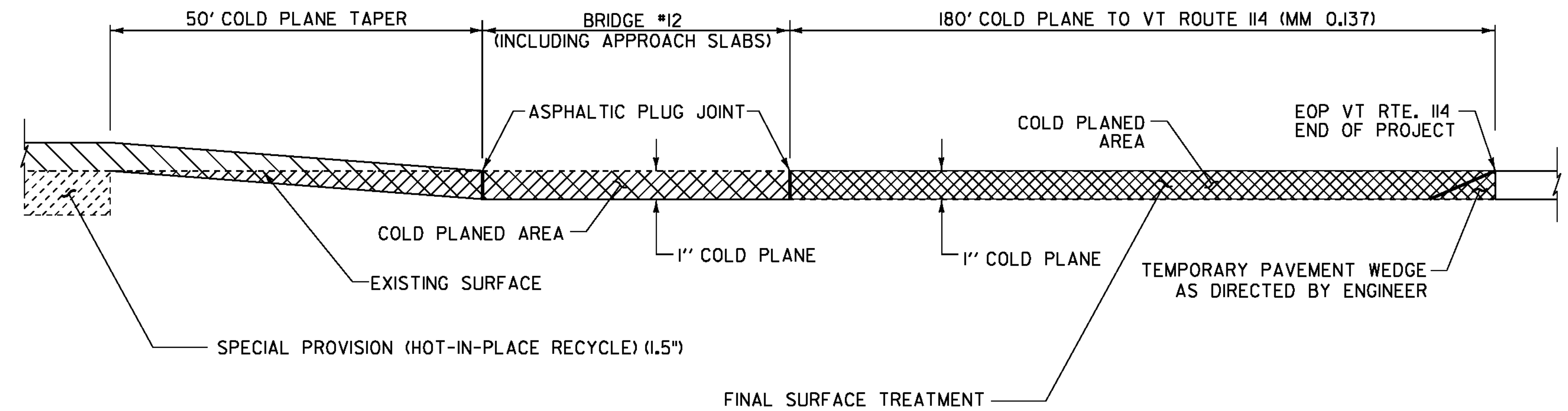
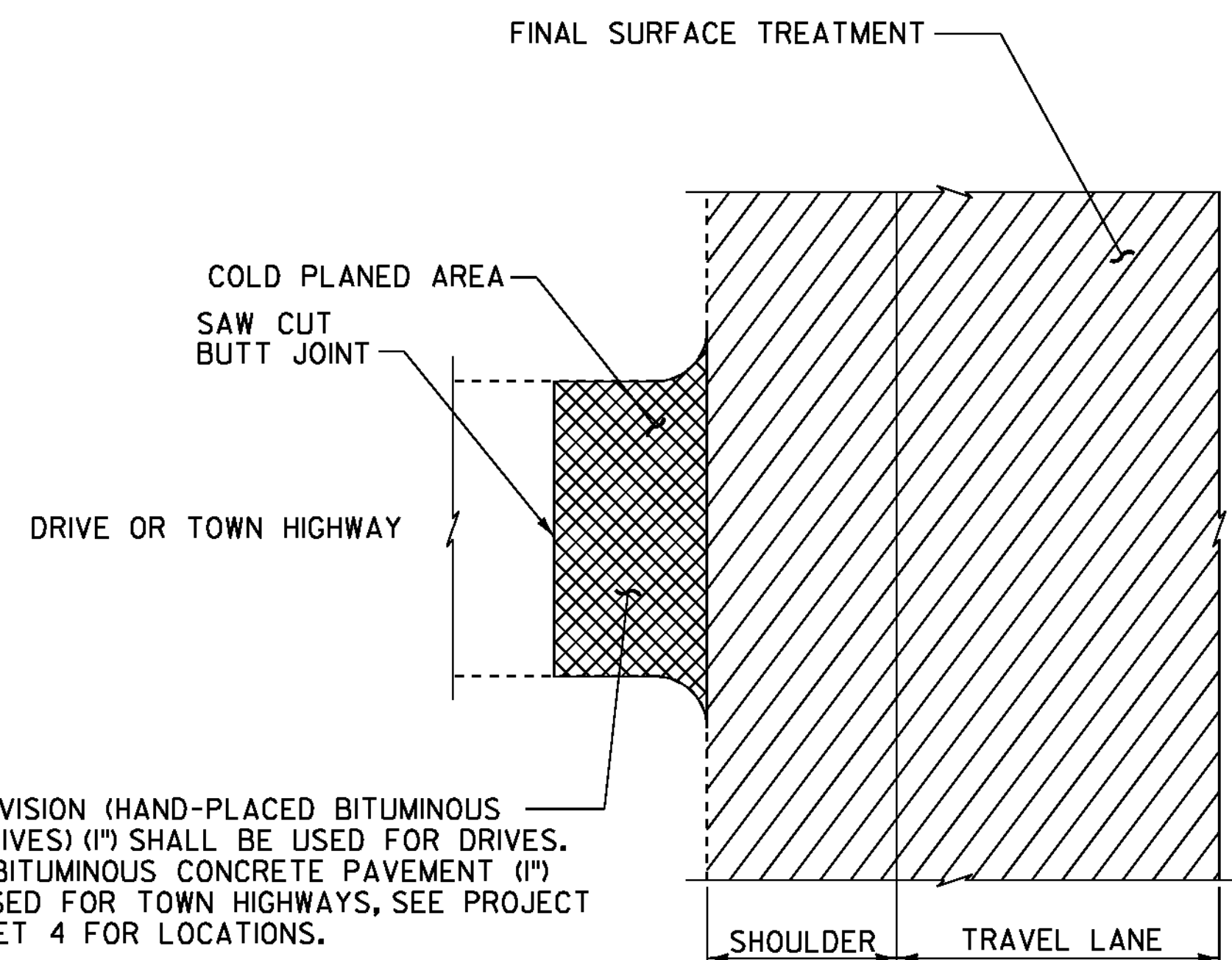


COLD PLANE DETAIL AT BEGINING OF PROJECT
REFER TO BRIDGE COLD PLANE DETAIL FOR END OF PROJECT.

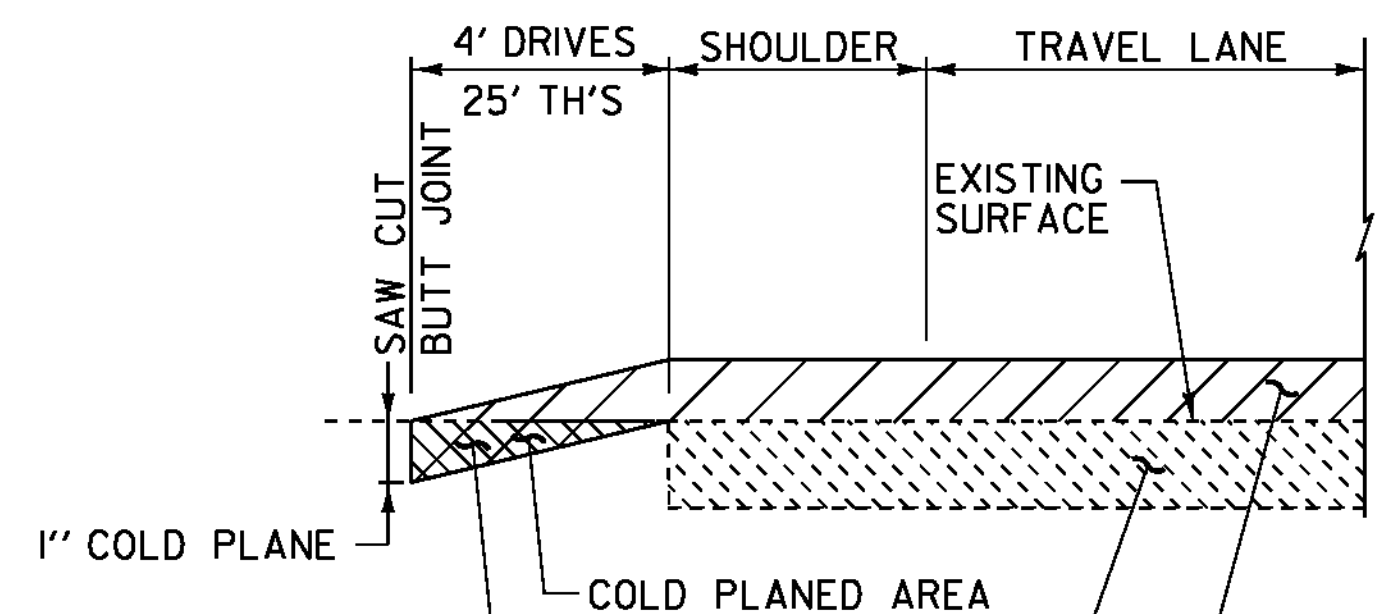


BRIDGE COLD PLANE DETAIL

- NOTES:
1. REFER TO ASPHALTIC PLUG JOINT DETAIL SHEET. ALL NEW JOINTS TO BE PAID FOR UNDER ITEM 516.10, BRIDGE EXPANSION JOINT, ASPHALTIC PLUG.
 2. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID DAMAGING DRAINAGE STRUCTURES AND EXPANSION JOINTS. ANY DAMAGE CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT THE EXPENSE OF THE CONTRACTOR.
 3. THE CONTRACTOR SHALL TAKE PRECAUTIONS TO AVOID THE ACCUMULATION OF DEBRIS IN THE DRAINAGE STRUCTURES LOCATED AT CURB LINE AND IN THE EXPANSION JOINTS. THE CONTRACTOR SHALL EXAMINE THESE BRIDGE FEATURES ON A DAILY BASIS TO ENSURE THAT DEBRIS HAS NOT ACCUMULATED. ANY DEBRIS WHICH IS PRESENT SHALL BE REMOVED BY THE CONTRACTOR AT NO COST TO THE STATE.
 4. THE CONTRACTOR SHALL USE CAUTION WHEN COLD PLANING AND PAVING OPERATIONS OCCUR ON BRIDGE DECKS. SHOULD ANY DAMAGE OCCUR TO THE DECK OR MEMBRANE AS A RESULT OF THESE OPERATIONS THE ENGINEER SHALL CONTACT THE VTRANS CONSTRUCTION STRUCTURES ENGINEER TO PROVIDE AN ASSESSMENT OF THE DAMAGE AND RECOMMEND ANY NECESSARY REPAIRS. THE CONSTRUCTION STRUCTURES ENGINEER WILL ALSO DETERMINE IF THE DAMAGE WAS AVOIDABLE. IF THE CONTRACTOR IS DETERMINED BY THE RESIDENT ENGINEER TO BE AT FAULT FOR THE DAMAGE, THE RECOMMENDED REPAIRS SHALL BE COMPLETED BY THE CONTRACTOR AT NO COST TO THE STATE.



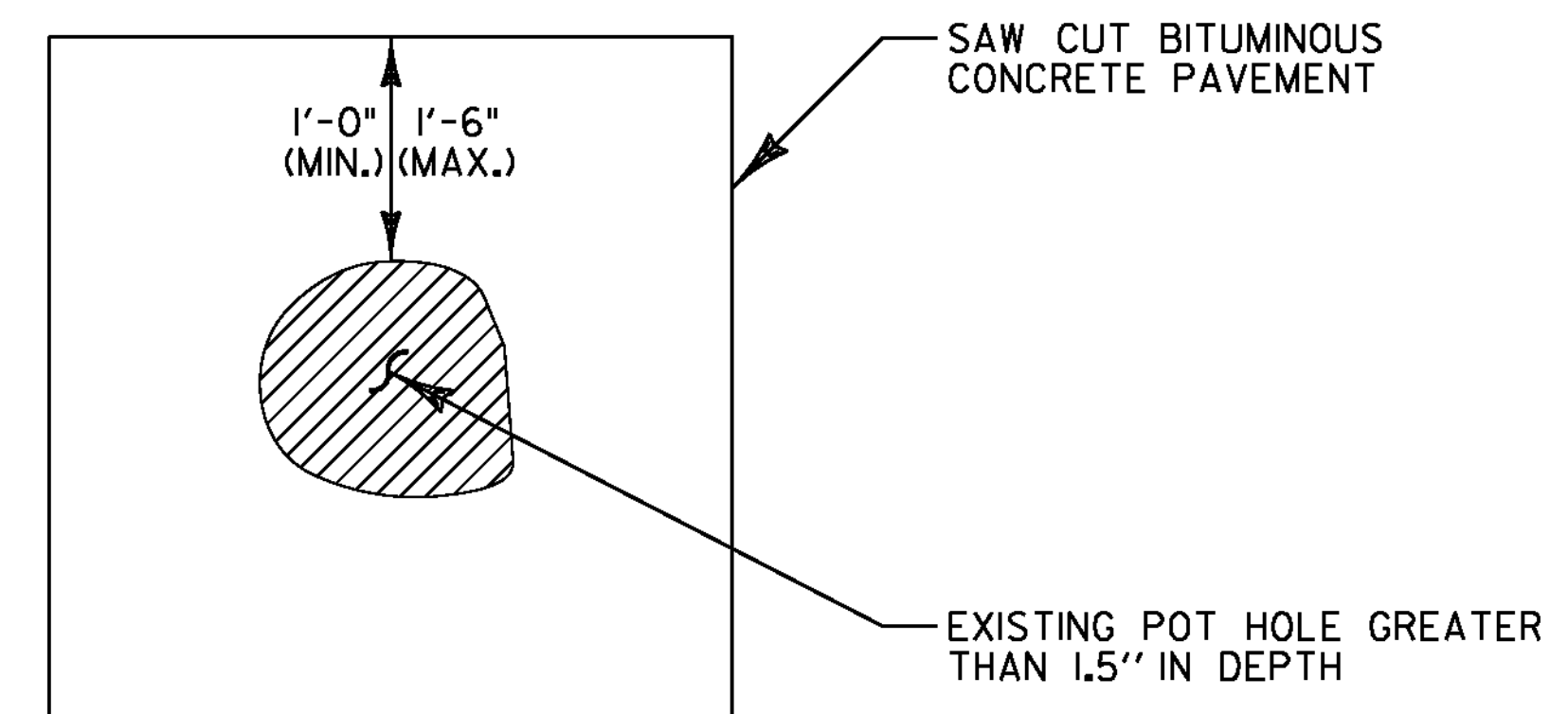
SPECIAL PROVISION (HAND-PLACED BITUMINOUS MATERIAL, DRIVES) (1") SHALL BE USED FOR DRIVES. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (1") SHALL BE USED FOR TOWN HIGHWAYS, SEE PROJECT LAYOUT SHEET 4 FOR LOCATIONS.



SPECIAL PROVISION (HAND-PLACED BITUMINOUS MATERIAL, DRIVES) (1") SHALL BE USED FOR DRIVES. SUPERPAVE BITUMINOUS CONCRETE PAVEMENT (1") SHALL BE USED FOR TOWN HIGHWAYS, SEE PROJECT LAYOUT SHEET 4 FOR LOCATIONS.

SPECIAL PROVISION (HOT-IN-PLACE RECYCLE) (1.5")

COLD PLANE DETAILS AT DRIVES & TOWN HIGHWAYS



TYPICAL - POT HOLE REPAIR

- NOTES:
- 1) TO BE USED IF HOT-IN-PLACE RECYCLE CANT REPAIR EXISTING POTHOLES, AS DIRECTED BY THE ENGINEER.
 - 2) EMULSIFIED ASPHALT SHALL BE APPLIED AT ALL PATCH INTERFACES AT A RATE OF 0.25 - 0.50 GAL/SY. EMULSIFIED ASPHALT SHALL MEET THE REQUIREMENTS OF SECTION 404 AND WILL BE CONSIDERED INCIDENTAL TO THE PATCHING MATERIAL.

NOT TO SCALE

PROJECT NAME:	MORGAN - BRIGHTON
PROJECT NUMBER:	STP SURF(4I)
FILE NAME: pl3c552_Morgan-Brighton.dgn	PLOT DATE: 14-MAY-2014
PROJECT LEADER: J. HARRINGTON	DRAWN BY: N. PAPPAS
DESIGNED BY: N. PAPPAS	CHECKED BY: PAYT MGMT
PROJECT COLD PLANE DETAILS	SHEET 5 OF 16