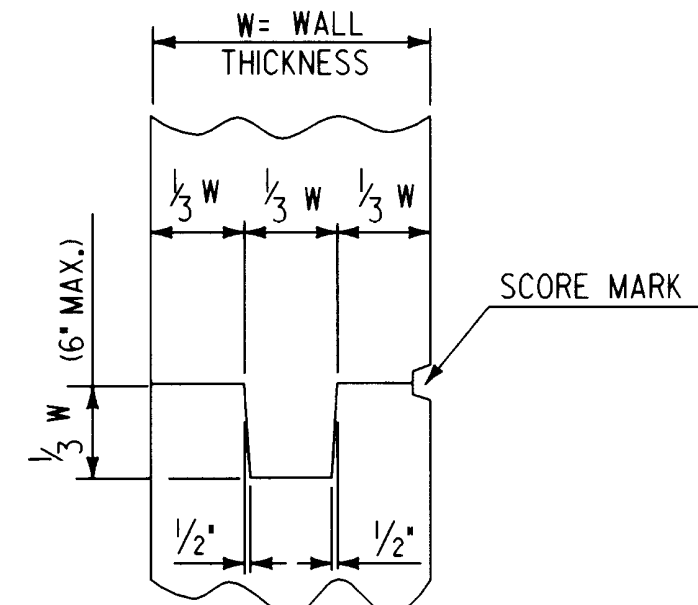
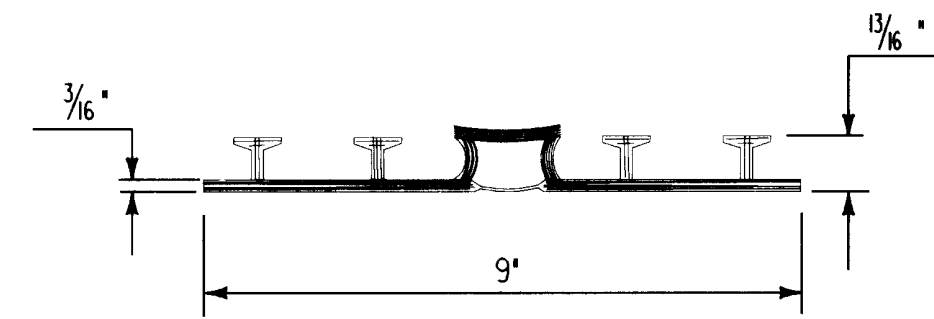


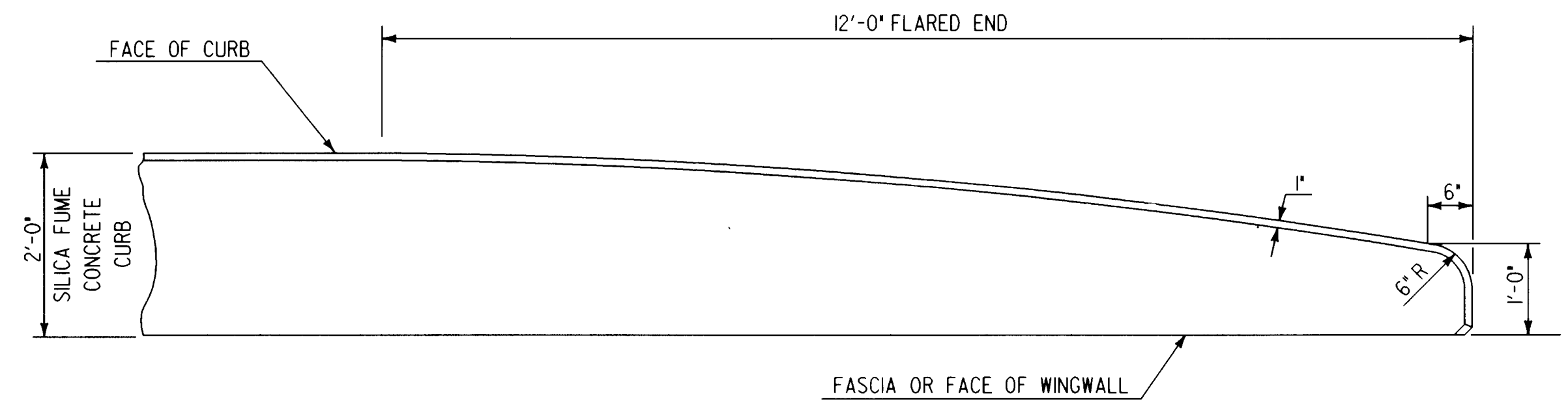
HAUNCH DETAIL
SCALE: 1" = 1'-0"



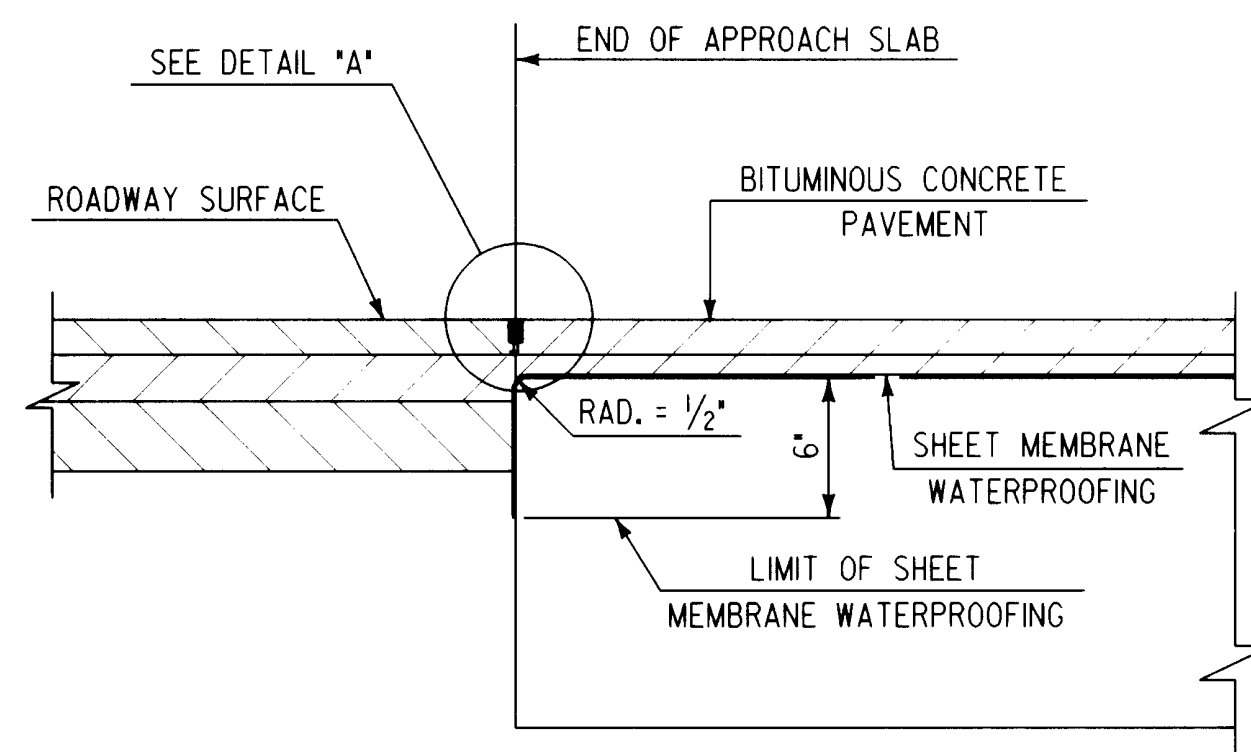
TYPICAL CONCRETE CONSTRUCTION JOINT
NOT TO SCALE



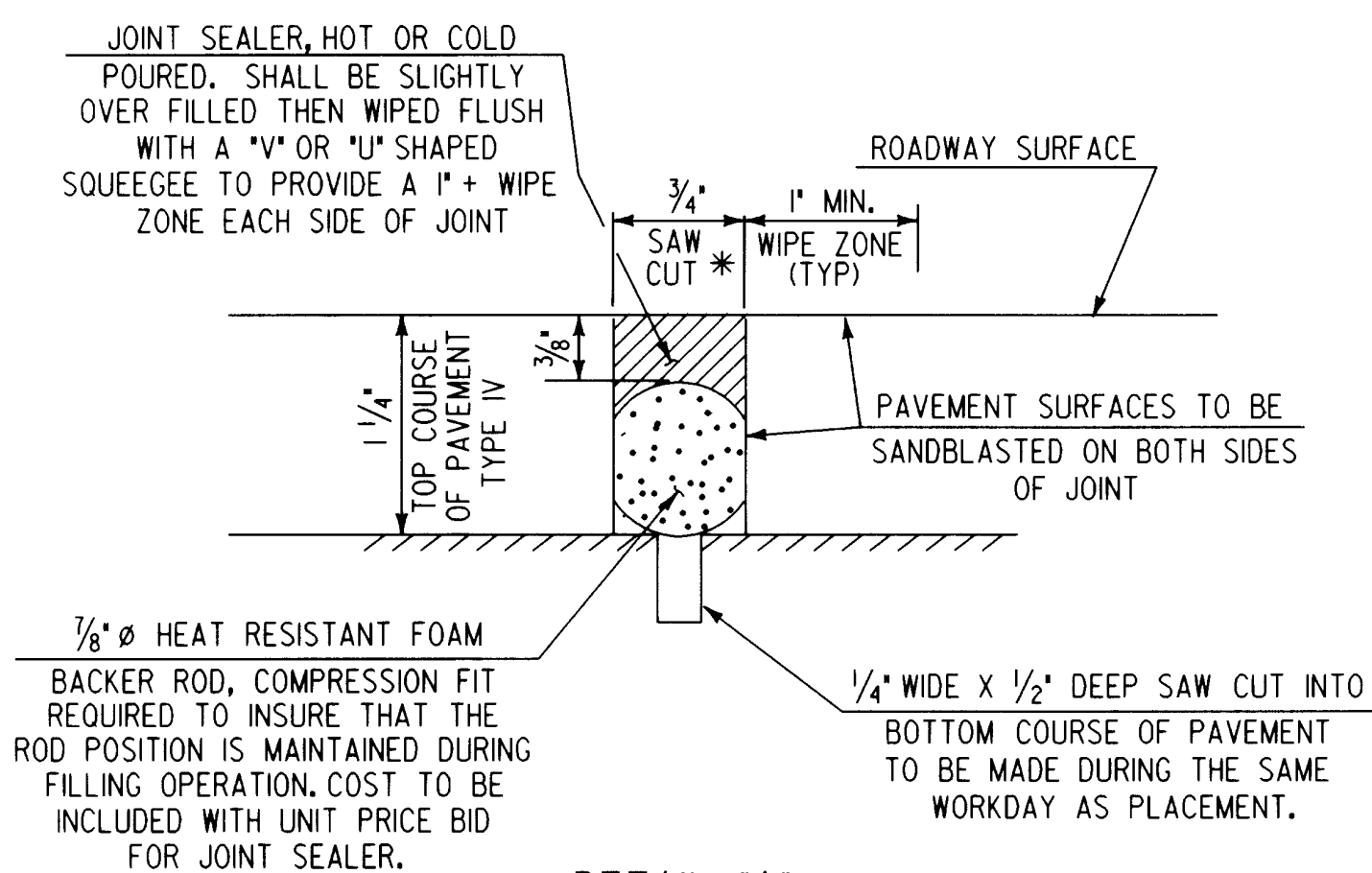
P. V. C. WATERSTOP FOR EXPANSION JOINTS
NOT TO SCALE
THE COSTS FOR P. V. C. WATERSTOP SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CONCRETE. OTHER CONFIGURATIONS MAY BE USED UPON APPROVAL OF THE STRUCTURES ENGINEER.



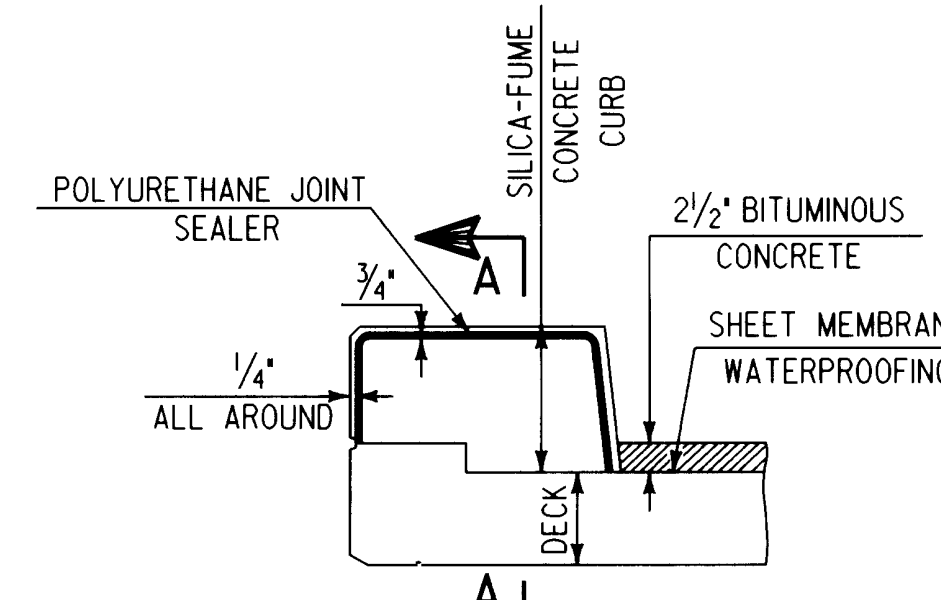
FLARED END DETAIL FOR 2'-0" CURB
SCALE: 3/4" = 1'-0"
CURB REINFORCING STIRRUP BARS SHALL BE TURNED AS REQUIRED TO FIT FLARED END.



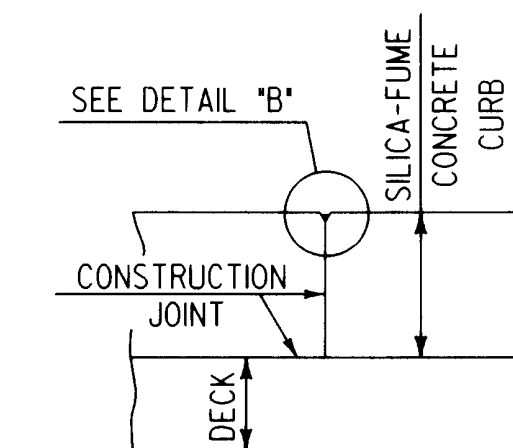
DETAIL FOR JOINT IN PAVEMENT AND SHEET MEMBRANE AT END OF APPROACH SLAB
NOT TO SCALE



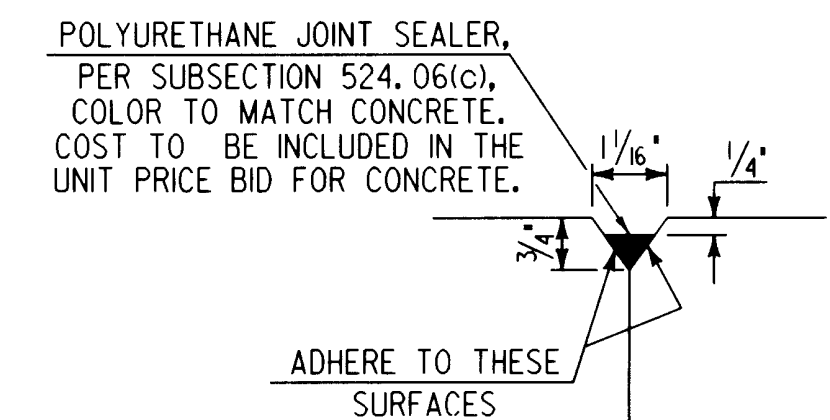
DETAIL 'A'
NOT TO SCALE
* JOINT IS TO BE LOCATED ACCURATELY BY STRING LINING, OR OTHER MEANS, PRIOR TO PAVING, SO THAT THE SAW CUTS WILL BE MADE DIRECTLY OVER THE END OF CONCRETE DECK. JOINT SHALL BE CUT DRY IN A SINGLE PASS AND BE SEALED WITHIN 24 HOURS OR PRIOR TO EXPOSURE TO TRAFFIC. JOINT SHALL BE CLEANED PRIOR TO APPLYING THE JOINT SEALER. SEE VT. SPECIFICATION 524 AND SPECIAL PROVISIONS.



TYPICAL SECTION THROUGH CONCRETE CURB CONSTRUCTION JOINT
NOT TO SCALE



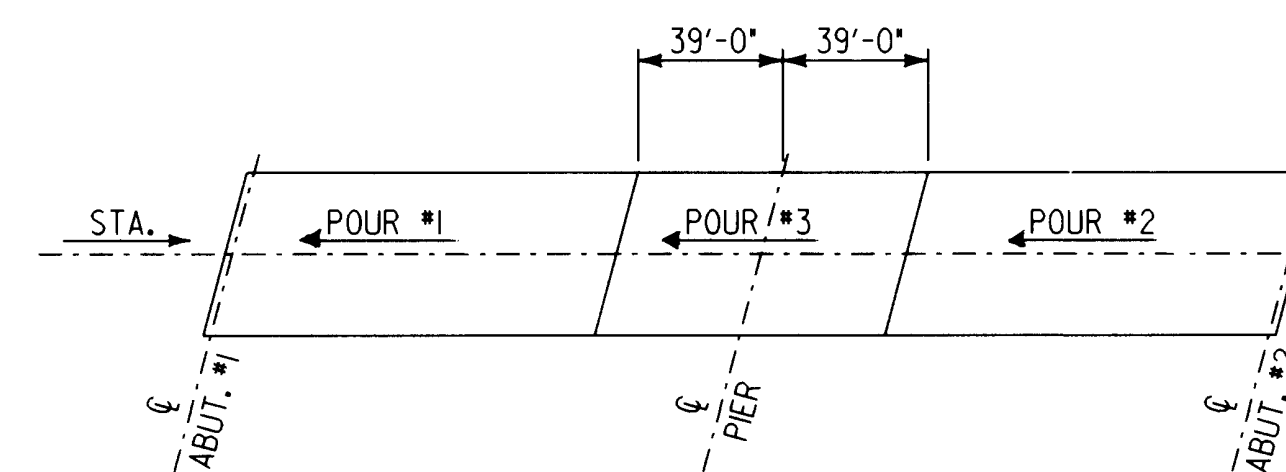
SECTION A - A
NOT TO SCALE



DETAIL 'B'
NOT TO SCALE

NOTES:

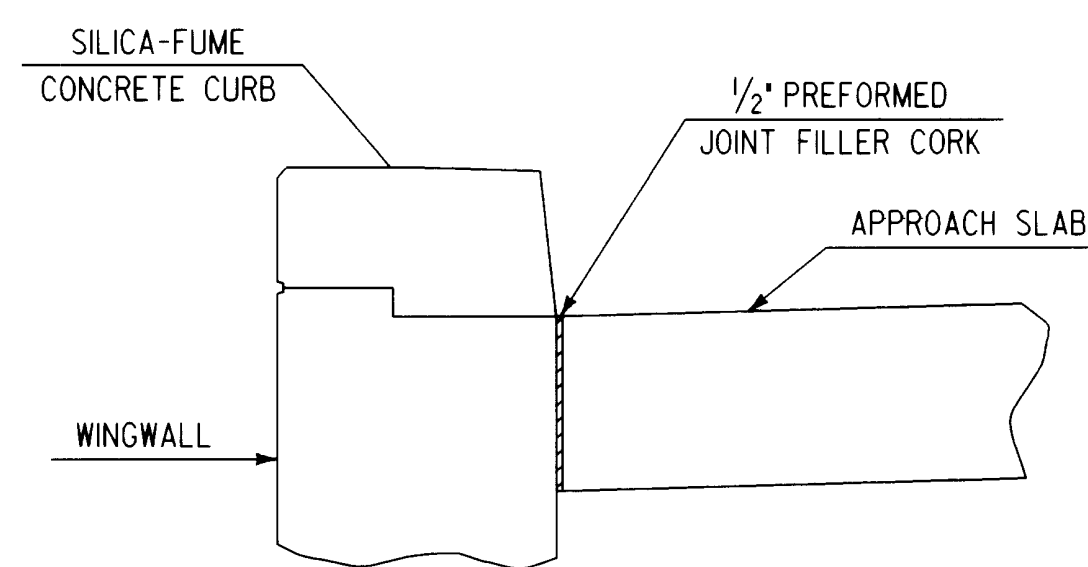
- CONSTRUCTION JOINTS THROUGH CONCRETE CURBS SHALL BE SPACED MAXIMUM 15'-0" CENTER TO CENTER AND SHALL BE 1'-6" MINIMUM FROM THE CENTER OF THE NEAREST BRIDGE RAIL POST. CONCRETE SHALL BE PLACED IN ALTERNATING SECTIONS WITH A MINIMUM OF 48 HOURS DELAY BETWEEN ADJACENT POURS.
- LONGITUDINAL REINFORCING SHALL PASS THROUGH CONCRETE CURB CONSTRUCTION JOINTS.
- POLYURETHANE MEMBRANE AND BLAST CLEANING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SHEET MEMBRANE WATERPROOFING.
- SHEET MEMBRANE WATERPROOFING SHALL EXTEND TO FACE OF CURB AS SHOWN ABOVE LEFT.
- ALL CONCRETE IN CURBS SHALL BE SILICA-FUME CONCRETE.



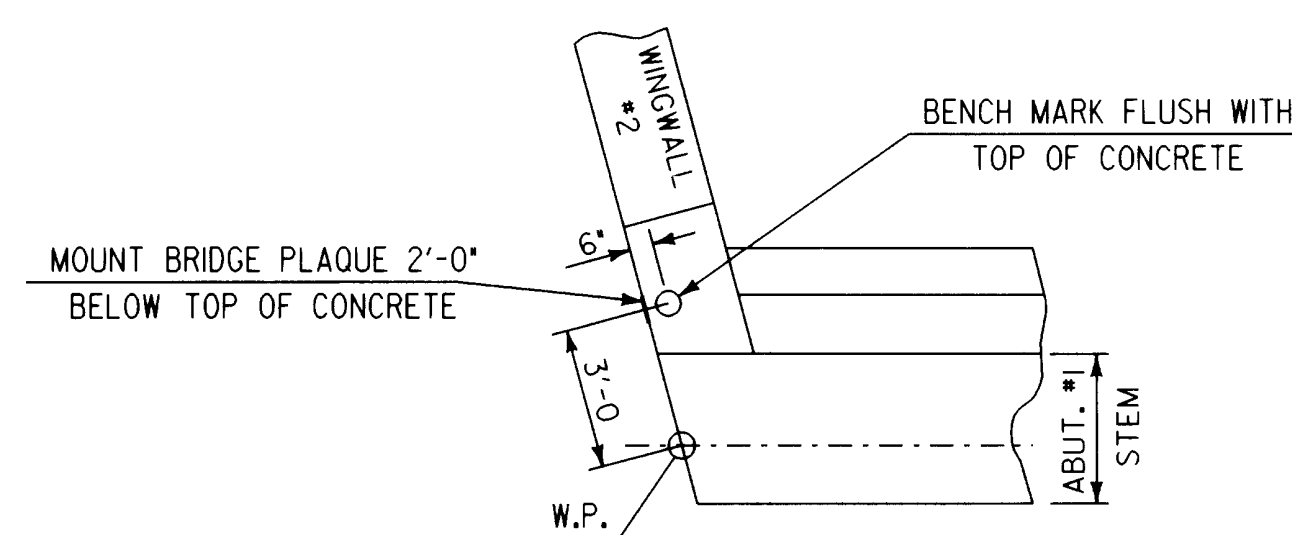
POUR SEQUENCE
NOT TO SCALE

NOTES:

- THE SEQUENCE OF POURS 1 & 2 MAY BE INTERCHANGED, A MINIMUM OF 96 HOURS BETWEEN POURS IS REQUIRED.
- IF APPROVED BY THE ENGINEER, POURS 1 & 2 OR ALL SECTIONS MAY BE Poured THE SAME DAY PROVIDED THE MAXIMUM TIME OF THE POUR IS 8 HOURS AND THE INDICATED POURING SEQUENCE IS FOLLOWED AND A RETARDING ADMIXTURE IS USED SO THAT THE DECK CONCRETE WILL NOT SET UP UNTIL ALL THE POUR HAS BEEN MADE. THE RETARDING ADMIXTURE WILL BE SUBSIDIARY TO THE ITEM 504.22, "CONCRETE CLASS A".
- ALL POURS SHALL START FROM THE LOW END (ELEVATION WISE).
- IF ALL SECTIONS ARE NOT Poured IN THE SAME DAY, USE TRANSVERSE BRIDGE SLAB CONSTRUCTION JOINTS. (SEE DETAIL THIS SHEET).
- BRIDGE DECK CONCRETE SHALL BE DEPOSITED PARALLEL TO THE CENTERLINE OF BEARING.

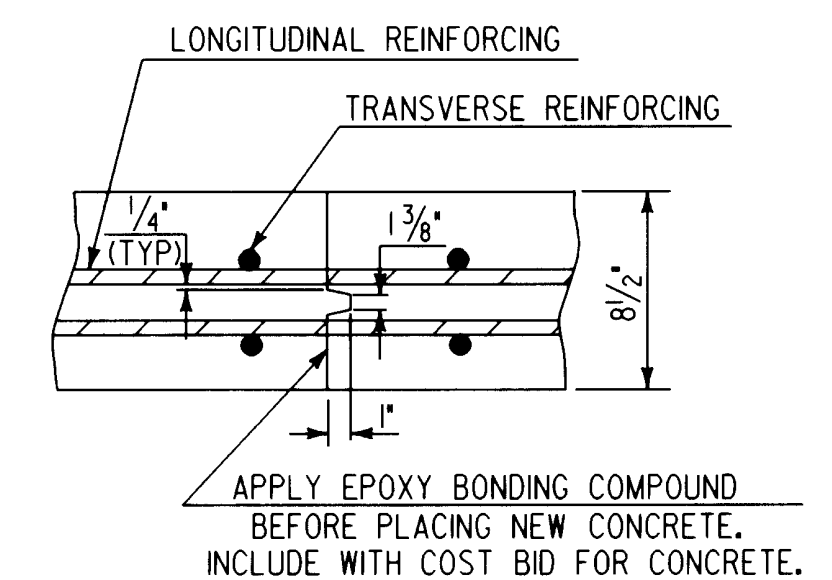


DETAIL OF APPROACH SLAB AGAINST WINGWALL
SCALE: 3/4" = 1'-0"



BENCH MARK AND BRIDGE PLAQUE LOCATION DETAIL
NOT TO SCALE

THE BENCH MARK AND BRIDGE PLAQUE WILL BE SUPPLIED BY THE AGENCY OF TRANSPORTATION AND SHALL BE INSTALLED BY THE CONTRACTOR AT ABUTMENT #1 ON THE RIGHT SIDE AS SHOWN OR AS DIRECTED BY THE ENGINEER.



TRANSVERSE BRIDGE SLAB CONSTRUCTION JOINT DETAILS
NOT TO SCALE

Revised 7/9/96

STATE OF VERMONT AGENCY OF TRANSPORTATION

Town Of	SPRINGFIELD	Bridge No.	44
Highway No.	U.S. ROUTE 5	Log Sta.	
		Surv. Sta.	
U.S. ROUTE 5 OVER BLACK RIVER			
MISCELLANEOUS DETAILS			
Designed By	S. BAKI	Drawn By	W. GAYNOR
Checked By	S. BAKI	Date	6-95
		Bridge Design Supervisor	P. PIERCE
		Date	5/96
PROJECT	SPRINGFIELD	PROJECT NO.	BRS-RS 0113 (8)
I.G.C. Info.	cr\usr\j9145622\spslabpl.dgn		
Bridge Sheet No.	BR19	Sheet	48 of 90