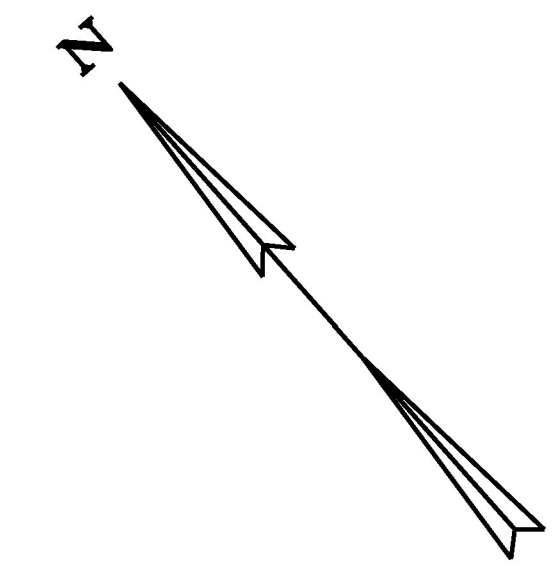
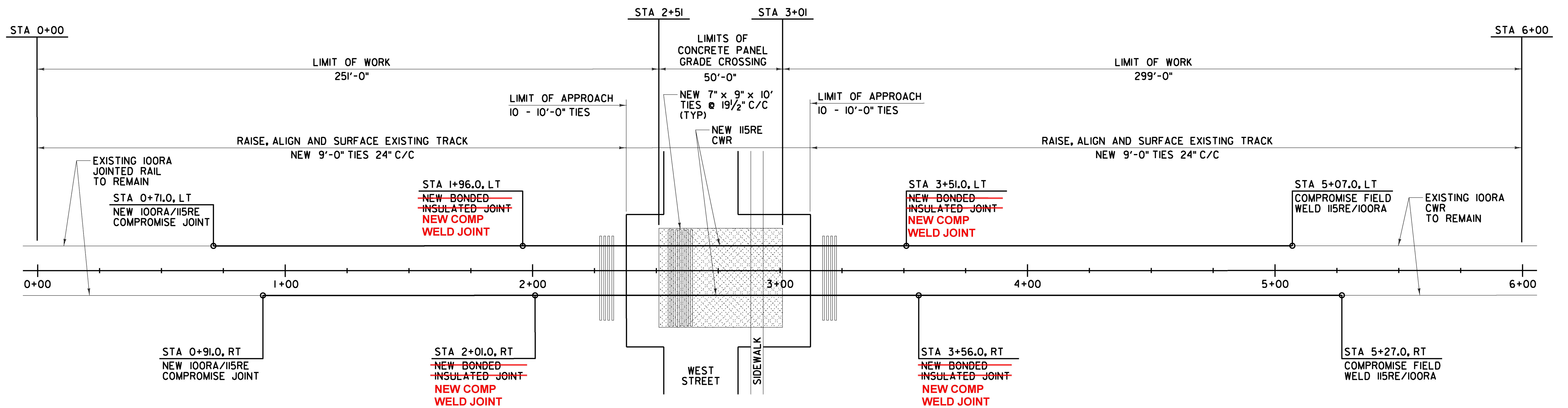


Robert Hensel, Director of Signals & Communications of Genesee and Wyoming Railroads (GWRR) an affiliate of New England Central Railroad (NECR), notified Resident Engineer Josh Hulett on June 16, 2017 that a "...PMD3 was used at the West Street Extension crossing and did not require insulated joints for this style of circuit." Ron Bocash, VP Signal & Train Control, GWRR, confirmed this in an email dated June 20, 2017 to Resident Engineer Hulett and Alan Clark of FWW that, "...insulated joints are not required at MP 6.45 West Street. Circuits are motion sensor only." Rick Boucher of GWRR noted (via email) on July 28, 2017 that, "...they (GWRR) preferred that these (joints) were Comp weld vs. Comp Bar.



REFERENCE CHANGE ORDER #3



**SCHEMATIC PLAN VIEW
AARDOT 247-723Y
MP 6.45
HIGHWAY CROSSING**

 LIMITS OF CONCRETE GRADE CROSSING PANELS

NOT TO SCALE	
PROJECT NAME: ESSEX JUNCTION	
PROJECT NUMBER: NH 2956(2)	
FILE NAME: z15v026rrschp.dgn	PLOT DATE: 4/19/2016
PROJECT LEADER: D.GOZALKOWSKI	DRAWN BY: C. KAHLBAUGH
DESIGNED BY: M. MOKEY	CHECKED BY: D. EMERICH
AARDOT 247-723Y RAILROAD PLAN VIEW	SHEET 165 OF 171

