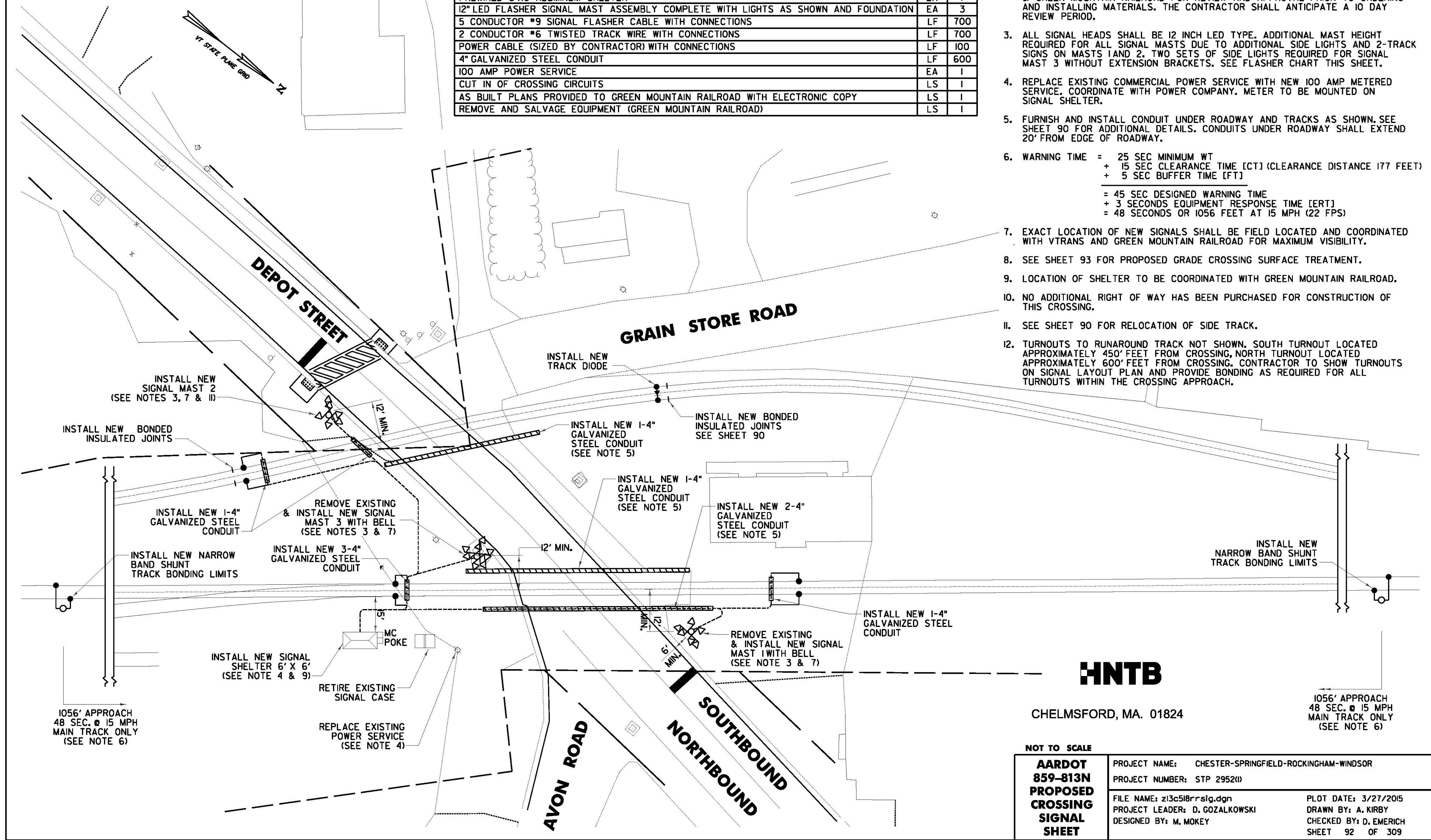


FLASHER CHART SIDELIGHTS REQUIRED IN ADDITION TO STANDARD BACK TO BACK SET		
MAST	ADDITIONAL LIGHT SETS	FOCUSED TOWARD
#1	1	AVON ROAD
#2	1	GRAIN STORE ROAD
#3	2	GRAIN STORE RD. & STATION PARKING LOT

ITEM 900.645 SPECIAL PROVISION (RAIL-HIGHWAY CROSSING ACTIVE WARNING SYSTEM) (AARDOT 859-813N)		
DESCRIPTION	UNIT	QTY
DETAIL SIGNAL DESIGN NEW FLASHER CROSSING IN 6'X6' HOUSE COMPLETE	LS	1
FURNISH & INSTALL NEW FLASHER CROSSING COMPLETE	EA	1
PREWIRED 6'X6' ALUMINUM SHELTER	EA	1
12" LED FLASHER SIGNAL MAST ASSEMBLY COMPLETE WITH LIGHTS AS SHOWN AND FOUNDATION	EA	3
5 CONDUCTOR #9 SIGNAL FLASHER CABLE WITH CONNECTIONS	LF	700
2 CONDUCTOR #6 TWISTED TRACK WIRE WITH CONNECTIONS	LF	700
POWER CABLE (SIZED BY CONTRACTOR) WITH CONNECTIONS	LF	100
4" GALVANIZED STEEL CONDUIT	LF	600
100 AMP POWER SERVICE	EA	1
CUT IN OF CROSSING CIRCUITS	LS	1
AS BUILT PLANS PROVIDED TO GREEN MOUNTAIN RAILROAD WITH ELECTRONIC COPY	LS	1
REMOVE AND SALVAGE EQUIPMENT (GREEN MOUNTAIN RAILROAD)	LS	1

NOTES:

1. THE WORK UNDER ITEM 900.645 SPECIAL PROVISION (RAIL-HIGHWAY CROSSING ACTIVE SYSTEM) (AARDOT 859-813N) SHALL BE PERFORMED IN ACCORDANCE WITH THE SPECIAL PROVISIONS AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND A.R.E.M.A. RECOMMENDED PRACTICES.
2. CROSSING SHALL BE ACTIVATED BY MOTION SENSOR AND STYLE "C" TRACK CIRCUITS. THE CONTRACTOR SHALL PROVIDE A DETAILED DESIGN TO VTRANS & GREEN MOUNTAIN RAILROAD FOR REVIEW AND APPROVAL PRIOR TO ORDERING AND INSTALLING MATERIALS. THE CONTRACTOR SHALL ANTICIPATE A 10 DAY REVIEW PERIOD.
3. ALL SIGNAL HEADS SHALL BE 12 INCH LED TYPE. ADDITIONAL MAST HEIGHT REQUIRED FOR ALL SIGNAL MASTS DUE TO ADDITIONAL SIDE LIGHTS AND 2-TRACK SIGNS ON MASTS 1 AND 2. TWO SETS OF SIDE LIGHTS REQUIRED FOR SIGNAL MAST 3 WITHOUT EXTENSION BRACKETS. SEE FLASHER CHART THIS SHEET.
4. REPLACE EXISTING COMMERCIAL POWER SERVICE WITH NEW 100 AMP METERED SERVICE. COORDINATE WITH POWER COMPANY. METER TO BE MOUNTED ON SIGNAL SHELTER.
5. FURNISH AND INSTALL CONDUIT UNDER ROADWAY AND TRACKS AS SHOWN. SEE SHEET 90 FOR ADDITIONAL DETAILS. CONDUITS UNDER ROADWAY SHALL EXTEND 20' FROM EDGE OF ROADWAY.
6. WARNING TIME = 25 SEC MINIMUM WT
+ 15 SEC CLEARANCE TIME [CT] (CLEARANCE DISTANCE 177 FEET)
+ 5 SEC BUFFER TIME [FT]
= 45 SEC DESIGNED WARNING TIME
+ 3 SECONDS EQUIPMENT RESPONSE TIME [ERT]
= 48 SECONDS OR 1056 FEET AT 15 MPH (22 FPS)
7. EXACT LOCATION OF NEW SIGNALS SHALL BE FIELD LOCATED AND COORDINATED WITH VTRANS AND GREEN MOUNTAIN RAILROAD FOR MAXIMUM VISIBILITY.
8. SEE SHEET 93 FOR PROPOSED GRADE CROSSING SURFACE TREATMENT.
9. LOCATION OF SHELTER TO BE COORDINATED WITH GREEN MOUNTAIN RAILROAD.
10. NO ADDITIONAL RIGHT OF WAY HAS BEEN PURCHASED FOR CONSTRUCTION OF THIS CROSSING.
11. SEE SHEET 90 FOR RELOCATION OF SIDE TRACK.
12. TURNOUTS TO RUNAROUND TRACK NOT SHOWN. SOUTH TURNOUT LOCATED APPROXIMATELY 450' FEET FROM CROSSING, NORTH TURNOUT LOCATED APPROXIMATELY 600' FEET FROM CROSSING. CONTRACTOR TO SHOW TURNOUTS ON SIGNAL LAYOUT PLAN AND PROVIDE BONDING AS REQUIRED FOR ALL TURNOUTS WITHIN THE CROSSING APPROACH.



HNTB

CHELMSFORD, MA. 01824

NOT TO SCALE

**AARDOT
859-813N
PROPOSED
CROSSING
SIGNAL
SHEET**

PROJECT NAME: CHESTER-SPRINGFIELD-ROCKINGHAM-WINDSOR	PLOT DATE: 3/27/2015
PROJECT NUMBER: STP 29520I	DRAWN BY: A. KIRBY
FILE NAME: z13c518rslg.dgn	CHECKED BY: D. EMERICH
PROJECT LEADER: D. GOZALKOWSKI	SHEET 92 OF 309
DESIGNED BY: M. MOKEY	