

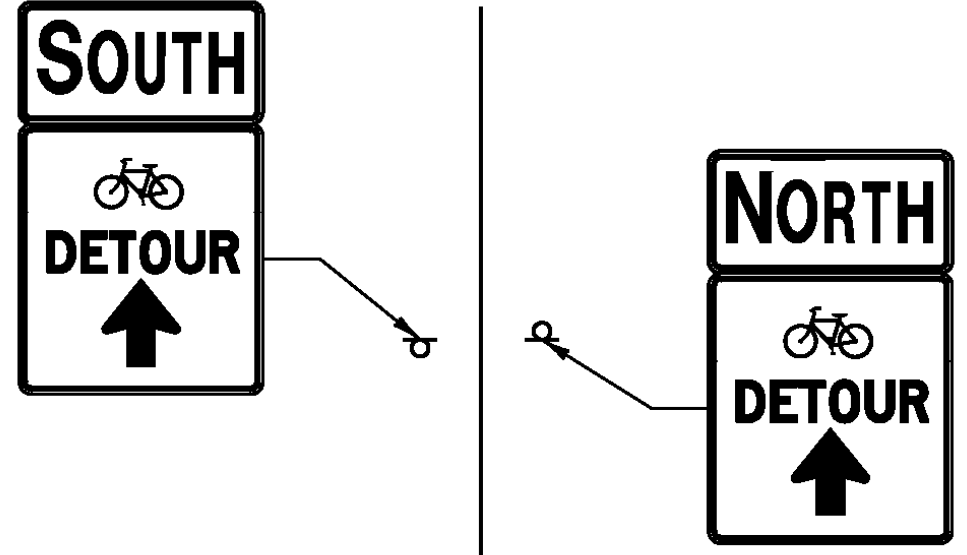
**NOTE:**

B = BIGGER SIGNS FOR FREEWAY SIGN STANDARD. SEE BR 62A TRAFFIC DETOUR PLAN (4 OF 4) SHEET.

= CONFIRMATORY BIKE DETOUR SIGNS

**TRAFFIC CONTROL NOTES:**

1. ALL SIGNS SHALL BE LOCATED SO THEY ARE VISIBLE AND ABLE TO BE READ BY THE TRAVELING PUBLIC. SIGNS SHALL BE INSTALLED SO AS NOT TO OBSTRUCT EXISTING SIGNS.
2. ALL SIGNS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "STANDARD HIGHWAY SIGNS" BOOK (SHS) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION (FHWA).
3. SOLID SUBSTRATE CONSTRUCTION SIGNS SHALL HAVE RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING "AMERICAN SOCIETY FOR TESTING MATERIALS" (ASTM D 4956) TYPE VII, VIII, IX REQUIREMENTS, UNLESS OTHERWISE NOTED. SOLID SUBSTRATE REGULATORY SIGNS (WHITE BACKGROUND) SHALL HAVE A RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING ASTM D 4956 TYPE III.
4. SIGNS SHALL BE ERECTED BEFORE THE START OF ANY WORK AND SHALL BE COVERED UNTIL WORK COMMENCES, AND UPON COMPLETION OF THE WORK. EACH SIGN SHALL BE ERECTED IN A NEAT AND WORKMANLIKE MANNER. SIGNS SHALL BE REMOVED UPON COMPLETION OF THE WORK AT THE DISCRETION OF THE ENGINEER.
5. FIXED SIGNS SHALL BE SET SECURELY IN THE GROUND. THE BOTTOM OF A SIGN SHALL BE AT LEAST SEVEN FEET ABOVE THE EDGE OF PAVEMENT. THE NEAREST EDGE OF A SIGN SHALL BE AT LEAST SIX FEET OUTSIDE THE SHOULDER POINT OR FOUR FEET OUTSIDE GUARDRAIL. ALL SIGNS SHALL BE INSTALLED WITHIN VTRANS OR TOWN'S RIGHT-OF-WAY (ROW). IF THE SIGN CANNOT BE INSTALLED IN THE ROW, THE CONTRACTOR SHALL GET PERMISSION FROM THE LANDOWNER.
6. WHERE SIGN INSTALLATIONS ARE NOT PROTECTED BY GUARDRAIL OR OTHER APPROVED TRAFFIC BARRIERS, ALL SIGN STANDS AND POST INSTALLATIONS SHALL BE "NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM" (NCHRP) REPORT 350 COMPLIANT. NO SIGN POSTS SHALL EXTEND OVER THE TOP OF THE SIGN INSTALLED ON SAID POST(S). WHEN ANCHORS ARE INSTALLED, STUB SHALL NOT BE GREATER THAN FOUR INCHES ABOVE EXISTING GROUND.
7. WHEN EXISTING ROUTE MARKER ASSEMBLIES ARE LOCATED AT THE INTERSECTIONS OR ALONG THE DETOUR ROUTE, THE DETOUR ROUTE MARKER ASSEMBLIES SHALL BE INSTALLED ADJACENT TO THE EXISTING ROUTE MARKER ASSEMBLIES.
8. THE PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE USED IN ACCORDANCE WITH SECTION 6F.60 OF THE MUTCD. PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) SHALL BE PLACED OFF THE EDGE OF THE ROADWAY, OUTSIDE THE CLEAR ZONE, BUT SHALL BE VISIBLE FROM THE ROADWAY. ANY VEGETATION THAT INTERFERES WITH VISIBILITY OF THE PCMS SHALL BE REMOVED. THE MESSAGE SIGN SHALL BE A MINIMUM OF 7 FEET ABOVE THE ROADWAY IN URBAN AREAS AND 5 FEET ABOVE THE ROADWAY IN RURAL AREAS WHEN IT IS IN THE OPERATING MODE. REMOVAL OF THE VEGETATION SHALL BE INCIDENTAL TO ITEM 641.15, "PORTABLE CHANGEABLE MESSAGE SIGN". WHEN PLACED BEHIND GUARDRAIL, THE BOTTOM OF THE SIGN FACE SHALL BE ABOVE THE TOP OF THE GUARDRAIL.
9. SEE BR 62A TRAFFIC DETOUR PLAN (2 AND 3) FOR DETAILS A-G.
10. THE COSTS OF ALL DETOUR SIGNS AND REQUIRED SIGN POSTS & INSTALLATION SHALL BE INCLUDED IN ITEM 641.10, "TRAFFIC CONTROL".



CONFIRMATORY BIKE DETOUR SIGNS  
NOT TO SCALE

TRAFFIC CONTROL PLAN



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PROJECT LEADER: M.A. COLGAN	DRAWN BY: M.C. SCOTT
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BR 62A TRAFFIC DETOUR PLAN (1 OF 4)	SHEET 17 OF 55