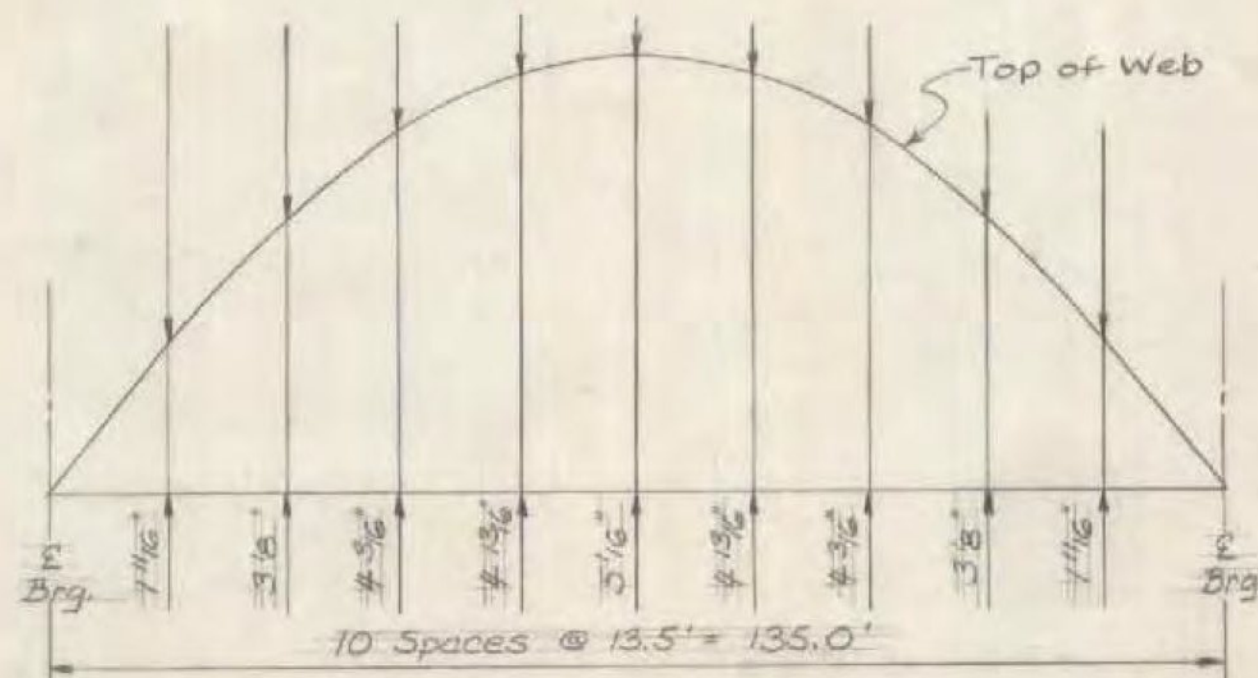
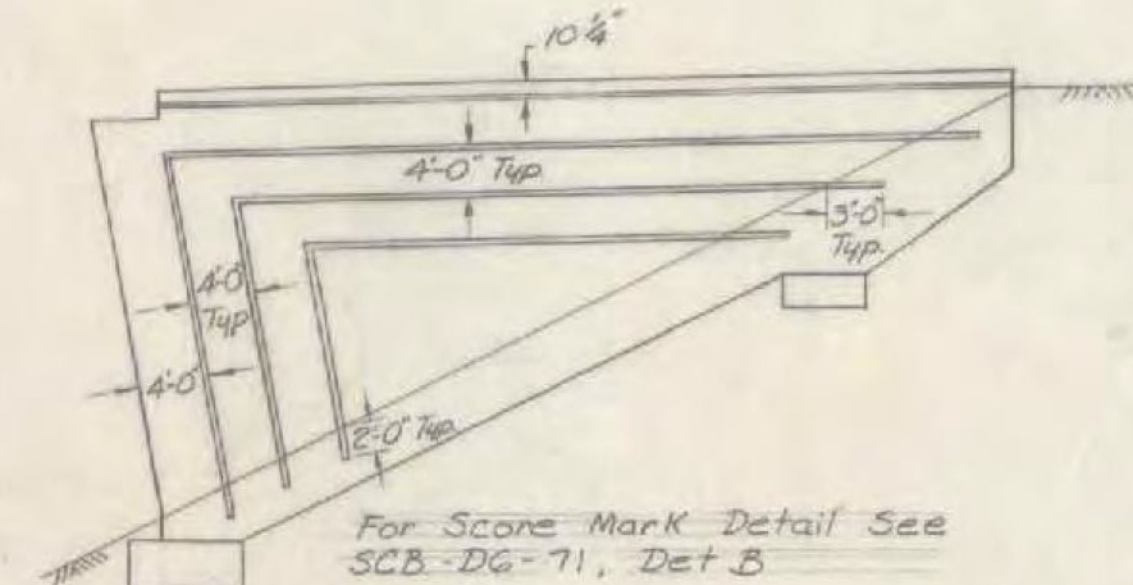


Due to weight of Girder, Bracing, Slab, Curb, Railing and Pavement  
**DEAD LOAD DEFLECTION DIAGRAM**  
 SCALE: Horizontal 1"=20'; Vertical 1"=2"

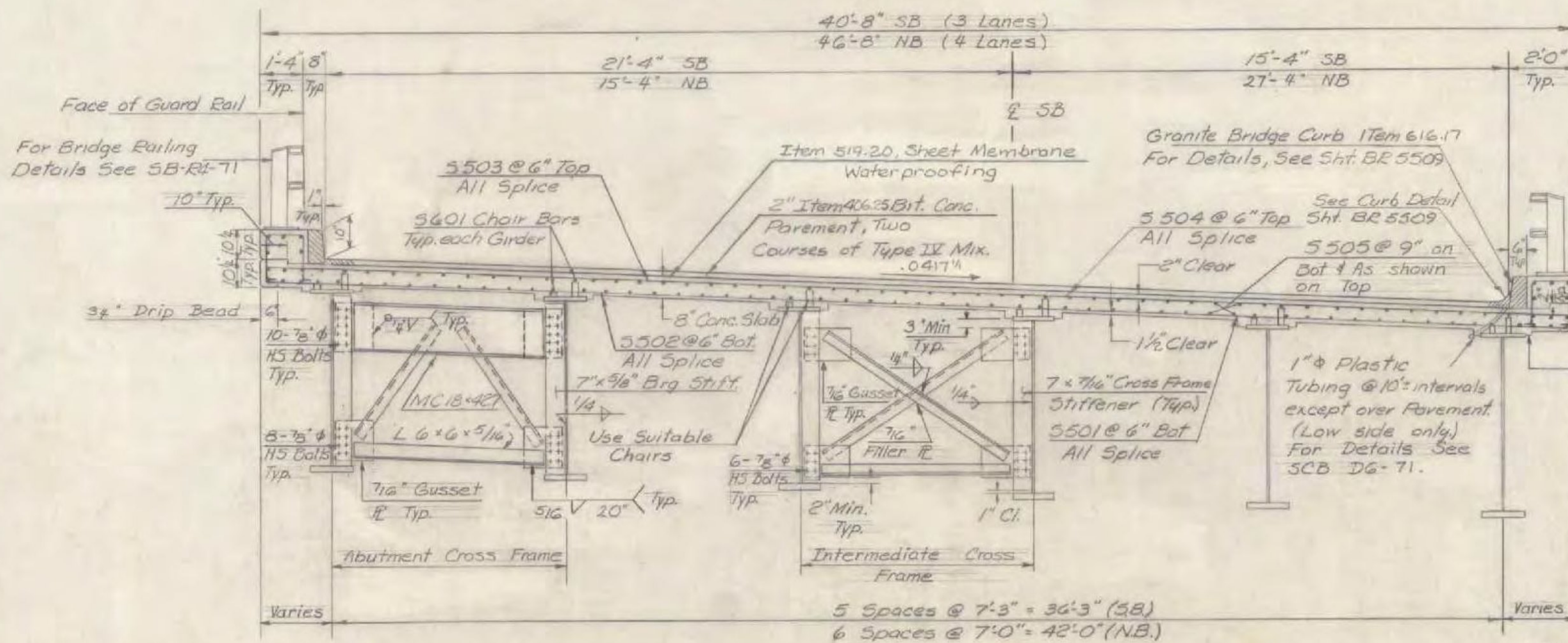


**CAMBER DIAGRAM (TYP)**  
 SCALE: Horizontal 1"=20'; Vertical 1"=2"



**SCORE MARK DETAIL 3**  
 Not to Scale  
 Typical all Wingwalls

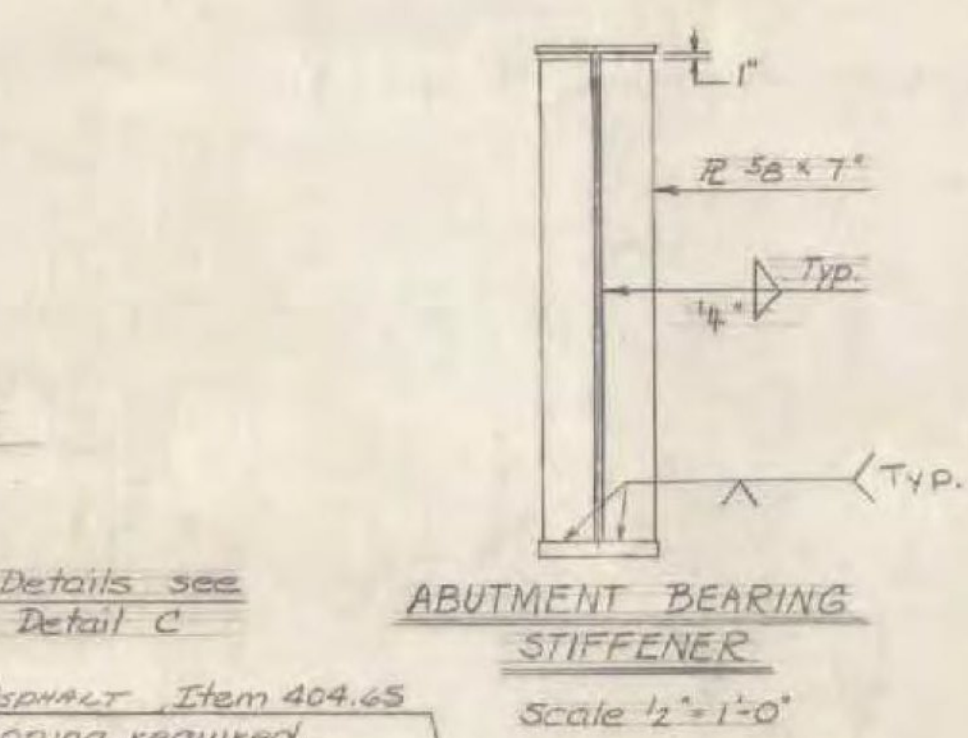
(RE-ADVERTISED)  
 BENNINGTON - MT TABOR  
 BF BPNT (16)  
 PROJECT BRIDGE 16N&S  
 SHEET 17 OF 23  
 FOR INFORMATION ONLY



**TYPICAL SECTION**  
 Scale 3/8" = 1'-0" SB  
 Not to Scale NB

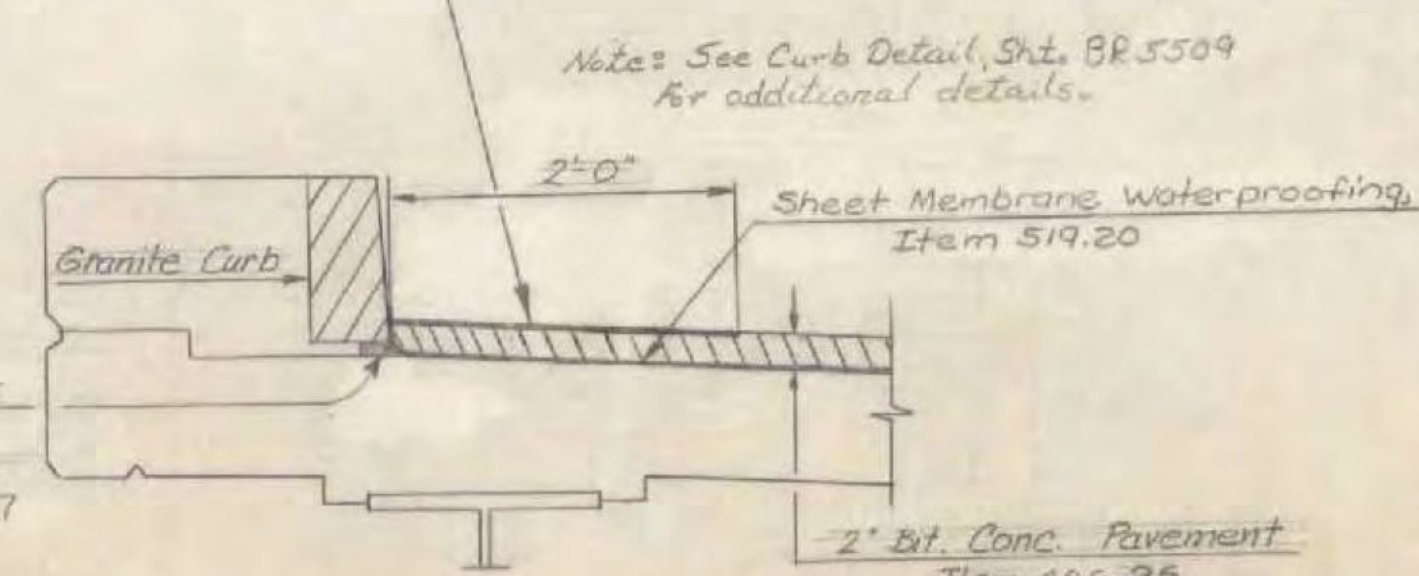
REVISION: REPLACED TAR EMULSION ON 2' STRIP NEXT TO THE CURB WITH EMULSIFIED ASPHALT.  
 Rev. By: GSR  
 Chd. By: DCW  
 DATE 8-4-78

Revised Stage III curb details.  
 Rev. By: EOB  
 Chd. By: GSR  
 Date 9-73



**NOTES:**

- Girders shall be cambered as shown in the Camber Diagram.
- All structural Steel shall be ASTM A36.
- Bearing stiffeners shall be vertical in the erected position; intermediate cross-frames shall be perpendicular to the flanges.
- Ends of girders shall be vertical in the erected position.
- Clip 1"x1" on inside corners of all stiffeners.
- All cross frame angles are L 4"x4"x3/8 except as noted. All angles shall be welded to their connections with 1/8 inches of 1/4 inch fillet weld at each end except as noted.
- Field bend transverse bars at curb line, as necessary, to maintain minimum vertical clearance in curb area.



STATE OF VERMONT  
 DEPARTMENT OF HIGHWAYS

PROJECT BENNINGTON  
 TOWN OF BENNINGTON  
 ROAD No. ART US 7 STA. 570+23

ARTERIAL US 7 OVER RAMP L  
 TYPICAL SECTION, GIRDER ELEVATION,  
 SCORE MARK DET.

SCALE AS SHOWN

SECTION SUPERVISOR: E.W. Balkom Betz + E. WAIBEL / OUELLETTE  
 DRAWN BY: / GARCIA CHECKED BY: E. WAIBEL

PROJECT No. DP-BF 019-1(12)  
 SHEET 209 OF 480 BR-5506