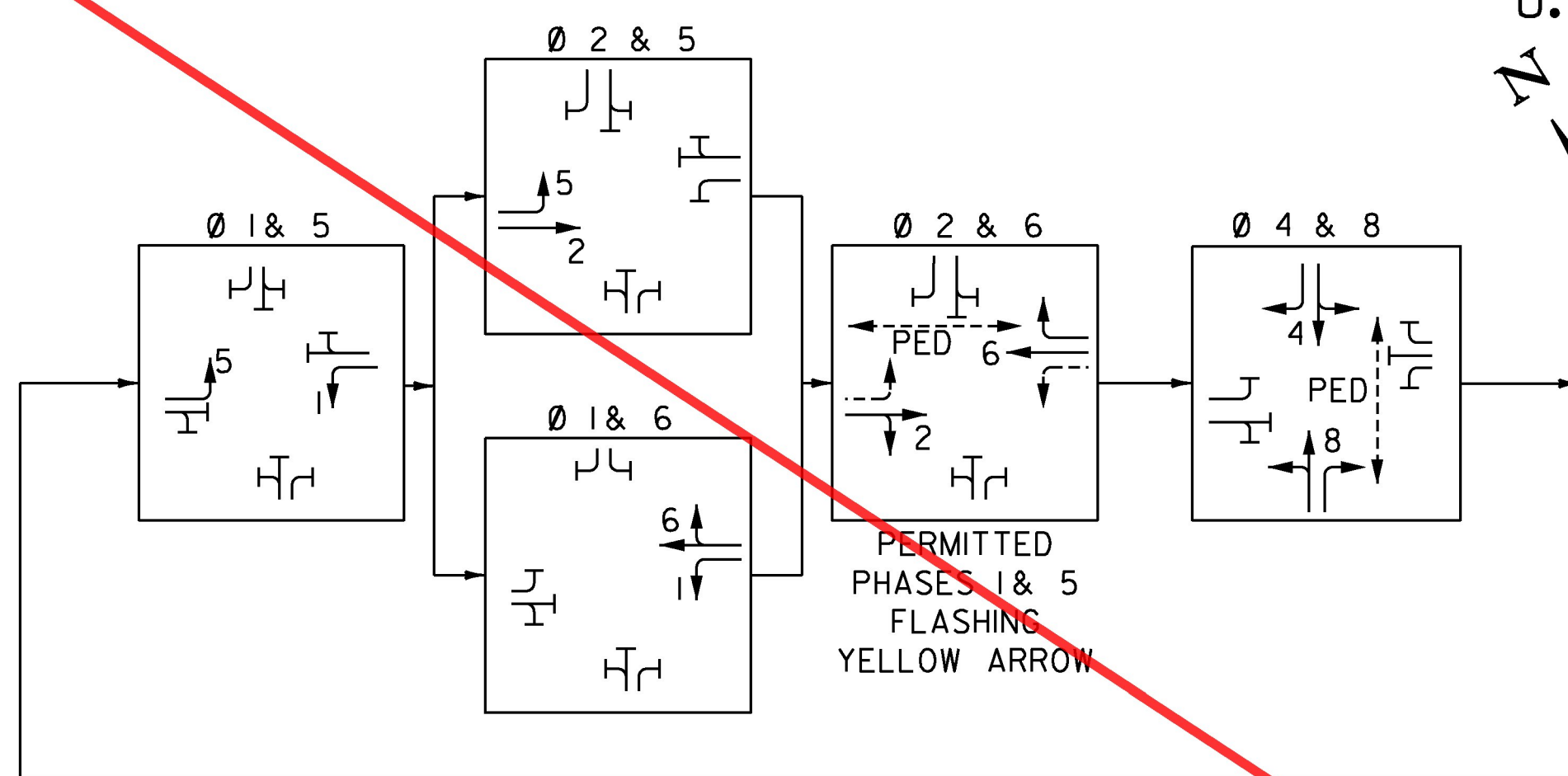


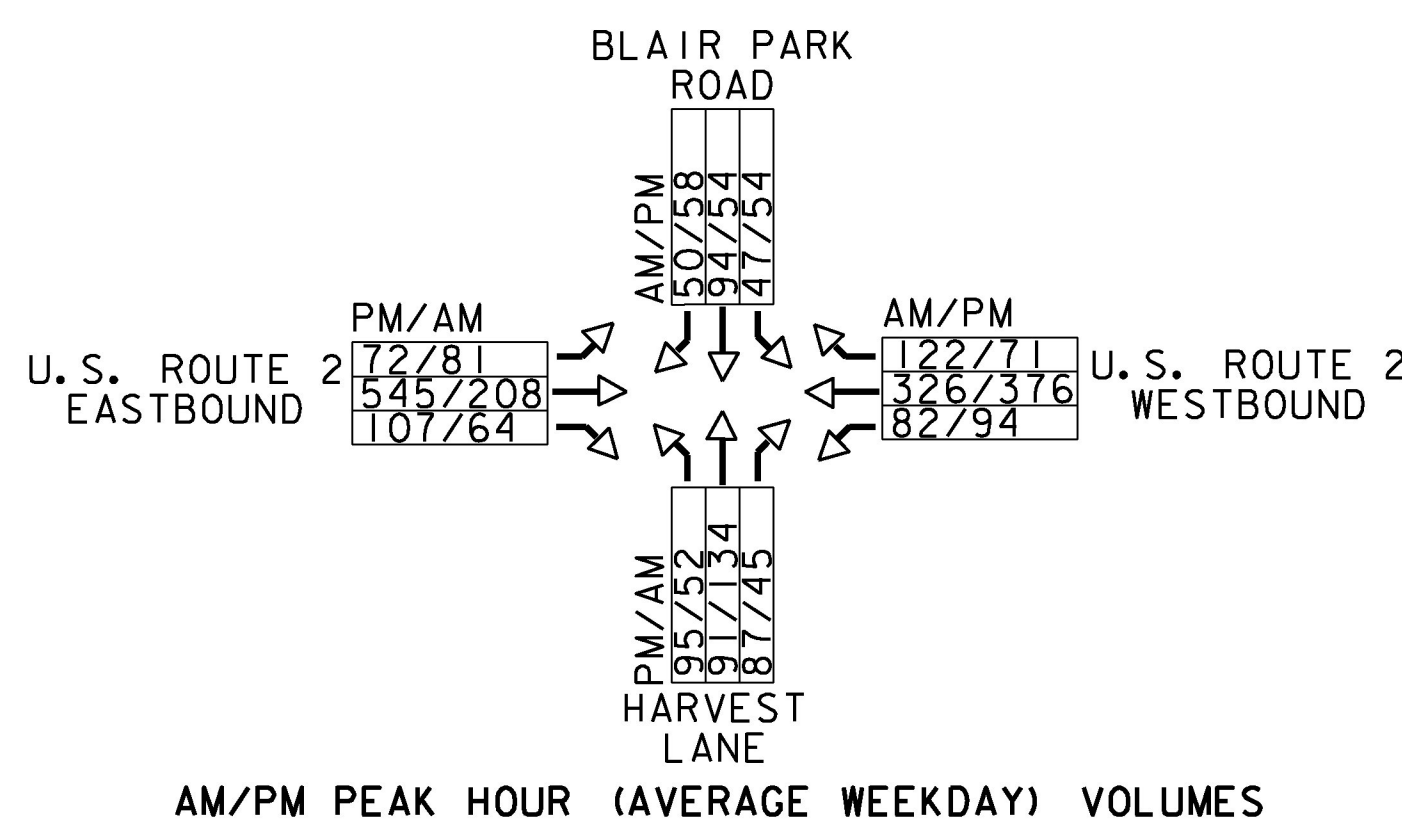
PHASING DIAGRAM



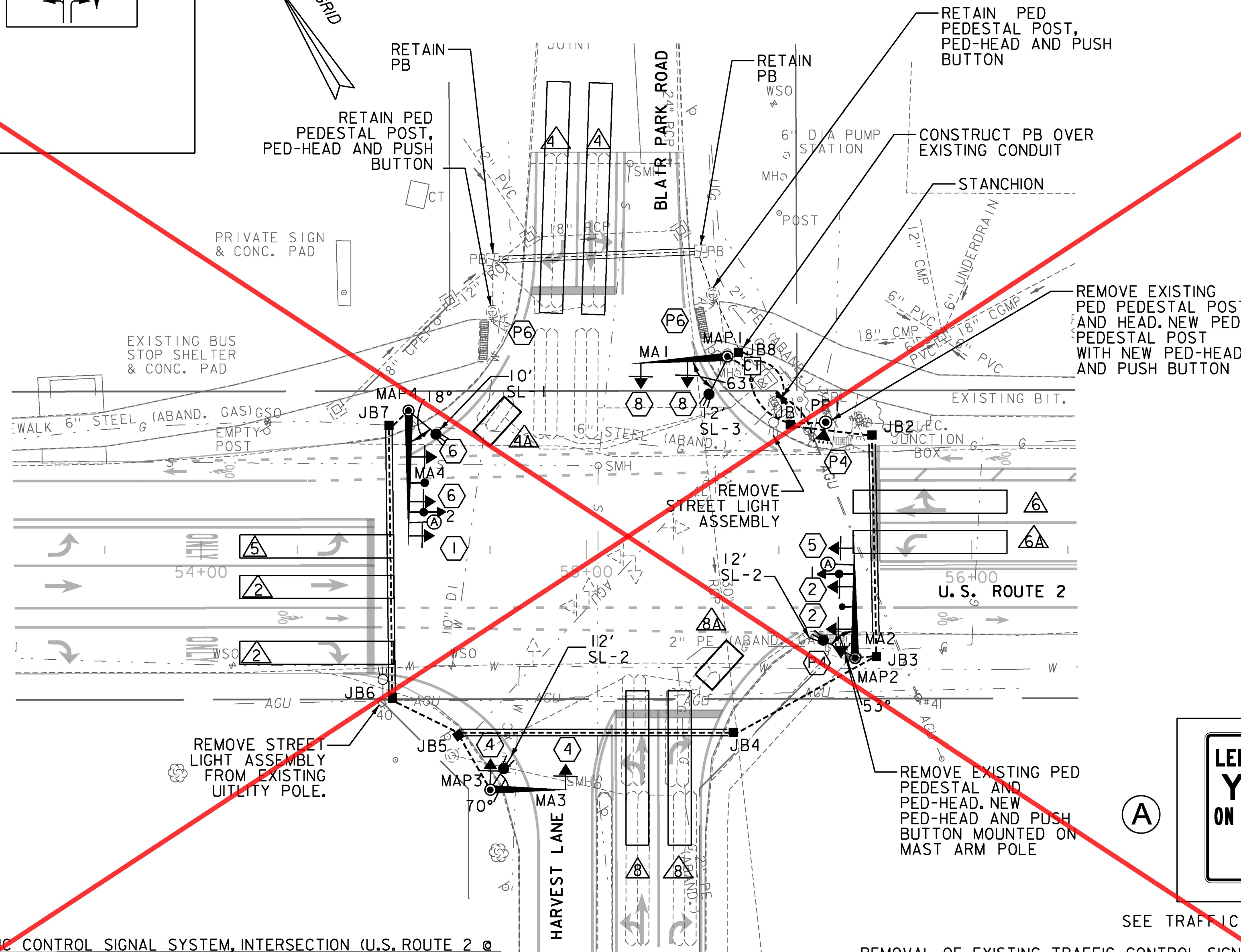
U. S. ROUTE 2 / BLAIR PARK ROAD / HARVEST LANE INTERSECTION

NOTES:

1. TRAFFIC ITEMS LISTED APPROXIMATE LOCATIONS AND MAY BE MODIFIED BY THE ENGINEER IN THE FIELD TO ENSURE CONFORMANCE WITH THE MUTCD AND VTRANS STANDARD DRAWINGS.
2. UNLESS OTHERWISE NOTED, THE REMOVAL OF ALL EXISTING TRAFFIC SIGNAL EQUIPMENT THAT SHALL NOT BE REINSTALLED SHALL BECOME THE PROPERTY OF THE CONTRACTOR. WORK REQUIRED TO PERFORM THIS ACTIVITY WILL BE PAID FOR UNDER CONTRACT ITEM 678.45 REMOVAL OF EXISTING TRAFFIC CONTROL SIGNAL SYSTEM.
3. REMOVAL OF EXISTING TRAFFIC SIGNAL CONTROL SYSTEM WILL NOT OCCUR UNTIL THE PROPOSED SYSTEM IS CONSTRUCTED AND OPERATIONAL.
4. FOR A LIST OF MAJOR EQUIPMENT CONDUIT SCHEDULE, AND CONTROLLER DATA, SEE SHEET I13.
5. ALL REMOVED STREET LIGHTING SHALL BE SALVAGED TO THE DISTRICT 5 SIGNAL TECHNICIAN. REMOVAL OF EXISTING STREET LIGHTING SYSTEM WILL BE PAID FOR UNDER ITEM 679.24 REMOVE STREET LIGHT ASSEMBLY.
6. ALL REMOVED STRAIN POLES SHALL BE DISPOSED OF BY THE CONTRACTOR.
7. STOP BAR DETECTION SYSTEM SHALL BE OPERATIONAL PRIOR TO CUTTING EXISTING LOOPS. EXISTING VEHICLE DETECTOR LOOPS SHALL BE CUT AT THE CURB LINE PRIOR TO COLD PLANING AS DIRECTED BY THE ENGINEER. PAYMENT WILL BE CONSIDERED INCIDENTAL TO ITEM 678.45 REMOVAL OF EXISTING TRAFFIC CONTROL SIGNAL SYSTEM.
8. RIGHT OF WAY IS APPROXIMATE.
9. UNDERGROUND UTILITIES ARE APPROXIMATE AND SHALL BE VERIFIED BY THE CONTRACTOR.
10. EXISTING SIGNAL INTERCONNECT CABLE EXISTS BETWEEN THE BROWNELL ROAD INTERSECTION AND THE HARVEST LANE/BLAIR PARK ROAD INTERSECTION. CONTRACTOR SHALL LOCATE INTERCONNECTING CABLE AND PROTECT DURING CONSTRUCTION. ALSO, INTERCONNECTION OF NEW SIGNAL EQUIPMENT BETWEEN THESE INTERSECTIONS SHALL BE REESTABLISHED. THIS WORK IS PAID FOR UNDER ITEM 678.15 TRAFFIC CONTROL SIGNAL SYSTEM, INTERSECTION (U.S. ROUTE 2 @ BLAIR PARK ROAD AND HARVEST LANE).



AM/PM PEAK HOUR (AVERAGE WEEKDAY) VOLUMES



| LEGEND   |     |                                 |
|----------|-----|---------------------------------|
| EXISTING | NEW | DESCRIPTION                     |
|          |     | UTILITY POLE                    |
|          |     | LUMINAIRE                       |
|          |     | LIGHT POLE                      |
|          |     | STRAIN POLE/CANTILEVER POLE     |
|          |     | CONTROLLER CABINET              |
|          |     | PULLBOX/JUNCTION BOX            |
|          |     | PEDESTRIAN SIGNAL HEAD          |
|          |     | SIGNAL HEAD                     |
|          |     | SIGNAL HEAD WITH LOUVERED PLATE |
|          |     | CONDUIT (SIGNAL)                |
|          |     | VEHICLE LOOPS                   |
|          |     | VEHICLE DETECTION AREA          |
|          |     | PEDESTAL POST/LIGHTING POLE     |
|          |     | CANTILEVER                      |
|          |     | DRAINAGE STRUCTURE              |
|          |     | VEHICLE DETECTOR                |
|          |     | SLEEVE                          |
|          |     | PREEMPT OPTICAL DETECTOR        |
|          |     | PREEMPTION STROBE               |
|          |     | PEDESTRIAN PUSHBUTTON ASSEMBLY  |
|          |     | POWER DROP STANCHION            |
|          |     | ELECTRICAL WIRING               |
|          |     | L.E.D REGULATORY SIGN           |

TRAFFIC CONTROL SIGNAL SYSTEM, INTERSECTION (U.S. ROUTE 2 @ BLAIR PARK ROAD AND HARVEST LANE)  
SEE LIST OF MAJOR EQUIPMENT, ON SHEET I13

CONSTRUCT MAST ARM POLES  
(JUNCTION BOX, HEAVY DUTY)  
STA. 54+54 37' LT (MAP4)  
STA. 54+75 61' RT (MAP3)  
STA. 55+37 51' LT (MAP1)  
STA. 55+70 27' RT (MAP2)

CONSTRUCT CONTROLLER CABINET (GROUND MOUNTED)  
STA. 55+43 50' LT

WIRED CONDUIT (2") (SCH 80)  
SEE CONDUIT SCHEDULE, ON SHEET I13

ELECTRICAL CONDUIT (2") (SCH 80)  
SEE CONDUIT SCHEDULE, ON SHEET I13

SPECIAL PROVISION  
(JUNCTION BOX, HEAVY DUTY)  
STA. 54+49 37' LT (JB7)  
STA. 54+49 36' RT (JB6)  
STA. 54+67 46' RT (JB5)  
STA. 55+38 45' RT (JB4)  
STA. 55+42 52' LT (JB8)  
STA. 55+53 33' LT (JB1)  
STA. 55+74 30' LT (JB2)  
STA. 55+74 27' RT (JB3)

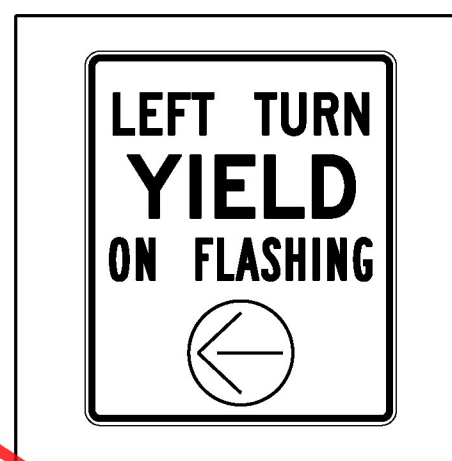
BRACKET ARM  
STA. 54+54 37' LT  
STA. 54+75 61' RT  
STA. 55+37 51' LT  
STA. 55+70 27' RT

REMOVAL OF EXISTING TRAFFIC CONTROL SIGNAL SYSTEM,  
U.S. ROUTE 2 AT BLAIR PARK ROAD AND HARVEST LANE

LUMINAIRE  
STA. 54+54 37' LT (JB7)  
STA. 54+75 61' RT (JB6)  
STA. 55+37 51' LT (JB4)  
STA. 55+70 27' RT (JB3)

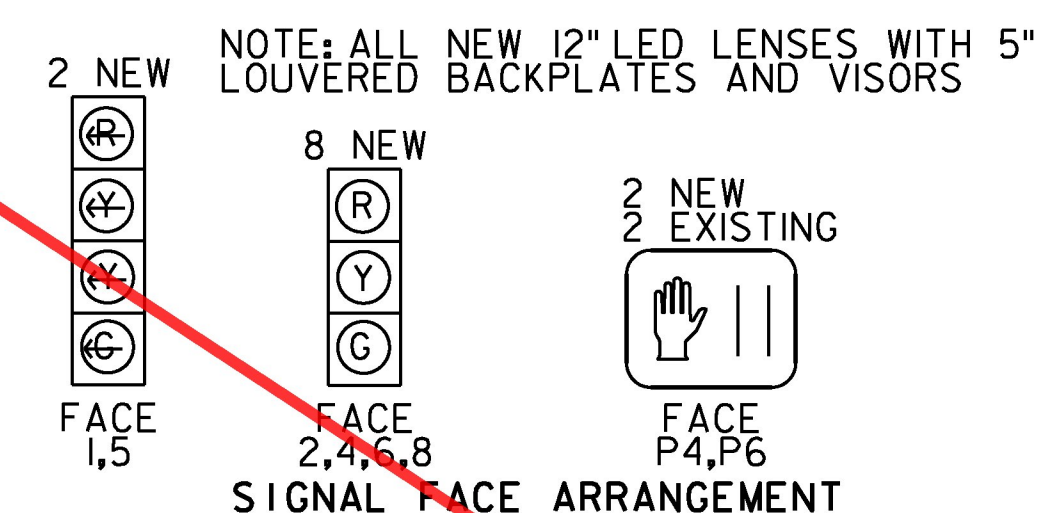
REMOVAL STREET LIGHT ASSEMBLY  
STA. 54+47 38' RT (JB6)  
STA. 55+48 38' LT (JB3)

SPECIAL PROVISION (HORIZONTAL DIRECTIONAL DRILLING) (12" CASING PIPE)  
STA. 54+49 37' LT TO 54+49 36' RT (73')  
STA. 54+67 46' RT TO 55+38 45' RT (71')  
STA. 55+74 30' LT TO 55+74 27' RT (57')



SEE TRAFFIC SIGN DETAIL SHEETS

**SHEET DISCONTINUED  
SEE REPLACEMENT  
SHEET #112R**



|                                |                            |
|--------------------------------|----------------------------|
| PROJECT NAME:                  | SOUTH BURLINGTON-WILLISTON |
| PROJECT NUMBER:                | NH 2944(I)                 |
| FILE NAME:                     | z1d340bdrslg.dgn           |
| PLOT DATE:                     | 3/1/2017                   |
| PROJECT LEADER:                | J. LITTLE                  |
| DRAWN BY:                      | P. ARMATA                  |
| DESIGNED BY:                   | D. DEBAIE                  |
| CHECKED BY:                    | T. LUTHER                  |
| TRAFFIC SIGNAL SYSTEMS SHEET 3 | SHEET 112 OF 249           |