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STANDARD SHEET 3-30 SUBMITTED TO THE CHIEF ENGINEER ON AUG 15, 1933.		

STATE OF VERMONT  
STATE HIGHWAY DEPARTMENT

PLAN AND PROFILE OF PROPOSED  
STATE HIGHWAY

NATIONAL RECOVERY SECONDARY PROJECT

TOWNS OF SOUTH BURLINGTON, WILLISTON

SHELBURNE ST. GEORGE

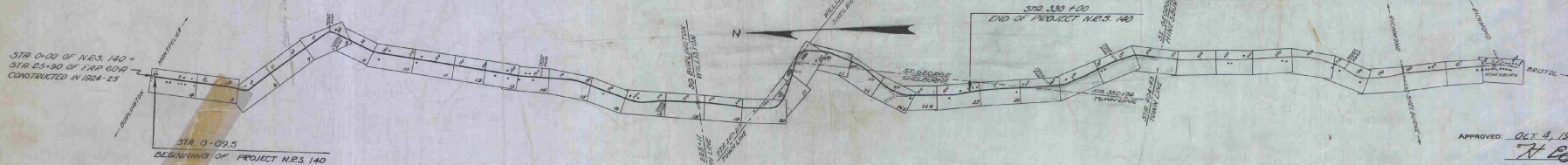
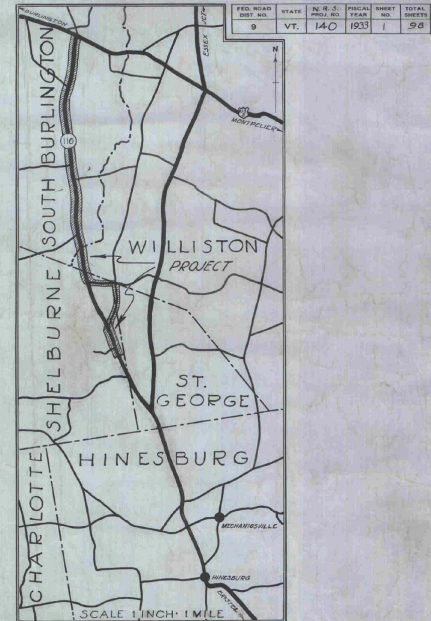
BURLINGTON-BRISTOL ROAD

FROM ITS INTERSECTION WITH THE BURLINGTON

- MONTPELIER ROAD SOUTHERLY 6.592 MILES TO

THE HOUSE OF FRANK MAXWELL

LENGTH OF PROJECT = 34808.0 FT.  
= 6.592 MILES



APPROVED: *Oct 8, 1933*  
*H. C. Stewart*  
COMMISSIONER OF HIGHWAYS  
SUBMITTED BY ORDER OF THE STATE HIGHWAY BOARD

CONVENTIONAL SIGNS

- COUNTY LINE
- TOWN LINE
- FENCE LINE
- STONE WALL
- UNFENCED PROPERTY
- GUARD RAIL
- TRAVELED WAY
- RAILROAD
- RETAINING WALL
- CENTER LINE
- SURVEY LINE
- CULVERT
- DROP INLET
- TROLLEY POLE
- POWER POLE
- TELEPHONE POLE
- TREES
- HEDGE

- GROUND ELEVATION
- GRADE ELEVATION

- CURVE DATA
- DEFLECTION ANGLE
  - DEGREE OF CURVE
  - RADIUS OF CURVE
  - TANGENT DISTANCE
  - LENGTH OF CURVE
  - EXTERNAL DISTANCE
  - POINT OF INTERSECTION
  - POINT OF CURVE
  - POINT OF TANGENT
  - POINT ON TANGENT

TITLE	SCALE
PLAN	1" = 2000'
PROFILE	1" = 50'
CROSS SECTIONS	1" = 5'

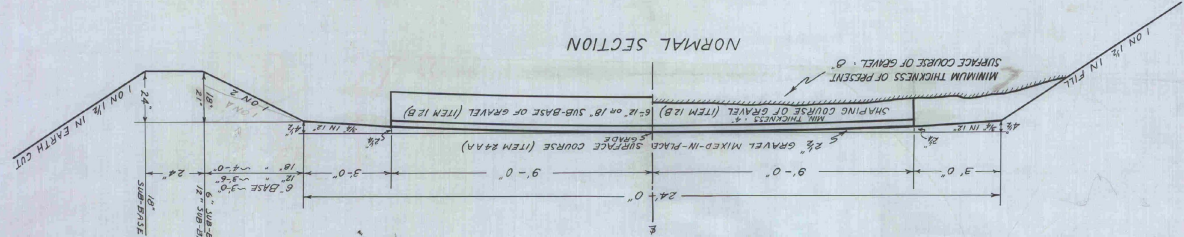
*Note*  
These plans are subject to such revisions as may be required by the Bureau of Public Roads or the Commissioner of Highways.  
Construction is to be carried on in accordance with the standard road and bridge specifications of 1930 with all subsequent approved revisions and such specifications and special provisions as are submitted with the plans.  
Attention of contractors is called to the fact that the State Highway Board reserves the right to extend this project southerly of the same contract prices. Said cost of extension not to exceed 25% of the amount of contract.

RECOMMENDED [Signature]  
DISTRICT ENGINEER BUREAU OF PUBLIC ROADS  
RECOMMENDED FOR APPROVAL [Signature]  
CHIEF ENGINEER BUREAU OF PUBLIC ROADS  
APPROVED [Signature]  
DIRECTOR BUREAU OF PUBLIC ROADS

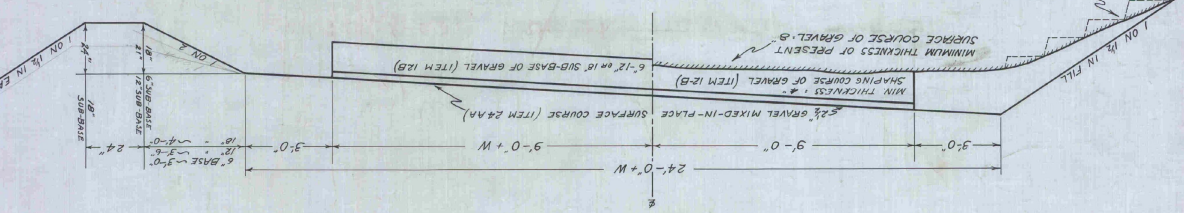


TYPICAL SECTIONS

2 1/2% GRAVEL MIXED-IN-PLACE SURFACE COURSE ITEM 24AA

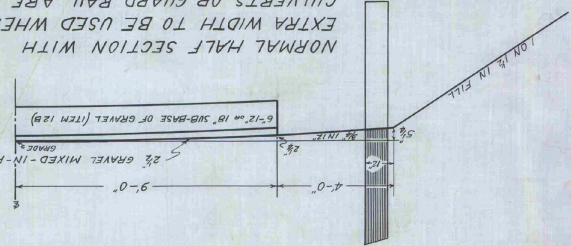


NORMAL SECTION

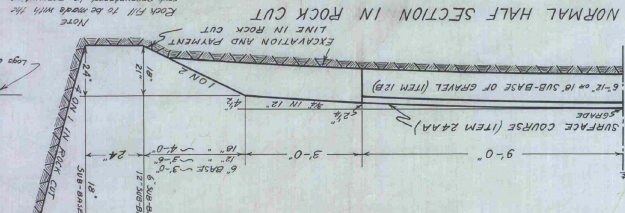


BANKED AND WIDENED SECTION

NORMAL HALF SECTION WITH  
 EXTRA WIDTH TO BE USED WHERE  
 CULVERTS OR GUARD RAIL ARE  
 REQUIRED

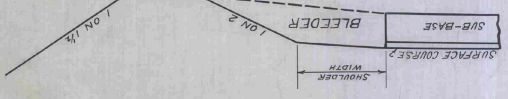


When embankments are to be made on a hillside, the slope of the original ground on which the embankment is to be made shall be allowed deeply or cut into steps before filling is commenced. See pamphlet B, item 10, paragraph 103.



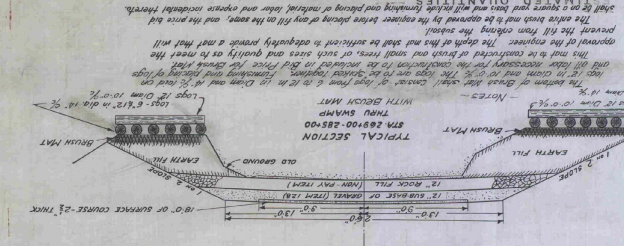
NORMAL HALF SECTION IN ROCK CUT

Members of Contractors to consult the fact that the State Highway Board reserves the right to extend this item whenever, in the case of extension, it is deemed desirable.



Notes—  
 Backfill of gravel for culvert trench. At culverts where a (A) four foot clearance between sub-grade and top of pipe can not be obtained and the soil is other than sand or gravel, the backfill shall be constructed on a base of gravel and the trench backfilled with gravel. Construction of sub-base of gravel to be paid for as sub-base of gravel.  
 Item 12-B limits to be stripped to the satisfaction of the engineer, practicable at driveways.  
 Shoulders shall be constructed of suitable material and thoroughly rolled or satisfactorily compacted. The work shall be considered as subsidiary work pertaining to excavation or borrow. See pamphlet B, item 10, paragraph 103.  
 Driveways to be constructed to provide satisfactory approaches to road surface at locations shown on cross sections.  
 A minimum thickness of 12" sub-base of gravel shall be used under road surface thru rock cuts. This sub-base to be extended thirty feet beyond each end of rock section.  
 The attention of the engineer is called to the removal of sod, wide and located every one hundred feet and staggered except on banked curves. These drains are to provide drainage for the sub-base construction methods measurement and payment are to be as the specifications for sub-base. Bleeders or side drains are to be constructed at the same time the sub-base is constructed as shown on the plans or as ordered by the engineer, to meet the actual field conditions as encountered during construction.

Gravel mixed-in-place surface course with gravel sub-base. If gravel mixed-in-place surface course with gravel sub-base is ordered by the engineer, the contractor will be required to verify the sub-base of gravel prior to the application of the track coat of tar to remove the largest stones and debris from the surface. The gravel sub-base shall be re-rolled as required.



TYPICAL SECTION

END AREAS IN SQUARE FEET

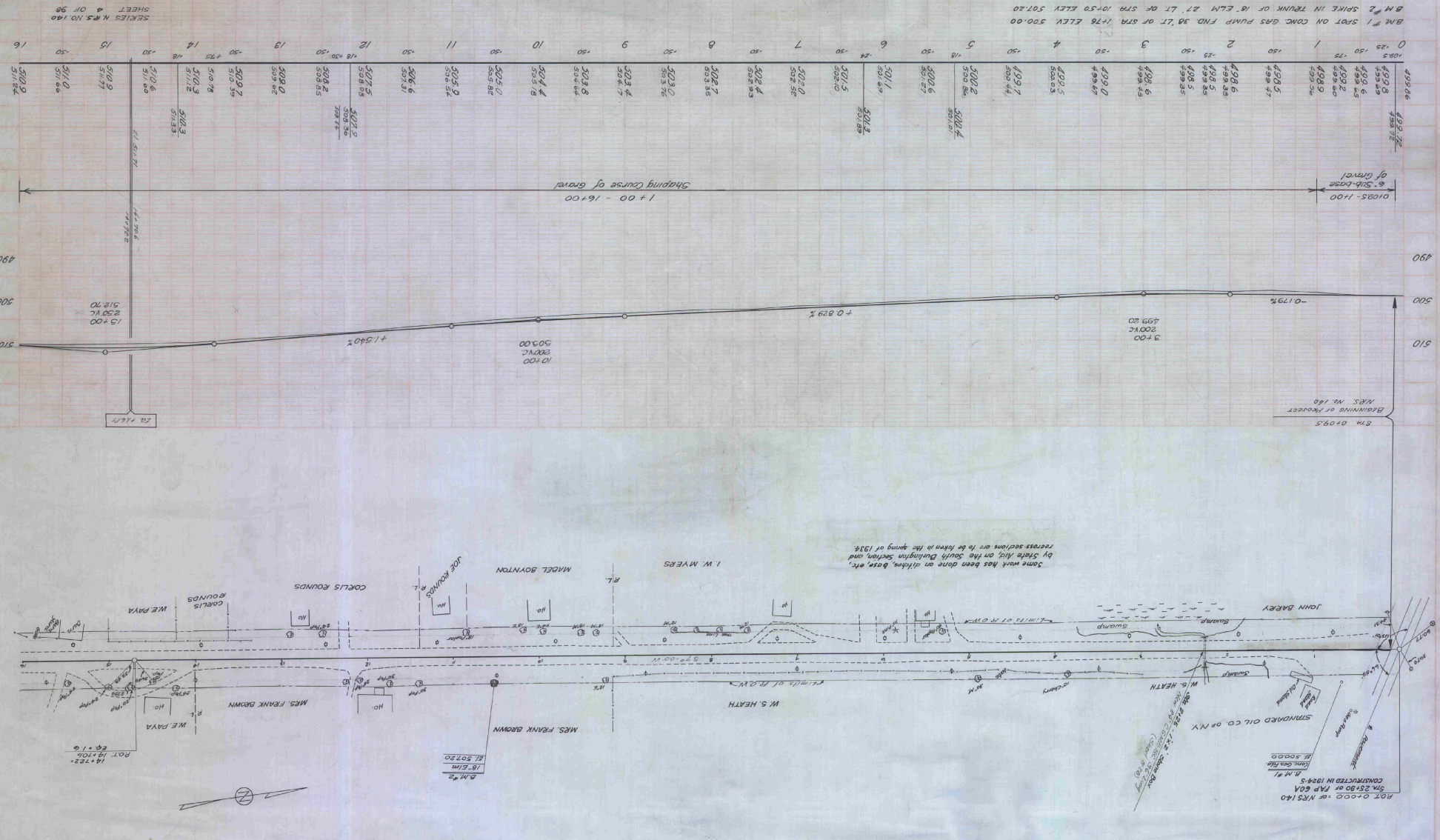
WIDTH	10'	20'	30'	40'
GRAVEL	10'	27.0	30.0	31.5
Sub-base	12'	18.0	20.0	21.0
Sub-base	6'	9.0	10.0	10.5
2 1/2% Gravel	3.75	4.17	4.37	
Width	20'	20'	20'	20'

ESTIMATED QUANTITIES

shall be for a square yard base and will include spreading and placing of material, labor and expense incidental thereto. The entire base and is approved by the engineer before placing of any fill on the same, and the price will prevent the fill from settling the wheel.

NOTE—  
 Keon fill to be made with the approval of the engineer. The depth of this sand shall be sufficient to adequately provide a road that will not be constructed of brush and wood chips and gravel to meet the requirements of the contract. The gravel shall be placed in the sub-base and the top of the sub-base shall be compacted to the same density as the gravel in the sub-base. The bottom of gravel sub-base shall consist of logs from 6" to 12" in diam and 18" to 24" long. Logs 5" to 6" in diam 18" long.

Checked by A. D. WOOD  
 Drawn by C. S. DUTTON  
 Traced by L. F. CONE  
 Series M.S. No. 140  
 Sheet 2 of 541 Sheets



Station	Elevation	Notes
0+00	499.86	B.M. #1 SPOT ON CONC GAS PUMP FND 36 LT OF STR 10+50 ELEV 500.00
0+25	499.82	
0+50	499.81	
0+75	499.85	
1+00	499.80	
1+25	498.57	
1+50	498.5	
1+75	498.5	
2+00	498.6	
2+25	498.48	
2+50	498.5	
2+75	498.5	
3+00	498.6	
3+25	499.15	
3+50	499.0	
3+75	499.67	
4+00	499.5	
4+25	499.08	
4+50	499.44	
4+75	499.7	
5+00	500.2	
5+25	500.86	
5+50	500.6	
5+75	501.1	
6+00	501.69	
6+25	501.5	
6+50	500.10	
6+75	502.0	
7+00	502.5	
7+25	502.4	
7+50	502.53	
7+75	502.7	
8+00	503.0	
8+25	503.7	
8+50	503.85	
8+75	503.4	
9+00	503.4	
9+25	504.17	
9+50	504.64	
9+75	504.4	
10+00	504.18	
10+25	505.0	
10+50	505.82	
10+75	505.9	
11+00	506.4	
11+25	507.5	
11+50	508.08	
11+75	508.2	
12+00	508.5	
12+25	509.0	
12+50	509.82	
12+75	509.7	
13+00	509.78	
13+25	510.3	
13+50	510.3	
13+75	510.3	
14+00	510.6	
14+25	510.9	
14+50	510.77	
14+75	511.0	
15+00	511.66	
15+25	511.84	
15+50	511.84	
15+75	511.84	
16+00	511.84	

B.M. #1 SPOT ON CONC GAS PUMP FND 36 LT OF STR 10+50 ELEV 500.00  
 B.M. #2 SPIKE IN TRUNK OF 18' CLM 27' LT OF STR 10+50 ELEV 507.20

SERIES N.C.S. NO. 140  
 SHEET 4 OF 98

NOT 0+00 OF N.C.S. 140  
 ON 25+00 OF MAP 60A  
 CONSTRUCTED IN 1934-5  
 R.M. #1  
 100' STATION MARKS  
 100' STATION MARKS

Some work has been done on the South Burlington section, and by State Aid, on the South Burlington section, and by State Aid, on the South Burlington section, etc.

75.0 1000  
 2.5 1000  
 1/16 1000

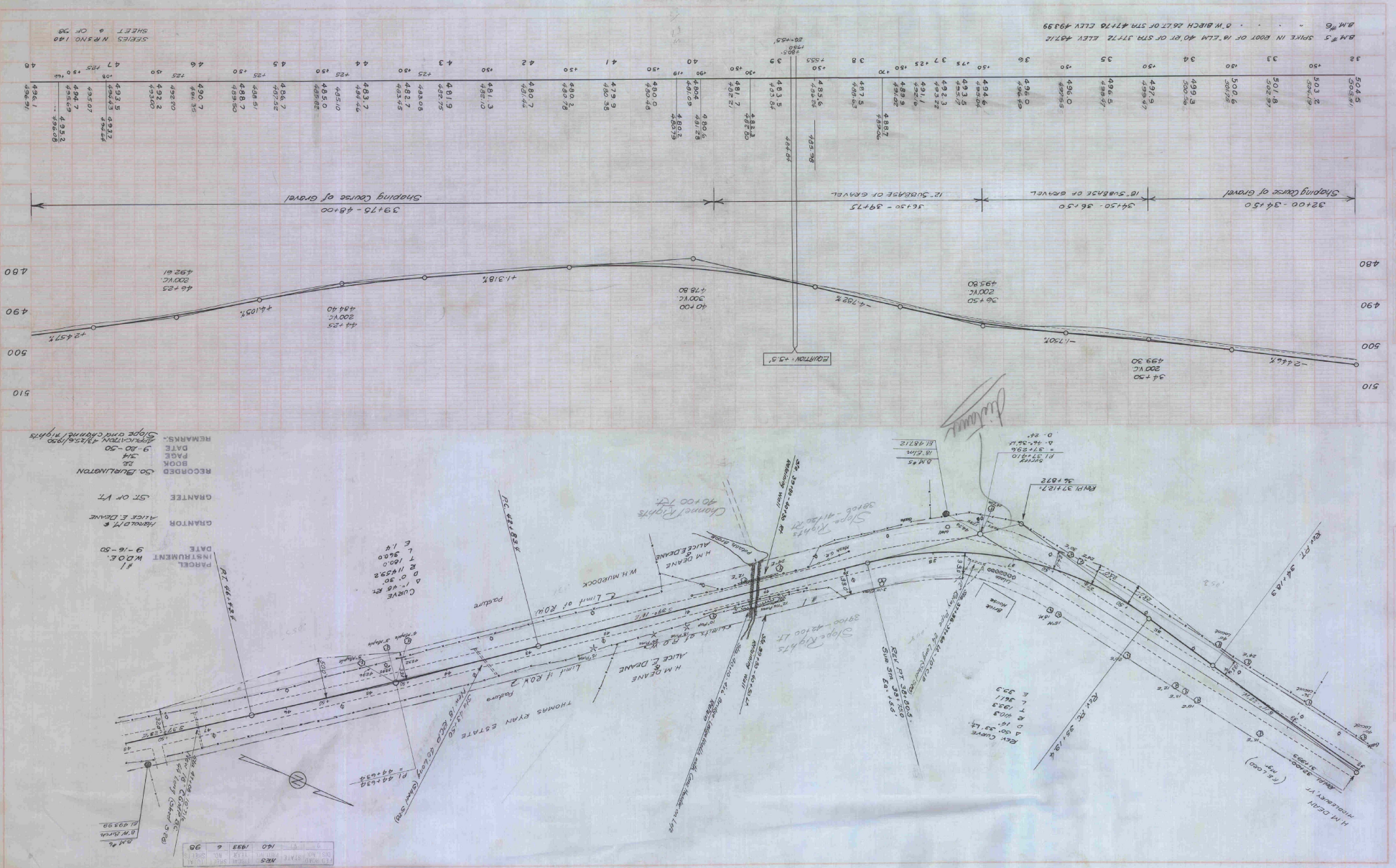
Fixed  
 Station  
 Point



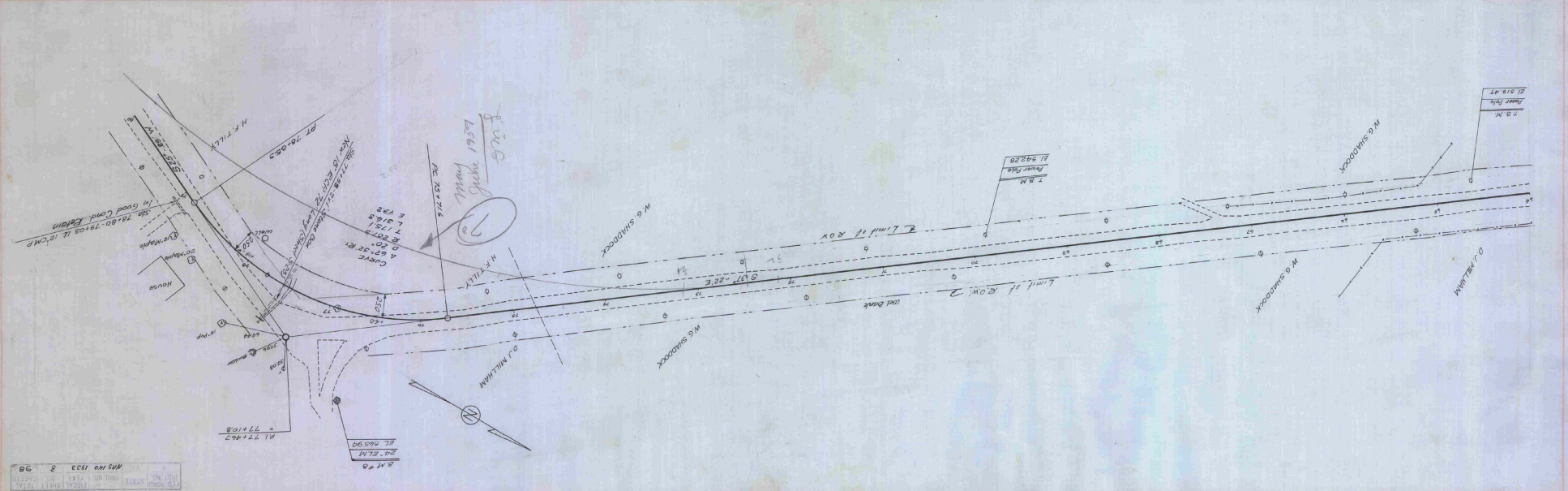
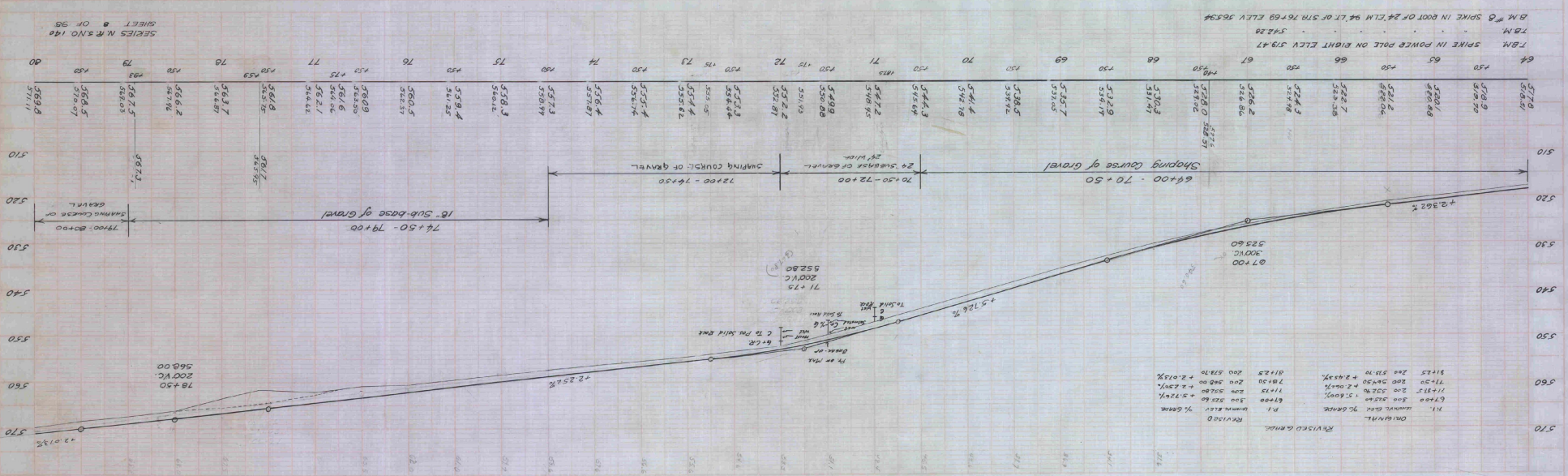
123  
12/11

123  
12/11

123  
12/11







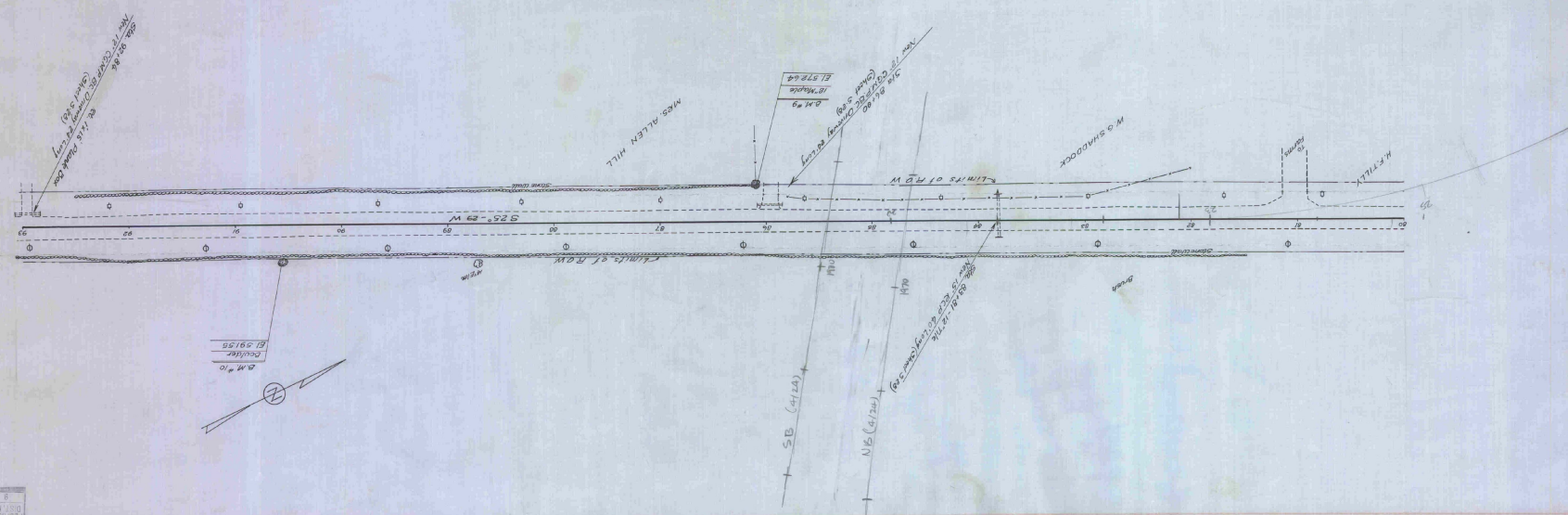
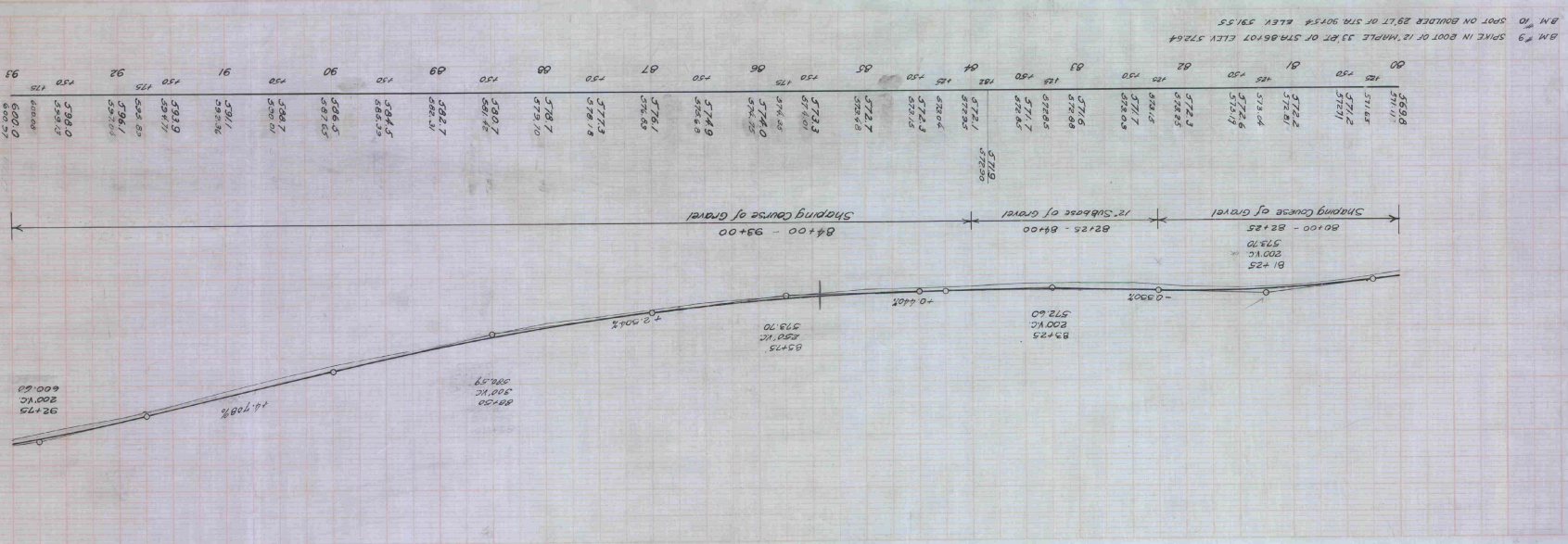
100' SCALE  
 SHEET NO. 8  
 PROJECT SHEET TOTAL  
 DATE  
 DRAWN BY  
 CHECKED BY  
 APPROVED BY

100' SCALE  
 SHEET NO. 8  
 PROJECT SHEET TOTAL  
 DATE  
 DRAWN BY  
 CHECKED BY  
 APPROVED BY

1:2 Scale  
 1/4" = 10'  
 1/8" = 5'

W. S. SHADDOCK  
 CIVIL ENGINEER

500  
 490  
 500



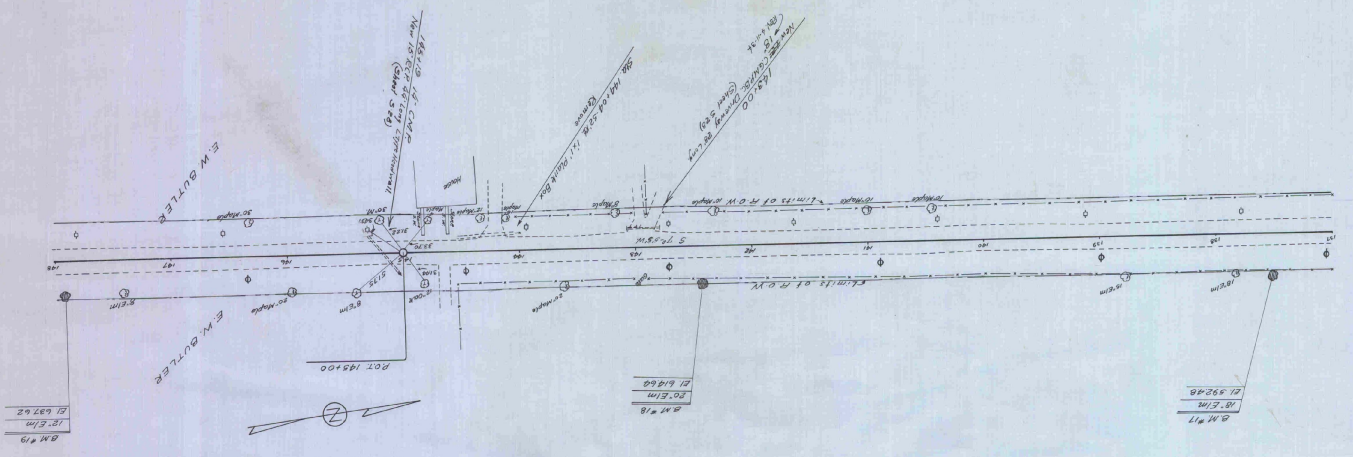
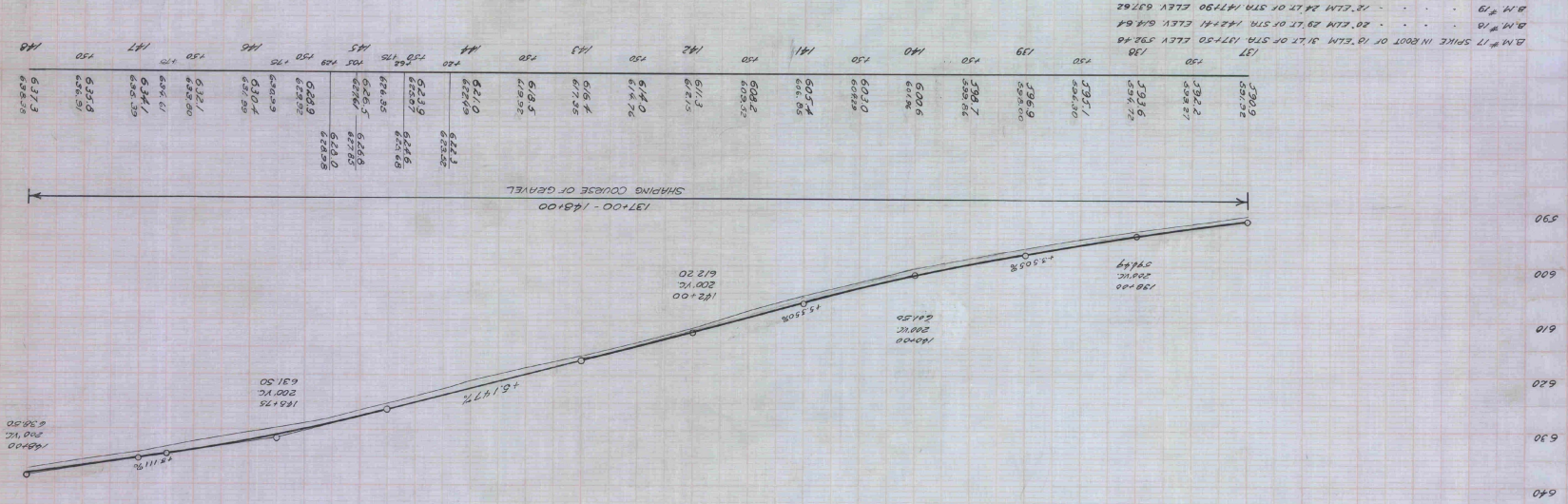
SEVERAL N. 1/2 OF 50  
 SHEET 4 OF 50

1:2 Scale  
 1/4" = 10'  
 1/8" = 5'



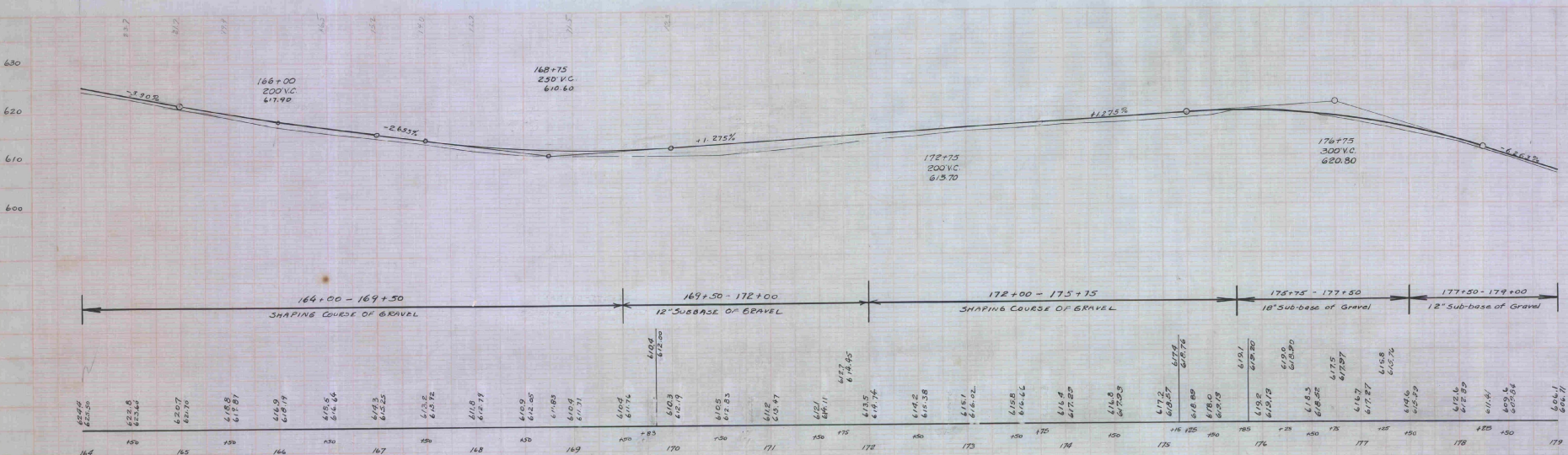
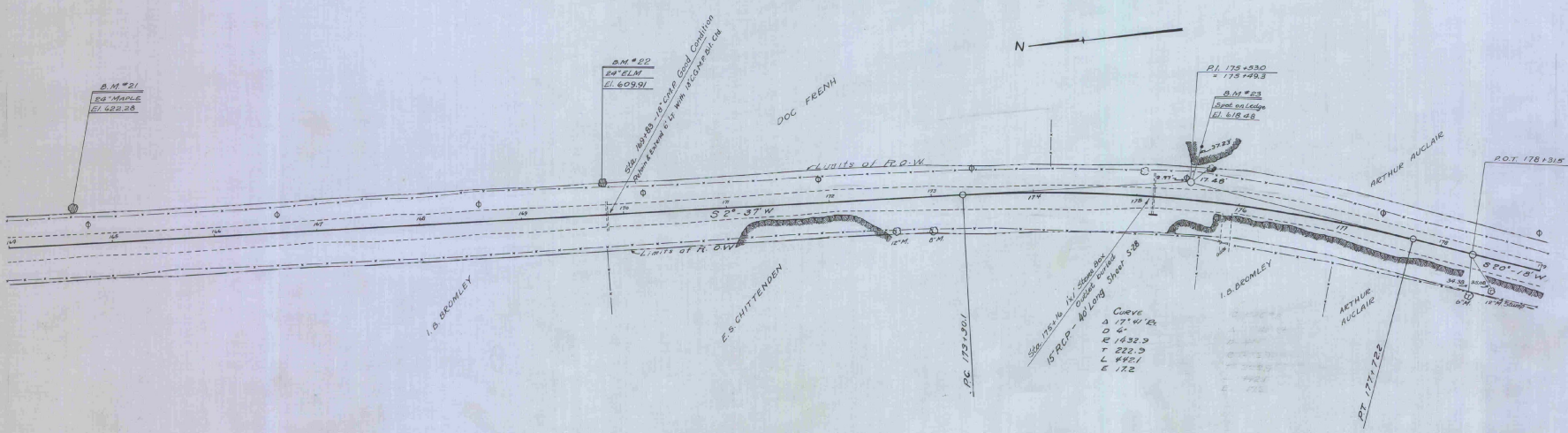






SERIES N.E.S. N. 140  
 SHEET 13 OF 36





B.M. #21 SPIKE IN ROOT OF 24' MAPLE 30' LT. OF STA. 164+53 ELEV. 622.28  
 B.M. #22 ELM 28' LT. OF STA. 169+70 ELEV. 609.91  
 B.M. #23 PAINT SPOT ON LEDGE 53' LT. OF STA. 175+58 ELEV. 618.48

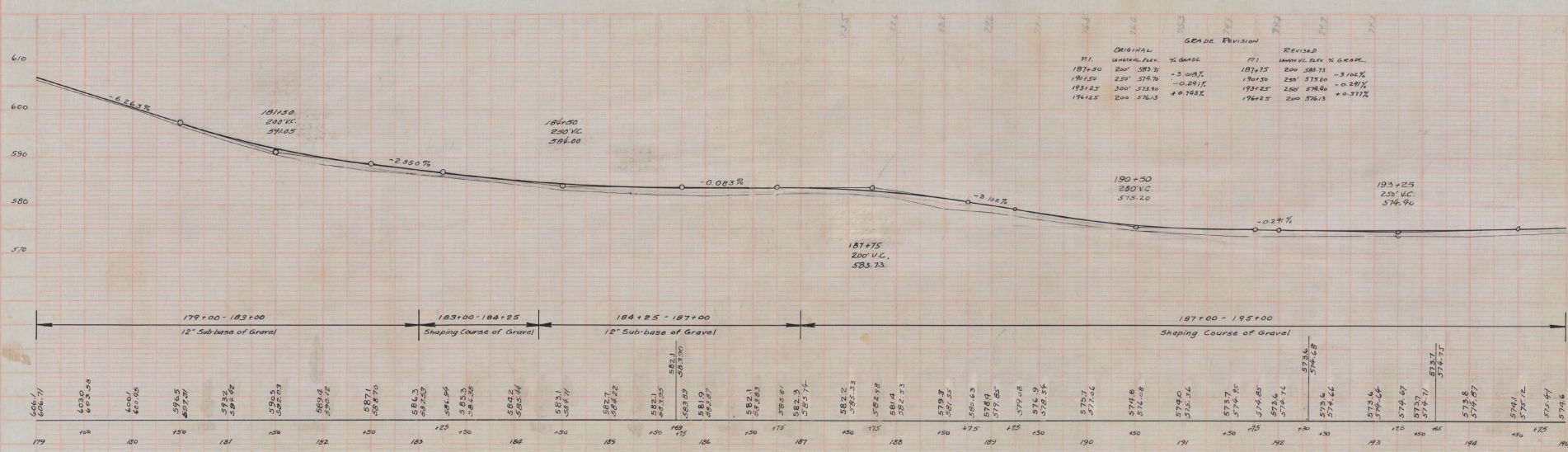
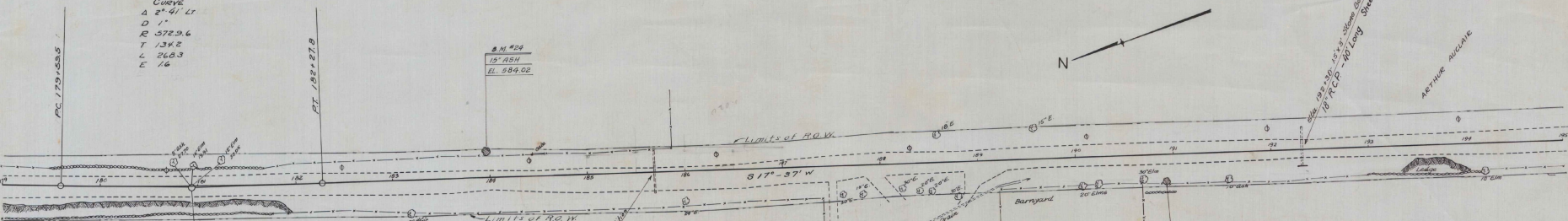
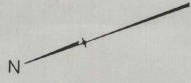
Curve  
 $\Delta$  2° 41' LT  
 D 1'  
 R 572.96  
 T 134.2  
 L 260.3  
 E 16

B.M. #25  
 20' ELM  
 EL. 584.02

B.M. #25  
 20' ELM  
 EL. 579.96

See notes on page 100 for  
 Right of Way  
 1/4" = 100' on Scale 5/8"

B.I. 180+33.7  
 = 180+33.6

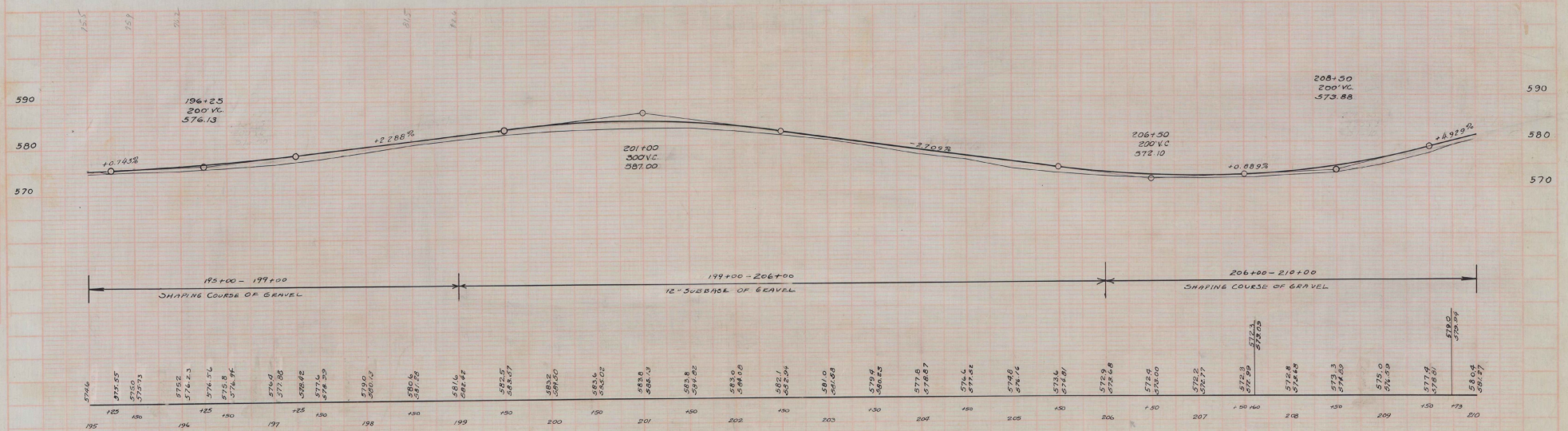
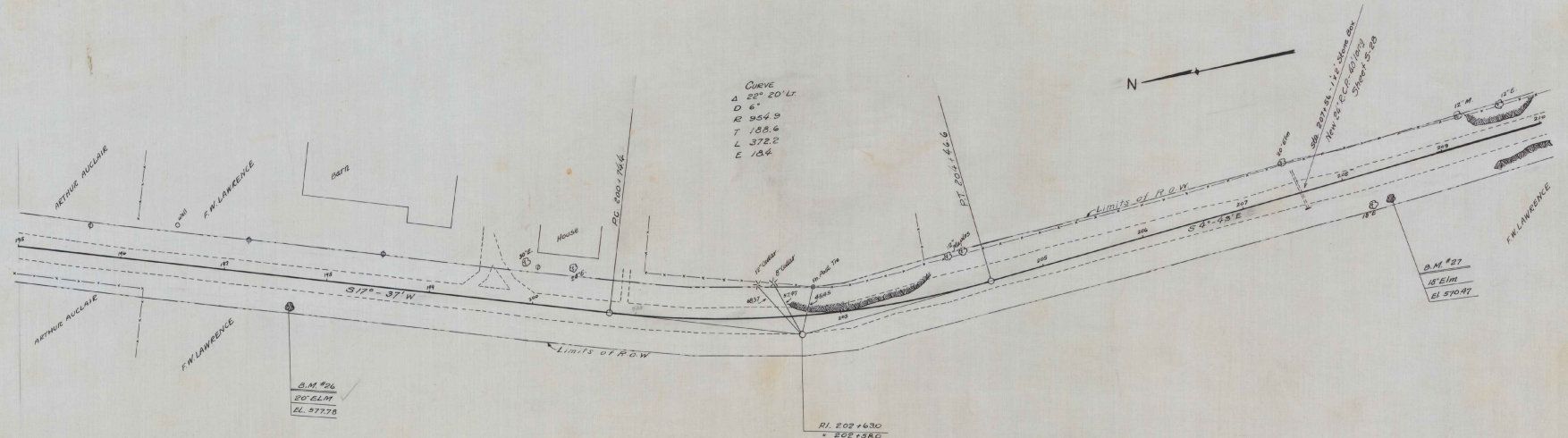


B.M. #24 SPIKE IN ROOT OF 15' ASH 21' LT. OF STR. 183+25 ELEV. 584.02  
 B.M. #25 20' ELM 24' RT. OF STR. 190+31 ELEV. 579.96

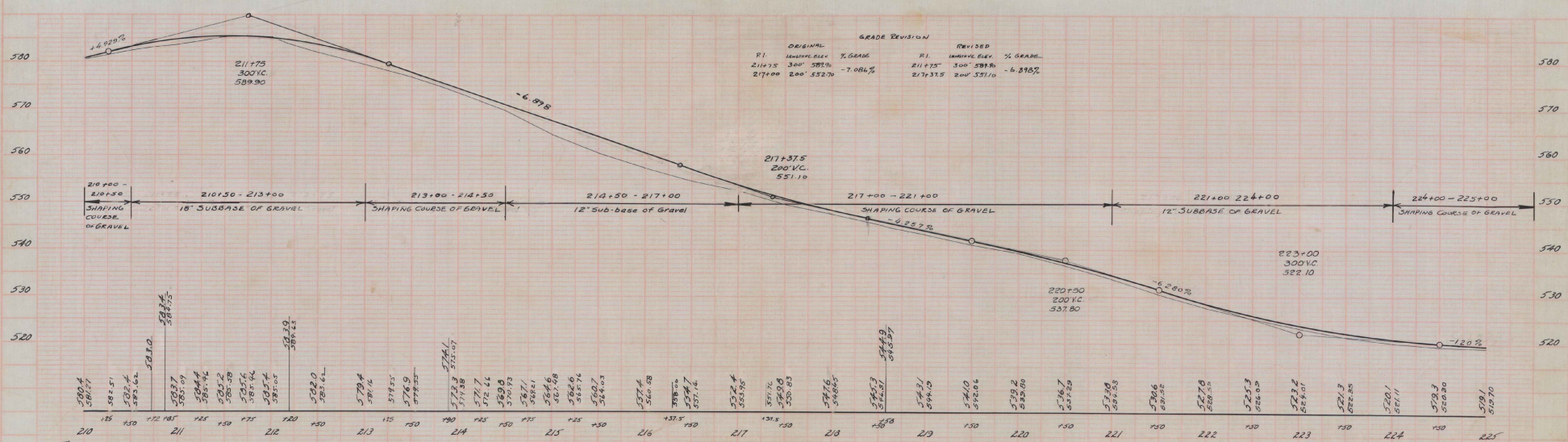
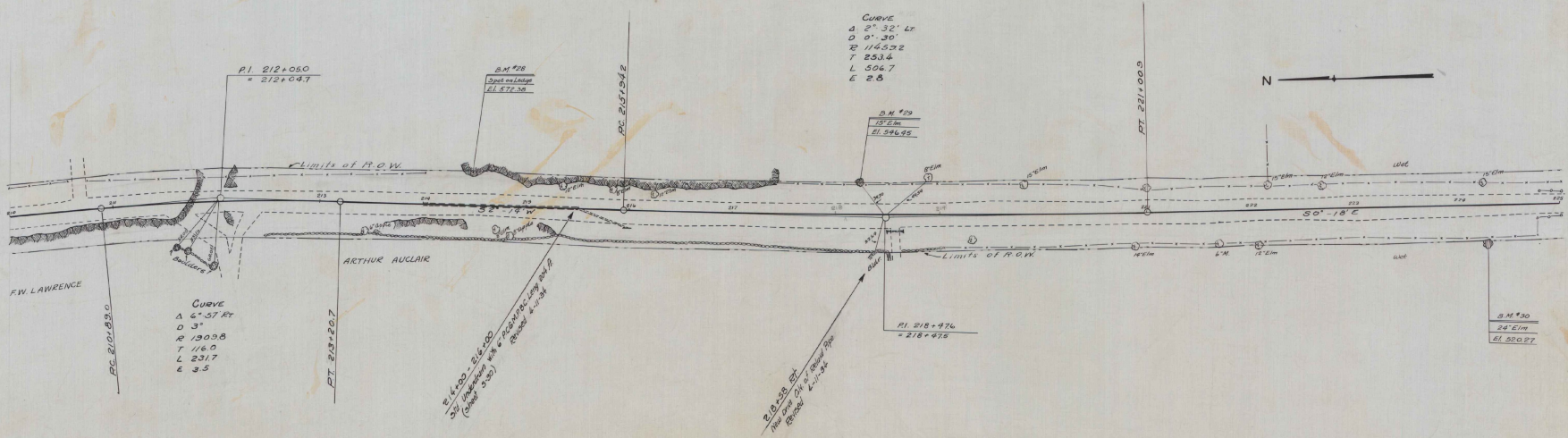
PLAN  
 Vertical Curves  
 Limits of Right-of-Way  
 Proposed Roadway  
 Proposed Gravel Course  
 Proposed Subbase of Gravel

PROFILE  
 Proposed Roadway  
 Proposed Gravel Course  
 Proposed Subbase of Gravel

CURVE  
 Δ 20° 20' LT  
 D 6"  
 R 954.9  
 T 188.4  
 L 372.2  
 L 18.4



B.M. #26 SPIKE IN ROOT OF 20' ELM 24 FT. OF STA. 197+67 ELEV. 577.78  
 B.M. #27 18' ELM 26 FT. OF STA. 200+40 ELEV. 570.47

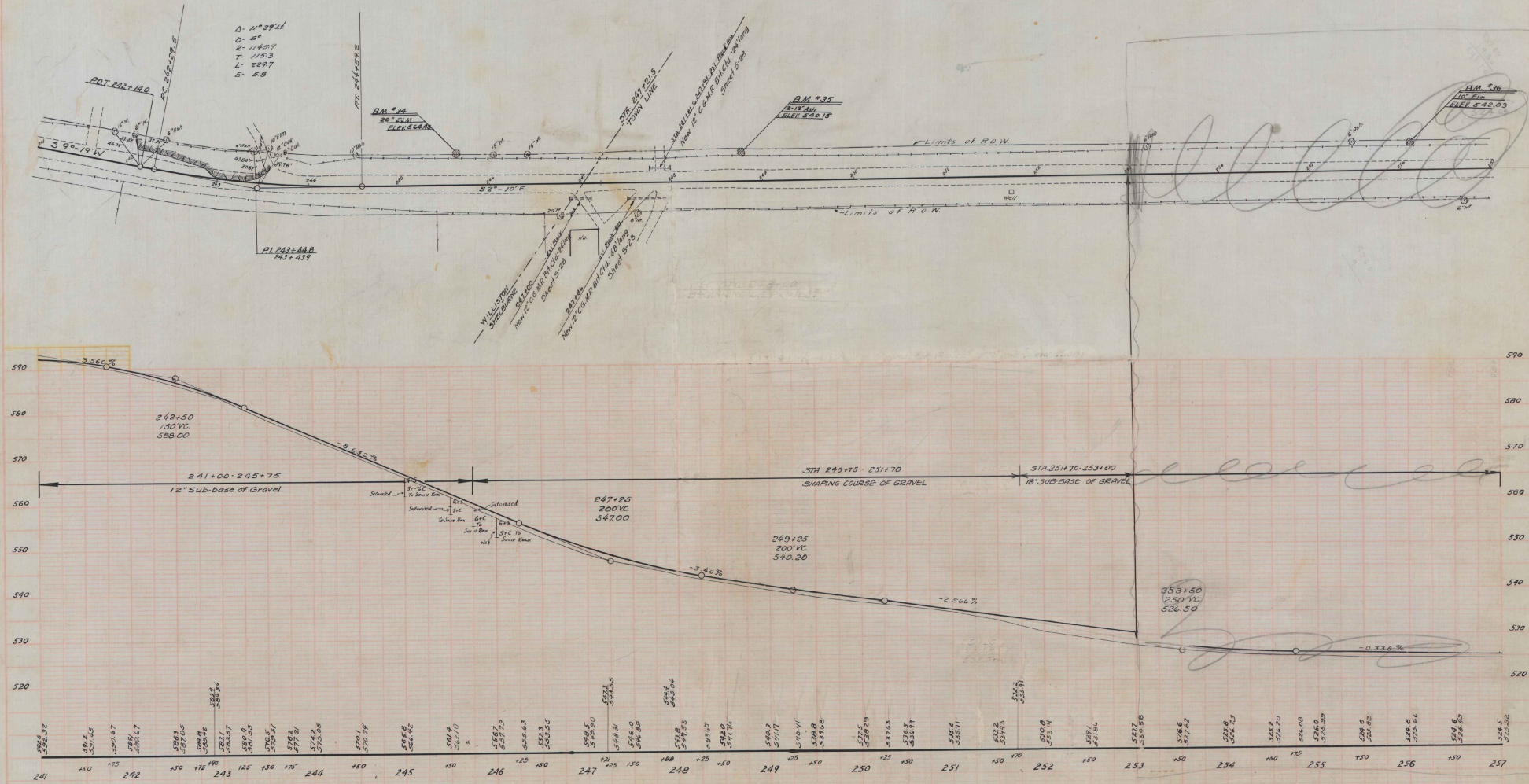
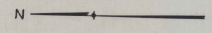


B.M. #28 PAINT SPOT ON LEDGE 29' LT. OF STA 214+47 ELEV. 572.30  
 B.M. #29 SPIKE IN ROOT OF 15' ELM 27 LT. OF STA 210+21 ELEV. 546.45  
 B.M. #30 20' ELM 34 FT. OF STA 224+28 ELEV. 520.27



WOOD  
BRIDLEY  
BICELOW

11,000  
2.11



Δ: 17°29'14"  
D: 24'  
R: 1145.9  
T: 115.3  
L: 229.7  
E: 5.8

DM #34  
20' ELN  
ELEV 546.82

STA 247+25  
TOWN LINE

DM #35  
20' TWN  
ELEV 540.12

DM #36  
10' TWN  
ELEV 542.03

PI 242+44.8  
243+43.9

WILLOUGHBY  
STATE BRIDGE  
New P.C. to M.B. 241+00  
Slope 10:50

BRIDGE  
New P.C. to M.B. 241+00  
Slope 10:50

Limits of R.O.W.

Limits of H.O.W.

241+00 - 245+75  
12" Sub-base of Gravel

SHAPE COURSE OF GRAVEL

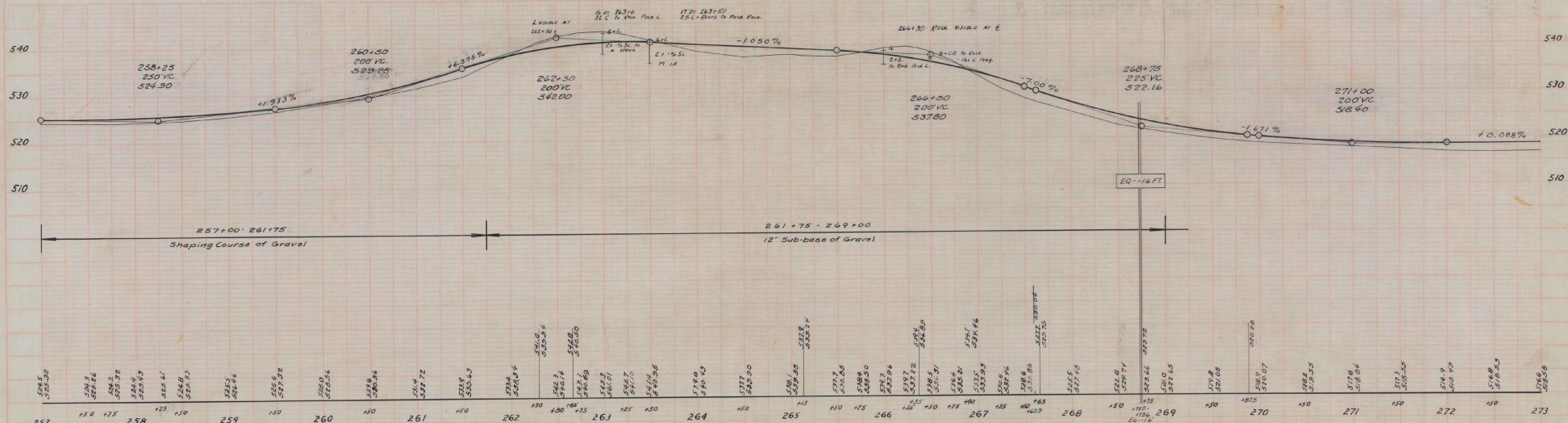
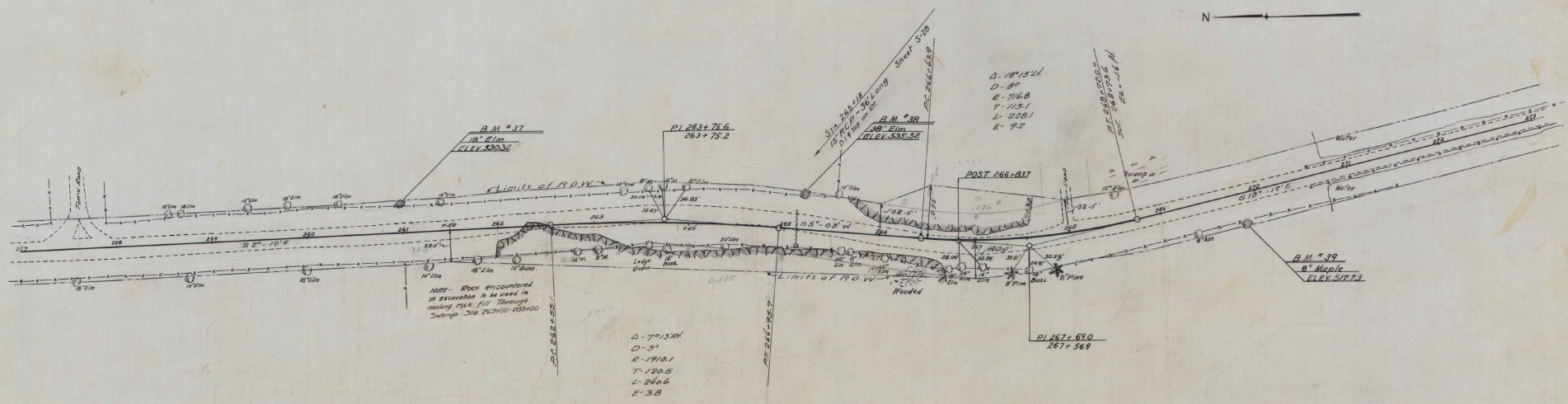
18" SUB-BASE OF GRAVEL

B.M. #34 Spike in Root 20' Elm 29' L. Sta 245+63 Elev 544.43  
B.M. #35 " " " 12' Twin Aak 26' L. Sta 248+75 Elev 540.15

SERIES N.R. 5140  
SHEET 28 OF 30

BUCKLEY  
 BIGELOW  
 Traced  
 11/1900  
 7/1/19

FED. ROAD STATE N.R. 5 FISCAL SHEET TOTAL  
 DIST. NO. 140 PROJ. NO. YEAR NO. SHEETS  
 9 190 1933 27 98



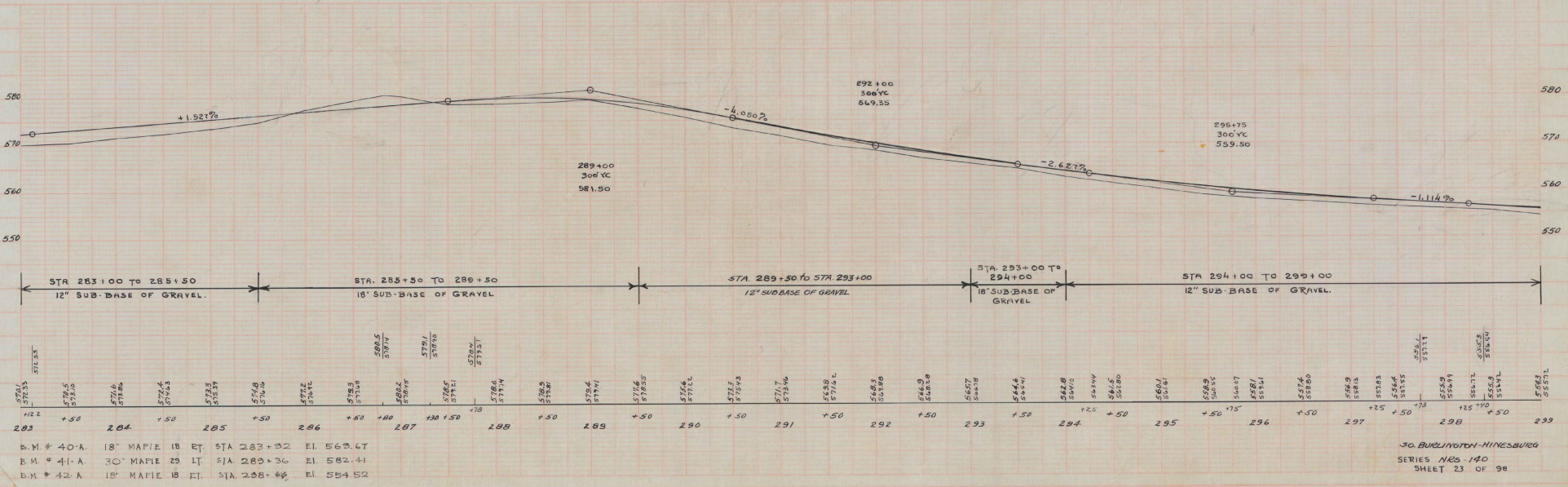
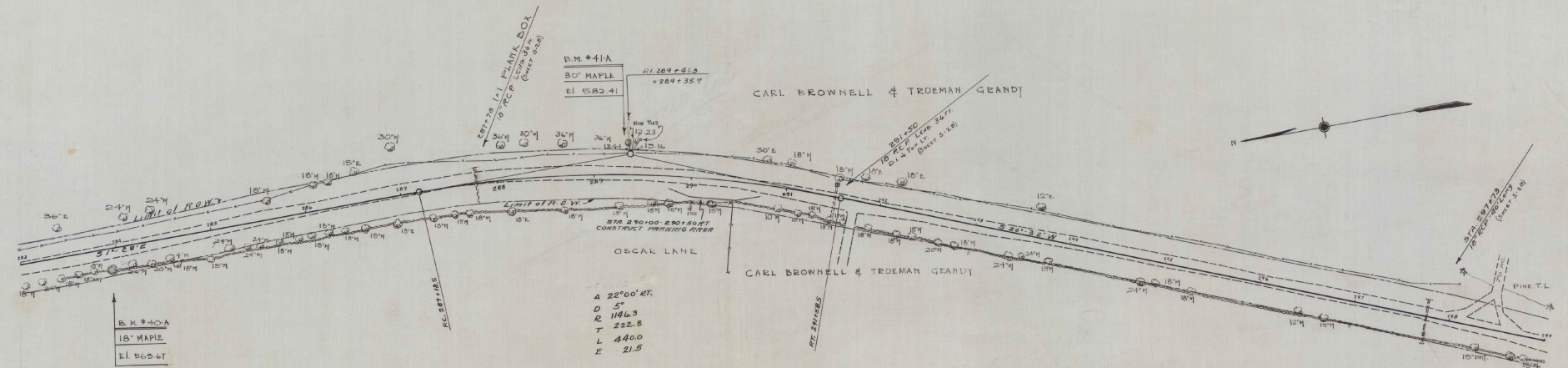
257 BM # 37 spike in root of 18" elm 24 Ft. lt. of Sta. 260-98 Elev. 530.32  
 258 BM # 38 spike in root of 18" elm 35 Ft. lt. of Sta. 265-18 Elev. 535.53  
 261 BM # 39 spike in root of 6" maple 28 Ft. lt. of Sta. 269-32 Elev. 517.73

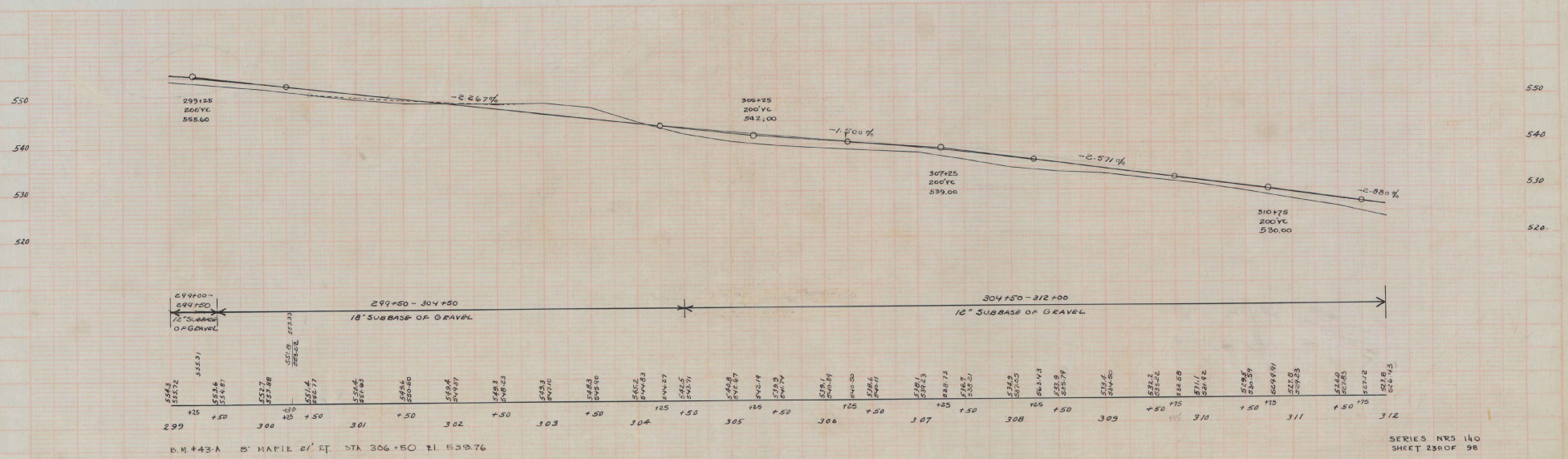
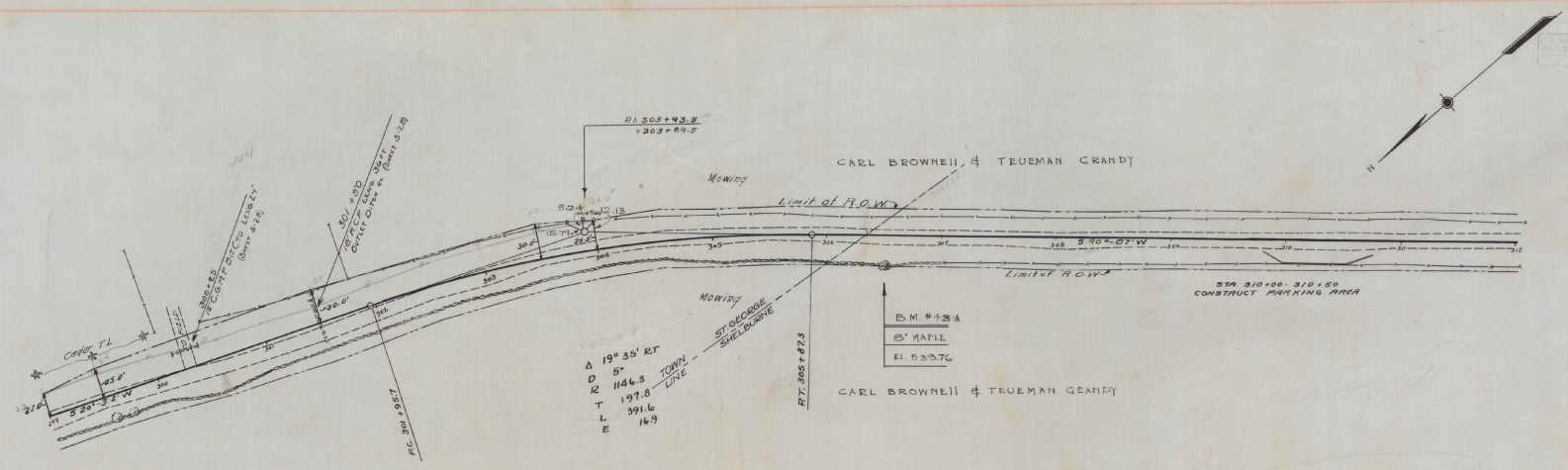
SERIES N.R. 140  
 SHEET 21 of 98





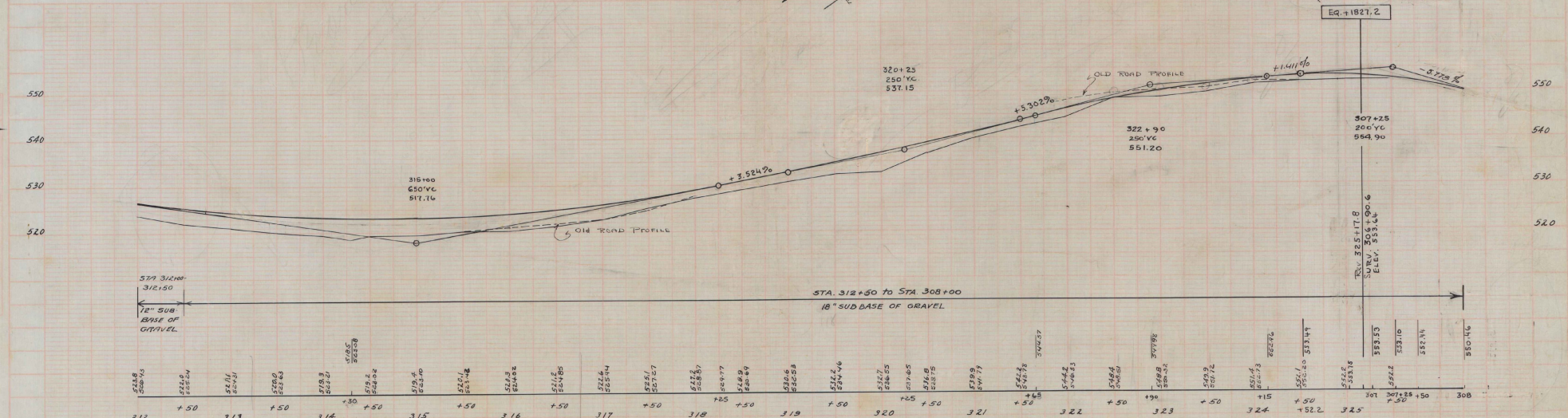
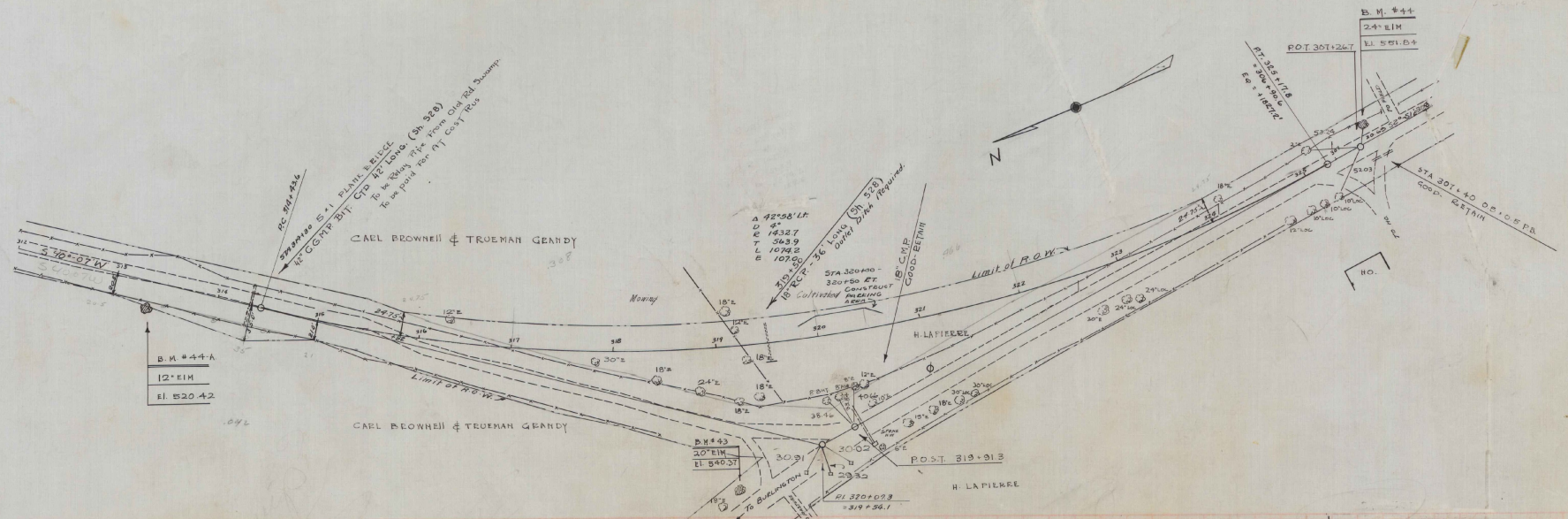






SUBDIVISION PLAN  
 1/11/34

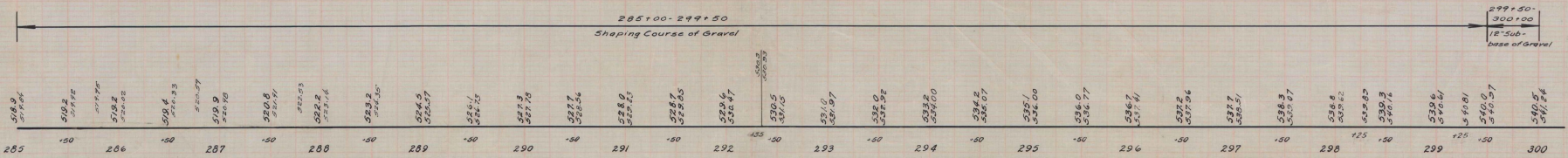
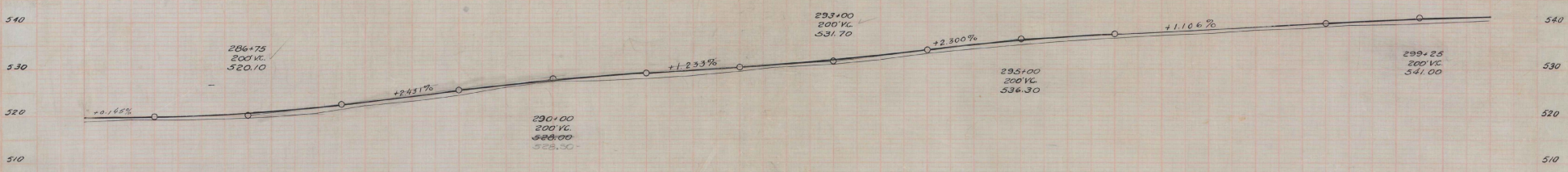
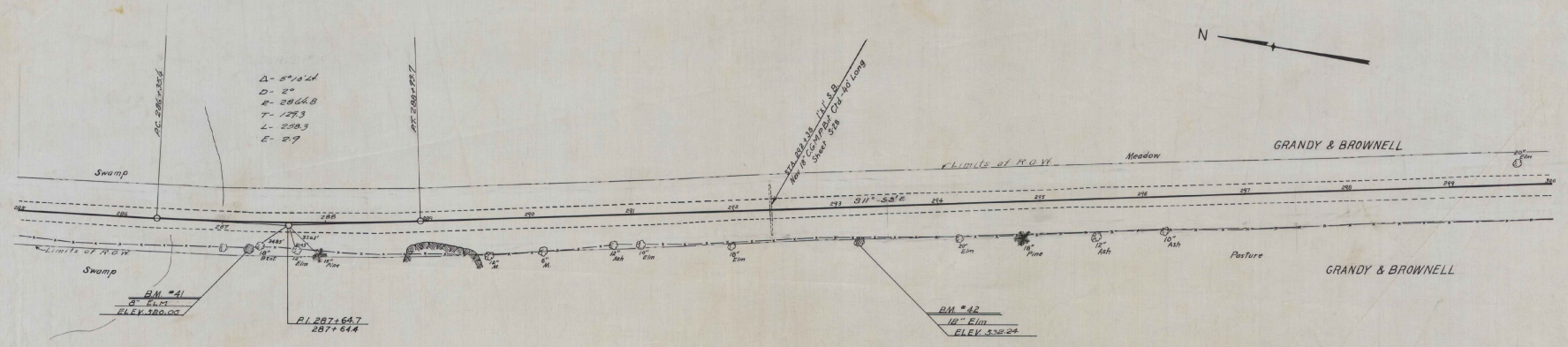
COMPARISON CURVE  
 4/11/34



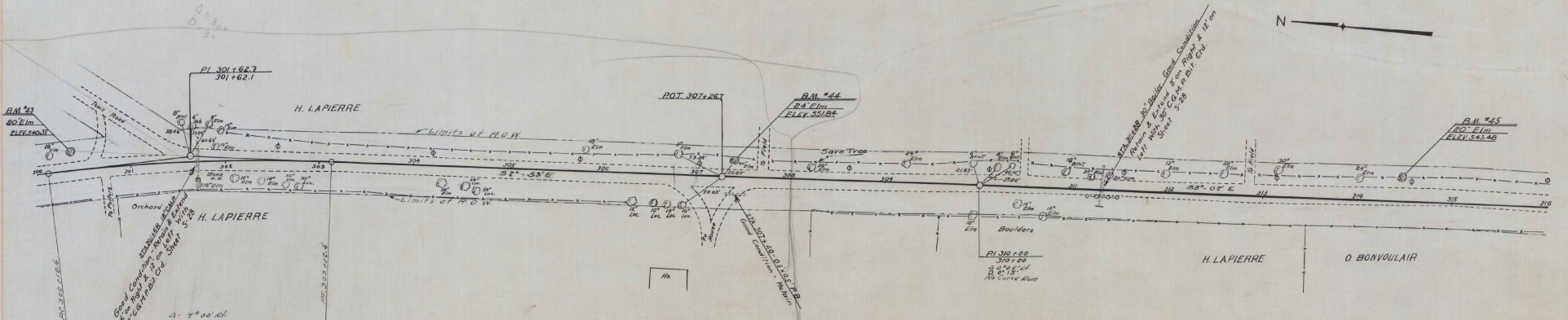
D.M. #44-A 12" EIM 22' FT STA. 313+31 EL. 520.42  
 B.M. #43-5 PIECE IN ROOT 20" ELM 17' LF STA. 307+38 EL. 540.47  
 B.M. #44-5 PIECE IN ROOT 24" ELM 17' FT STA. 307+35 EL. 520.44

WOOD SUGLEY  
 BICELOW  
 TRACED

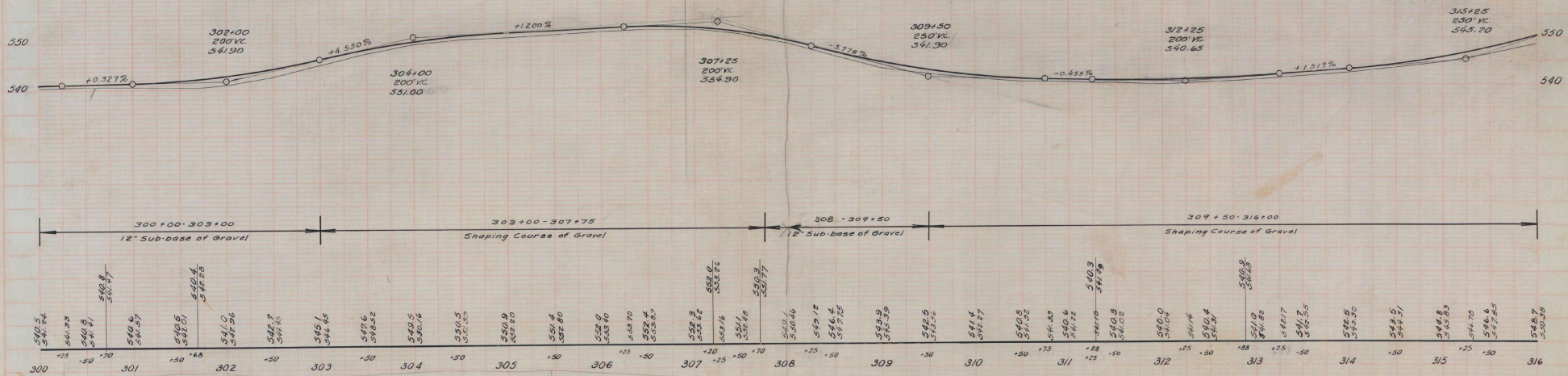
WOOD 7-11-33



BM # 41 spike in root of 8" elm 25 ft. ht. of sta 287+25. Elev. 520.00  
 BM # 42 spike in root of 12" elm 31 ft. ht. of sta 293+20. Elev. 532.24



A = 4° 00' 00"  
 D = 300  
 R = 140.1  
 T = 150.8  
 L = 300.0  
 E = 57



Vermont Agency of  
Transportation  
Phase III- Interstate  
#090303-01



INITIALS

Hanger 3568<sup>ONE</sup>

1934

So. Burlington-Williston-  
N.R.S. 140 Shelburne

1934