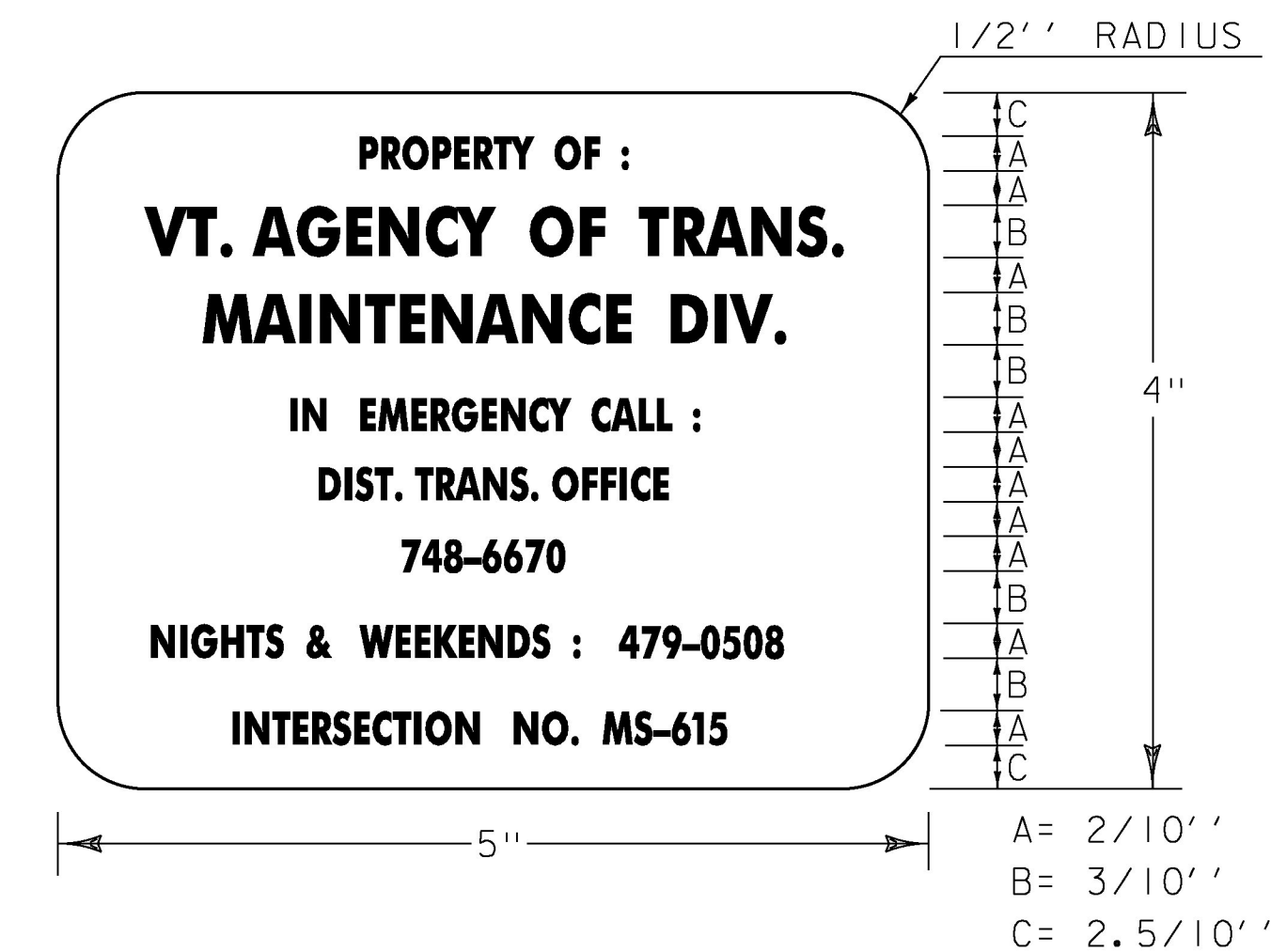


**NOTES:**

1. THE TRAFFIC SIGNAL SYSTEM SHEET IS NOT TO SCALE AND SHALL ONLY BE USED AS A GUIDE FOR THE PLACEMENT OF THE HARDWARE LISTED. THE CONTRACTOR SHALL CONFIRM ALL LOCATIONS IN THE FIELD WITH THE ENGINEER PRIOR TO INSTALLATION. LOCATIONS MAY BE REVISED AS A RESULT OF THE SITE SURVEY.
2. ALL SIGNAL HEADS SHALL BE 12" POLYCARBONATE. THE SIGNAL HEADS SHALL BE FEDERAL YELLOW WITH BLACK VISORS TO MATCH EXISTING.
3. THE TRAFFIC SIGNAL CONTROLLER AND RELATED EQUIPMENT SHALL BE AN ECONOLITE ASC/3-2100 (NEMA TS2) IN A NEW GROUND MOUNT P-44 NEMA TS-2 CABINET (16 PHASE FOR FUTURE FYA'S) W/EXTENSION INSTALLED ON A NEW CONCRETE PAD PER STD E-171B AT THE LOCATION SHOWN ON THE PLANS. TRAFFIC CONTROL CABINET SHALL BE ORIENTED SUCH THAT THE DOOR DOES NOT FACE THE ROADWAY.
4. ALL SIGNAL HEADS SHALL HAVE RED, YELLOW AND GREEN L.E.D. SIGNALS WITH A VISIBLE BEAM SPREAD OF 80 DEGREES OFF AXIS.
5. THE BOTTOM OF ALL NEW SIGNAL HEADS SHALL BE AT THE SAME ELEVATION AS EXISTING HEADS. EXTENSIONS MAY BE USED.
6. ALL NEW SIGNAL AND PEDESTRIAN EQUIPMENT SHALL BE PAINTED FLAT BLACK. EXCEPT AS SPECIFIED OTHERWISE.
7. ALL SIGNAL EQUIPMENT AND SPAN WIRE MOUNTED SIGNS SHALL HAVE SAFETY CABLES.
8. SWITCH-OVER TO NEW SIGNAL SYSTEM SHALL NOT OCCUR DURING PEAK TRAFFIC OPERATING PERIODS. UNIFORMED TRAFFIC OFFICERS SHALL CONTROL TRAFFIC DURING SWITCH-OVER.
9. EXISTING SIGNAL TIMINGS SHALL BE UTILIZED. FOR EXISTING SIGNAL TIMING INFORMATION, PLEASE CONTACT THE TRAFFIC SIGNAL OPERATIONS ENGINEER, DEREK LYMAN AT 802-249-5079.
10. STOP BAR DETECTOR MOUNTING LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR IN ACCORDANCE WITH THE MANUFACTURER'S GUIDANCE FOR THE TYPE OF DETECTOR SUPPLIED. THE CONTRACTOR SHALL SUBMIT PROPOSED MOUNTING LOCATION AND DOCUMENTATION OF CONFORMANCE WITH THE MANUFACTURER'S GUIDANCE TO THE ENGINEER FOR APPROVAL.
11. ALL VEHICLE DETECTORS SHALL BE PLACED SUCH THAT OCCLUSION IS MINIMIZED AND PHASING IS NOT AFFECTED.
12. STOP BAR DETECTION AREAS SHALL EXTEND FIVE FEET PAST THE STOP BAR. FINAL DETECTION ZONES TO BE SET UP FOR OPTIMAL DETECTION BY THE CONTRACTOR BASED ON THE FINAL PAVEMENT MARKINGS.
13. VEHICLE DETECTION SYSTEM SHALL BE ECONOLITE ACCUSCAN BRAND, WAVETRONIX BRAND OR SMARTCO TRAFFIC RADAR.
14. THERE SHALL BE NO WIRING SPLICES BETWEEN THE SIGNAL CONTROLLER EQUIPMENT AND THE VEHICLE DETECTORS EXCEPT IN AN APPROVED MANUFACTURERS JUNCTION BOX.
15. ALL CONDUIT SHALL BE IN SCH 80 PVC CONDUIT AND SHALL BE CONSIDERED INCIDENTAL TO SPECIAL PROVISION (REMOVE AND REPLACE CONTROLLER CABINET). SEPARATE CONDUITS SHALL BE USED FOR DETECTION, SIGNAL CABLE AND POWER. MATERIAL AND CONSTRUCTION TO BE IN ACCORDANCE WITH SECTION 678.
16. STOP BAR DETECTION SYSTEM IS TO BE OPERATIONAL PRIOR TO DISCONNECTING EXISTING LOOPS. EXISTING VEHICLE DETECTOR LOOPS SHALL BE CUT BY THE CONTRACTOR AT THE CURB LINE PRIOR TO COLD PLANING/RESURFACING AS DIRECTED BY THE ENGINEER. PAYMENT SHALL BE CONSIDERED INCIDENTAL TO CONTRACT ITEM 900.620 "SPECIAL PROVISION (VEHICLE STOP BAR DETECTION SYSTEM) (VT 14 @ VT 63)". IF EXISTING VEHICLE DETECTION LOOPS ARE DAMAGED DUE TO THE CONTRACTOR'S OPERATION PRIOR TO INSTALLATION OF NEW STOP BAR DETECTION, TEMPORARY DETECTION WILL BE PROVIDED BY THE CONTRACTOR WITHIN 72 HOURS AT NO COST TO THE AGENCY.
17. FOR INFORMATION REGARDING THE INSTALLATION OF THE ACCESSIBLE PEDESTRIAN PUSH BUTTON ASSEMBLIES (ORIENTATION, HEIGHT, ECT.), SEE SECTION "4E08-PEDESTRIAN DETECTORS" IN THE 2009 EDITION OF THE MUTCD.
18. SEE THE PLANS AND THE SPECIAL PROVISIONS FOR A DETAILED LIST OF EQUIPMENT.

19. ALL ELECTRICAL WIRING SHALL BE DONE BY A LICENSED ELECTRICIAN AND OVERSEEN BY A MASTER ELECTRICIAN.
20. SEE STANDARD E-171A FOR ADDITIONAL NOTES.
21. CONTRACTOR TO SCHEDULE FINAL INSPECTION AND OBTAIN WRITTEN APPROVAL OF WORK FROM VTRANS TRAFFIC SIGNAL TECHNICIAN.
22. ADVANCED VEHICLE DETECTION SHALL BE PROVIDED ON PHASES 2 AND 6. ADVANCE DETECTION SHALL BE CAPABLE OF DETECTING APPROACHING VEHICLES IN A CONTINUOUS RANGE FROM 200-600 FEET AND EXTENDING THE GREEN TIME BY 1-2 SECONDS.
23. ALL MATERIALS AND CONSTRUCTION SHALL CONFORM TO THE STATE OF VERMONT AGENCY OF TRANSPORTATION'S (VAOT) "STANDARD SPECIFICATIONS FOR CONSTRUCTION" DATED 2011, WITH CURRENT MODIFICATIONS.
24. A UNIFORMED TRAFFIC OFFICER WITH A BLUE LIGHT SHALL BE PRESENT DURING ALL LANE CLOSURES.
25. ALL NEW SIGNAL WIRING SHALL BE #14 GAUGE.
26. CALL DIGSAFE PRIOR TO PERFORMING ANY EXCAVATION WORK. 1-888-DIG-SAFE
27. CALL DISTRICT SIGNAL TECH PRIOR TO PERFORMING ANY EXCAVATION. 802-655-1580
28. SEE UTILITIES SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.



LEGEND: - BLACK (NON-REFL.) - STAMPED PRIOR TO PAINTING  
BACKGROUND: NATURAL ALUMINUM OR BRASS SURFACE

**NOTES:**

1. THE PLAQUE SHALL BE MOUNTED ON THE TRAFFIC SIGNAL CONTROLLER CABINET. IT SHALL BE FASTENED TO THE CONTROLLER CABINET IN SUCH A MANNER AS TO BE NOT EASILY REMOVED, SUCH AS WELDED, RIVETED OR BOLTED WITH VANDAL PROOF BOLTS.
2. THE LETTERS SHALL BE PUNCHED OR STAMPED, SUCH STAMPING SHALL PENETRATE AT LEAST 1/2 THE BASE MATERIAL THICKNESS.
3. THE BASE MATERIAL FOR THE PLAQUE SHALL BE BRASS OR ALUMINUM WITH A MINIMUM THICKNESS OF 1/16".
4. THE COST OF PROVIDING AND INSTALLING THE CONTROLLER IDENTIFICATION PLAQUE SHALL BE CONSIDERED INCIDENTAL TO ITEM 900.620 SPECIAL PROVISION (REMOVE AND REPLACE TRAFFIC SIGNAL CONTROLLER AND CABINET) (VT 14 @ VT 63)

**CONTROLLER IDENTIFICATION PLAQUE**

PROJECT NAME:	BERLIN-BARRE TOWN
PROJECT NUMBER:	STP 2943(I)
FILE NAME:	z11b338det.dgn
PROJECT LEADER:	C. LATHROP
DESIGNED BY:	S. SOLLA
TRAFFIC SIGNAL SYSTEM NOTES	
PLOT DATE:	4/26/2016
DRAWN BY:	S. SOLLA
CHECKED BY:	C. LATHROP
SHEET	67 OF 202

NOT TO SCALE