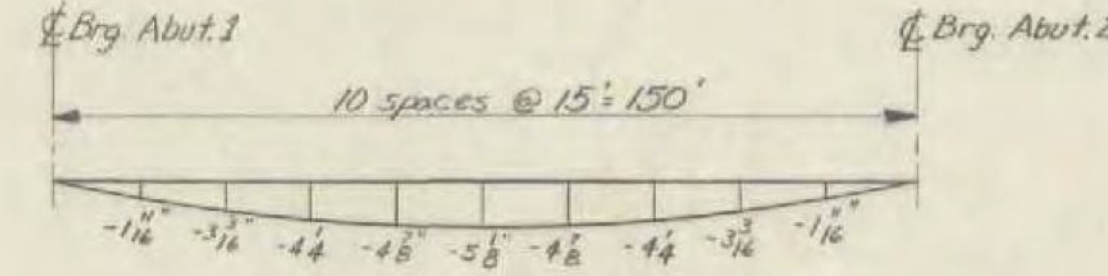


Max. Live Load Deflection at mid-span = -2"
The girders shall be cambered for Dead Load Deflection plus 1/8" Permanent Camber at mid-span, plus 3/8" vertical curve camber.

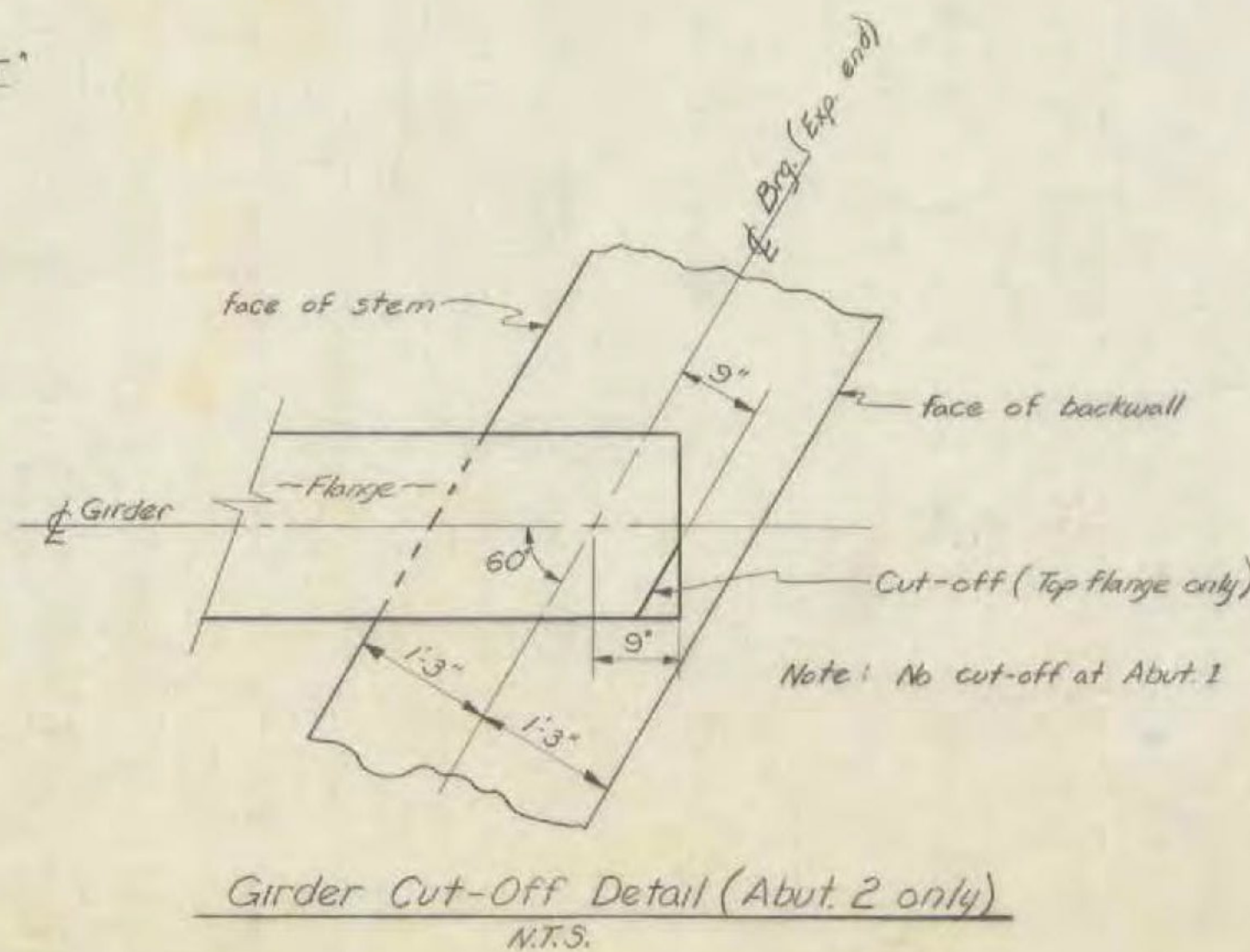
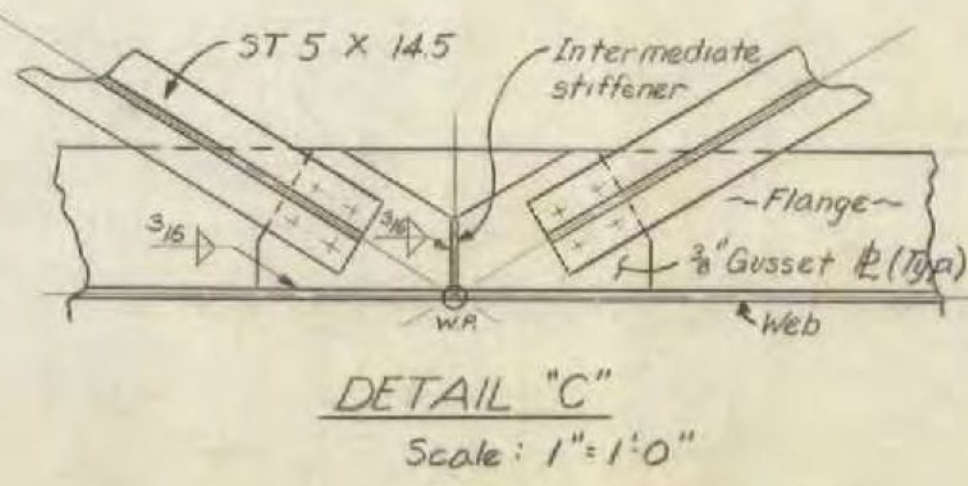
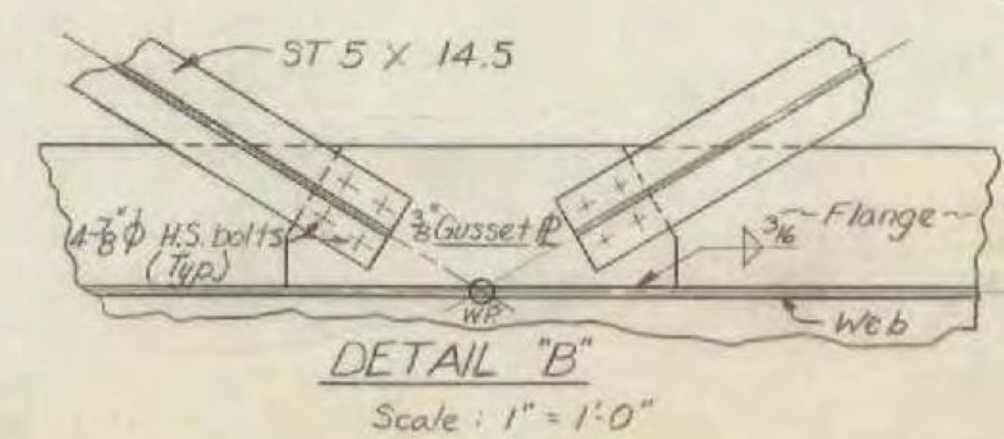
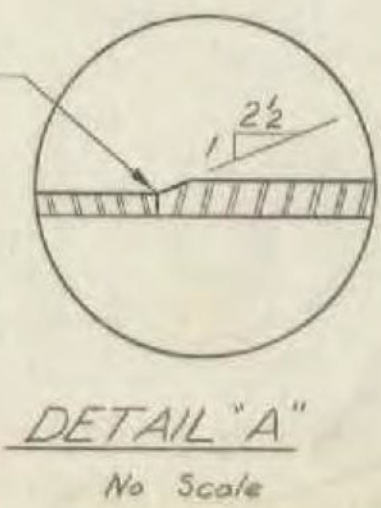
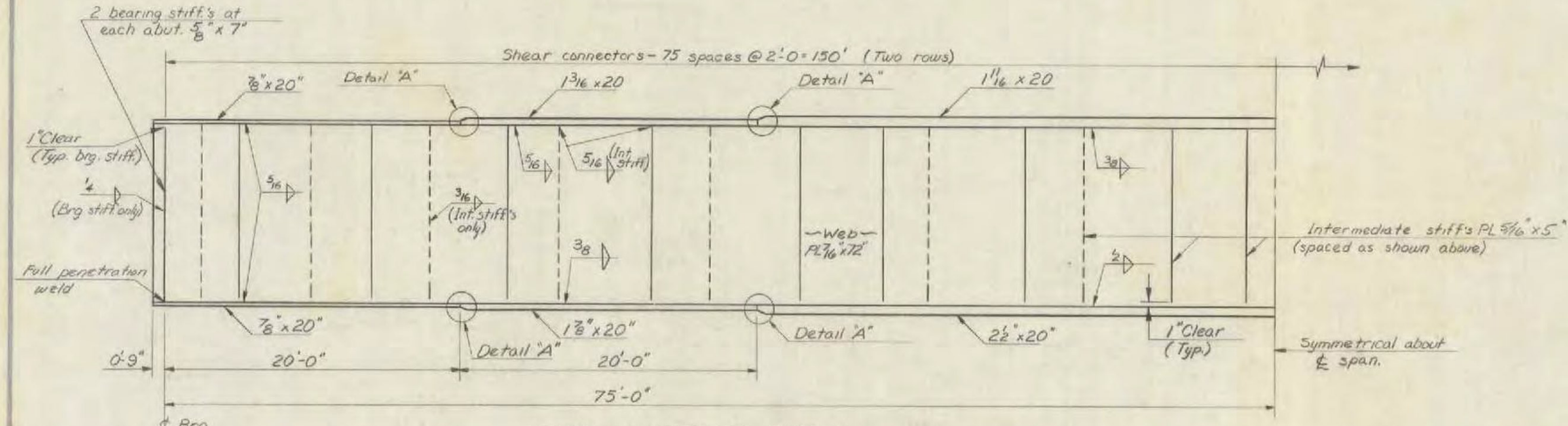
Trough Notes:
1) See Br. 5007 for additional trough details.
2) The shaded area indicates the area where the slope of the trough is warped. All the rest of the trough is sloped 45° from the vertical.
3) * The slope of the trough between the limits shown shall be warped to fit. In the exact plane of section A-A it is a 1 on 1 slope.



GENERAL NOTES

- For GENERAL NOTES see Br. 5002
- All Structural Steel shall conform to ASTM designation A-36, unless otherwise noted. All field connections shall be made with 7/8" φ ASTM A-325 bolts in 5/16" φ holes.
- Bearing stiffeners may be ground to bear on the bottom flange in lieu of welding if desired.

(RE - ADVERTISED) BENNINGTON - MTTABOR
BF BPNT (16)
PROJECT BRIDGE 11
SHEET 12 OF 23
FOR INFORMATION ONLY



Stage I Construction

STATE OF VERMONT
DEPARTMENT OF HIGHWAYS

TOWN OF Bennington

ROUTE No. US 7 Conn. Sta. 23+25
US 7 Conn. Over Roaring Branch

Framing Plan & Girder Details

SCALE As Noted

SECTION SUPERVISOR A. BUSHANAN

DRAWN BY Armstrong, CHECKED BY Couch

PROJECT No. DE-U-F 212-1(7)

Br. 5005 SHEET 22 OF 156