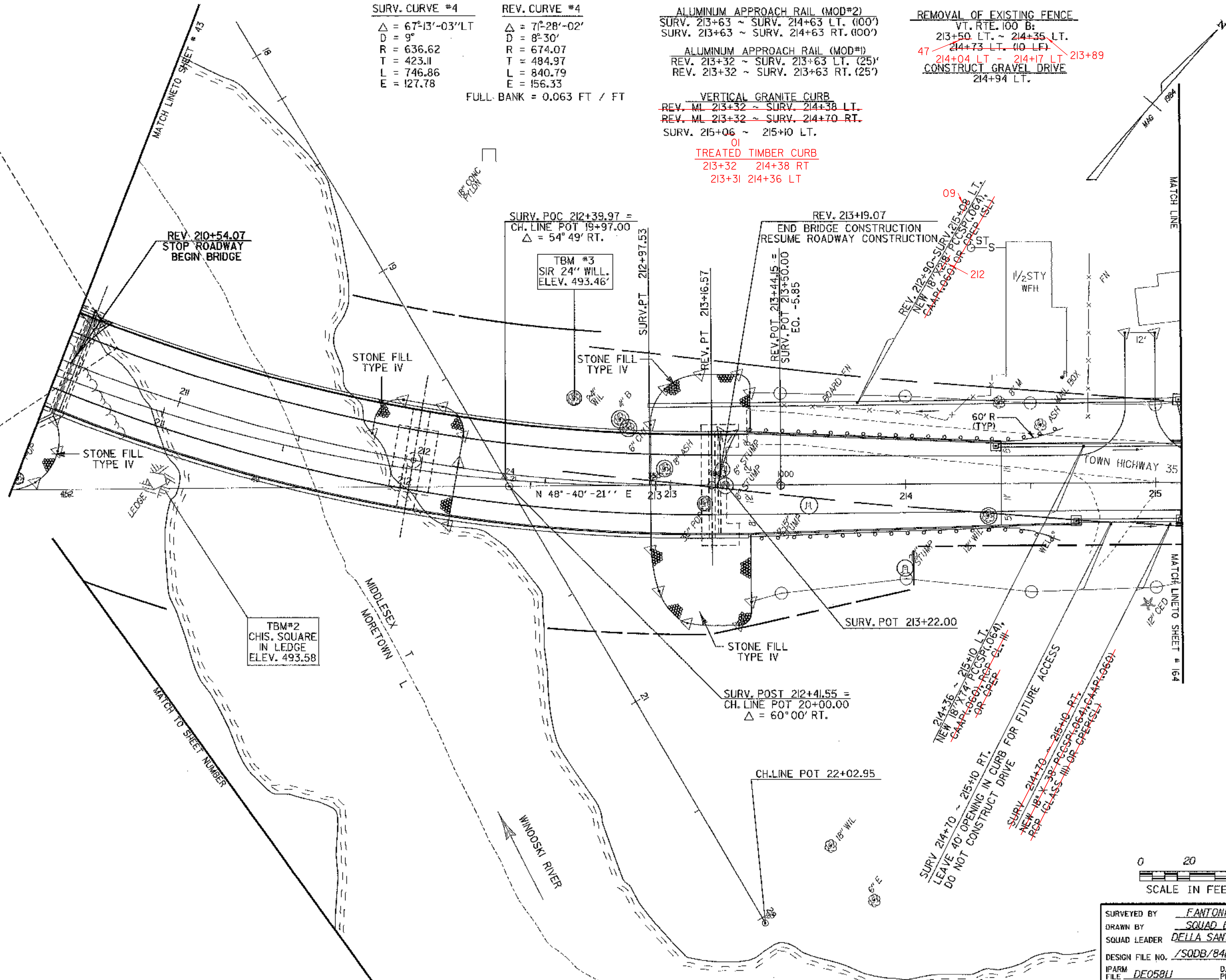
SURV. CURVE #4
 $\Delta = 67^{\circ}13'-03''$ LT
 D = 9'
 R = 636.62
 T = 423.11
 L = 746.86
 E = 127.78

REV. CURVE #4
 $\Delta = 71^{\circ}28'-02''$
 D = 8'-30"
 R = 674.07
 T = 484.97
 L = 840.79
 E = 156.33
 FULL BANK = 0.063 FT / FT

ALUMINUM APPROACH RAIL (MOD#2)
 SURV. 213+63 ~ SURV. 214+63 LT. (100')
 SURV. 213+63 ~ SURV. 214+63 RT. (100')
 ALUMINUM APPROACH RAIL (MOD#1)
 REV. 213+32 ~ SURV. 213+63 LT. (25')
 REV. 213+32 ~ SURV. 213+63 RT. (25')

REMOVAL OF EXISTING FENCE
 VT. RTE. 100 B:
 213+50 LT. ~ 214+35 LT.
 47' ~~214+73 LT. (10 LF)~~
 214+04 LT. - 214+17 LT. 213+89
 CONSTRUCT GRAVEL DRIVE
 214+94 LT.

VERTICAL GRANITE CURB
 REV. ML 213+32 ~ SURV. 214+38 LT.
 REV. ML 213+32 ~ SURV. 214+70 RT.
 SURV. 215+06 ~ 215+10 LT.
 TREATED TIMBER CURB
 213+32 214+38 RT
 213+31 214+36 LT



TBM#2
 CHIS. SQUARE
 IN LEDGE
 ELEV. 493.58

TBM #3
 SIR 24" WILL.
 ELEV. 493.46'

SURV. POC 212+39.97 =
 CH. LINE POT 19+97.00
 $\Delta = 54^{\circ}49'$ RT.

REV. 213+19.07
 END BRIDGE CONSTRUCTION
 RESUME ROADWAY CONSTRUCTION

REV. POT 213+44.15 =
 SURV. POT 213+50.00
 SURV. EC. -5.85

SURV. POST 212+41.55 =
 CH. LINE POT 20+00.00
 $\Delta = 60^{\circ}00'$ RT.

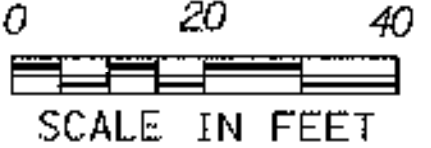
CH. LINE POT 22+02.95

SURV. POT 213+22.00

214+36 ~ 215+10 LT.
 NEW 18" X 18" PCCSP (1.06%)
 OR CPEP
 OR CPEP (CLASS III) OR CPEP (SL)

SURV. 214+70 ~ 215+10 RT.
 NEW 18" X 36" PCCSP (1.06%)
 OR CPEP (CLASS III) OR CPEP (SL)

LEAVE 40' OPENING IN CURB FOR FUTURE ACCESS
 DO NOT CONSTRUCT DRIVE



SURVEYED BY	FANTONI	DATE	1/87
DRAWN BY	SQUAD B	DATE	8/87
SQUAD LEADER	DELLA SANTA		
DESIGN FILE NO.	/SODB/B4E058/DE058.DGN		
IPARM FILE	DE058U	DATE PLOTTED	01-MAR-2000
PROJ. NAME	MORETOWN - MIDDLESEX		
PROJ. NO.	BR50167(II)		
SHEET	162 of 243	SHEETS	

DATUM
 VERTICAL NGVD 1929
 HORIZONTAL N/A

○ DENOTES TREE OR STUMP REMOVAL