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**STANDARDS**

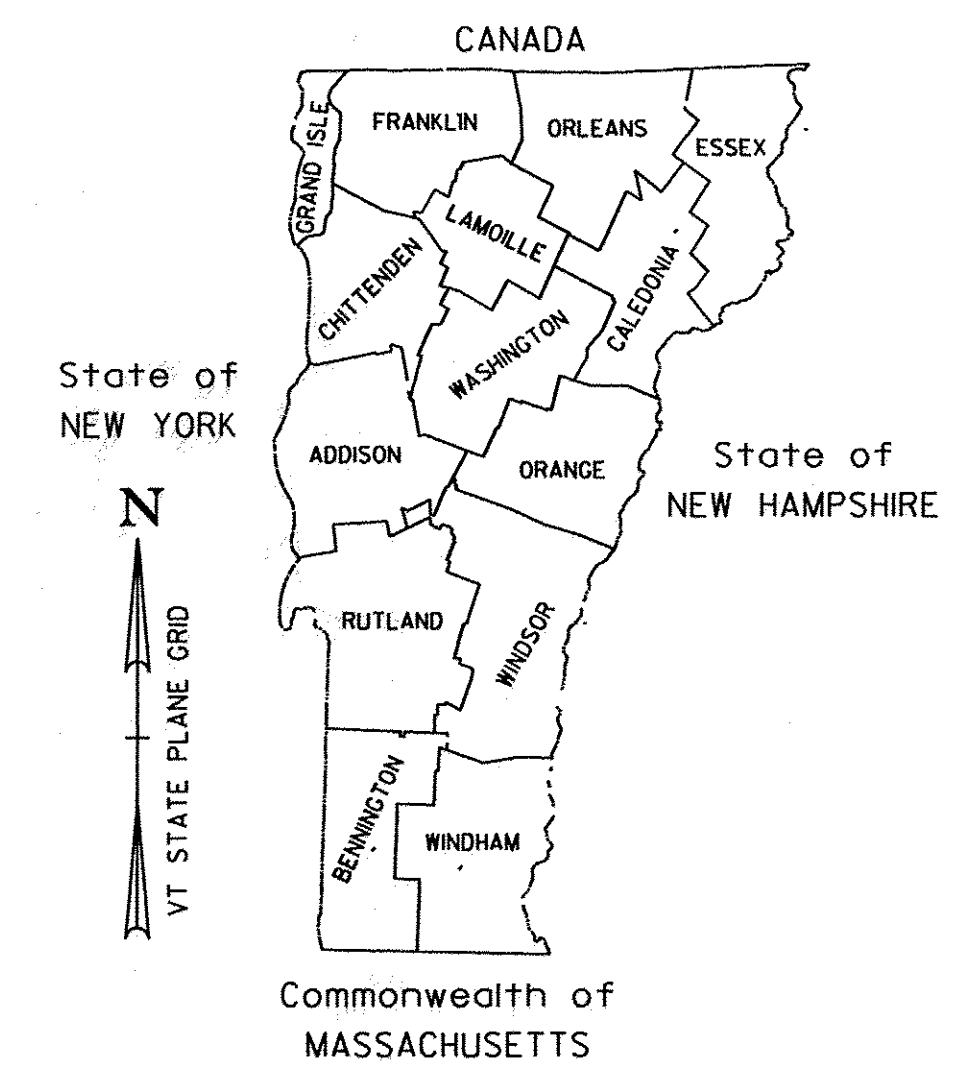
E-106 TRAFFIC CONTROL MISCELLANEOUS DETAILS 03/01/04  
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# STATE OF VERMONT AGENCY OF TRANSPORTATION



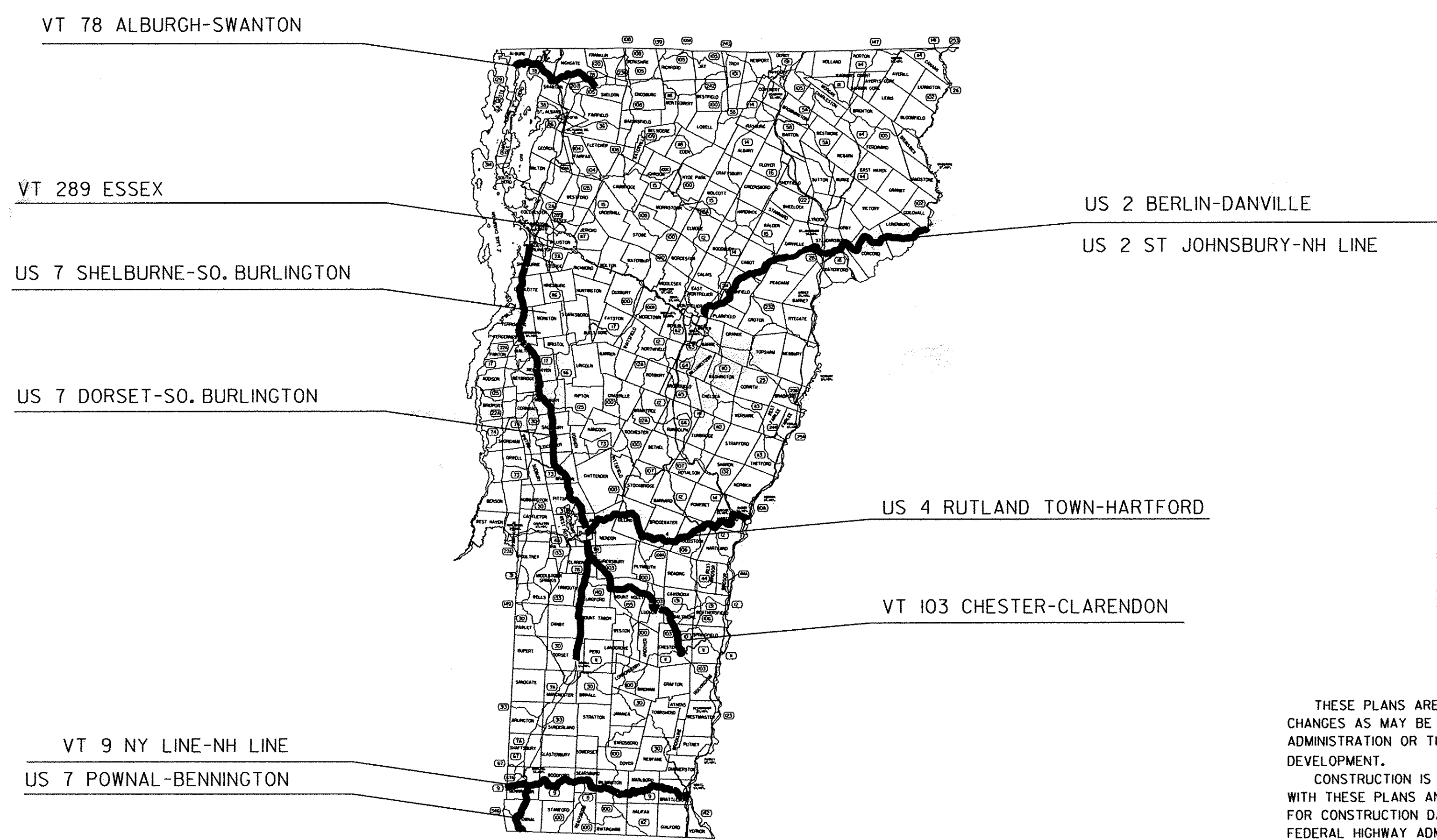
## PROPOSED IMPROVEMENT STATEWIDE NATIONAL HIGHWAY SYSTEM MARKING NHG MARK (205)

LENGTH OF PROJECT 268.98 MILES



**PROPOSED IMPROVEMENT  
 NATIONAL HIGHWAY SYSTEM  
 PAVEMENT MARKING**

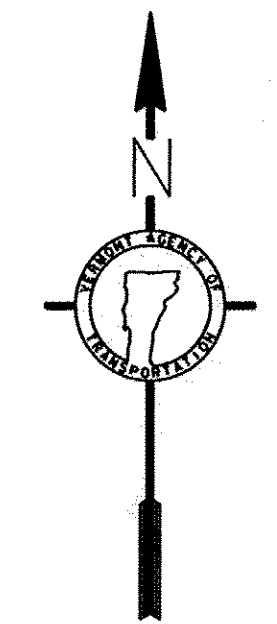
WORK TO BE PERFORMED UNDER THIS PROJECT INCLUDES GRINDING CENTERLINE RUMBLE STRIPS, NEW PAVEMENT MARKINGS INCLUDING CENTERLINES, EDGELINES, LANE LINES, DASHED AND DOTTED ACCELERATION AND DECELERATION LANES, GORE MARKINGS, STOP BARS AND STOP LETTERS, SYMBOLS, AND CROSSWALKS.



RECORD PLANS	
CONTRACTOR:	L&D SAFETY MARKINGS CORP.-BERLIN, VT
RESIDENT ENGINEER:	TIM POCKETT
CONSTRUCTION BEGAN:	JULY 18, 2011
CONSTRUCTION COMPLETE:	OCTOBER 20, 2011
RECORD PLANS BY:	TIM POCKETT
I HEREBY CERTIFY THAT ALL THE CONSTRUCTION REQUIRED BY THIS SET OF DRAWINGS HAS BEEN ACCOMPLISHED AS INDICATED HEREIN.	
BY	RESIDENT ENGINEER
DATE	03-02-2012
NOTE: Any further information concerning final quantities, amounts or other details relative to this project may be found at Central Files in the electronic archives.	

**CONVENTIONAL SYMBOLS**

COUNTY LINE	
TOWN LINE	
LIMITS OF ACCESS	
POINT OF ACCESS	
FENCE LINE	
STONE WALL	
TRAVELED WAY	
GUARD RAIL	
RAILROAD	
SURVEY LINE	
CULVERT	
POWER POLE	
TELEPHONE POLE	
TREES	
CONTROL OF ACCESS	
PROPERTY LINE	
R.O.W. TAKING LINE	
SLOPE RIGHTS	
TOP OF CUT	
TOE OF SLOPE	



SURVEYED BY : N/A  
 SURVEYED DATE : N/A

DATUM  
 VERTICAL N/A  
 HORIZONTAL N/A

THESE PLANS ARE SUBJECT TO SUCH ENGINEERING CHANGES AS MAY BE REQUIRED BY THE FEDERAL HIGHWAY ADMINISTRATION OR THE DIRECTOR OF PROGRAM DEVELOPMENT.  
 CONSTRUCTION IS TO BE CARRIED ON IN ACCORDANCE WITH THESE PLANS AND THE STANDARD SPECIFICATIONS FOR CONSTRUCTION DATED 2006, AS APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION ON JUNE 15, 2006 FOR USE ON THIS PROJECT, INCLUDING ALL SUBSEQUENT REVISIONS AND SUCH REVISED SPECIFICATIONS AND SPECIAL PROVISIONS AS ARE INCORPORATED IN THESE PLANS.

DIRECTOR OF PROGRAM DEVELOPMENT	
APPROVED _____	DATE _____
PROJECT MANAGER : A GAMBLE	
PROJECT NAME : STATEWIDE	
PROJECT NUMBER : NHG MARK (205)	
SHEET 1 OF 26 SHEETS	





## SUMMARY OF LONG LINE QUANTITIES BY ROUTE

ROUTE	TOWN	MM	TO	MM	4' ' WHITE LINE	MATERIAL	4' ' YELLOW LINE	MATERIAL	REMARKS
VT 103	ROCKINGHAM	0.00	-	6.75					MM 0.00 - 6.75 ROCKINGHAM 2009 PROJECT (NO WORK)
VT 103	CHESTER	0.00	-	2.00					MM 0.00 - 6.75 ROCKINGHAM 2009 PROJECT (NO WORK)
VT 103	CHESTER	2.00	-	2.60	6,350	PAINT	6,350	PAINT	
VT 103	CHESTER	4.27	-	9.88	58,802	PAINT	53,647	PAINT	
VT 103	CAVENDISH	0.00	-	3.74	20,477	PAINT	14,432	PAINT	
VT 103	LUDLOW	0.00	-	1.29	13,335	PAINT	13,245	PAINT	
VT 103	LUDLOW	2.86	-	5.78	38,026	PAINT	32,090	PAINT	
VT 103	MT HOLLY	0.00	-	7.86	83,820	PAINT	67,755	PAINT	
VT 103	WALLINGFORD	0.00	-	2.13	22,492	PAINT	22,025	PAINT	
VT 103	SHREWSBURY	0.00	-	4.24	45,304	PAINT	36,250	PAINT	
VT 103	CLARENDON	0.00	-	1.97	20,804	PAINT	14,850	PAINT	
	VT 103 TOTAL				309,748	PAINT	258,543	PAINT	
US 4	FAIR HAVEN	0.00	-	0.15	-	-	800	PAINT	
US 4	RUTLAND TOWN	0.00	-	1.23	14,375	PAINT	13,628	PAINT	
US 4	MENDON	0.00	-	5.93	69,360	PAINT	63,235	PAINT	
US 4	KILLINGTON	0.00	-	8.49	102,734	PAINT	68,414	PAINT	
US 4	BRIDGEWATER	0.00	-	7.50	79,300	PAINT	71,300	PAINT	
US 4	WOODSTOCK	0.00	-	5.65	59,700	PAINT	53,200	PAINT	
US 4	WOODSTOCK	7.22	-	9.65	25,300	PAINT	25,300	PAINT	
US 4	HARTLAND	0.00	-	0.77	8,130	PAINT	8,130	PAINT	
US 4	HARTFORD	0.00	-	9.35	99,792	PAINT	93,544	PAINT	
	US 4 TOTALS				447,206		394,860		

NOTE: MATERIAL USED FOR PAVEMENT MARKING  
 PAINT = WATERBORNE TRAFFIC PAINT  
 POLYUREA

**ROUTE QUANTITIES**  
**SHEET 2**

PROJECT NAME: <b>STATEWIDE</b>	PLOT DATE: 31-MAY-2012 13:10
PROJECT NO.: <b>NHG MARK (205)</b>	DRAWN BY: KAS
DESIGN FILE NAME: llb010.dgn	SHEET: <b>4R</b> OF 26
PROJECT MANAGER: A GAMBLE	
IPARM FILE NAME: llb010r.q2.i	

# SUMMARY OF LONG LINE QUANTITIES BY ROUTE

ROUTE	TOWN	MM	TO	MM	4' ' WHITE LINE	MATERIAL	4' ' YELLOW LINE	MATERIAL	REMARKS
US 2	BERLIN	0.00	-	1.39	14,680	PAINT	14,290	PAINT	
US 2	BARRE TOWN	0.00	-	0.09	910	PAINT	570	PAINT	
US 2	EAST MONTPELIER	0.00	-	4.82	50,900	PAINT	44,730	PAINT	
US 2	PLAINFIELD	0.00	-	1.49	15,788	PAINT	12,618	PAINT	
US 2	MARSHFIELD	0.00	-	8.21	86,785	PAINT	84,540	PAINT	
US 2	CABOT	0.00	-	1.50	-	-	-	-	2011 PROJECT NO WORK
US 2	CABOT	1.50	-	6.23	51,760	PAINT	48,260	PAINT	
US 2	DANVILLE	0.00	-	4.05	43,020	PAINT	39,090	PAINT	
US 2	DANVILLE	4.05	-	4.75	-	-	-	-	2011 PROJECT NO WORK
US 2	DANVILLE	4.75	-	6.50	18,480	PAINT	18,480	PAINT	
US 2	ST JOHNSBURY	4.70	-	8.64	41,760	PAINT	40,660	PAINT	
US 2	KIRBY	0.00	-	0.97	10,190	PAINT	10,190	PAINT	
US 2	CONCORD	0.00	-	10.78	112,930	PAINT	94,320	PAINT	
US 2	LUNENBURG	0.00	-	9.03	94,616	PAINT	80,696	PAINT	
US 2	GUILDHALL	0.00	-	1.16	12,220	PAINT	10,450	PAINT	
	TOTAL US 2				556,400		498,143		

NOTE: MATERIAL USED FOR PAVEMENT MARKING  
 PAINT = WATERBORNE TRAFFIC PAINT  
 POLYUREA

**ROUTE QUANTITIES**  
**SHEET 3**

PROJECT NAME: **STATEWIDE**  
 PROJECT NO.: **NHG MARK (205)**  
 DESIGN FILE NAME: IIB010.dgn  
 PROJECT MANAGER: A GAMBLE  
 IPARM FILE NAME: IIB010r03.i

PLOT DATE: 31-MAY-2012 13:11  
 DRAWN BY: KAS  
 SHEET: **5R** OF 26

## SUMMARY OF LONG LINE QUANTITIES BY ROUTE

ROUTE	TOWN	MM	TO	MM	4' ' WHITE LINE	MATERIAL	4' ' YELLOW LINE	MATERIAL	REMARKS
US 7	POWNAL	0.00	-	7.99	86,624	PAINT	79,694	PAINT	
US 7	BENNINGTON	0.00	-	2.16	22,810	PAINT	22,810	PAINT	
US 7	DORSET	1.75	-	6.72	-	-	-	-	2011 PAVING PROJECT NH SURF (24) MM 1.75 DORSET - 2.47 DANBY
US 7	MT TABOR	0.00	-	4.18	-	-	-	-	
US 7	DANBY	0.00	-	2.47	-	-	-	-	
US 7	WALL ING FORD	0.00	-	6.48	67,494	PAINT	62,536	PAINT	
US 7	RUTLAND TOWN	1.18	-	3.97	30,938	PAINT	27,444	PAINT	
US 7	PITTSFORD	0.00	-	7.59	81,796	PAINT	74,430	PAINT	
US 7	BRANDON	0.00	-	0.70	44,930	PAINT	35,505	PAINT	MM 0.70 BRANDON-3.36 2010 + 2011 PROJECT
US 7	BRANDON	4.35	-	7.45					
US 7	LEICESTER	0.00	-	3.43	36,220	PAINT	22,630	PAINT	
US 7	SALISBURY	0.00	-	4.50	47,520	PAINT	31,825	PAINT	
US 7	MIDDLEBURY	0.00	-	4.28	64,557	PAINT	51,712	PAINT	
US 7	MIDDLEBURY	5.71	-	7.48					
US 7	NEW HAVEN	0.00	-	7.70	74,980	PAINT	67,480	PAINT	
US 7	WALTHAM	0.00	-	1.64	17,320	PAINT	15,620	PAINT	
US 7	FERRISBURGH	0.00	-	7.29	76,980	PAINT	69,380	PAINT	
US 7	CHARLOTTE	0.00	-	6.55	72,243	PAINT	51,953	PAINT	
US 7	SHELBURNE	0.00	-	2.60	29,631	PAINT	28,621	PAINT	
US 7	SO. BURLINGTON	0.98	-	1.72	9,120	PAINT	15,000	PAINT	
	US 7 TOTALS				718,884		633,666		

NOTE: MATERIAL USED FOR PAVEMENT MARKING  
PAINT = WATERBORNE TRAFFIC PAINT  
POLYUREA

**ROUTE QUANTITIES**  
**SHEET 4**

PROJECT NAME: **STATEWIDE**  
PROJECT NO.: **NHG MARK (205)**

DESIGN FILE NAME: IIB010.dgn  
PROJECT MANAGER: A GAMBLE  
IPARM FILE NAME: IIB010r.q4.i

PLOT DATE: 31-MAY-2012 13:11  
DRAWN BY: KAS  
SHEET: **6R** OF 26



# SUMMARY BY INTERCHANGE OF QUANTITIES & MATERIALS

TOWN	ROUTE	MM	EXIT	DIR	ROADWAY TYPE	6" WHITE LINE	MTL	6" YELLOW LINE	MTL	12" WHITE LINE	MTL	LETTER & SYM.	MTL	STOP BAR	MTL	RR XING	4" WHITE LINE	CROSS WALK	4" YELLOW LINE	REMARKS
<b>US 4 DIVIDED HIGHWAY</b>																				
FAIR HAVEN	US 4	1.70	2	EB	OFF+ON RAMP	1,900	P	1,950	P	865	P	14	P	40	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
			2	WB	OFF+ON RAMP	1,995	P	2,760	P	885	P	14	P	53	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
FAIR HAVEN	US 4	2.55	3	EB	OFF+ON RAMP	710	P	710	P	445	P	-	-	-	-	-	-	-	-	
			3	WB	OFF+ON RAMP	980	P	975	P	425	P	10	P	21	-	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
CASTLETON	US 4	5.45	4	EB	OFF+ON RAMP	2,125	P	2,105	P	1,245	P	23	P	31	P	-	-	-	-	STOP(3) AHEAD(1) YIELD(1) WRONG WAY ARROW(1)
			4	WB	OFF+ON RAMP	2,165	P	2,010	P	1,482	P	23	P	23	P	-	-	-	-	STOP(3) AHEAD(1) YIELD(1) WRONG WAY ARROW(1)
CASTLETON	US 4	8.65	5	EB	OFF+ON RAMP	1,450	P	1,440	P	796	P	18	P	32	P	-	-	-	-	STOP(3) AHEAD(1) WRONG WAY ARROW(1)
			5	WB	OFF+ON RAMP	1,465	P	1,475	P	785	P	14	P	31	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
IRA	US 4	11.20		WB	PARKING AREA	0	P	0	P	630	P	0	P	-	-	-	250	-	-	HC(1) <del>WRONG WAY ARROW(1)</del>
WEST RUTLAND	US 4	14.90	6	EB	OFF+ON RAMP	1,705	P	1,615	P	970	P	14	P	28	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
			6	WB	OFF+ON RAMP	1,595	P	1,760	P	925	P	9	P	45	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
RUTLAND	US 4	18.13	-	EB	US 4	-	-	-	-	-	-	22	P	-	-	-	-	-	-	SIGNAL(2) AHEAD(2)
RUTLAND	US 4	18.22	-	EB	US 4	-	-	-	-	-	-	-	-	-	-	2	-	-	-	RR XING(2)
RUTLAND	US 4	18.36	-	EB	US 4	-	-	-	-	-	-	-	-	0	P	-	-	-	-	
RUTLAND	US 4	18.57	-	WB	US 4	-	-	-	-	-	-	22	P	-	-	-	-	-	-	SIGNAL(2) AHEAD(2)
RUTLAND	US 4	18.53	-	WB	US 4	-	-	-	-	-	-	-	-	-	-	2	-	-	-	RR XING(2)
RUTLAND	US 4	18.36	-	WB	US 4	-	-	-	-	-	-	-	-	0	P	-	-	-	-	
RUTLAND	US 4	18.82	-	EB	JCT US 7	0	P	-	-	293	P	25	P	38	P	-	-	-	-	ARROW(9) ONLY(7)4
RUTLAND			-	WB	JCT US 7															
<b>US 4 TOTALS</b>						<b>16,090</b>		<b>16,800</b>		<b>9,746</b>		<b>208</b>		<b>342</b>		<b>4</b>	<b>250</b>			
<b>US 7 DIVIDED HIGHWAY</b>																				
BENNINGTON	US 7	5.16	1	NB	OFF+ON RAMP	0	P	0	P	0	P	-	-	-	-	-	-	-	-	NO WORK DUE TO BENNINGTON BYPASS PROJECT
			1	SB	OFF+ON RAMP	0	P	0	P	0	P	-	-	-	-	-	-	-	-	
BENNINGTON	US 7	6.40	2	NB	OFF+ON RAMP	5,175	P	5,175	P	0	P	0	P	0	P	-	-	-	-	STOP(2) AHEAD(1) YIELD(1) WRONG WAY ARROW(1)
			2	SB	OFF+ON RAMP	3,700	P	3,772	P	2,016	P	15	P	0	P	-	-	-	-	YIELD(2) AHEAD(1) WRONG WAY ARROW(1)
SUNDERLAND	US 7	1.56	3	NB	OFF+ON RAMP	1,856	P	1,536	P	0	P	14	P	33	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
		1.56	3	SB	OFF+ON RAMP	3,575	P	3,575	P	2,158	P	24	P	24	P	-	-	-	-	STOP(2) AHEAD(1) YIELD(2) WRONG WAY ARROW(1)
MANCHESTER	US 7	4.68	4	NB	OFF+ON RAMP	1,500	P	1,411	P	0	P	24	P	19	P	-	-	-	-	STOP(2) AHEAD(1) YIELD(1) WRONG WAY ARROW(1)
		4.68	4	SB	OFF+ON RAMP	616	P	842	P	1,194	P	28	P	35	P	-	-	-	-	STOP(3) AHEAD(1) ARROW(4)2 WRONG WAY ARROW(1) ONLY(2)
<b>US 7 TOTALS</b>						<b>15,422</b>		<b>17,311</b>		<b>5,368</b>		<b>105</b>		<b>117</b>						

NOTE: MAT. = MATERIAL USED FOR PAVEMENT MARKING  
P=WATERBORNE TRAFFIC PAINT  
TP=PAINTED OVER OLD DURABLE MARKINGS

REMOVAL=ALIGNING ARROWS AND ONLY'S TO 2009 MUTCD

**INTERCHANGE QUANTITIES  
AND MATERIALS  
SHEET 1**

PROJECT NAME: STATEWIDE  
PROJECT NO.: NHG MARK (205)  
DESIGN FILE NAME: IIB010.dgn  
PROJECT MANAGER: A GAMBLE  
IPARM FILE NAME: IIB010Iql.I

PLOT DATE: 31-MAY-2012 13:11  
DRAWN BY: KAS  
SHEET: 8R OF 26

# SUMMARY BY INTERCHANGE OF QUANTITIES & MATERIALS

TOWN	ROUTE	MM	EXIT	DIR	ROADWAY TYPE	6" WHITE LINE	MTL	6" YELLOW LINE	MTL	12" WHITE LINE	MTL	LETTER & SYM.	MTL	STOP BAR	MTL	REM	6" WHITE LINE	CROSS WALK	4" YELLOW LINE	REMARKS
<b>VT 289 LIMITED ACCESS HIGHWAY</b>																				
ESSEX	VT 289	7.86	7	WB	OFF RAMP	1,400	RPOLY	1,400	RPOLY	-	-	24	P	36	P	128	-	-	-	SIGNAL(1) AHEAD(1) ARROW(4) ONLY(2) WRONG WAY ARROW(1)
		7.86	7	EB	ON RAMP	1,650	RPOLY	1,650	RPOLY	-	-	-	-	-	-	-	-	-	-	
ESSEX	VT 289	9.35	9	EB	OFF+ON RAMP	1,600	RPOLY	1,550	RPOLY	65	POLY	21	P	31	P	-	-	32	-	SIGNAL(1) AHEAD(1) YIELD(1) STOP(1) WRONG WAY ARROW(1)
		9.35	9	WB	OFF+ON RAMP	1,975	RPOLY	1,950	RPOLY	33	POLY	21	P	32	P	-	-	51-	-	SIGNAL(1) AHEAD(1) YIELD(1) STOP(1) WRONG WAY ARROW(1)
ESSEX	VT 289	10.13	10	EB	OFF+ON RAMP	1,775	RPOLY	1,650	RPOLY	116	POLY	17	P	0	P	-	-	-	-	SIGNAL(1) AHEAD(1) YIELD(2) WRONG WAY ARROW(1)
		10.13	10	WB	OFF+ON RAMP	1,975	RPOLY	1,850	RPOLY	100	POLY	17	P	0	P	-	-	0	-	SIGNAL(1) AHEAD(1) YIELD(2) WRONG WAY ARROW(1)
ESSEX	VT 289	10.13	11	EB	U-TURN RAMP	400	RPOLY	350	RPOLY	0	POLY	14	P	26	P	-	-	-	-	STOP(2) AHEAD(1) WRONG WAY ARROW(1)
ESSEX	VT 289	11.83	12	EB	OFF RAMP	1,300	RPOLY	1,250	RPOLY	124	POLY	10	P	19	P	-	-	-	-	SIGNAL(1) AHEAD(1) ARROW(2) YIELD(1) WRONG WAY ARROW(1)
	VT 289	11.83	12	WB	ON RAMP	1,500	RPOLY	1,400	RPOLY	0	POLY	10	P	-	-	-	-	-	-	YIELD(1) WRONG WAY ARROW(1)
VT 289 TOTALS						13,575		13,050		438	P	130		144		128	107	83		
<b>VT 279 LIMITED ACCESS HIGHWAY</b>																				
BENNINGTON	VT 279		1	EB	OFF+ON RAMP	2,618	P	2,520	P	0	P	0	P	0	P	40	-	-	-	SIGNAL(1) AHEAD(1) YIELD(1) ARROW(2) ONLY(2) WRONG WAY ARROW(1)
			1	WB	OFF+ON RAMP	1,745	P	1,680	P	807	P	0	P	0	P	40	-	-	-	SIGNAL(1) AHEAD(1) YIELD(1) ARROW(2) ONLY(2) WRONG WAY ARROW(1)
VT 279 TOTALS						4,363		4,200		807		0		0		80				
						WORK NOT DONE BECAUSE OF BENNINGTON NH 019-1(54) PROJECT														

NOTE: MAT. = MATERIAL USED FOR PAVEMENT MARKING  
P=WATERBORNE TRAFFIC PAINT  
POLY=POLYUREA  
RPOLY=RECESSED POLYUREA

REMOVAL=ALIGNING ARROWS AND ONLY'S TO 2009 MUTCD

**INTERCHANGE QUANTITIES  
AND MATERIALS  
SHEET 2**

PROJECT NAME: STATEWIDE  
PROJECT NO.: NHG MARK (205)  
DESIGN FILE NAME: IIB010.dgn  
PROJECT MANAGER: A GAMBLE  
IPARM FILE NAME: IIB010Iq2.I

PLOT DATE: 31-MAY-2012 13:11  
DRAWN BY: KAS  
SHEET: 9R OF 26

# SUMMARY OF HAND WORK BY ROUTE

ROUTE	TOWN	MM	STENCIL	4" WHITE LINE	8" YELLOW LINE	8" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	REMOVAL	RR XING	REMARKS	
VT 9	WOODFORD	0.83	SCHOOL(1)	-	-	-	-	<del>6</del>	-	-	-		
VT 9	WOODFORD	1.19	SCHOOL(1)	-	-	-	-	<del>6</del>	-	-	-		
VT 9	WILMINGTON	3.00	CROSSWALK	-	-	-	-	-	<del>40 FEET</del>	-	-		
VT 9	WILMINGTON	3.06	STOP BAR	-	-	-	<del>12 FEET</del>	-	-	-	-	JCT TH 8	
VT 9	WILMINGTON	3.08	STOP BAR	NOT DONE DUE TO DAMAGE FROM IRENE STORM		-	<del>12 FEET</del>	-	<del>40 FEET</del>	-	-	JCT VT 100 NB	
VT 9	WILMINGTON	3.10	STOP BAR			-	<del>12 FEET</del>	-	<del>40 FEET</del>	-	-	JCT VT 100 NB	
VT 9	WILMINGTON	3.21	SCHOOL(1)			-	-	-	<del>6</del>	-	-	-	
VT 9	WILMINGTON	3.25	CROSSWALK	-	-	-	-	-	<del>28 FEET</del>	-	-		
VT 9	WILMINGTON	3.30	SCHOOL(1)	-	-	-	-	<del>6</del>	-	-	-		
VT 9	WILMINGTON	3.95	ARROW(4) ONLY(2)	<del>230 FEET</del>	<del>160 FEET</del>	-	-	<del>12</del>	-	-	-	JCT SHAWS+RITE AID	
VT 9	MARLBORO	5.50	SCHOOL(1)	-	-	-	-	6	-	-	-		
VT 9	MARLBORO	5.80	SCHOOL(2)	-	-	-	-	12	-	-	-		
VT 9	BRATTLEBORO	3.23	ARROW(2) ONLY(1)	73 FEET	200 FEET	-	-	6	-	-	-	JCT WEST BROOK COURT	
VT 9	BRATTLEBORO	3.23	4 ARROW( <del>3</del> ) ONLY(3)	137 FEET	300 FEET	-	-	16	-	-	-	JCT WEST BROOK COURT	
VT 9	BRATTLEBORO	3.33	ARROW(2) ONLY(1)	160 FEET	120 FEET	-	-	6	-	-	-	JCT WEST GATE DRIVE	
VT 9	BRATTLEBORO	3.55	ARROW(8)	-	250 FEET	-	-	8	-	-	-	2-WAY TURN LANE (CUMBY'S)	
	TOTAL VT 9			370 FEET	870 FEET		0 FEET	54	0 FEET				
VT 78	ALBURGH	0.00	STOP (1)	-	-	-	0 FEET	0	-	-	-	JCT US 2	
VT 78	ALBURGH	0.23	STOP (1) AHEAD(1)	DONE BY DISTRICT FORCES		-	-	0	-	-	-		
VT 78	ALBURGH	2.65	RR XING(1)			-	-	-	-	-	-	1	
VT 78	ALBURGH	2.72	ARROW(2) ONLY(1)			255 FEET	534 FEET	-	-	6	-	-	JCT TH 50
VT 78	ALBURGH	2.90	RR XING(1)	-	-	-	-	-	-	-	1		
VT 78	SWANTON	7.50	ARROW(15) ONLY(10)	-	770 FEET	-	0 FEET	0	0 FEET	-	-		
	TOTAL VT 78			255 FEET	1,304 FEET		0 FEET	6	0 FEET	50 FEET 50 FEET	2		
VT 103	CHESTER	7.80	ARROW(5) ONLY(3) YIELD(1)	600 FEET	215 FEET	250 FEET	-	22	65 FEET	-	-	JCT VT 10	
VT 103	CAVENDISH	2.10		-	-	695 FEET	-	-	-	-	-		
VT 103	CAVENDISH	2.70	RR XING(1)	-	-	-	-	-	4" YELLOW LINE	-	1		
VT 103	CAVENDISH	2.90	RR XING(1)	-	-	-	-	-	4" YELLOW LINE	-	1		
VT 103	LUDLOW	4.05	ARROW(6) ONLY(4)	730 FEET	-	-	-	22	820 FEET	-	-	JCT VT 100 NB	
VT 103	SHREWSBURY	2.18	RR XING(1)	-	-	-	-	-	-	-	1		
VT 103	SHREWSBURY	2.45	RR XING(1)	-	-	-	-	-	-	-	1	NOT DONE DUE DAMAGE DONE BY IRENE	
VT 103	SHREWSBURY	3.79	RR XING(1)	-	-	-	-	-	-	-	1		
VT 103	SHREWSBURY	3.95	RR XING(1)	-	-	-	-	-	-	-	1		
VT 103	CLARENDON	3.95	YIELD MARKINGS(5)	-	165 FEET	341 FEET	13 FEET	10	755 FEET	12" YELLOW LINE	-	JCT US 7	
	TOTAL VT 103			1,895 FEET	380 FEET	1,361 FT	13 FEET	64		101 FEET 101 FEET	6		
VT 103	ROCKINGHAM	0.05	ARROW(2) ONLY(2)	180 FEET				10					

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 1**

PROJECT NAME: STATEWIDE  
PROJECT NO.: NHG MARK (205)

DESIGN FILE NAME: IIB010.dgn  
PROJECT MANAGER: A GAMBLE  
IPARM FILE NAME: IIB010hwl1

PLOT DATE: 31-MAY-2012 13:11  
DRAWN BY: KAS  
SHEET: 10R OF 26

# SUMMARY OF HAND WORK BY ROUTE

ROUTE	TOWN	MM	STENCIL	4" WHITE LINE	8" YELLOW LINE	8" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	4" YELLOW LINE	REMARKS
US 4	RUTLAND TOWN	0.20	ARROW( 7) ONLY( 3) DBL ARROWS( 5)	36 FEET	160 FEET	0 FEET	46 FEET	29	-	120 FEET	JCT HOME DEPOT
US 4	RUTLAND TOWN	0.35	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 4	MENDON	0.00	ARROW( 4) ONLY( 2)	400 FEET	620 FEET	-	-	12	-	-	JCT TOWN LINE RD
US 4	MENDON	1.25	ARROW( 2) ONLY( 1)	45 FEET	1000 FEET	-	-	6	-	-	JCT MEADOWLAKE DRIVE
US 4	KILLINGTON	1.13	STOP ( 2) AHEAD( 1)	662 FEET	-	250 FEET	13 FEET	13	-	490 FEET	JCT JUGHANDLE
US 4	KILLINGTON	2.20	ARROW( 3) ONLY( 2)	270 FEET	1,393 FEET	925 FEET	-	11	-	-	JCT VT 100 NB
US 4	KILLINGTON	2.28	<del>2</del> ARROW( <del>3</del> ) ONLY( <del>2</del> ) <del>1</del>	213 FEET	-	-	-	6	-	180 FEET	JCT VT 100 NB
US 4	KILLINGTON	2.34	<del>6</del> ARROW( <del>7</del> ) ONLY( <del>5</del> ) <del>4</del>	695 FEET	757 FEET	-	-	22	-	181 FEET	JCT KILLINGTON ACCESS RD
US 4	BRIDGEWATER	7.20	SCHOOL( 1)	-	-	-	-	0	-	-	
US 4	BRIDGEWATER	7.28	CROSSWALK	-	-	-	-	-	0 FEET	-	
US 4	BRIDGEWATER	7.37	SCHOOL( 1)	-	-	-	-	0	-	-	
US 4	BRIDGEWATER	7.47	CROSSWALK	-	-	-	-	-	0 FEET	-	
US 4	WOODSTOCK	3.10	ARROW( 2) ONLY( 1)	228 FEET	296 FEET	-	-	0	-	487 FEET	JCT TH 35 (COVERED BRIDGE)
US 4	WOODSTOCK	4.45	SCHOOL( 1)	-	-	-	-	0	-	-	
US 4	WOODSTOCK	4.58	ARROW( 2) ONLY( 1)	200 FEET	270 FEET	-	-	0	-	435 FEET	JCT WOODSTOCK HIGH SCHOOL
US 4	WOODSTOCK	4.68	CROSSWALK	-	-	-	-	-	0 FEET	-	
US 4	WOODSTOCK	4.72	SCHOOL( 1)	-	-	-	-	0	-	-	
US 4	WOODSTOCK	7.05	CROSSWALK	-	-	-	-	-	0 FEET	-	
US 4	HARTFORD	3.20	CROSSWALK	-	-	-	-	-	0 FEET	-	QUECHEE GORGE BRIDGE
US 4	HARTFORD	3.27	CROSSWALK	-	-	-	-	-	0 FEET	-	QUECHEE GORGE BRIDGE
US 4	HARTFORD	3.40	CROSSWALK	-	-	-	-	-	0 FEET	-	QUECHEE WELCOME CENTER
US 4	HARTFORD	3.95	ARROW( 2) ONLY( 1)	248 FEET	275 FEET	-	-	0	-	625 FEET	JCT TH 51
US 4	HARTFORD	5.10	ARROW( 2) ONLY( 1)	244 FEET	-	-	-	0	-	-	JCT TH 49
US 4	HARTFORD	5.86	ARROW( 2) ONLY( 1)	263 FEET	-	-	-	0	-	-	JCT QUECHEE ST HWY ( I-89 SB)
US 4	HARTFORD	6.50	ARROW( 2) ONLY( 1)	228 FEET	0 FEET	-	-	0	-	-	JCT I-89 NB
US 4	HARTFORD	9.20	STOP( 1) AHEAD( 1)	-	-	-	-	0	-	-	
US 4	HARTFORD	9.35	STOP( 1) YIELD MARKINGS( 10)	235 FEET	51 FEET	451 FEET	0 FEET	0	-	687 FEET	JCT US 5
			ARROW( 2) DBL ARROW( 3)								REMOVAL=YIELD LETTERS
US 4	HARTFORD	8.50			235 FEET	1,705 FEET					TO YIELD MARKINGS
	TOTAL US 4			3,967 FEET	5,068 FEET	3,331 FEET	59 FEET	110	0 FEET	3,205 FEET	

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 2**

PROJECT NAME: **STATEWIDE**  
PROJECT NO.: **NHG MARK (205)**

DESIGN FILE NAME: IIB010.dgn

PROJECT MANAGER: A GAMBLE

IPARM FILE NAME: IIB010hw2.I

PLOT DATE: 31-MAY-2012 13:11

DRAWN BY: KAS

SHEET: **IIR** OF 26

# SUMMARY OF HAND WORK BY ROUTE

ROUTE	TOWN	MM	STENCIL	4" WHITE LINE	8" YELLOW LINE	8" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	REMOVAL	RR XING	REMARKS
US 2	EAST MONTPELIER	3.00	2010-2011 PROJECT			DURABLE MARKINGS 2011						JCT VT 14 NB
US 2	EAST MONTPELIER	3.00	2010-2011 PROJECT			DURABLE MARKINGS 2011						
US 2	PLAINFIELD	1.00	CROSSWALK	-	-	-	-	-	24 FEET	-	-	PLAINFIELD HEALTH CENTER
US 2	MARSHFIELD	7.88	ARROW( 2) ONLY( 1)	298 FEET	-	-	-	6	-	-	-	JCT VT 232
US 2	DANVILLE	1.73	CROSSWALK	-	-	-	-	-	46 FEET	-	-	WEST DANVILLE STORE
US 2	ST JOHNSBURY	6.54	YIELD MARKINGS( 5)	-	-	315 FEET	-	0	-	64 FEET	-	JCT VT 18 REMOVAL=YIELD LETTERS
US 2	LUNENBURG	4.45	SCHOOL( 1)	-	-	-	-	6	-	-	-	TO YIELD MARKINGS
US 2	LUNENBURG	4.62	CROSSWALK	650 FEET	-	562 FEET	-	-	35 FEET	-	-	
US 2	LUNENBURG	4.70	CROSSWALK	-	-	-	-	-	38 FEET	-	-	
US 2	LUNENBURG	4.85	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 2	DANVILLE	5.68	DASHES	30 FEET								
	TOTAL US 2			978 FEET		877 FEET		18	143 FEET	64 FEET		
US 7	POWNA	1.86	ARROW( 8) ONLY( 7)	66 FEET	0 FEET	-	-	36	-	-	-	JCT VT 346
US 7	POWNA	4.75	ARROW( 2) ONLY( 1)	21 FEET	0 FEET	-	-	6	-	-	-	JCT BARBER POND RD
US 7	POWNA	4.85	ARROW( 2) ONLY( 1)	21 FEET	0 FEET	-	-	6	-	-	-	JCT NORTH POWNA RD
US 7	POWNA	5.14	SCHOOL( 1)	-	-	-	-	0	-	-	-	
US 7	POWNA	5.37	SCHOOL( 1)	-	-	-	-	0	-	-	-	
US 7	BENNINGTON	3.80	ARROW( 3) ONLY( 2)	-	-	-	-	11	-	-	-	JCT VETERANS HOSPITAL
US 7	BENNINGTON	3.93	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	-	
US 7	BENNINGTON	4.13	ARROW( 7) ONLY( 5)	-	-	-	62 FEET	27	-	-	-	JCT KOSHER DRIVE NB
US 7	BENNINGTON	4.13	ARROW( 6) ONLY( 4)	-	-	-	55 FEET	22	-	-	-	JCT KOSHER DRIVE SB
US 7	BENNINGTON	4.29	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	-	
US 7	DORSET	1.32	RAILROAD CROSSING	-	-	-	-	-	-	-	1	
US 7	DORSET	1.46	STOP BARS	-	-	-	0 FEET	-	-	-	-	
US 7	DORSET	1.53	ARROW( 2) ONLY( 1)	-	-	-	-	6	-	-	-	JCT VT 7A
US 7	DORSET	1.63	RAILROAD CROSSING	-	-	-	-	-	-	-	1	
US 7	DORSET	5.20	ARROW( 6) ONLY( 4)	0 FEET	0 FEET	-	-	0	-	-	-	JCT EMERALD LAKE STATE PARK
US 7	WALLINGFORD	5.42	STOP BAR CROSSWALK				22 FEET	-	60 FEET	-	-	JCT VT 140
US 7	SUNDERLAND	1.56		210 FEET								
US 7	WALLINGFORD	6.48			302 FEET							

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 3**

PROJECT NAME: STATEWIDE  
 PROJECT NO.: NHG MARK (205)  
 DESIGN FILE NAME: lib010.dgn  
 PROJECT MANAGER: A GAMBLE  
 IPARM FILE NAME: lib010hw3.i

PLOT DATE: 31-MAY-2012 13:11  
 DRAWN BY: KAS  
 SHEET: 12R OF 26

# SUMMARY OF HAND WORK BY ROUTE

ROUTE	TOWN	MM	STENCIL	4" WHITE LINE	8" YELLOW LINE	8" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	4" YELLOW LINE	RR XING	REMARKS
US 7	RUTLAND TOWN	1.46	ARROW( 6) DBL( 6)	-	-	-	44 FEET	18	-	-	-	JCT POST RD
US 7	RUTLAND TOWN	1.61	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	-	
US 7	RUTLAND TOWN	1.65	8' ' WHITE LINE	100 FEET	-	323 FEET	-	-	-	-	-	JCT EAST PITTSFORD RD
US 7	RUTLAND TOWN	3.50	ARROW( 3) ONLY( 2)	0 FEET	-	-	-	0	-	-	-	JCT PINNACLE HILL RD
US 7	PITTSFORD	2.80	8' ' WHITE LINE	200 FEET	-	1,009 FEET	-	-	-	-	-	JCT VT 3
US 7	PITTSFORD	2.90	CROSSWALK	-	-	-	-	-	37 FEET	-	-	
US 7	PITTSFORD	3.34	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	PITTSFORD	3.42	CROSSWALK	-	-	-	-	-	21 FEET	-	-	
US 7	PITTSFORD	3.55	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	PITTSFORD	3.87	CROSSWALK	-	-	-	-	-	26 FEET	-	-	
US 7	PITTSFORD	5.02	ARROW( 6) ONLY( 4)	750 FEET	401 FEET	-	-	22	-	70 FEET	-	
US 7	BRANDON	0.51	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	BRANDON	4.40	ARROW( 2)	192 FEET	279 FEET	-	-	2	-	70 FEET	-	JCT MCDONALDS
US 7	BRANDON	5.35	ARROW( 6) ONLY( 4)	760 FEET	1,322 FEET	-	-	22	-	56 FEET	-	JCT TH 7
US 7	LEICESTER	1.35	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	LEICESTER	1.58	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	MIDDLEBURY	1.23	ARROW( 3) ONLY( 2)	424 FEET	212 FEET	230 FEET	-	11	-	-	-	JCT VT 125 EAST
US 7	MIDDLEBURY	3.85	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	-	
US 7	MIDDLEBURY	3.96	ARROW( 6) ONLY( 4) DBL( 3)	827 FEET	628 FEET	-	60 FEET	28	53 FEET	478 FEET	-	JCT MCDONALDS
US 7	MIDDLEBURY	4.08	ARROW( 4) ONLY( 2)	344 FEET	520 FEET	-	50 FEET	12	60 FEET	-	-	JCT CITIZENS BANK
US 7	MIDDLEBURY	4.20	ARROW( 9) ONLY( 1) DBL( 2)	174 FEET	202 FEET	-	-	12	-	-	-	JCT CHITTENDEN BANK
US 7	MIDDLEBURY	4.23	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	-	
US 7	MIDDLEBURY	6.55	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	MIDDLEBURY	6.73	SCHOOL( 1)	-	-	-	-	6	-	-	-	
US 7	NEW HAVEN	5.35	ARROW( 3) ONLY( 2)	138 FEET	618 FEET	-	-	11	-	-	-	JCT VT 17 EAST
US 7	NEW HAVEN	5.36	RR XING ( 1)	-	-	-	-	-	-	-	1	
US 7	NEW HAVEN	5.47	ARROW( 1)	77 FEET	675 FEET	-	-	1	-	-	-	JCT VT 17 WEST
US 7	NEW HAVEN	5.60	RR XING ( 1)	-	-	-	-	-	-	-	1	
US 7	FERRISBURGH	0.20	ARROW( 3) ONLY( 3)	625 FEET	-	-	-	3	-	-	-	JCT FERRISBURGH ST HWY
US 7	FERRISBURGH	0.50	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	-	
US 7	FERRISBURGH	0.64	ARROW( 6) ONLY( 3)	607 FEET	433 FEET	-	61 FEET	18	-	-	-	JCT MONKTON RD
US 7	FERRISBURGH	0.69	RR XING ( 1)	-	-	-	-	-	-	-	1	
US 7	FERRISBURGH	0.77	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	-	
US 7	FERRISBURGH	0.93	RR XING ( 1)	-	-	-	-	-	-	-	1	
US 7	FERRISBURGH	1.40	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	-	

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 4**

PROJECT NAME: STATEWIDE  
PROJECT NO.: NHG MARK (205)

DESIGN FILE NAME: IIB010.dgn  
PROJECT MANAGER: A GAMBLE  
IPARM FILE NAME: IIB010hw4.i

PLOT DATE: 31-MAY-2012 13:11  
DRAWN BY: KAS  
SHEET: 13R OF 26

# SUMMARY OF HAND WORK BY ROUTE

ROUTE	TOWN	MM	STENCIL	4" WHITE LINE	8" YELLOW LINE	8" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	4" YELLOW LINE	REMARKS
US 7	FERRISBURGH	1.50	ARROW( 6) ONLY( 3) YIELD( 1)	382 FEET	236 FEET	0 FEET	36 FEET	33	-	144 FEET	JCT VT 22A
			AHEAD( 1) YIELD MARKINGS( 5)								
US 7	FERRISBURGH	1.70	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	FERRISBURGH	3.07	SCHOOL( 1)	-	-	-	-	6	-	-	
US 7	FERRISBURGH	3.12	ARROW( 2) ONLY( 2)	289 FEET	737 FEET	-	-	10	-	-	JCT MIDDLE BROOK RD
US 7	FERRISBURGH	3.16	SCHOOL( 1)	-	-	-	-	6	-	-	
US 7	FERRISBURGH	6.95	ARROW( 2) ONLY( 2)	261 FEET	857 FEET	-	-	10	-	-	JCT STAGE RD
US 7	CHARLOTTE	3.23	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	CHARLOTTE	3.45	ARROW( 6) ONLY( 4) DBL ARROW( 6)	832 FEET	1,000 FEET	-	75 FEET	34	-	120 FEET	JCT F-5
US 7	CHARLOTTE	3.72	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	SHELBURNE	0.55	ARROW( 6) ONLY( 4)	506 FEET	606 FEET	-	-	22	-	132 FEET	JCT TEDDY BEAR FACTORY RD
US 7	SHELBURNE	1.02	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	SHELBURNE	1.14	STOP BAR	-	-	-	28 FEET	-	-	-	JCT BOSTWICK RD
US 7	SHELBURNE	1.23	ARROW( 3) ONLY( 2)	235 FEET	-	-	-	11	-	-	JCT SHELBURNE MUSEUM
US 7	SHELBURNE	1.27	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	SHELBURNE	1.67	CROSSWALK	-	-	-	-	-	39 FEET	-	
US 7	SHELBURNE	1.78	CROSSWALK	-	-	-	-	-	42 FEET	-	
US 7	SHELBURNE	1.80	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	SHELBURNE	2.00	ARROW( 4) ONLY( 2)	244 FEET	190 FEET	-	43 FEET	12	36 FEET	-	JCT HARBOR RD
US 7	SHELBURNE	2.10	SIGNAL( 1) AHEAD( 1)	-	-	-	-	11	-	-	
US 7	SHELBURNE	2.30	ARROW( 4) ONLY( 2)	259 FEET	250 FEET	-	-	12	-	40 FEET	JCT TD BANK NORTH
US 7	SHELBURNE	2.50	ARROW( 8) ONLY( 6)	569 FEET	683 FEET	-	-	32	-	149 FEET	JCT HARBOR INDUSTRIES
US 7	SHELBURNE	2.70	STOP BAR + CROSSWALK	-	150 FEET	-	54 FEET	-	42 FEET	160 FEET	JCT WEBSTER RD
US 7	SHELBURNE	3.10	STOP BAR + CROSSWALK	-	-	-	76 FEET	-	78 FEET	-	JCT LONG MEADOW DR
US 7	SHELBURNE	3.80	STOP BAR + CROSSWALK	-	-	-	76 FEET	-	78 FEET	-	JCT EXECUTIVE DR
US 7	SHELBURNE	4.24	STOP BAR + CROSSWALK	-	-	-	76 FEET	-	78 FEET	-	JCT HILLCREST RD
US 7	SHELBURNE	4.58	STOP BAR + CROSSWALK ARROW( 1)	-	-	-	77 FEET	1	78 FEET	-	JCT MARTINDALE RD
US 7	SHELBURNE	4.72	STOP BAR + CROSSWALK	-	-	-	80 FEET	-	80 FEET	-	JCT LOCUST HILL RD
US 7	SO. BURLINGTON	0.11	STOP BAR + CROSSWALK ARROW( 4) ONLY( 3)	-	-	-	77 FEET	16	78 FEET	-	JCT ALLEN RD
US 7	SO. BURLINGTON	0.23	STOP BAR + CROSSWALK ARROW( 2) ONLY( 1) BUS( 1)-	-	-	-	76 FEET	9	78 FEET	-	JCT HARBOR VIEW RD
US 7	SO. BURLINGTON	0.53	STOP BAR + CROSSWALK ARROW( 2) ONLY( 1)	-	-	-	75 FEET	6	78 FEET	-	JCT GREEN MTN DR
US 7	SO. BURLINGTON	0.87	STOP BAR + CROSSWALK ARROW( 4) ONLY( 4)	226 FEET	-	-	75 FEET	20	74 FEET	890 FEET	JCT IDX DR
US 7	SO. BURLINGTON	1.01	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS
US 7	SO. BURLINGTON	1.06	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS
US 7	SO. BURLINGTON	1.14	ARROW( 7) ONLY( 4)	531 FEET	-	-	94 FEET	23	75 FEET	-	JCT MC DONALDS

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 5**

PROJECT NAME: STATEWIDE	PLOT DATE: 31-MAY-2012 13:11
PROJECT NO.: NHG MARK (205)	DRAWN BY: KAS
DESIGN FILE NAME: lib010.dgn	SHEET: 14R OF 26
PROJECT MANAGER: A GAMBLE	
IPARM FILE NAME: lib010hw5.i	



## SUMMARY OF HAND WORK BY ROUTE (DIVIDED HIGHWAY)

ROUTE	TOWN	MM	STENCIL	6" WHITE LINE	12" YELLOW LINE	12" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	4" YELLOW LINE	REMARKS
US 7 NB	CLARENDON	0.75	ARROW( 3) ONLY( 2) STOP( 2)	-	-	-	56 FEET	19	-	142 FEET	JCT VT 7B
US 7 SB	CLARENDON	0.75	ARROW( 3) ONLY( 2)	-	-	-	-	11	-	-	JCT VT 7B
US 7 NB	CLARENDON	1.16	ARROW( 2) ONLY( 2)	-	-	-	-	11	-	-	JCT TH 26 ( BUMP RD)
US 7 SB	CLARENDON	1.16	ARROW( 2) ONLY( 1) STOP( 2)	-	-	-	26 FEET	14	-	26 FEET	JCT TH 26 ( BUMP RD)
US 7 NB	CLARENDON	1.46	ARROW( 5) ONLY( 2) STOP( 2)	-	-	-	26 FEET	21	-	26 FEET	JCT VT 7B
US 7 SB	CLARENDON	1.46	ARROW( 5) ONLY( 2)	-	-	-	-	13	-	-	JCT VT 7B ( MIDDLE RD)
US 7 NB	CLARENDON	2.43	NO REPAIR TO THERMAL NEEDED	-	-	-	-	-	-	-	PARKING AREA THERMAL NO STENCILS
US 7 SB	CLARENDON	2.54	NO REPAIR TO THERMAL NEEDED	-	-	-	-	-	-	-	PARKING AREA THERMAL NO STENCILS
US 7 NB	CLARENDON	3.18	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	
US 7 NB	CLARENDON	3.34	ARROW( 3) ONLY( 2)	-	-	-	0 FEET	11	-	-	JCT VT 103
US 7 SB	CLARENDON	3.34	ARROW( 3) ONLY( 2)	-	-	-	36 FEET	11	-	-	JCT VT 103
US 7 SB	CLARENDON	3.56	SIGNAL( 2) AHEAD( 2)	-	-	-	-	0	-	-	
US 7 NB	CLARENDON	4.77	ARROW( 3) ONLY( 2)	-	-	-	-	11	-	-	JCT VT 7B
US 7 SB	CLARENDON	4.77	ARROW( 2) ONLY( 1)	-	-	-	-	6	-	-	JCT VT 7B
US 7 NB	CLARENDON	5.05	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	
US 7 NB	CLARENDON	5.20	ARROW( 2) ONLY( 1)	-	-	-	36 FEET	6	-	-	JCT NORTH SHREWSBURY RD
US 7 SB	CLARENDON	5.20	ARROW( 4) ONLY( 2)	-	-	-	48 FEET	12	-	-	JCT NORTH CLARENDON RD
US 7 SB	CLARENDON	5.35	SIGNAL( 2) AHEAD( 2)	-	-	-	-	22	-	-	
US 7 NB	CLARENDON	5.54	ARROW( 2) ONLY( 1)	-	-	-	-	6	-	-	
US 7 NB	CLARENDON	5.58	SIGNAL( 2) AHEAD( 2)	-	-	-	-	0	-	-	
US 7 NB	CLARENDON	5.60	ARROW( 3) ONLY( 2)	-	-	-	-	11	-	-	JCT TRACTOR SUPPLY
			US 7 CLARENDON TOTALS	-	-	-	228 FEET	229	-	194 FEET	
US 7 NB	RUTLAND TOWN	0.11	ARROW( 2) ONLY( 1)	-	-	-	12 FEET	6	-	-	JCT WINDCREST RD
US 7 SB	RUTLAND TOWN	0.11	ARROW( 3) ONLY( 2)	-	-	-	0 FEET	0	-	-	JCT MIDDLE RD
US 7 NB	RUTLAND TOWN	0.36	ARROW( 6) ONLY( 4) YIELD MARKS( 5)	-	-	-	0 FEET	0	-	-	JCT DIAMOND RUN MALL ENT.
US 7 SB	RUTLAND TOWN	0.36	ARROW( 2) ONLY( 2)	-	-	-	0 FEET	0	-	-	JCT US 4 WB
US 7 NB	RUTLAND TOWN	0.68	ARROW( 3) ONLY( 2)	-	-	-	0 FEET	0	-	-	JCT DIAMOND RUN MALL ENT.
US 7 SB	RUTLAND TOWN	0.68	ARROW( 2) ONLY( 1)	-	-	-	0 FEET	0	-	-	JCT HOLIDAY INN
US 7 NB	RUTLAND TOWN	0.80	ARROW( 2) ONLY( 1)	-	-	-	0 FEET	0	0 FEET	-	JCT GREEN MOUNTAIN PLAZA
US 7 SB	RUTLAND TOWN	0.80	ARROW( 6) ONLY( 4)	-	-	-	0 FEET	0	-	-	JCT GREEN MOUNTAIN PLAZA
US 7 NB	RUTLAND TOWN	0.87	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS
US 7 NB	RUTLAND TOWN	0.91	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS
US 7 NB	RUTLAND TOWN	0.95	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS
US 7 NB	RUTLAND TOWN	0.99	ARROW( 2)	-	-	-	-	2	-	-	TWO-WAY TURN LANE ARROWS

NOTE: ALL HAND WORK IS WATERBORNE TRAFFIC PAINT

**HAND WORK  
QUANTITIES SHEET 7**

PROJECT NAME: **STATEWIDE**  
 PROJECT NO.: **NHG MARK (205)**  
 DESIGN FILE NAME: IIB010.dgn  
 PROJECT MANAGER: A GAMBLE  
 IPARM FILE NAME: IIB010hw7.i

PLOT DATE: 31-MAY-2012 13:11  
 DRAWN BY: KAS  
 SHEET: 16R OF 26



# SUMMARY OF LANE REDUCTION ARROWS BY ROUTE

ROUTE	TOWN	MM	STENCIL	6" WHITE LINE	12" YELLOW LINE	12" WHITE LINE	STOP BAR	LETTER & SYM.	CROSS WALK	REMOVAL	REMARKS
VT 9 EB	WOODFORD	4.95	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 WB	SEARSBURG	1.65	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 EB	WILMINGTON	6.90	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 WB	MARLBORO	1.20	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 WB	MARLBORO	3.26	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 WB	MARLBORO	4.15	LANE REDUCTION ARROW( 6)	-	-	-	-	0	-	-	EB AND WB TRUCK LANES END
VT 9 WB	MARLBORO	5.75	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	TURN TO SCHOOL DO NOT INSTALL
VT 9 WB	MARLBORO	6.90	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 9 WB	BRATTLEBORO	1.35	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 103NB	CAVENDISH	1.32	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 103NB	LUDLOW	5.62	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 103NB	MT HOLLY	2.40	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
VT 103SB	SHREWSBURY	3.38	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 EB	MARSHFIELD	7.68	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 EB	CABOT	2.45	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 EB	CABOT	5.14	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 WB	DANVILLE	2.63	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 EB	DANVILLE	3.41	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 WB	DANVILLE	5.46	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 2 EB	DANVILLE	5.48	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 4 EB	RUTLAND TOWN	1.06	LANE REDUCTION ARROW( 3)	-	-	-	-	2	-	-	
US 4 EB	MENDON	0.50	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 4 EB	KILLINGTON	0.55	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 4 WB	KILLINGTON	0.85	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 4 WB	KILLINGTON	2.60	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	POWNA	2.10	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	POWNA	4.45	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 SB	POWNA	6.65	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	SHAFTSBURY	2.25	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 SB	SHAFTSBURY	4.45	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	SHAFTSBURY	4.70	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	SUNDERLAND	5.05	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	SUNDERLAND	6.60	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 SB	SUNDERLAND	6.60	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 SB	MANCHESTER	3.95	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	
US 7 NB	MANCHESTER	7.30	LANE REDUCTION ARROW( 3)	-	-	-	-	3	-	-	

<b>LANE REDUCTION ARROWS</b>  <b>QUANTITY SHEET 1</b>	PROJECT NAME: <b>STATEWIDE</b>	PLOT DATE: 31-MAY-2012 13:11
	PROJECT NO.: <b>NHG MARK (205)</b>	DRAWN BY: KAS
	DESIGN FILE NAME: IIB010.dgn	SHEET: <b>18R</b> OF 26

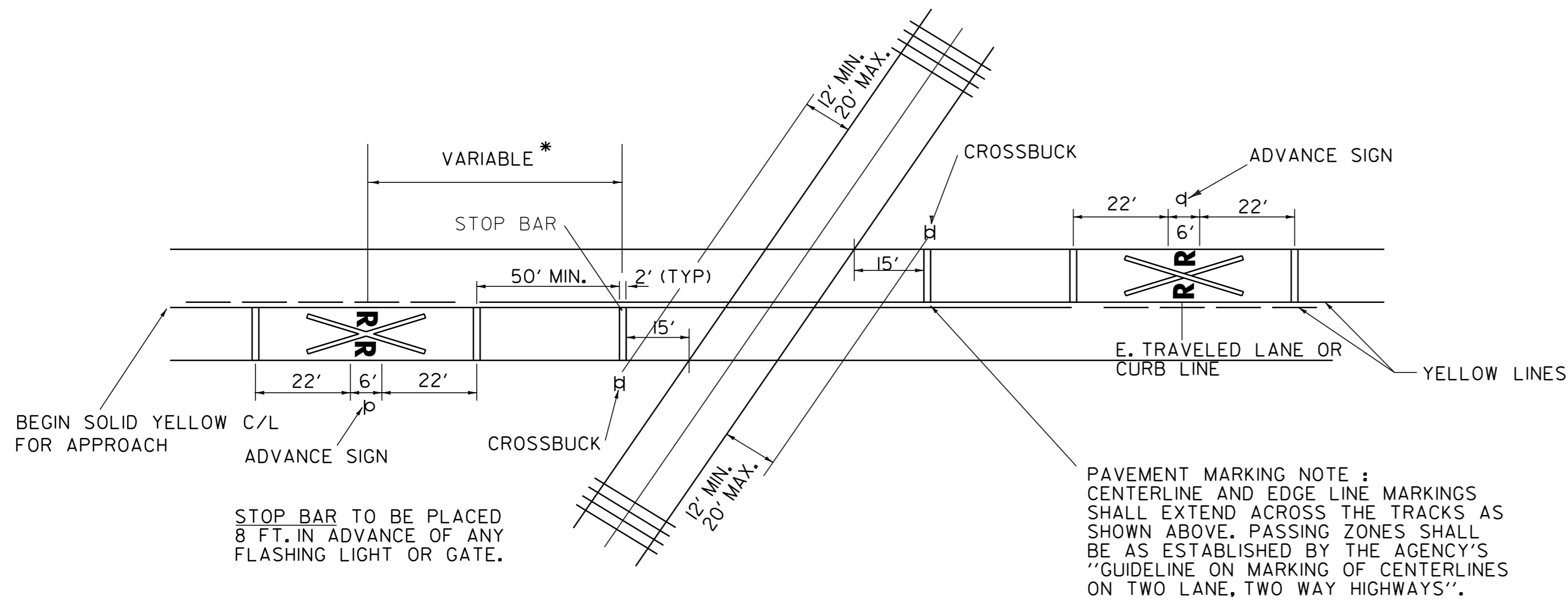
NOTE: ALL LANE REDUCTION ARROWS ARE WATERBORNE TRAFFIC PAINT



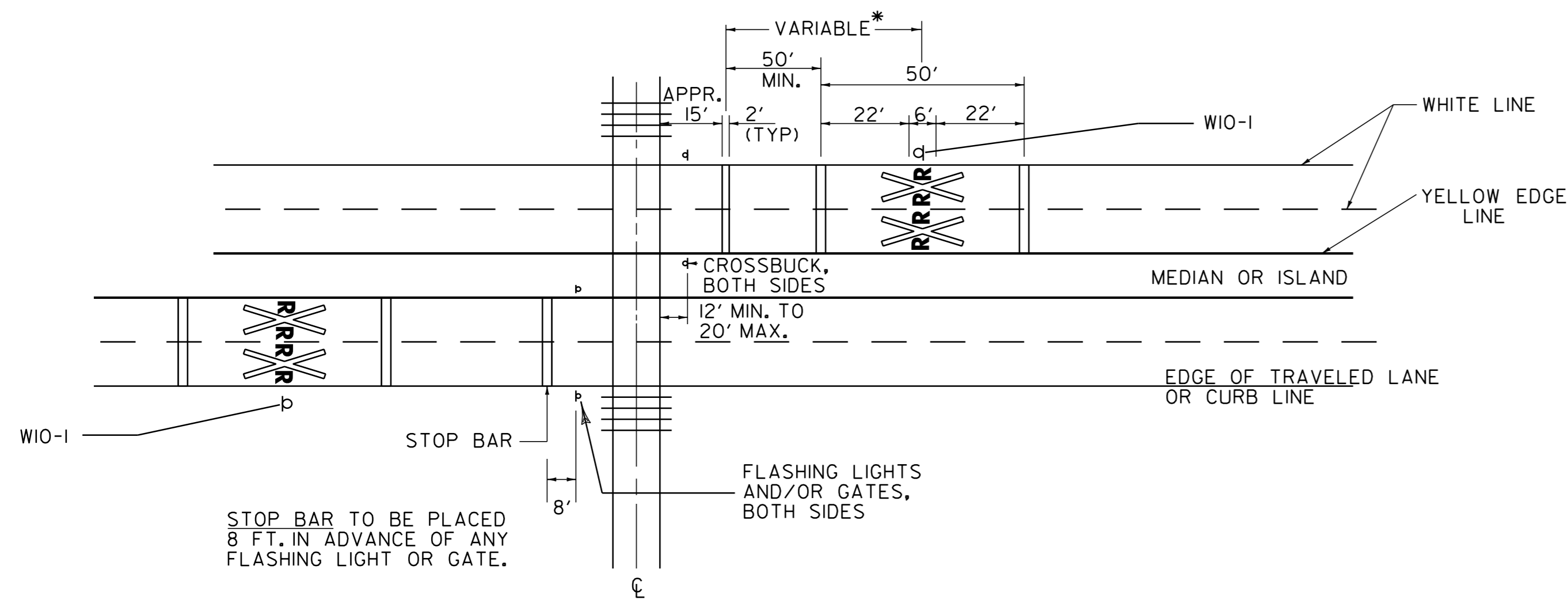
NOTES:

1. ALL MARKINGS SHALL BE PLACED WHERE EXISTING MARKINGS ARE LOCATED EXCEPT WHERE EXISTING MARKINGS OR STENCILS DO NOT CONFORM WITH DETAIL DRAWINGS. MARKINGS THAT DO NOT CONFORM WITH DETAIL DRAWINGS SHALL BE REMOVED AND REPLACED BY NEW MARKINGS.
2. MARKINGS INCLUDE TURN AND CLIMBING LANES, 8 INCH AND 12 INCH WHITE GORE MARKINGS AND 8 INCH YELLOW PAINTED ISLANDS.
3. CENTERLINE RUMBLE STRIPS SHALL BE GROUND INTO THE CENTER OF THE FOLLOWING ROADS:  
  
US 7 BRANDON MM 5.68 TO LEICESTER MM 1.25  
US 7 LEICESTER MM 1.97 TO MIDDLEBURY MM 1.12  
US 7 BENNINGTON MM 7.20 TO BENNINGTON MM 7.40 (APPROX. 1000')  
US FAIR HAVEN MM 0.03 TO FAIR HAVEN MM 0.15 (APPROX. 650')
4. EXISTING CENTERLINE MARKINGS LOCATED IN CENTERLINE RUMBLE STRIPS FOR THE FOLLOWING LOCATIONS THE PAINT APPLICATOR WILL NEED TO BE MODIFIED TO PAINT OVER RUMBLE STRIPS:  
  
US 4 MENDON MM 1.25 TO KILLINGTON MM 2.05  
VT 9 MARLBORO MM 0.60 TO BRATTLEBORO MM 1.60
5. ALL MARKINGS DONE ON US 7 IN SHELBURNE AND SO. BURLINGTON SHALL BE DONE AT NIGHT WITH UTO'S. SEE THE SPECIAL PROVISIONS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.

<b>PAVEMENT MARKING NOTES</b>	PROJECT NAME: <b>STATEWIDE</b>	
	PROJECT NO.: <b>NHG MARK (205)</b>	
	DESIGN FILE NAME: IIB010.dgn	PLOT DATE: 31-MAY-2012 13:11
	PROJECT MANAGER: A GAMBLE	DRAWN BY: KAS
	IPARM FILE NAME: IIB010pmm.i	SHEET: 20 OF 26



**2 LANE HIGHWAY PAVED**

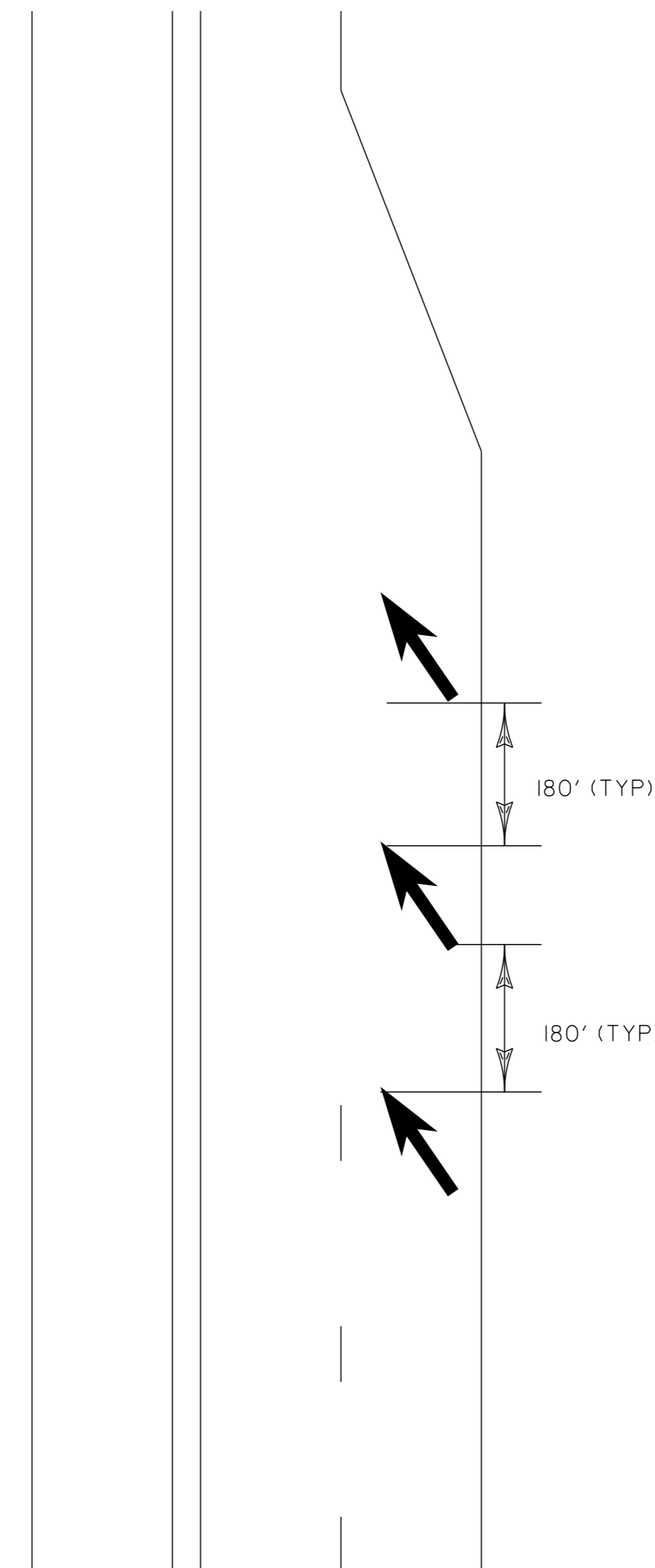


**4 LANE DIVIDED HIGHWAY**

**NOTES**

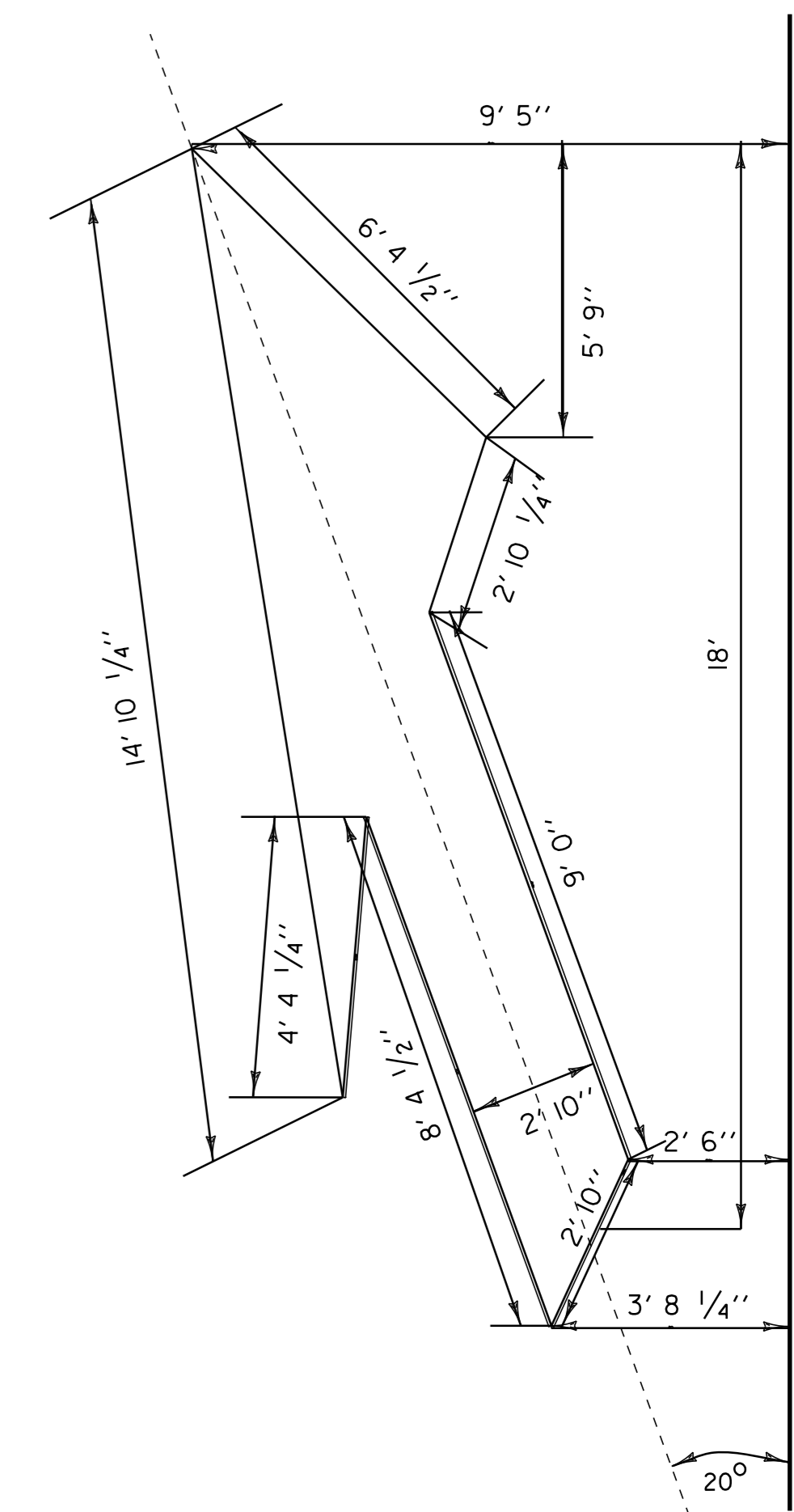
**1. PAVEMENT MARKINGS**

THESE MARKINGS SHALL BE PLACED, IF PHYSICALLY FEASIBLE, ON ALL PAVED APPROACHES TO RAILROAD CROSSINGS, WHERE THERE ARE SIGNALS AND/OR AUTOMATIC GATE AND AT ALL OTHER CROSSINGS WHERE THE PREVAILING SPEED OF HIGHWAY TRAFFIC IS 40 M.P.H. OR GREATER. THE MARKINGS SHALL ALSO BE PLACED AT CROSSINGS WHERE ENGINEERING STUDIES INDICATE THERE IS A SIGNIFICANT POTENTIAL CONFLICT BETWEEN VEHICLES AND TRAINS. ALL MARKINGS SHALL BE WHITE EXCEPT FOR MEDIAN EDGELINES AND CENTERLINE ON 2 LANE ROADS WHICH SHALL BE YELLOW. THE RAILROAD CROSSING SYMBOL SHALL CONSIST OF THE "X" FLANKED BY TWO "R"'S AND THREE 24" STOP BARS; TWO BRACKETING THE "X", ONE AT THE CROSSING. AT MINOR CROSSINGS OR IN URBAN AREAS, THESE MARKINGS MAY BE OMITTED IF AN ENGINEERING STUDY INDICATES THAT OTHER DEVICES INSTALLED PROVIDE SUITABLE PROTECTION.



**LANE REDUCTION TRANSITION MARKINGS**

FIRST LANE REDUCTION ARROW SHALL BE PLACED OPPOSITE THE LAST DASH. ARROWS TO BE PLACED EVERY 180'(MAX.) BETWEEN LAST DASH AND BEGINNING OF LANE TAPER. FOR LEFT LANE USE MIRROR IMAGE.



**LANE DROP ARROW**

NOT TO SCALE

**PAVEMENT MARKING DETAILS**

**PAVEMENT MARKING DETAILS SHEET 1**

PROJECT NAME: STATEWIDE  
PROJECT NO.: NHG MARK (205)

DESIGN FILE NAME: 11b010.dgn

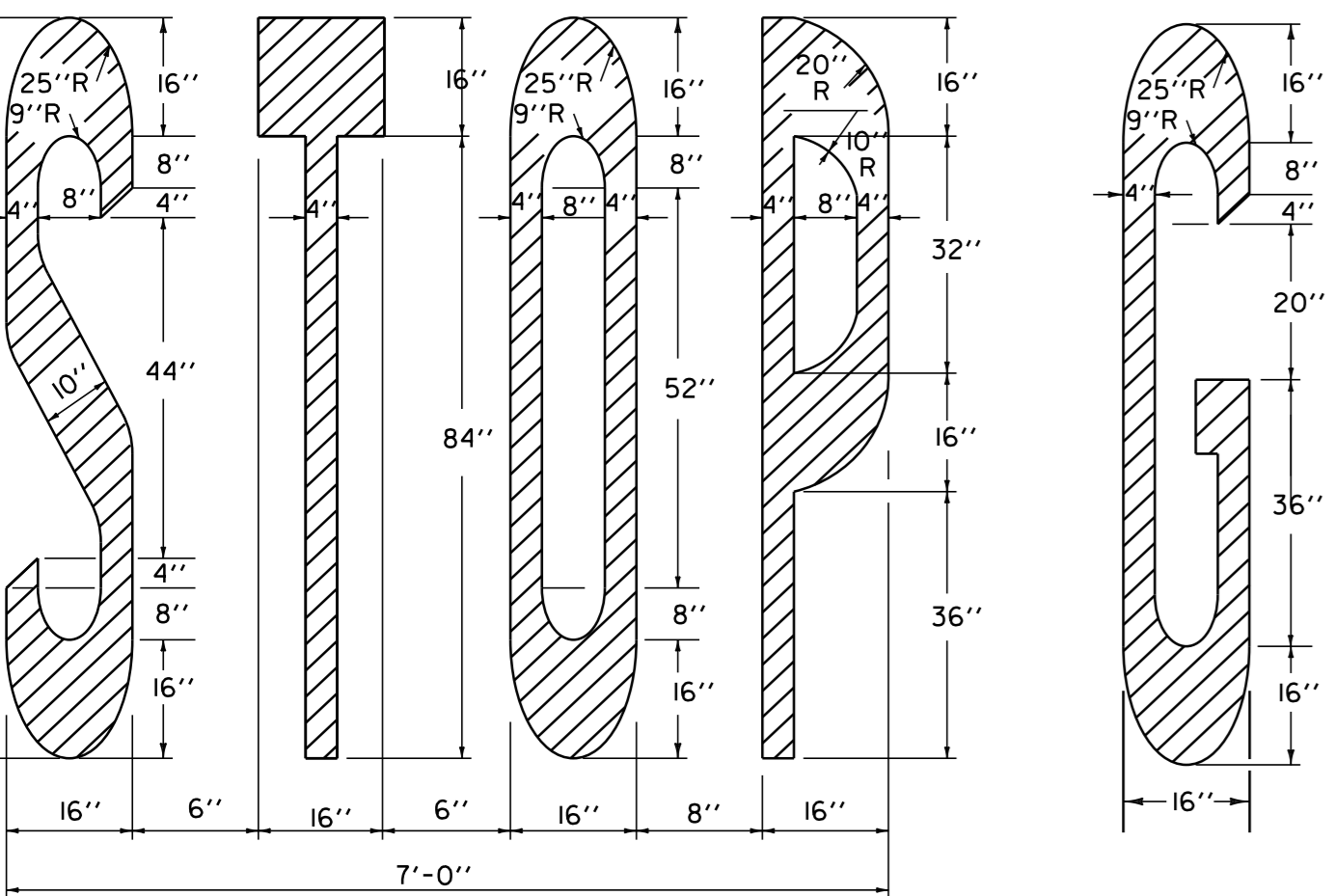
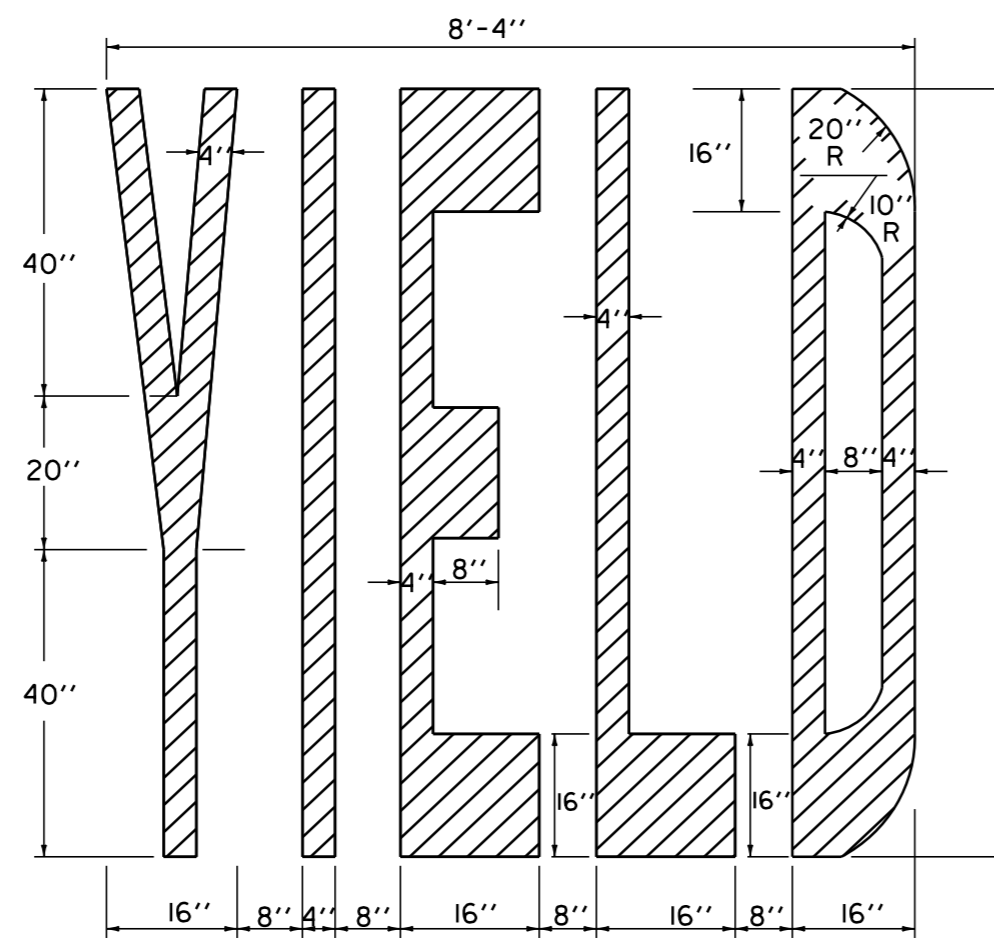
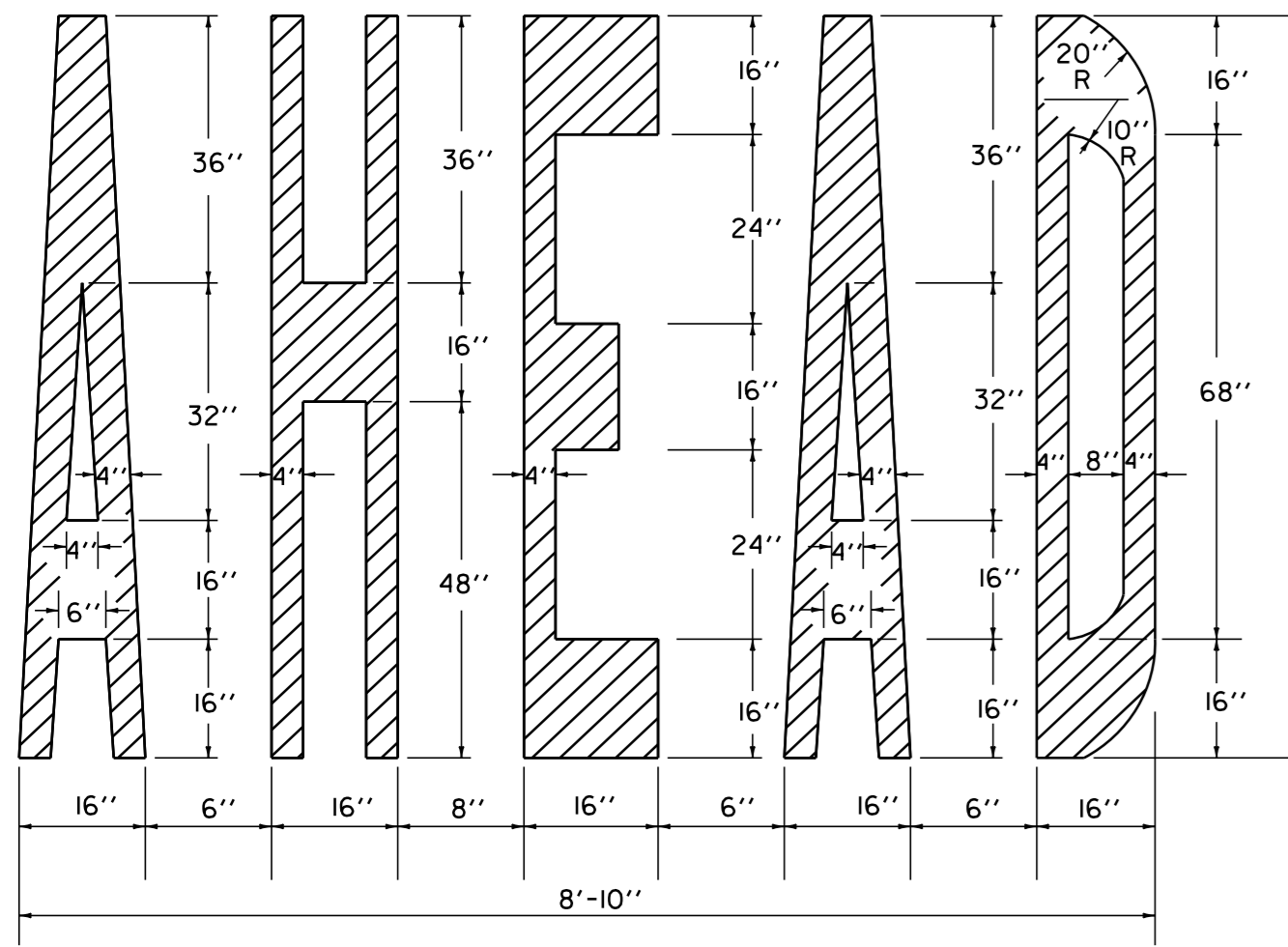
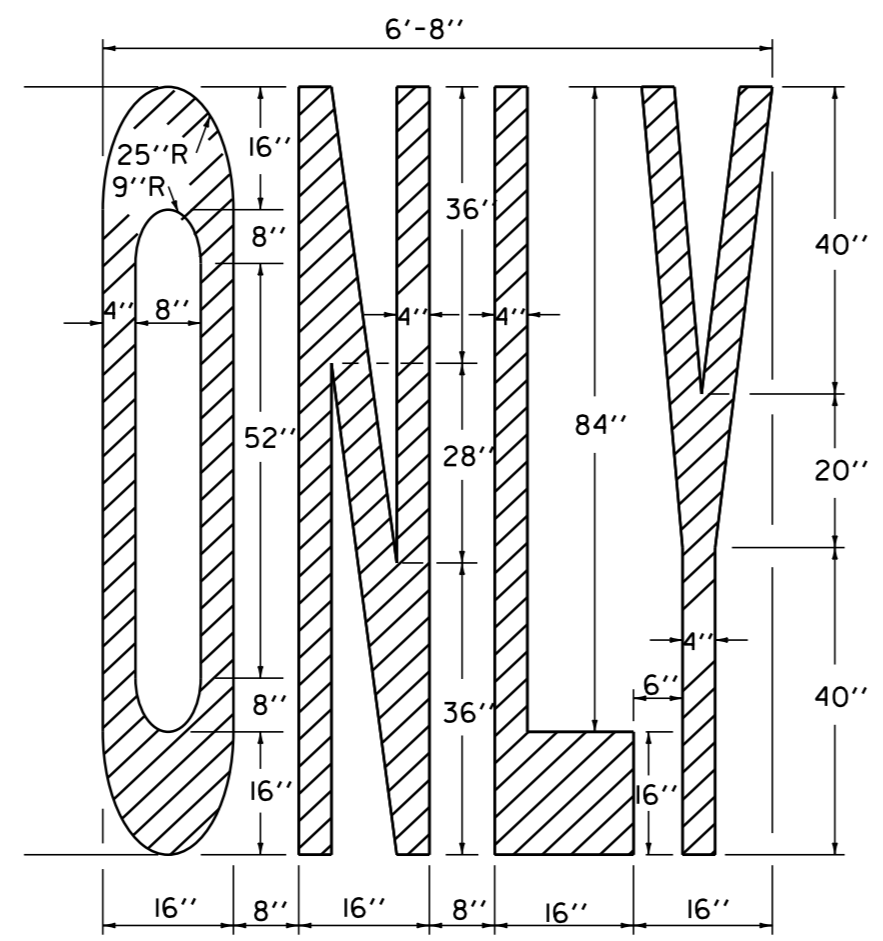
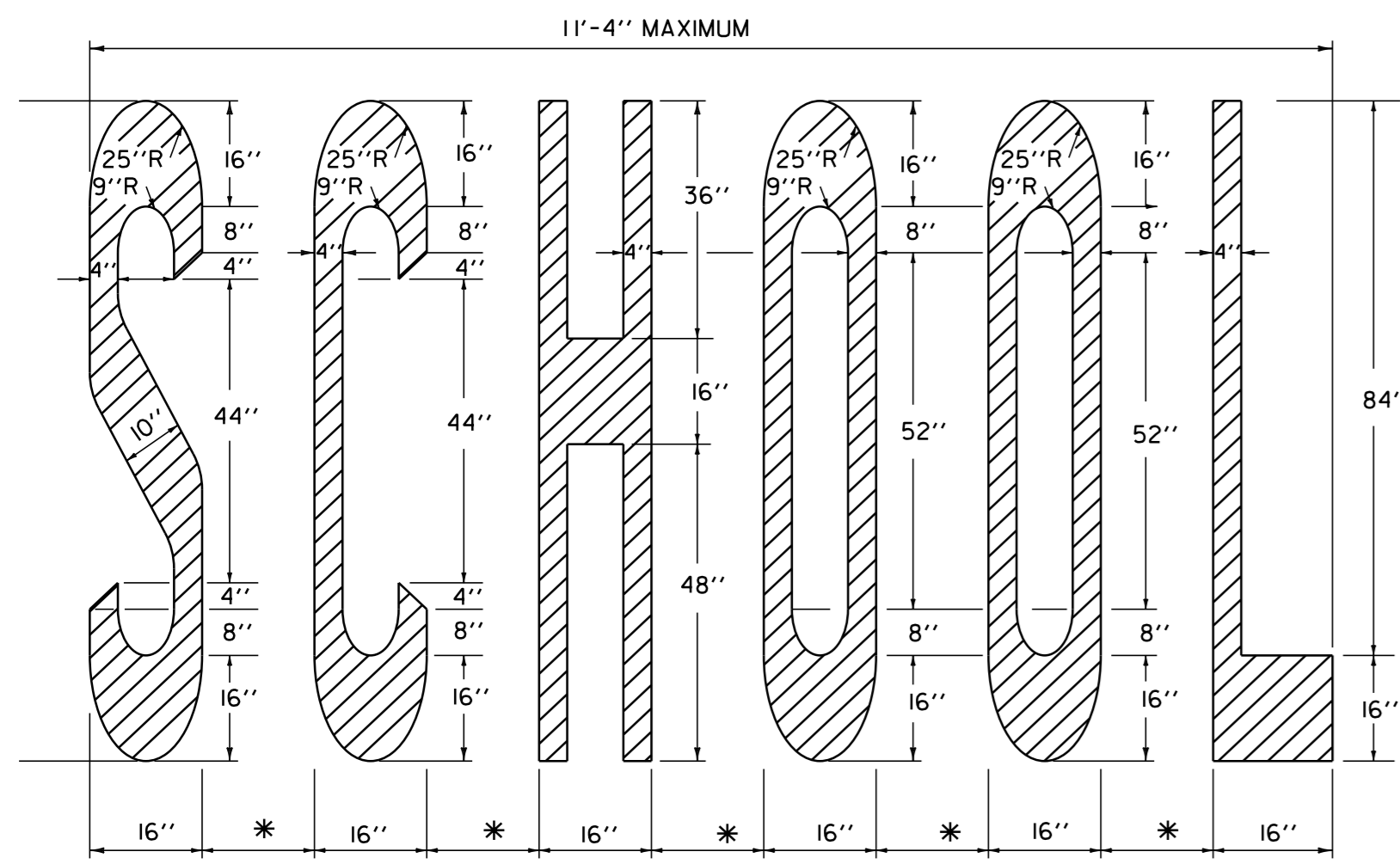
PROJECT MANAGER: A GAMBLE

IPARM FILE NAME: 11b010pml.i

PLOT DATE: 31-MAY-2012 13:11

DRAWN BY: KAS

SHEET: 21 OF 26

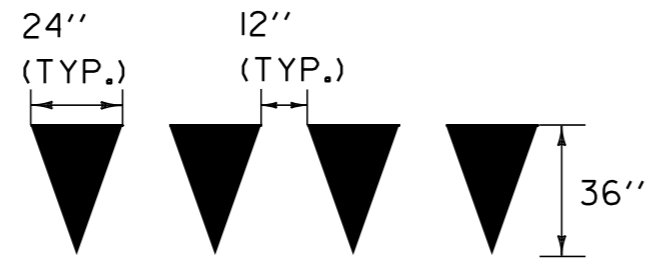


THE LETTER "G" PERTAINS TO THE WORD "SIGNAL" FOR OTHER LETTERS, SEE ABOVE.

**SIGNAL**  
6" LETTER SPACING

**LETTER HEIGHT**

DIMENSIONS ARE FOR 8' - 4" LETTER HEIGHTS. A LESSER HEIGHT OF 8' - 0" IS ACCEPTABLE AS LONG AS THE DIMENSIONS ARE PROPORTIONAL TO THE DETAILS SHOWN. SEE NOTE BELOW.

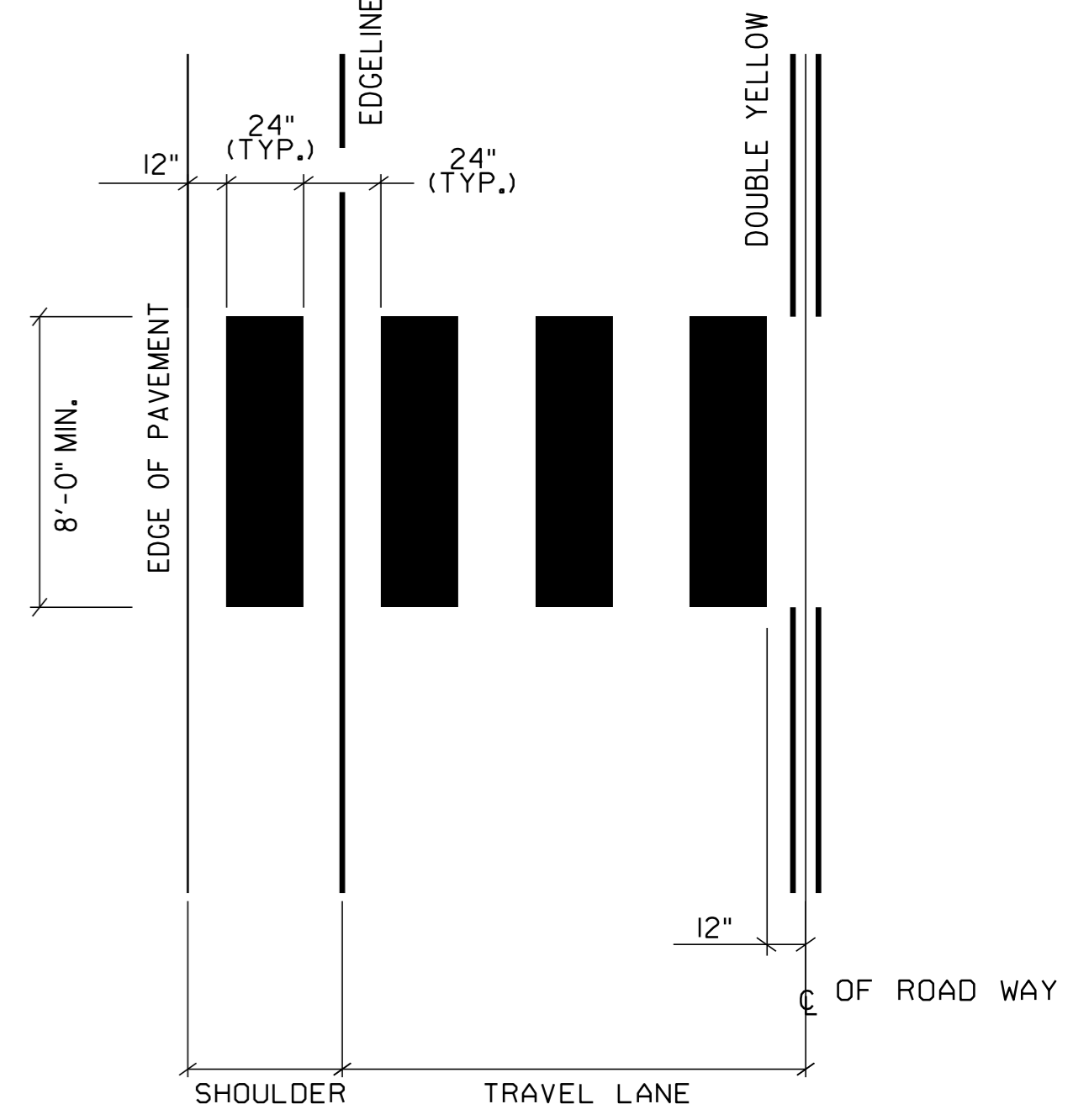
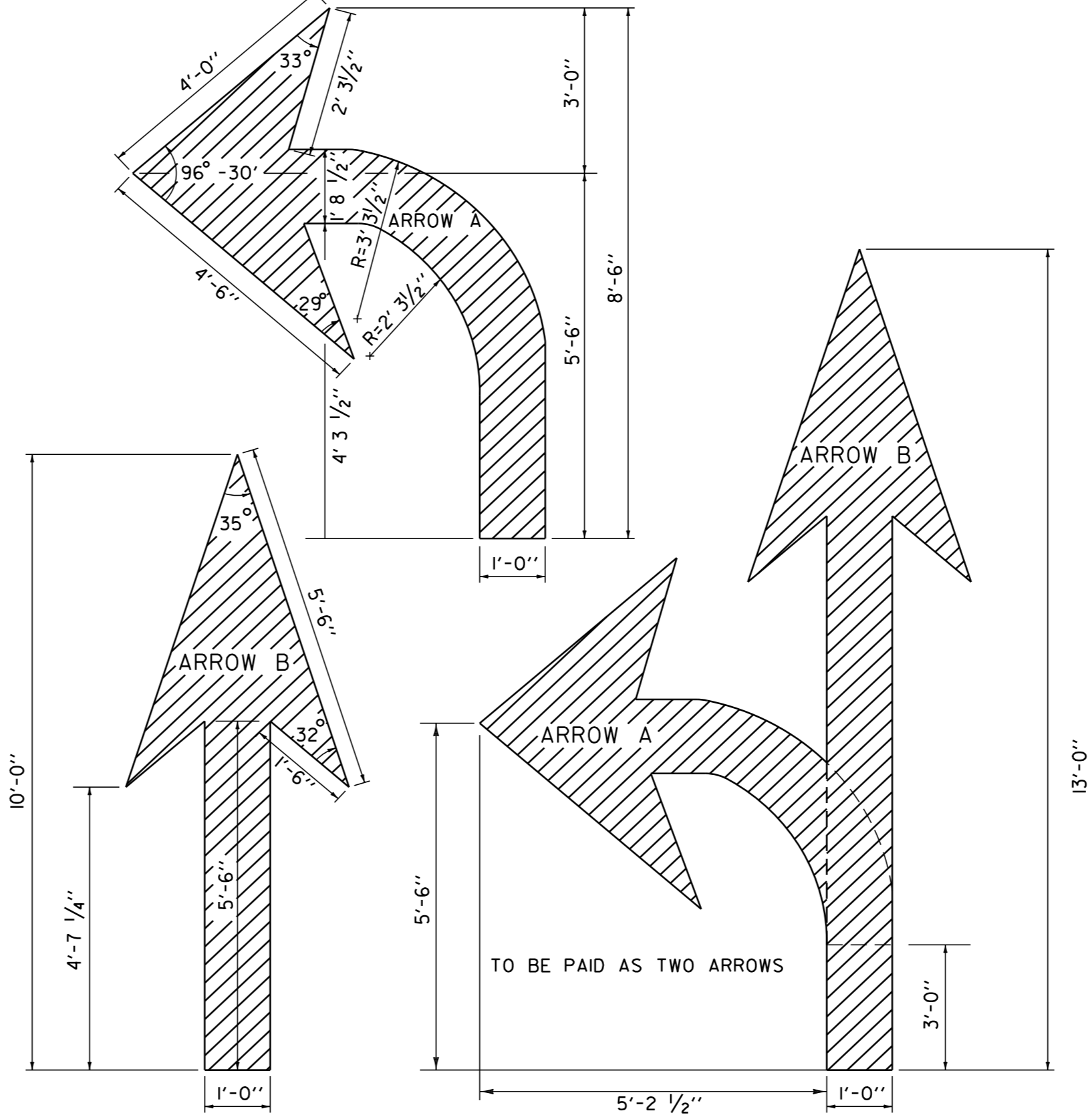


**YIELD LINE DETAILS**

ARROWS AND WORD MARKINGS THAT CONFORM TO THE DIMENSIONS SHOWN ON THIS SHEET OR AS DETAILED IN THE BOOKLET ENTITLED "STANDARD ALPHABETS FOR HIGHWAY SIGNS AND PAVEMENT MARKINGS" AND THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (CURRENT EDITION) PREPARED BY THE FEDERAL HIGHWAY ADMINISTRATION WILL BE ACCEPTABLE.

**LETTER IN WORD MARKING AND YIELD LINE DETAILS**

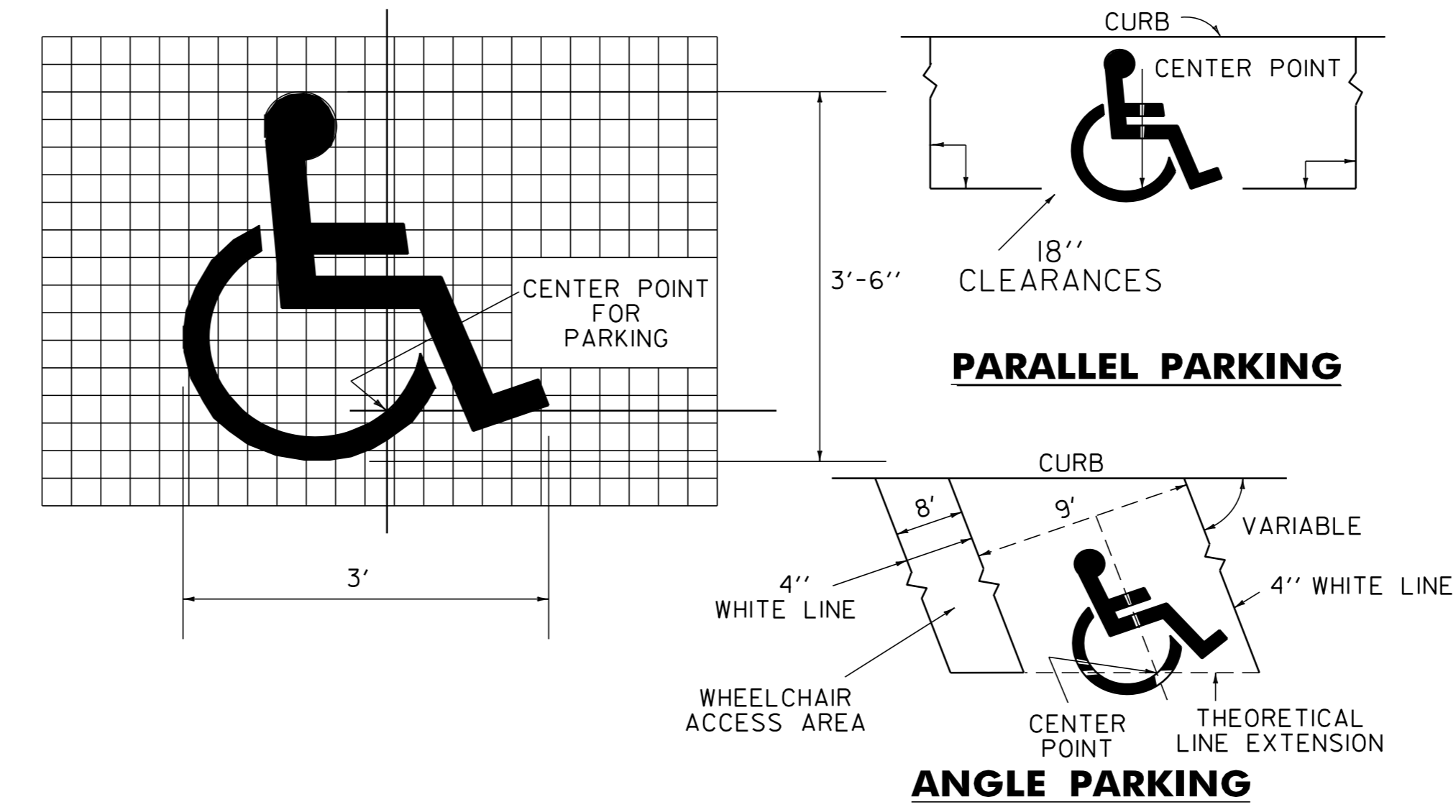
**ARROW DETAILS**



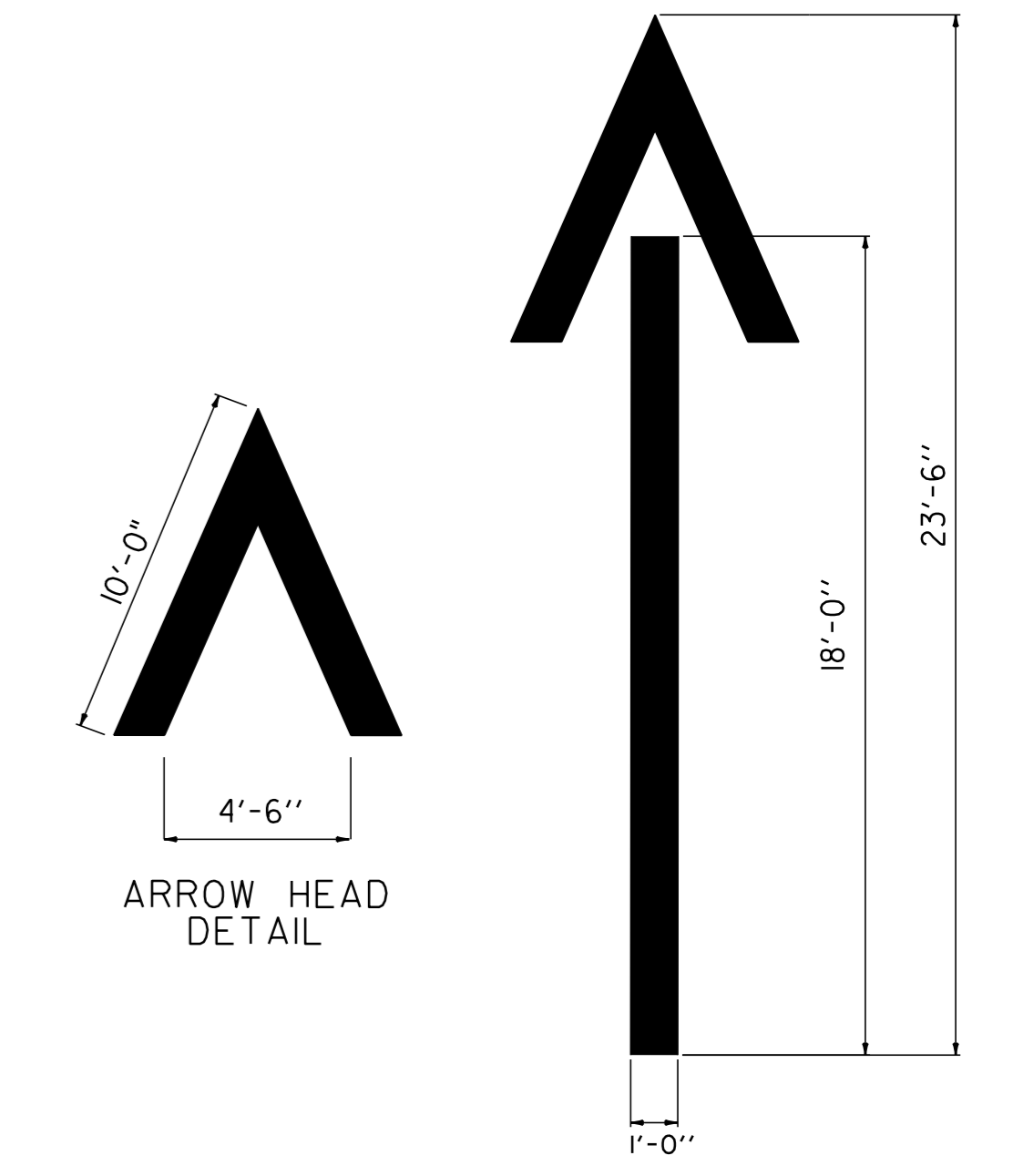
ADJUST SPACING (1 - 2 FT) TO AVOID WHEEL PATHS

**BLOCK PATTERN CROSSWALK DETAIL**

NOTES:  
THE BLOCK PATTERN IS THE VTRANS PREFERRED CROSSWALK MARKINGS. SEE MUTCD FOR APPROVED OPTIONS.



**HANDICAPPED PAVEMENT MARKING DETAILS**



**WRONG WAY ARROW**

**PAVEMENT MARKING DETAILS**

NOT TO SCALE

<b>PAVEMENT MARKING DETAILS SHEET 2</b>	PROJECT NAME: <b>STATEWIDE</b>	PLOT DATE: 31-MAY-2012 13:11
	PROJECT NO.: <b>NHG MARK (205)</b>	DRAWN BY: KAS
	DESIGN FILE NAME: I1b010.dgn	SHEET: 22 OF 26
	PROJECT MANAGER: A GAMBLE	
	IPARM FILE NAME: I1b010pm2.1	

**NOTE:**

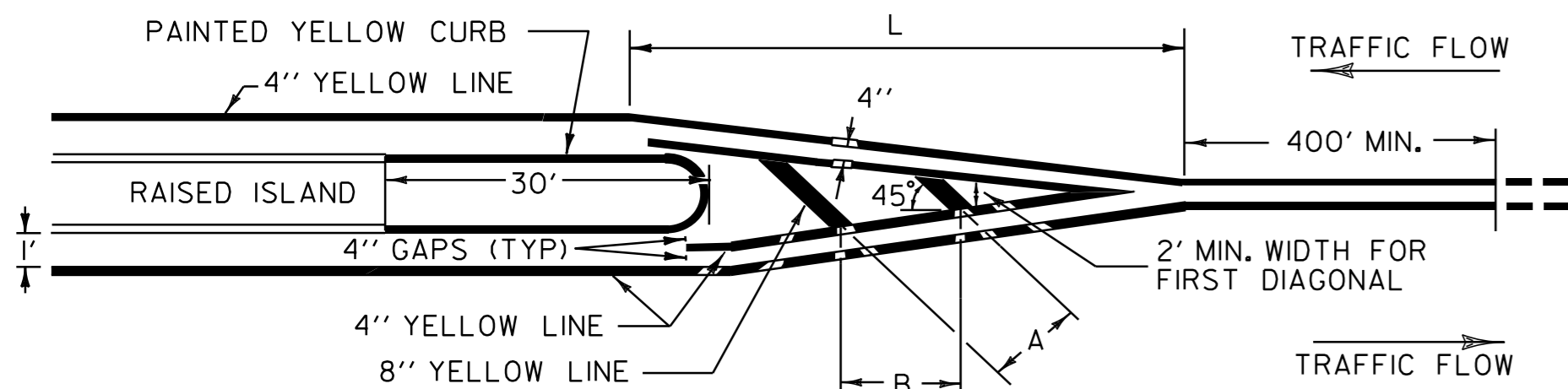
A SOLID LINE IN THE DIRECTION OF TRAVEL BEGINS AT A LOCATION 400 FEET MIN. IN ADVANCE OF THE BEGIN TAPER FOR THE PAVEMENT WIDTH TRANSITION.

PASSING ZONE SHOWN FOR REFERENCE ONLY, ACTUAL MARKINGS BASED ON FIELD CONDITIONS.

$$L = \frac{WS^2}{60}$$

$$L = WS \text{ WHEN } S = 45 \text{ MPH OR GREATER}$$

EDGELINES SHALL BE 1' - 0" MIN. FROM CURB



**RAISED ISLAND APPROACH MARKINGS**

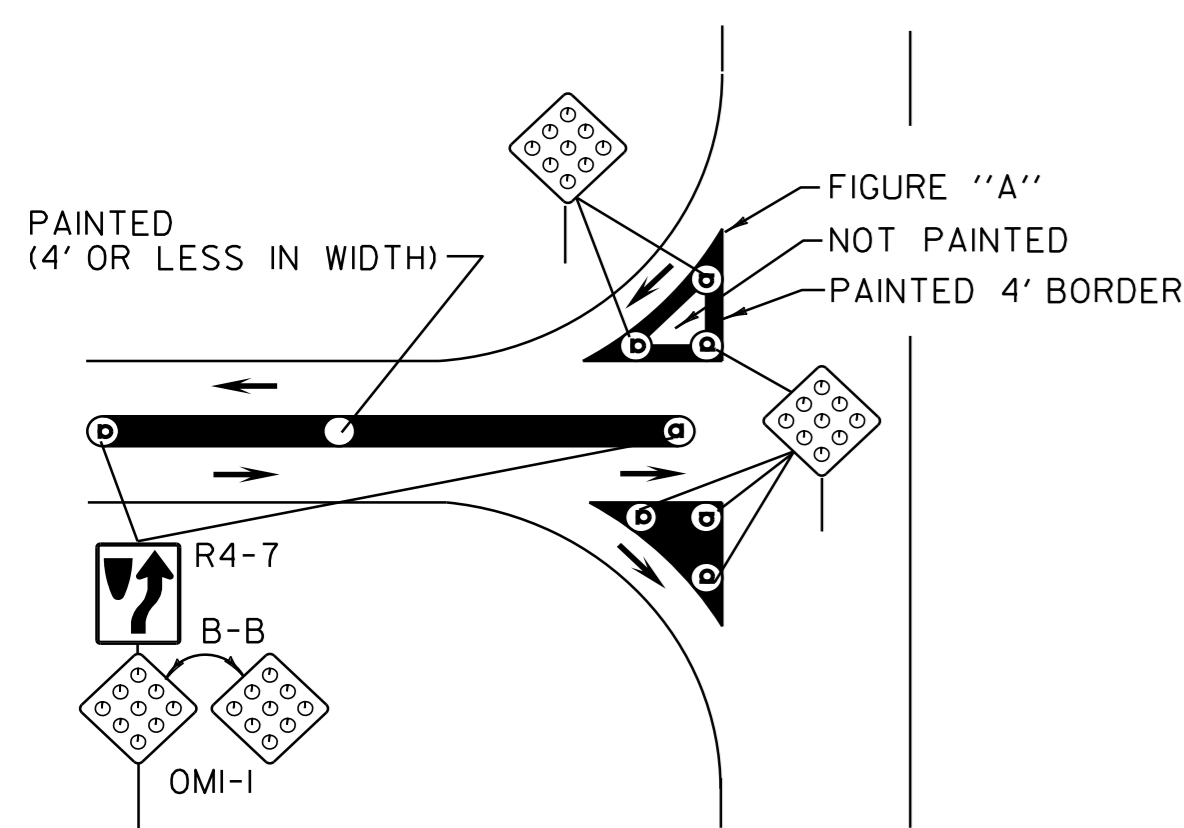
DIMENSIONS		CONDITIONS FOR USE	
A	B		
5'	7'	LOW SPEED OR HIGH SPEED/W POOR SIGHT DISTANCE, <200'	WHERE LENGTH OF DIAGONAL AREA IS 75' MAX.
10'	14'	HIGH SPEED AND GOOD SIGHT DISTANCE, ≥200'	WHERE LENGTH OF DIAGONAL AREA EXCEEDS 75'

THE FOLLOWING GUIDELINES WILL BE USED WHEN PAINTING ISLANDS ON STATE HIGHWAYS AND RECOMMENDED FOR ISLANDS ON OTHER SYSTEMS WITHIN THE STATE.

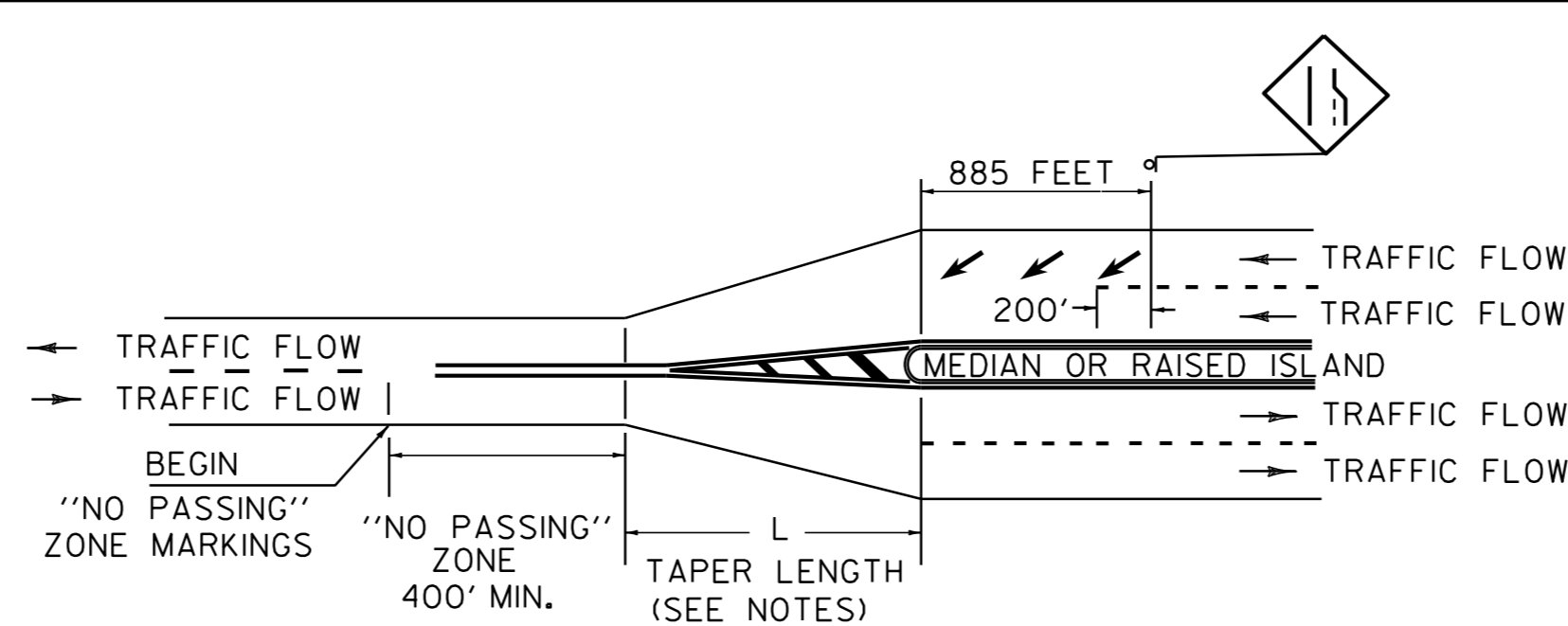
I. PAINTING: ISLANDS WHICH ARE FOUR FEET OR LESS IN WIDTH AND TRIANGULAR ISLANDS WHICH ARE LESS THAN FOUR HUNDRED AND FIFTY SQUARE FEET SHALL BE PAINTED ENTIRELY. TRIANGULAR ISLANDS WHICH ARE FOUR HUNDRED AND FIFTY SQUARE FEET AND LARGER SHALL BE PAINTED WITH A FOUR FOOT BORDER AS SHOWN ON FIGURE "A" BELOW.

II. SIGNS: ALL APPROACH NOSES TO THE ISLANDS IN THE LINE OF TRAFFIC WILL BE SIGNED WITH A KEEP RIGHT (R4-7) SIGN AS WELL AS OBJECT MARKERS (OMI-1) PLACED BACK TO BACK TO INDICATE THE BEGINNING (END) OF THE ISLAND.

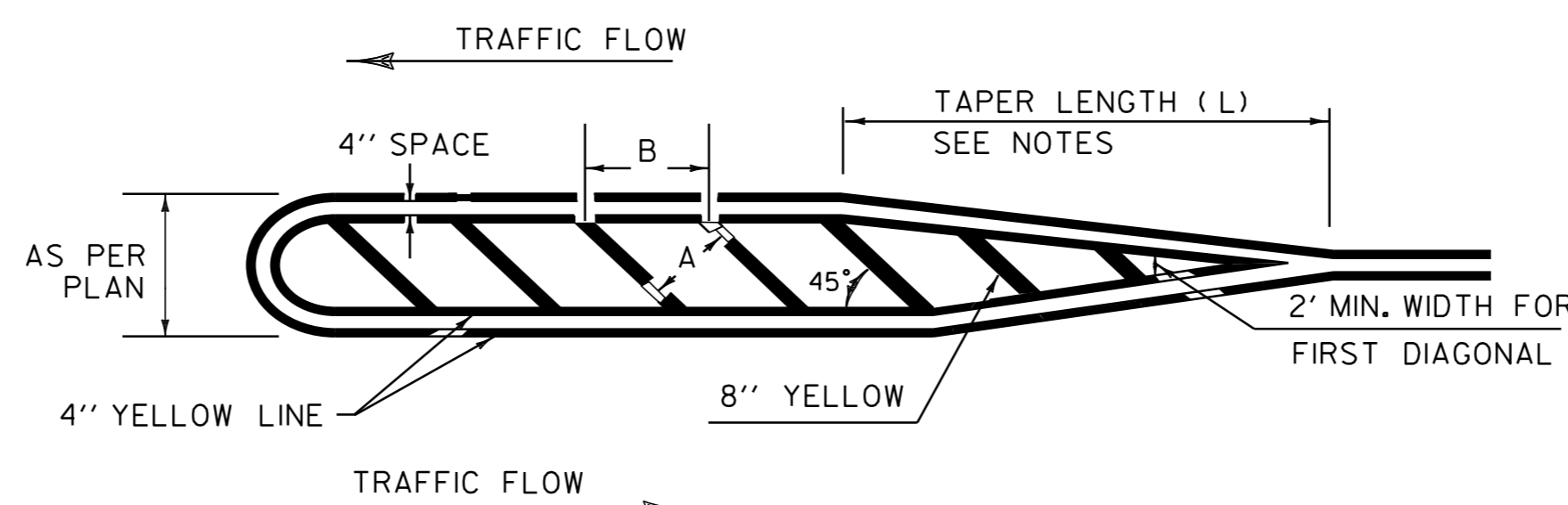
III. EXCEPTIONS: THERE WILL BE SPECIAL CASES WHICH REQUIRE AN EXCEPTION TO, OR MODIFICATION OF THIS GUIDELINE. THESE QUESTIONS SHOULD BE REFERRED TO THE TRAFFIC OPERATIONS ENGINEER OR DELEGATED REPRESENTATIVE.



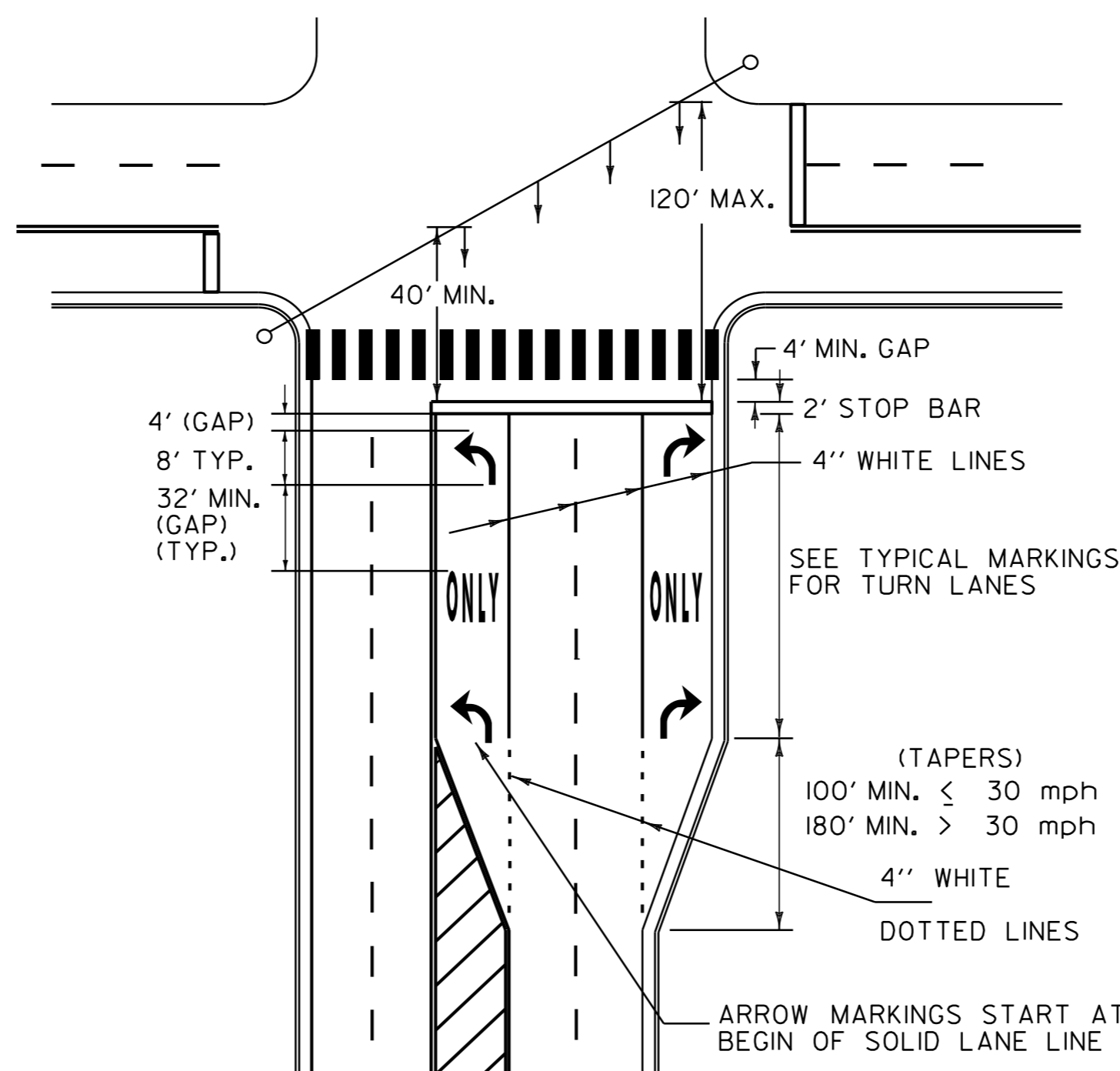
**PAINTED RAISED ISLANDS**



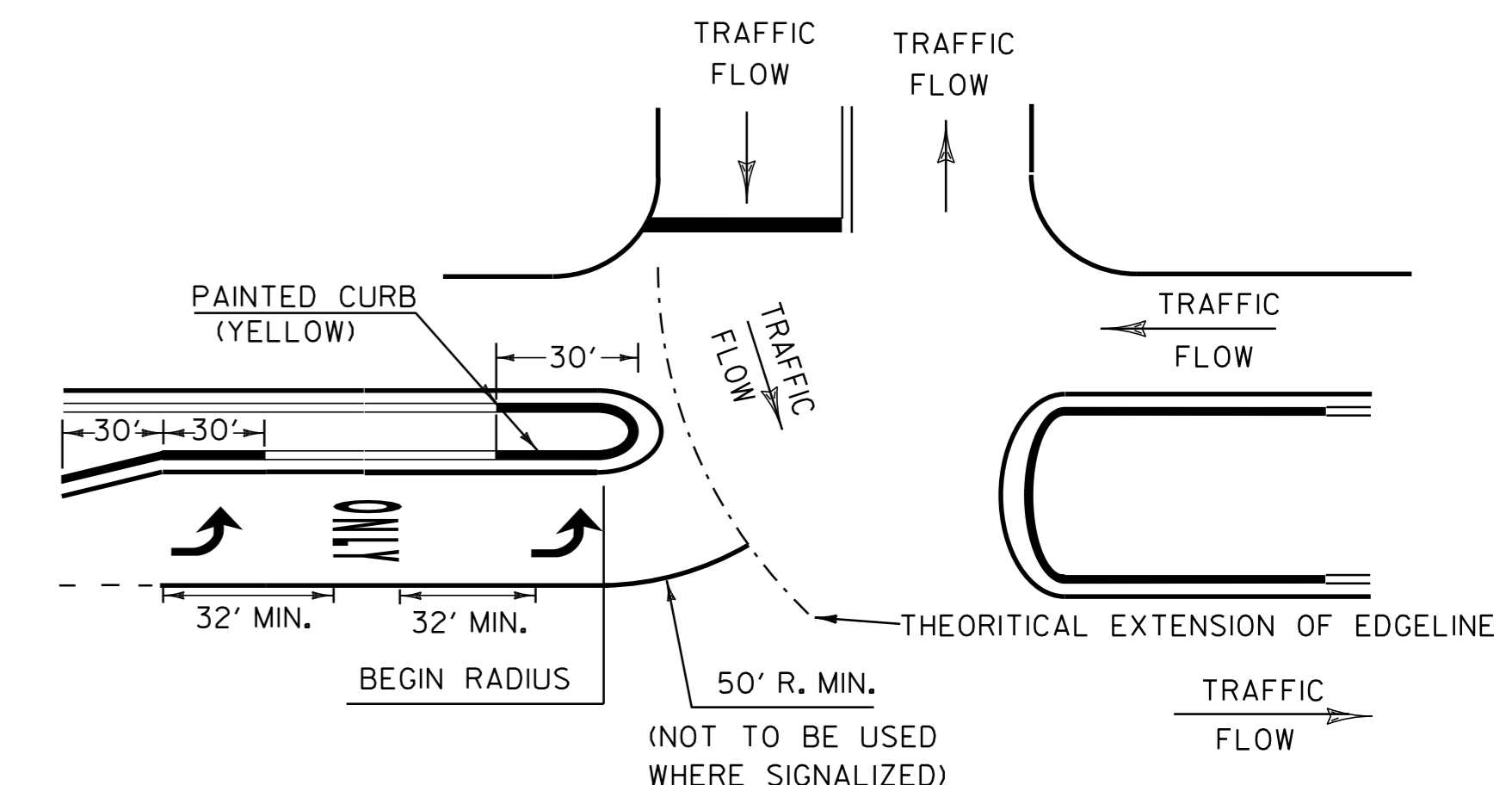
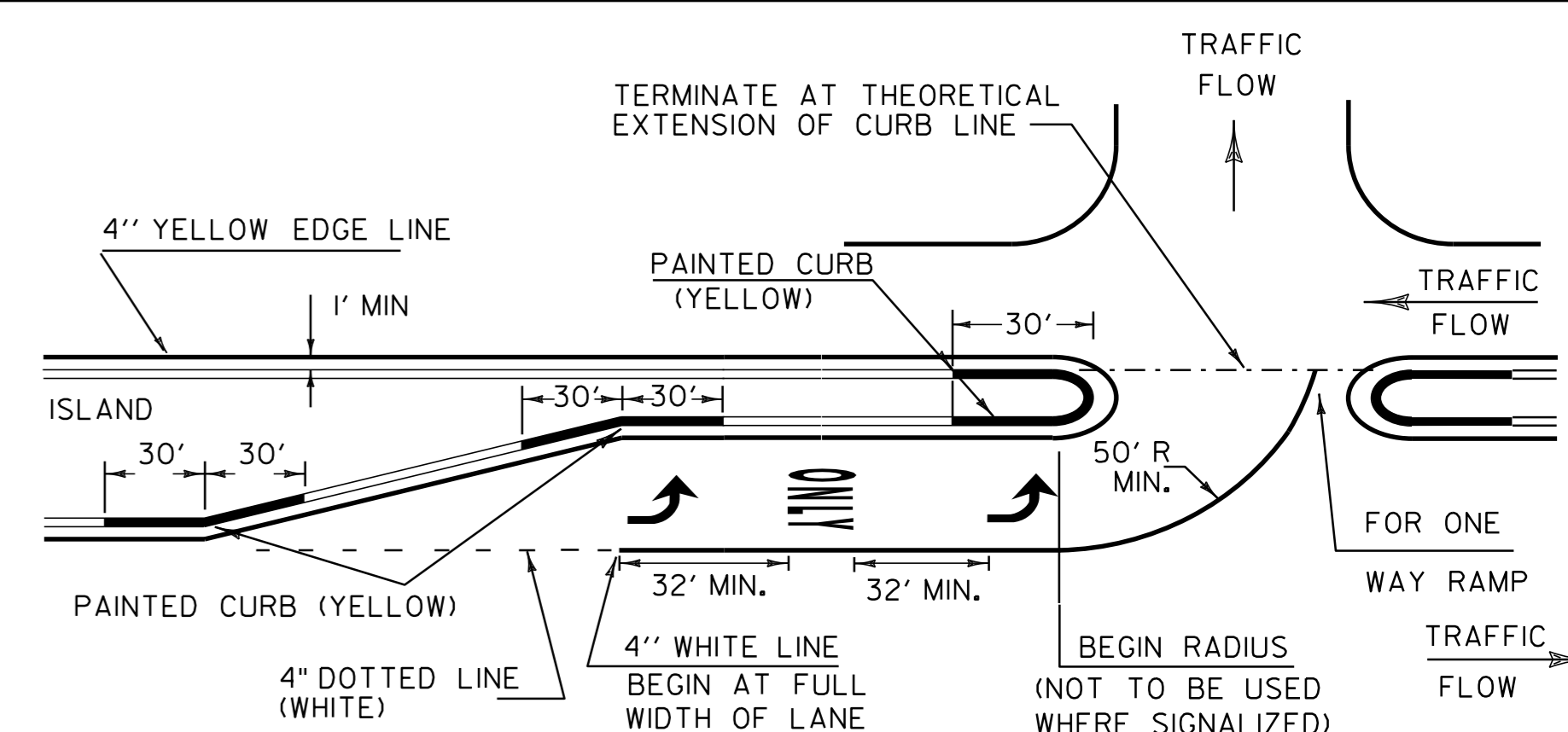
**"NO PASSING" ZONE LAYOUT IN PAVEMENT WIDTH TRANSITIONS, TWO LANE HIGHWAY TO DIVIDED HIGHWAY**



**MEDIAN ISLAND DETAIL**



**TYPICAL MARKINGS FOR SIGNALIZED INTERSECTION**



CURB SHALL BE PAINTED ON THE ISLAND NOSE AND TO A POINT 30 FEET FROM THE END OF THE ISLAND. IN ADDITION, CURB SHALL BE PAINTED 30 FEET FROM ANY CHANGE IN CURB DIRECTION.

**TURN LANE AND PAINTED CURB DETAIL**

**TYPICAL MARKINGS FOR TURN LANES**

EXCLUSIVE TURN (LEFT OR RIGHT) LANE(S) LINES SHALL BE SOLID AND EXTEND BACK FROM THE STOP LINE TO THE POINT OF FULL LANE WIDTH OF THE TURN LANE.

FOR DESIGN OF A LEFT OR RIGHT TURN LANE(S) REFER TO VTRANS "GUIDELINE FOR DETERMINING STORAGE, TAPER AND DECELERATION LENGTHS FOR LEFT AND RIGHT-TURN LANES AT INTERSECTIONS".

LEGEND(S) SHALL START AT THE BEGINNING OF SOLID LANE LINE.

THE FIRST AND LAST SYMBOLS SHALL BE LANE USE ARROWS. THE "ONLY" WORD MARKING SHALL BE USED TO SUPPLEMENT LANE-USE ARROW MARKINGS.

IF LANE LENGTH IS LESS THAN 50 FEET ONLY ONE TURN ARROW PLACED AT THE BEGINNING OF THE SOLID LANE LINE IS REQUIRED.

THE LONGITUDINAL SPACE BETWEEN LEGENDS SHOULD NOT BE MORE THAN TEN TIMES THE HEIGHT OF THE CHARACTERS UNDER ANY CONDITIONS.

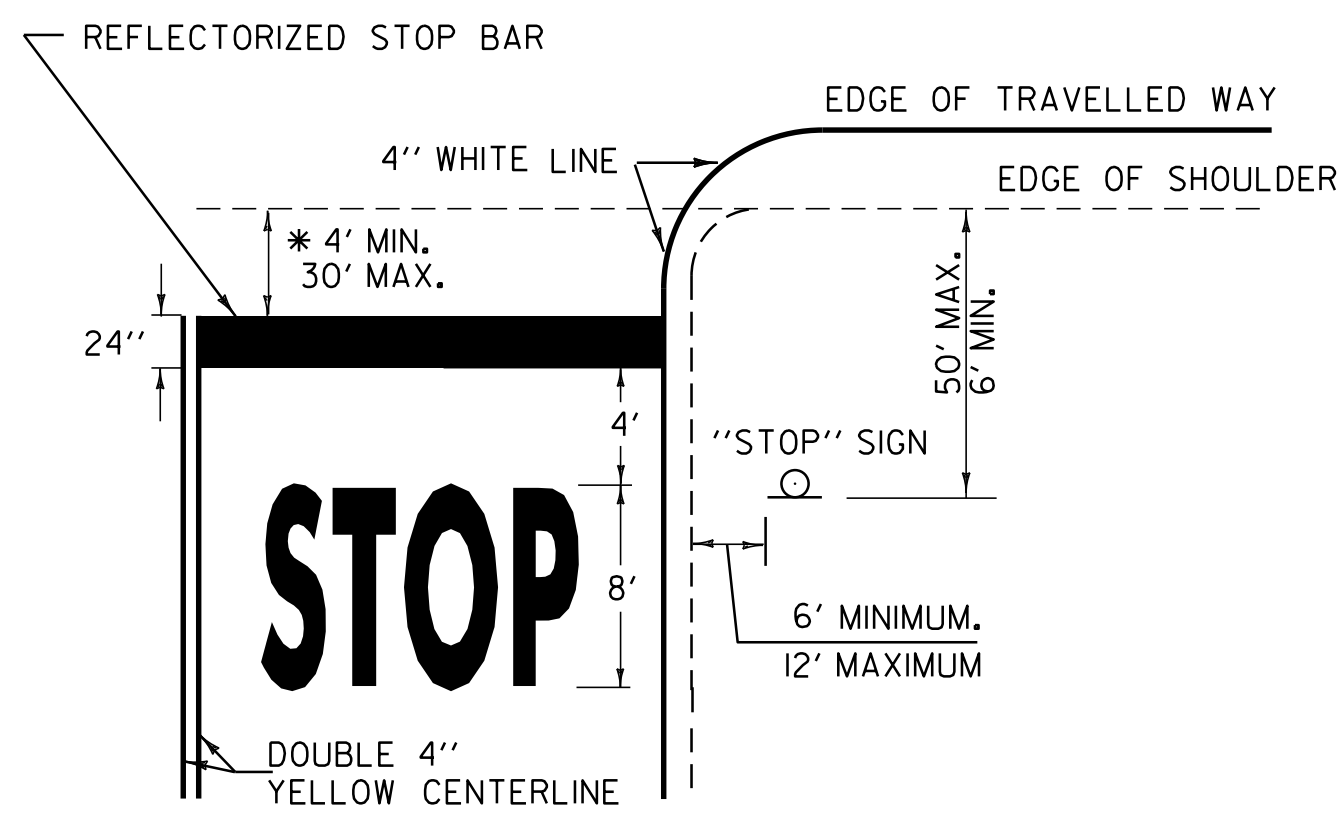
A SECOND "ONLY" IS OPTIONAL WHEN SPACE PERMITS. MARKINGS START AT THE BEGINNING OF THE SOLID LANE LINE.

NOT TO SCALE

**PAVEMENT MARKING DETAILS**

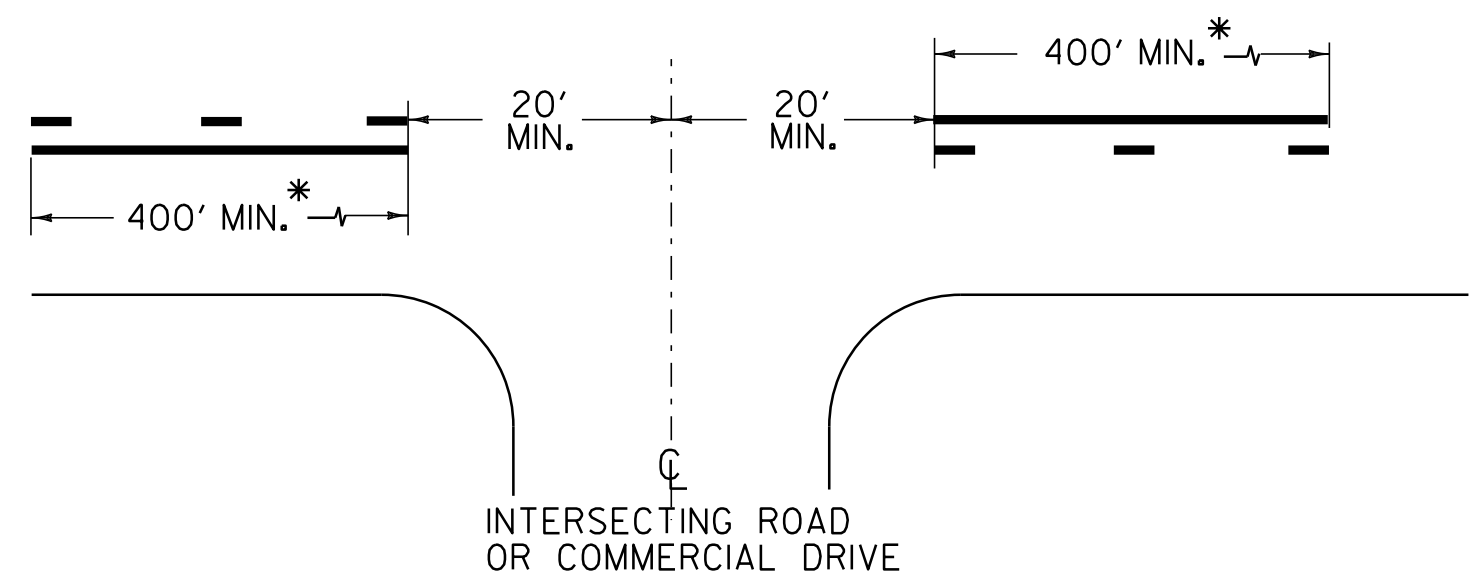
**PAVEMENT MARKING DETAILS SHEET 3**

PROJECT NAME: <b>STATEWIDE</b>	
PROJECT NO.: <b>NHG MARK (205)</b>	
DESIGN FILE NAME: 11b010.dgn	PLOT DATE: 31-MAY-2012 13:11
PROJECT MANAGER: A GAMBLE	DRAWN BY: KAS
IPARM FILE NAME: 11b010pm3.i	SHEET: 23 OF 26



\* THE "DESIRED STOPPING POINT" IS THE LOCATION BASED ON SITE CONDITIONS THAT BEST ALLOWS THE STOPPED VEHICLE TO VIEW THE APPROACHING TRAFFIC.

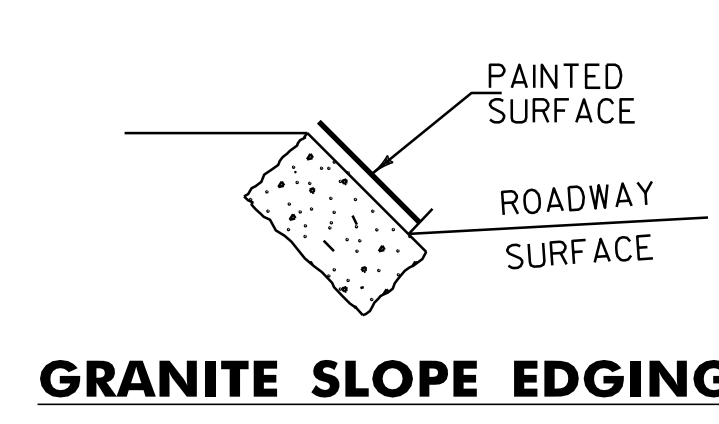
**STOP BAR LAYOUT**



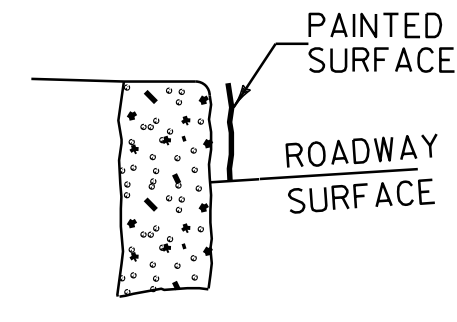
\* THE SOLID LINE SHALL BE PAIRED WITH EITHER A SOLID OR DASHED LINE DEPENDING ON SIGHT DISTANCE AVAILABILITY IN THE OPPOSING DIRECTION. ADJUSTMENTS TO THE 40 FOOT CENTERLINE OPENING MAY BE MADE TO ACCOMMODATE SKEWED INTERSECTIONS.

- CENTERLINE BREAKS:
- A. AT ALL STATE HIGHWAYS AND TOWN HIGHWAYS THAT HAVE STOP AND LEGAL LOAD LIMIT SIGNS INSTALLED.
  - B. COMMERCIAL DRIVES:
    1. WHERE A SEPERATE TURN LANE EXISTS ON THE MAIN LINE (LT. OR RT.)
    2. SIGNIFICANT TRAFFIC VOLUMES EXISTS.

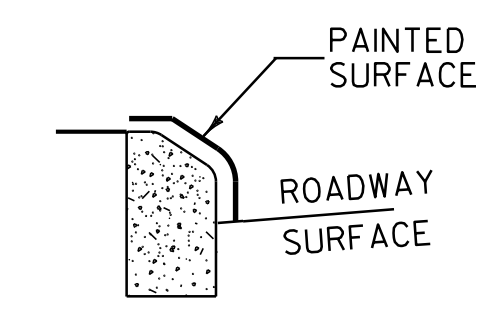
**CENTERLINE LAYOUT**



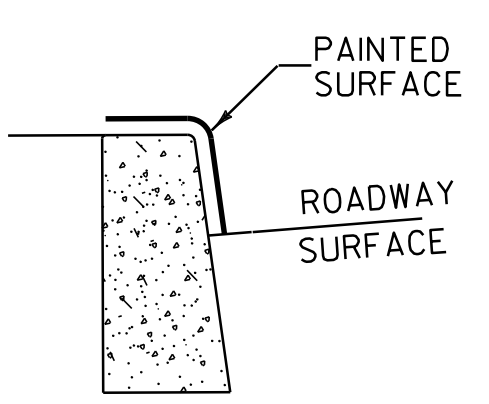
**GRANITE SLOPE EDGING**



**VERTICAL GRANITE CURB**

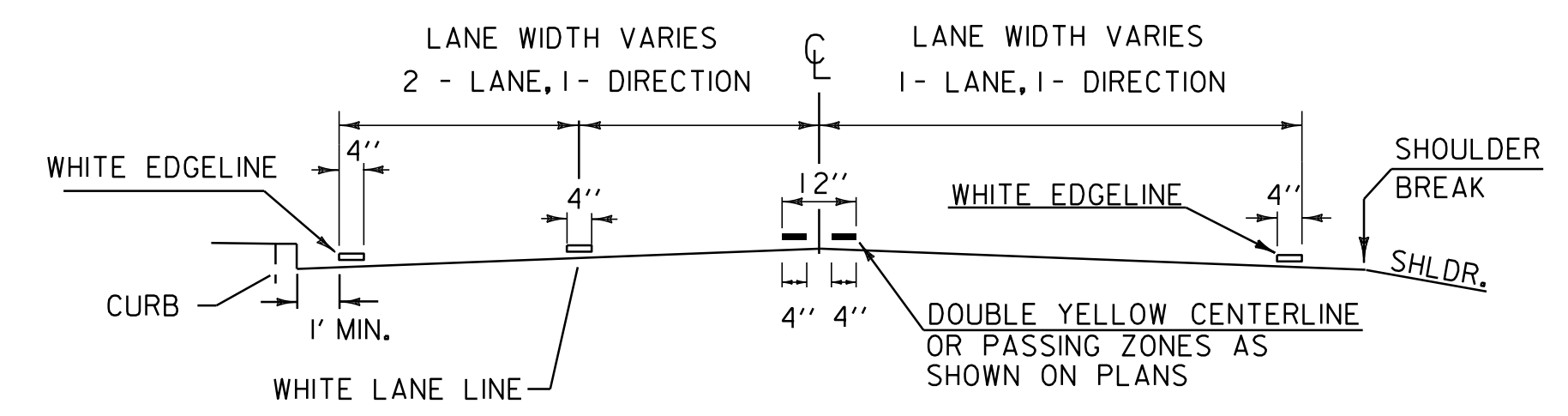


TYPE A (CONCRETE)

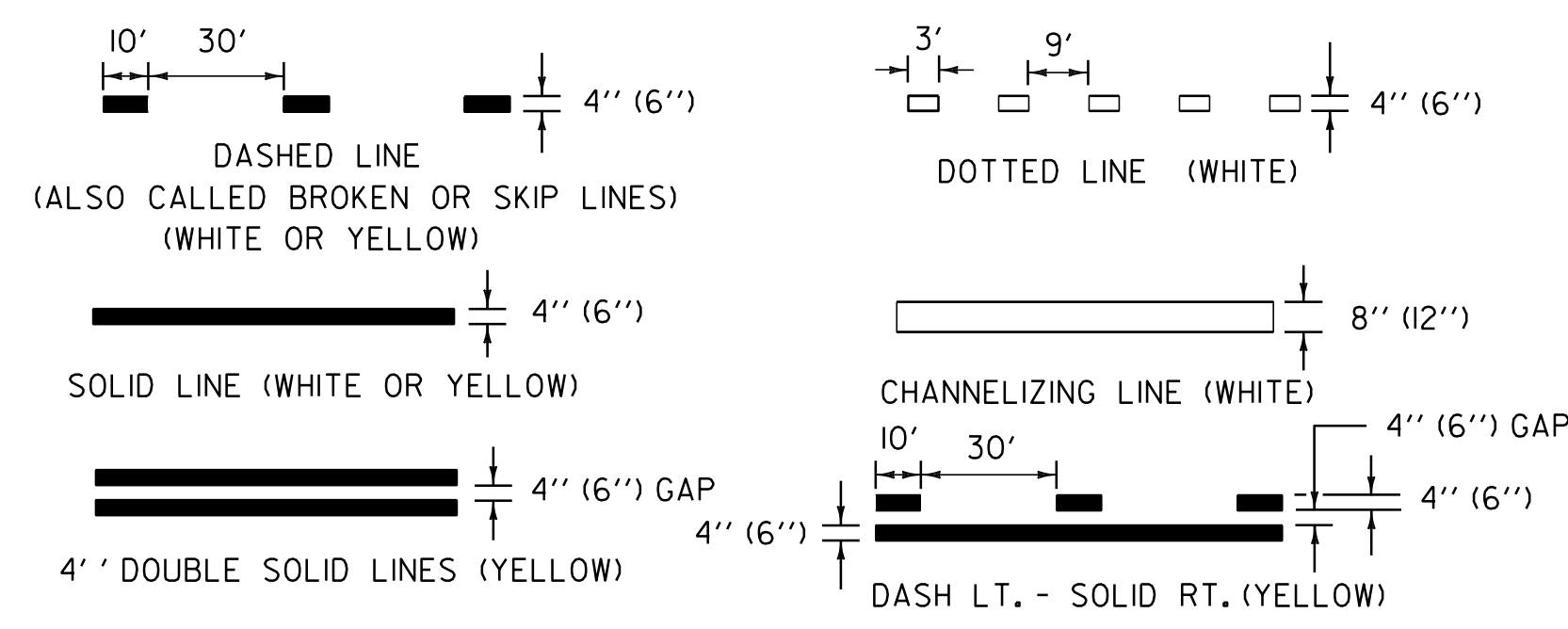


TYPE B (CONCRETE)

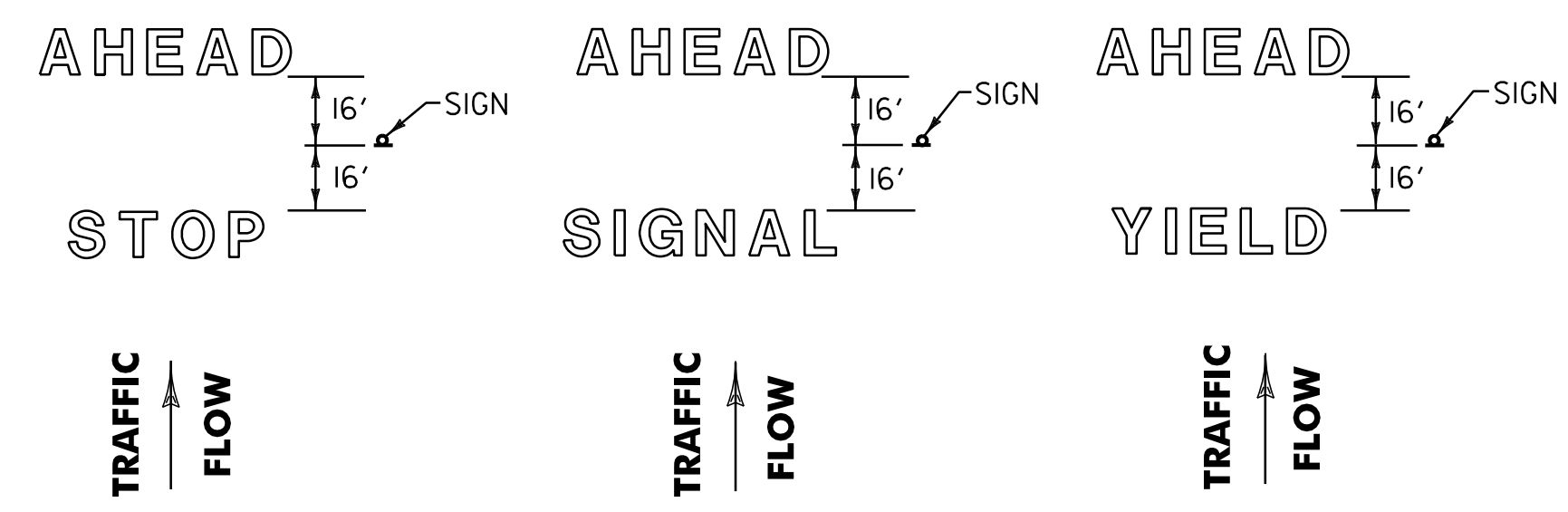
**PAINTED CURB**



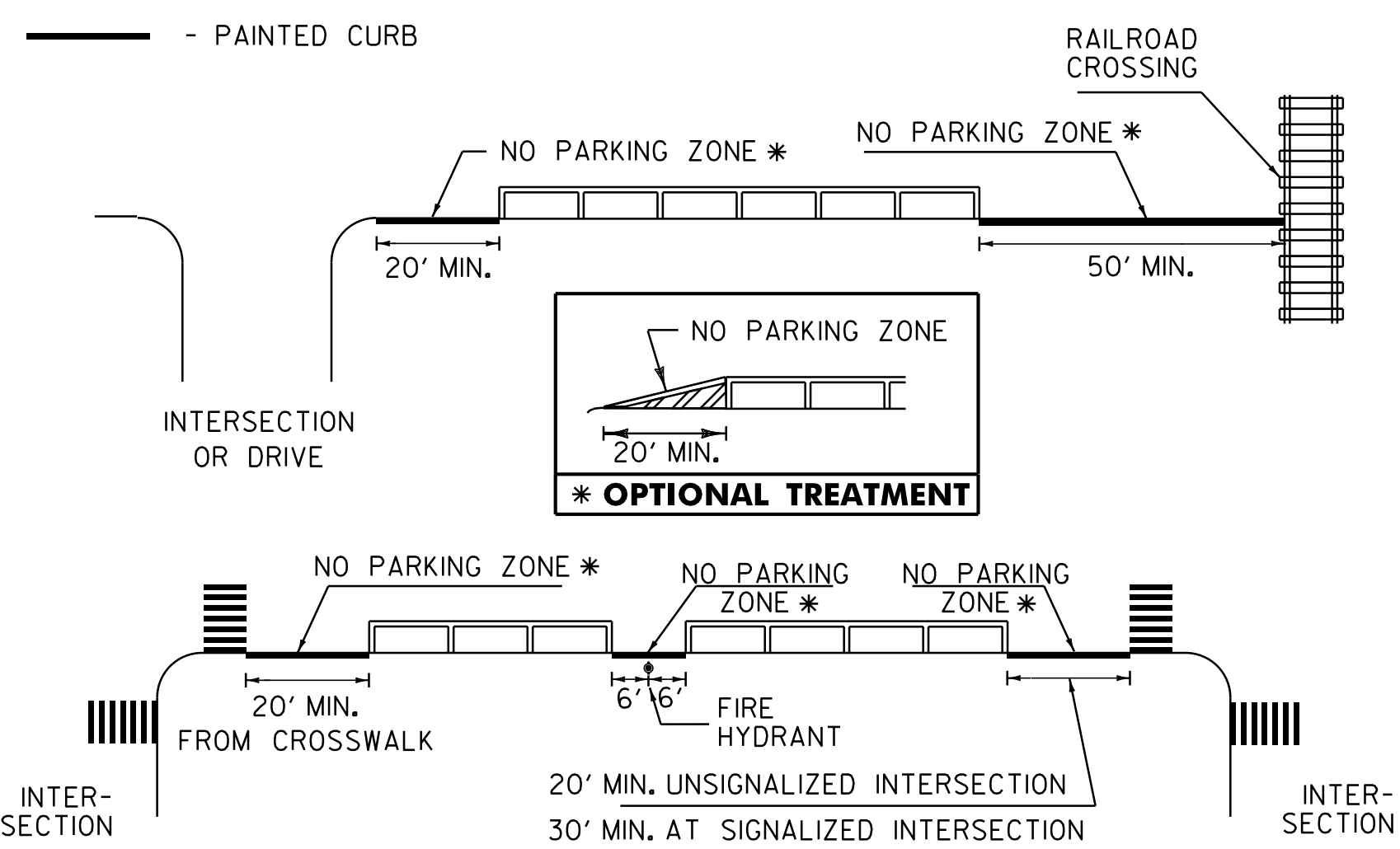
**PAVEMENT MARKING PLACEMENT DETAIL**



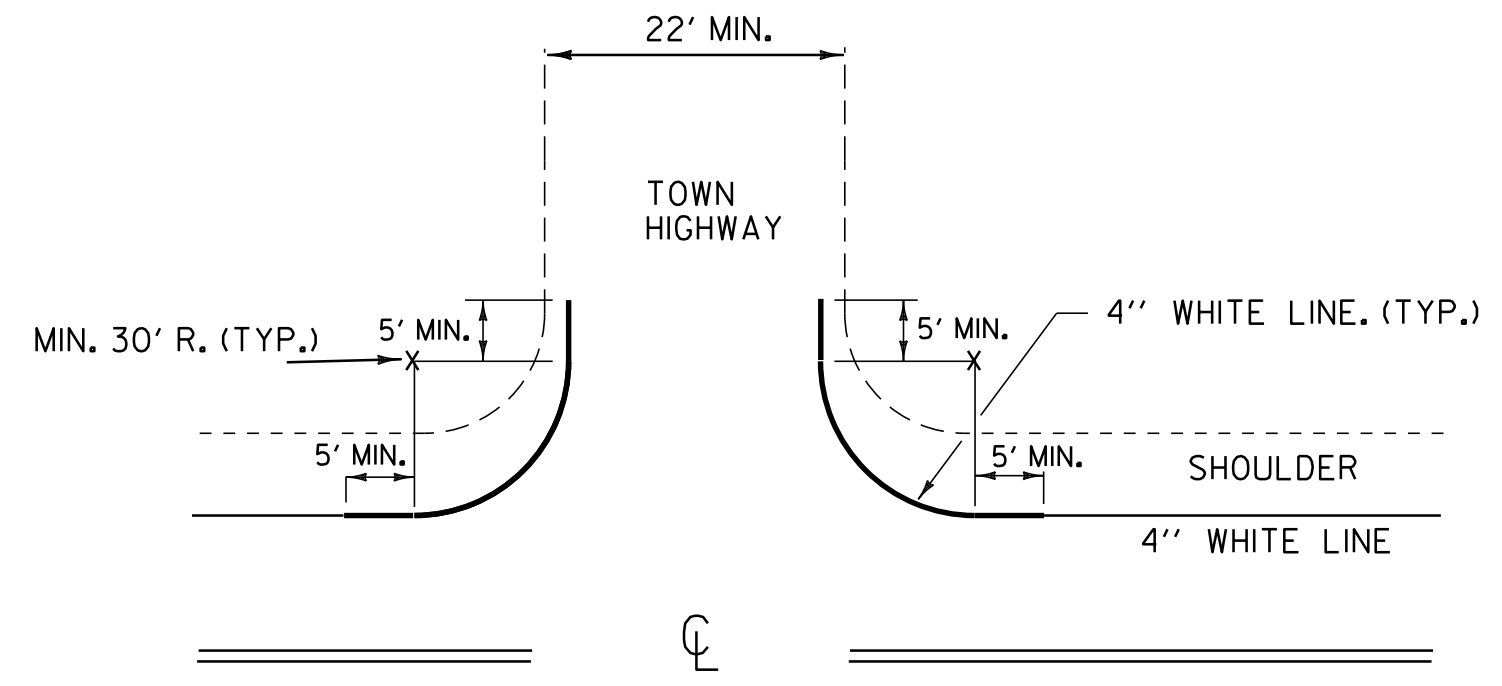
**PAVEMENT MARKING LINE DETAILS (X) = INTERSTATE OR LIMITED ACCESS**



**LETTER IN WORD MARKING SPACING DETAIL NOTE: SINGLE WORDS CENTERED ON SIGN ie: SCHOOL OR YIELD**



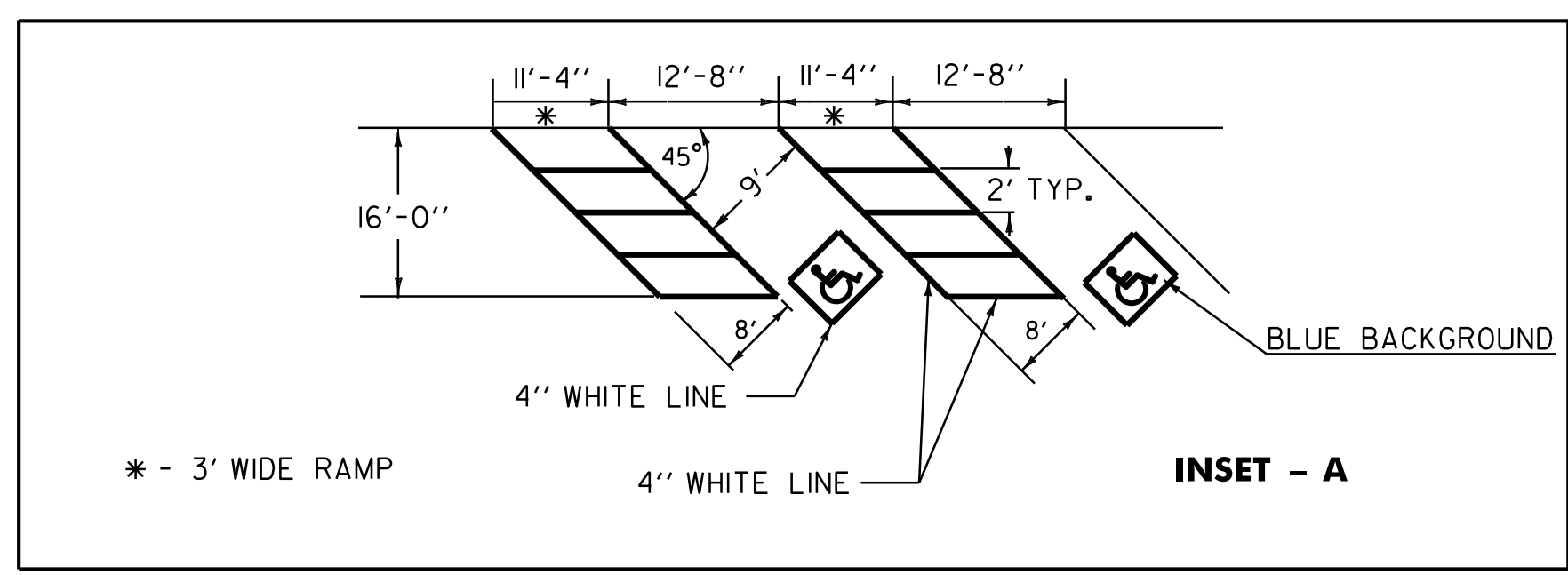
**NO PARKING LAYOUT DETAILS**



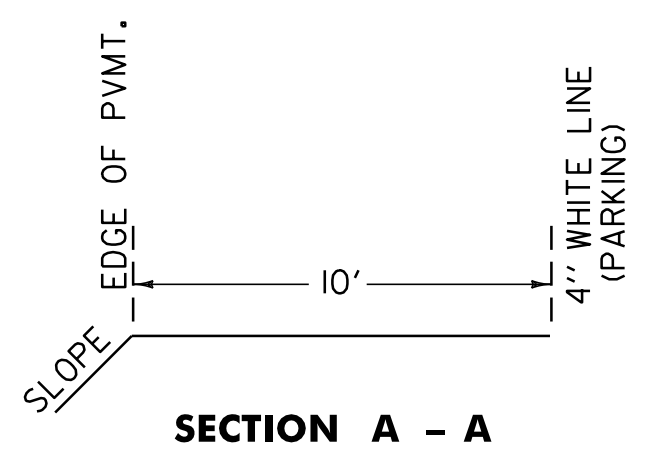
EDGE LINES SHALL BE APPLIED TO ALL STATE HIGHWAYS AND SHOULD BE MAINTAINED AT A CONSTANT DISTANCE FROM THE CENTERLINE UNLESS PAVEMENT WIDTH INCREASES TO ALLOW WIDER LANES.

APPLY EDGE LINE AS DETAILED ON ALL PAVED CLASS 1 & CLASS 2 TOWN HIGHWAYS AND ANY PAVED CLASS 3 TOWN HIGHWAY 22 FEET OR MORE IN WIDTH. IF MIN. 30 FOOT RADIUS CANNOT BE OBTAINED, OR THE TOWN HIGHWAY IS NOT PAVED, BREAK THE EDGE LINE USING AN 80 FOOT GAP AT INTERSECTION.

**EDGE LINE LAYOUTS**

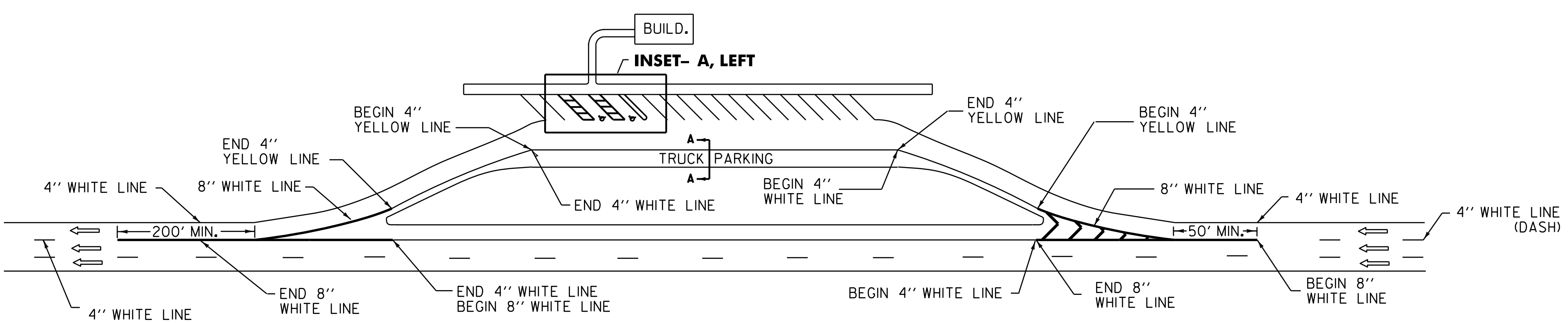


**INSET - A**



**SECTION A - A**

**TRUCK PARKING DETAIL**

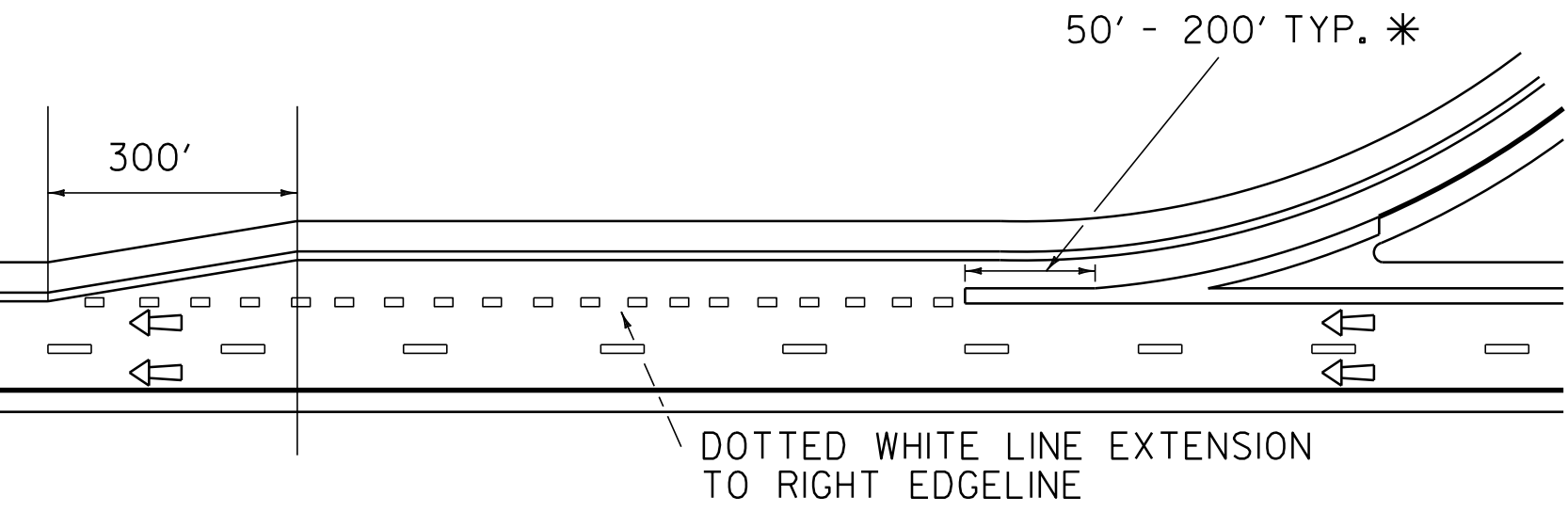


**REST AREA PARKING DETAILS**

NOT TO SCALE

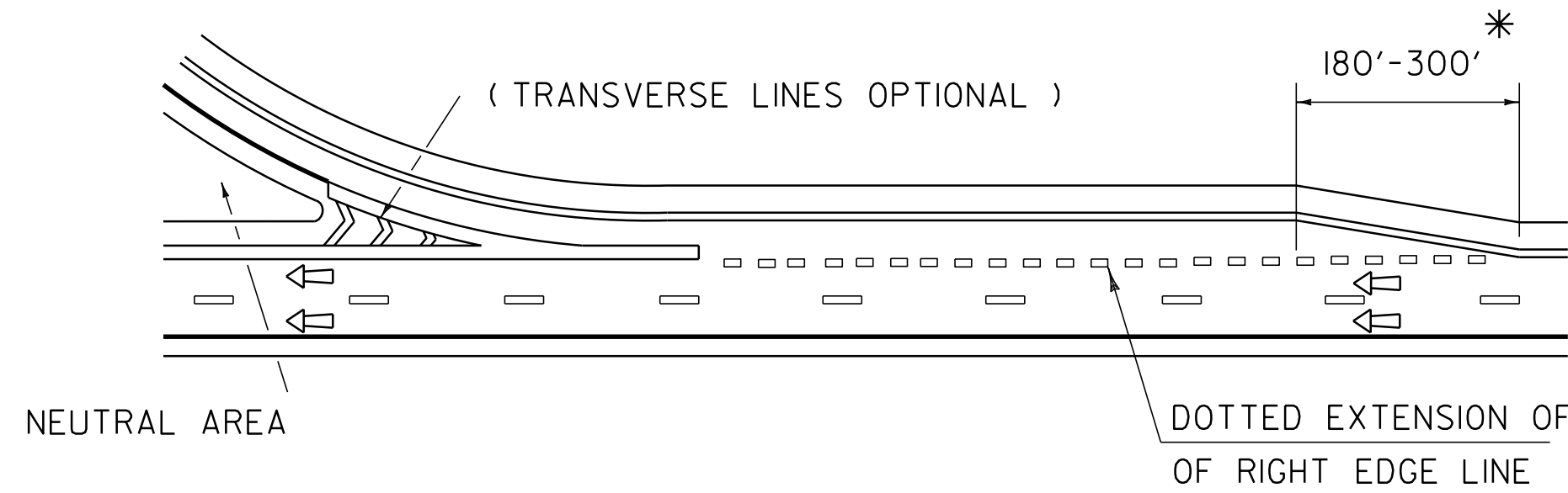
**PAVEMENT MARKING DETAILS**

<b>PAVEMENT MARKING DETAILS SHEET 4</b>	PROJECT NAME: <b>STATEWIDE</b>	PLOT DATE: 31-MAY-2012 13:11
	PROJECT NO.: <b>NHG MARK (205)</b>	DRAWN BY: KAS
	DESIGN FILE NAME: 11b010.dgn	SHEET: 24 OF 26
	PROJECT MANAGER: A GAMBLE	
	IPARM FILE NAME: 11b010pm4.i	



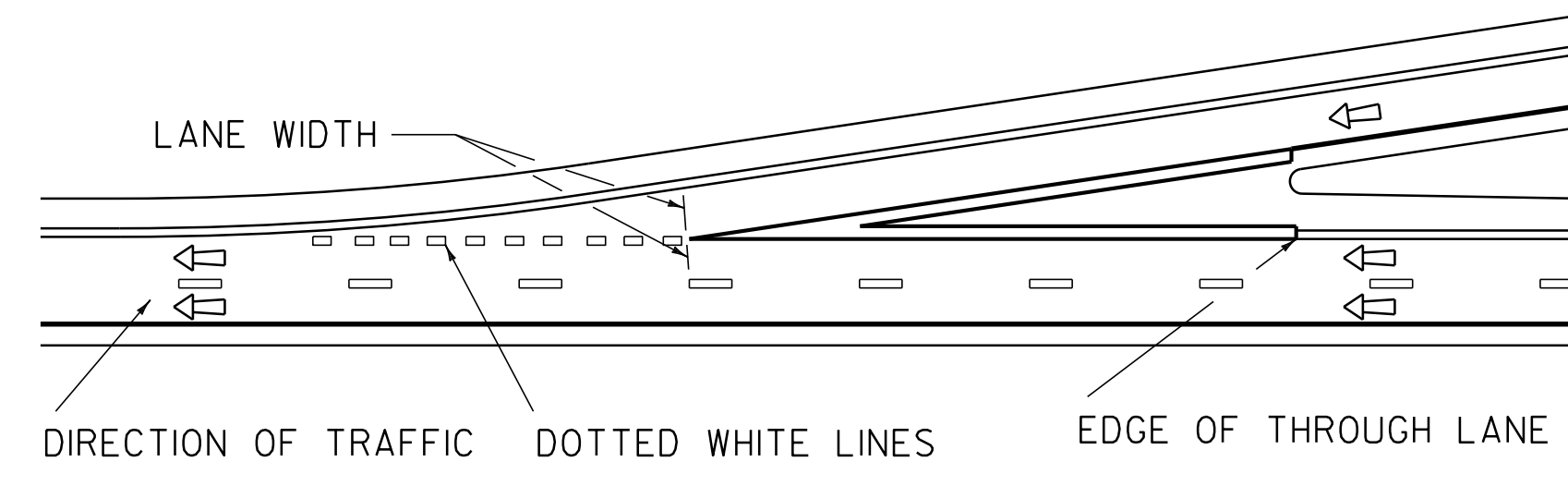
\* USE LONGER LENGTH TO EMPHASIZE SITUATIONS WHERE THE CROSSING REQUIRES UNUSUAL CARE SUCH AS HIGH VOLUME MERGE AREAS.

**PARALLEL ACCELERATION LANE**

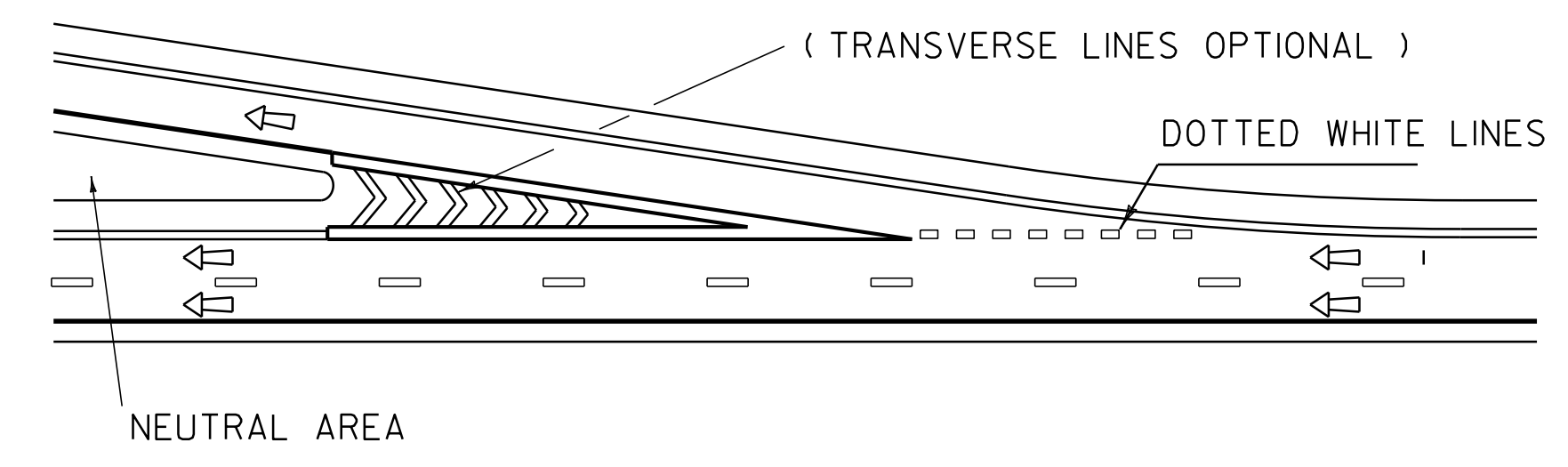


\* SHORTER TAPERS GIVE A BETTER TARGET VALUE, HOWEVER ALIGNMENT MAY DICTATE A LONGER TAPER. ENGINEER SHALL ESTIMATE LENGTH.

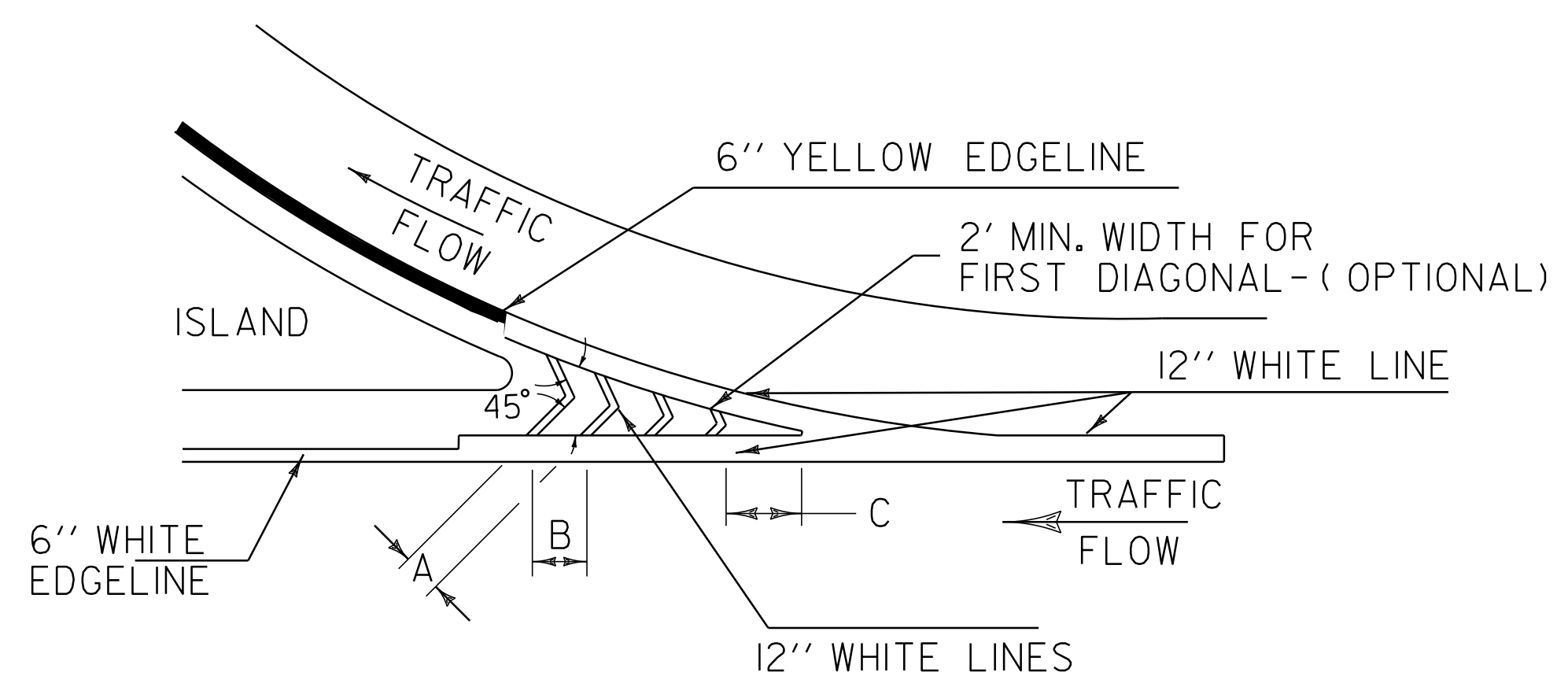
**PARALLEL DECELERATION LANE**



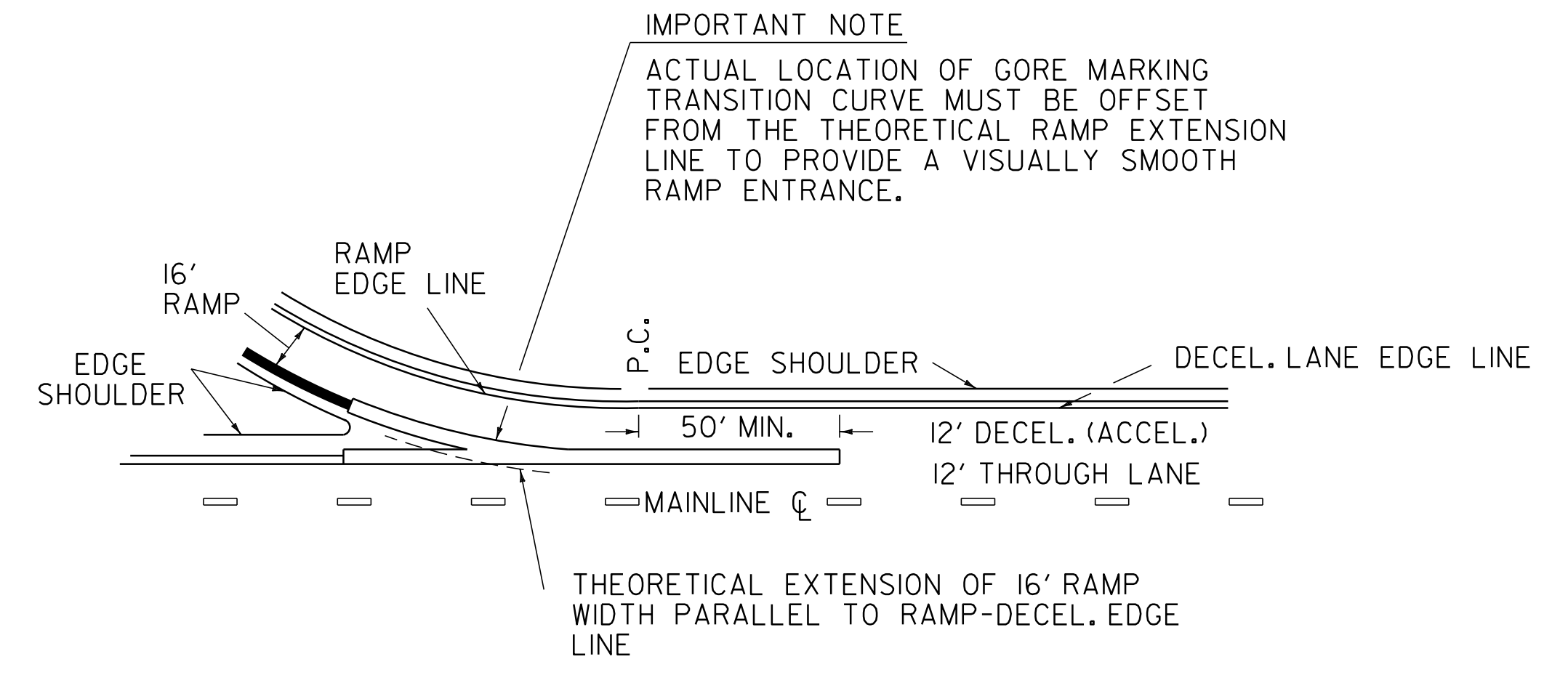
**TAPERED ACCELERATION LANE**



**TAPERED DECELERATION LANE**



**GORE MARKING DETAIL - EXIT TRANSVERSE LINES**



**DETAIL - GORE MARKING TRANSITION CURVE**

**LEGEND**

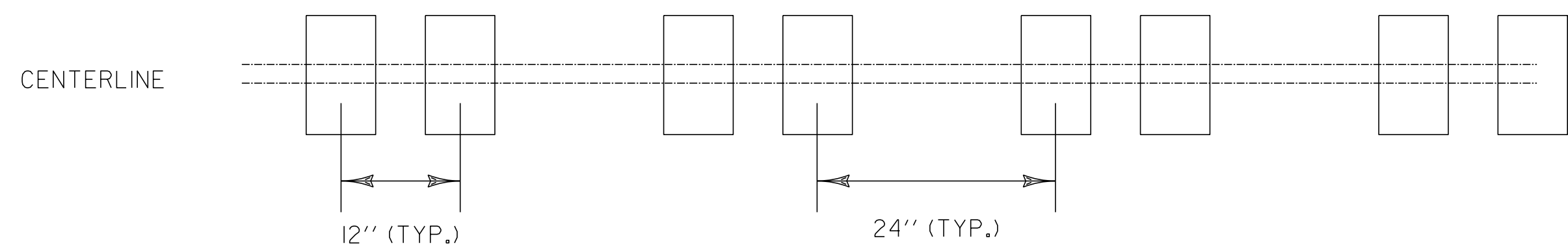
- WHITE LINES
- YELLOW LINES
- CHANNELIZATION WHITE LINES
- WHITE DOTTED LINES (3' SOLID - 9' GAP)
- DIRECTION OF TRAFFIC FLOW

DIMENSIONS			CONDITIONS FOR USE	
A	B	C		
5'	7'	2'	LOW SPEED OR HIGH SPEED/W POOR SIGHT DISTANCE, <200'	WHERE LENGTH OF DIAGONAL AREA IS 75' MAX.
10'	14'	4'	HIGH SPEED AND GOOD SIGHT DISTANCE, >200'	WHERE LENGTH OF DIAGONAL AREA EXCEEDS 75'

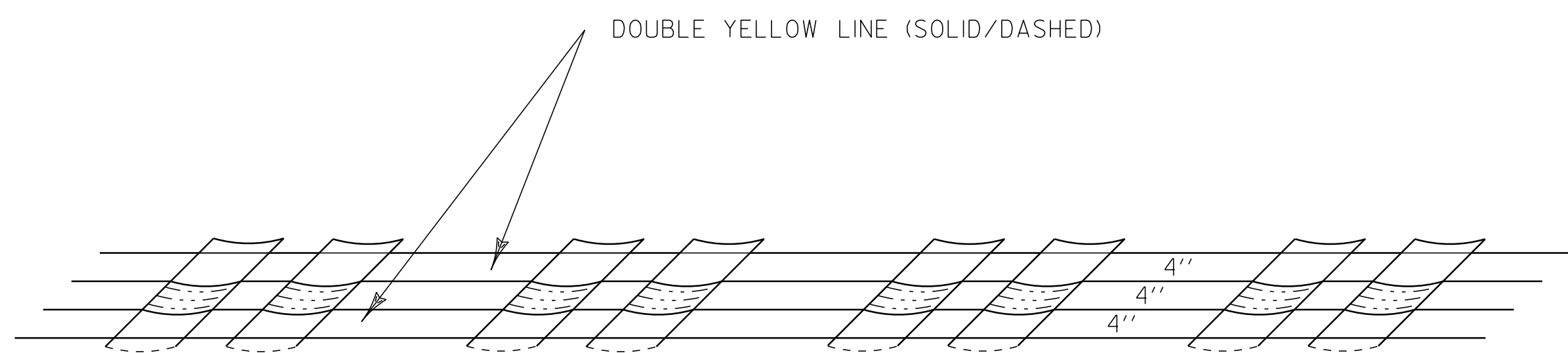
NOT TO SCALE

**PAVEMENT MARKING DETAILS**

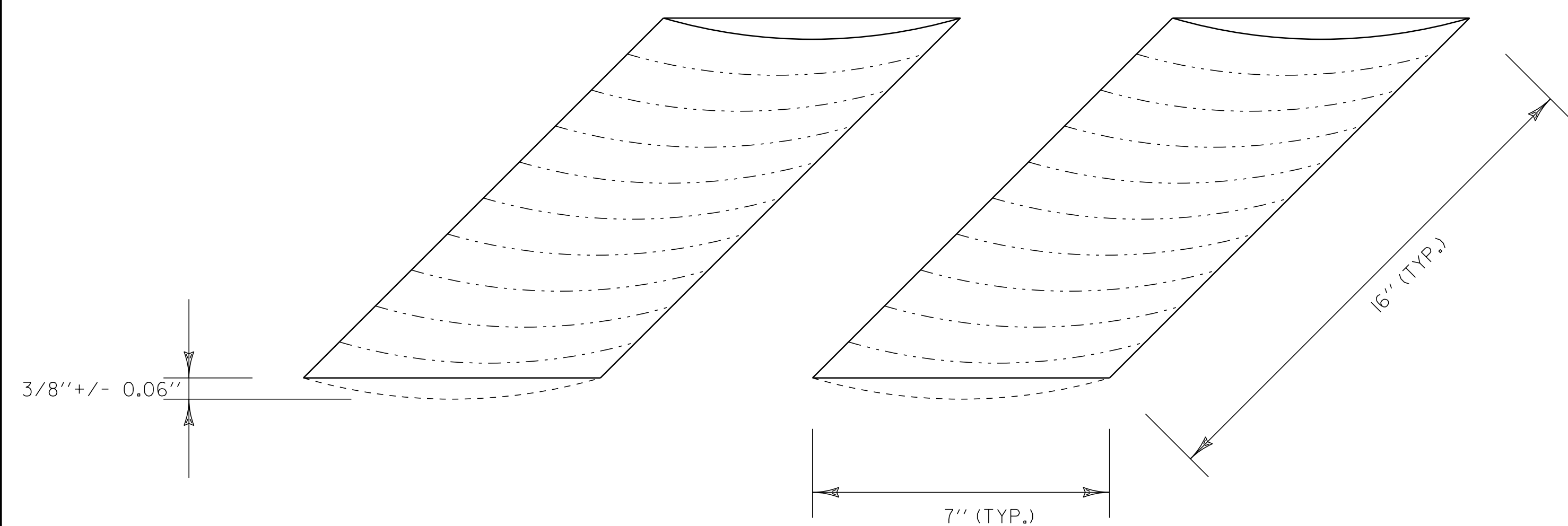
<b>PAVEMENT MARKING DETAILS SHEET 5</b>	PROJECT NAME: <b>STATEWIDE</b>	PLOT DATE: 31-MAY-2012 13:11		
	PROJECT NO.: <b>NHG MARK (205)</b>		DRAWN BY: KAS	
	DESIGN FILE NAME: IIB010.dgn			SHEET: 25 OF 26
	PROJECT MANAGER: A GAMBLE			
IPARM FILE NAME: IIB010pm5.i				



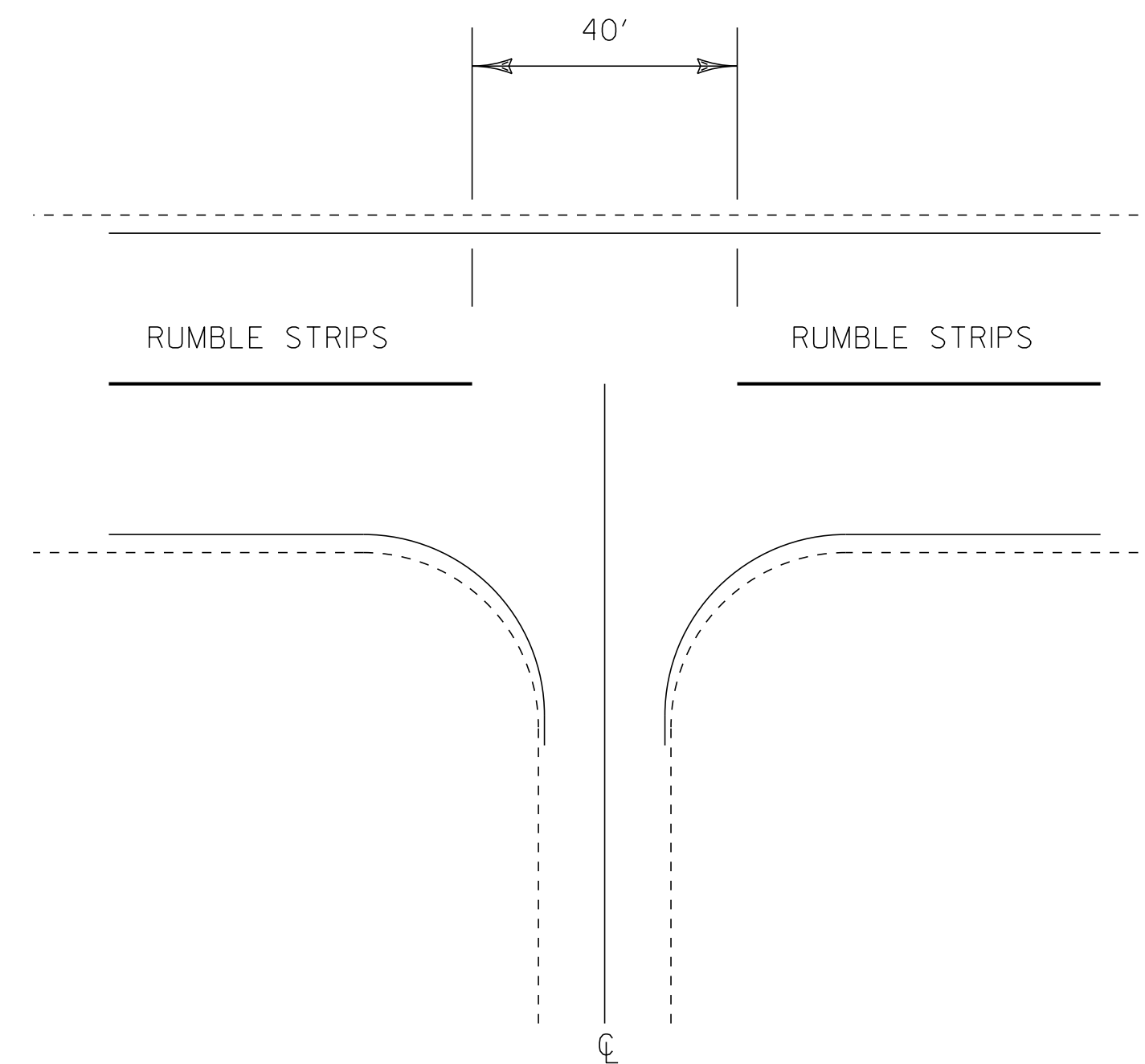
**RUMBLE STRIP LAYOUT**



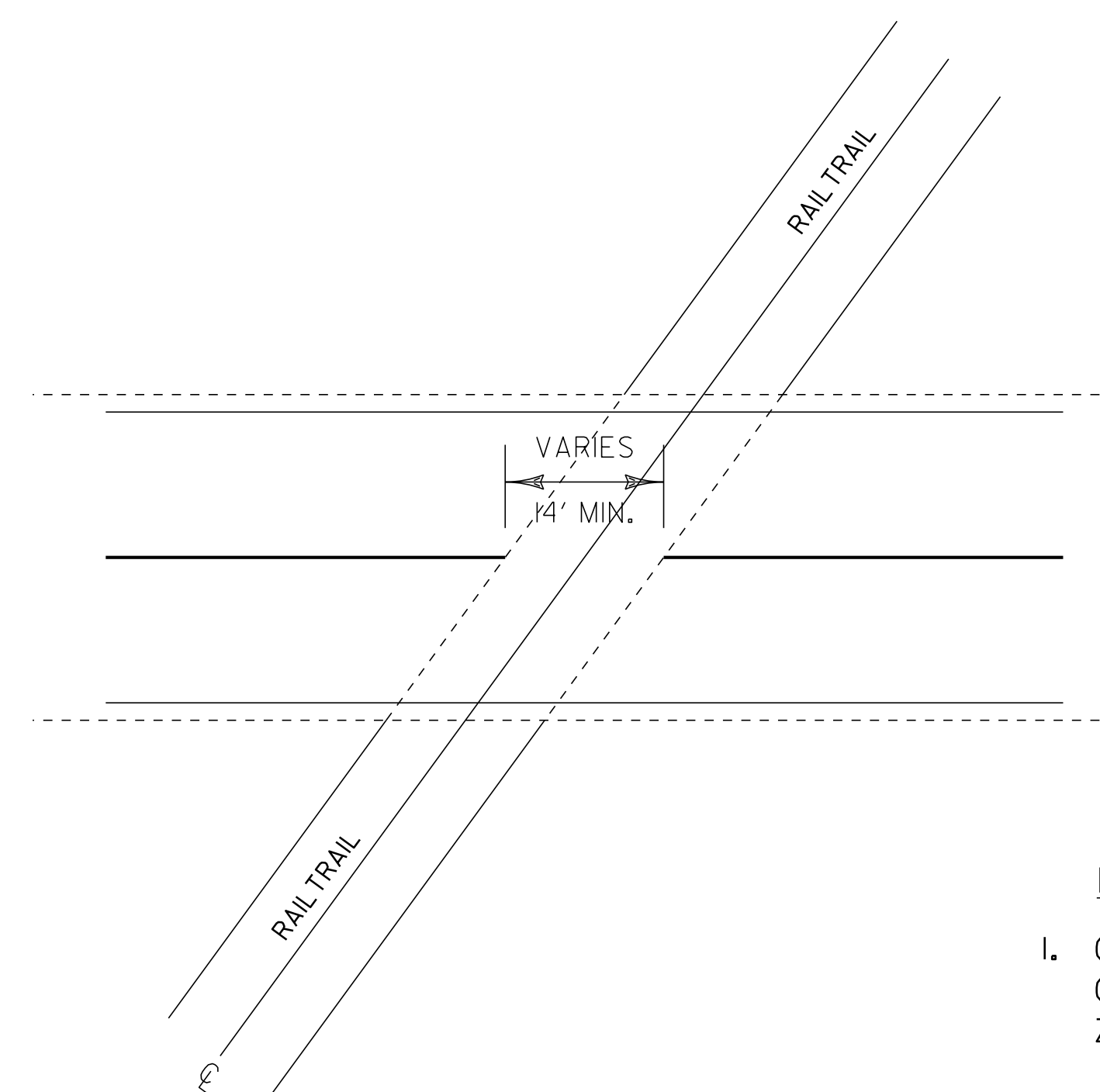
**PAVEMENT MARKINGS DETAIL**



**RUMBLE STRIP DETAIL**



**INTERSECTION DETAIL**



**RAILTRAIL CROSSING DETAIL**

**NOTES**

1. CENTERLINE NOT TO EXTEND BEYOND CENTERLINE RUMBLE STRIPS WHEN CENTERLINE IS A DOUBLE YELLOW LINE, WITH OR WITHOUT A PASSING ZONE.
2. CENTERLINE RUMBLE STRIP TO STOP 20 FEET PRIOR TO THE CENTERLINE OF AN INTERSECTION.
3. WIDTH OF GAP AT RAILTRAIL CROSSINGS WILL VARY WITH SKEW OF CROSSING WITH A MINIMUM 14 FOOT GAP TO BE PROVIDED AT ALL RAILTRAIL CROSSINGS.

NOT TO SCALE

**RUMBLE STRIP  
DETAIL SHEET**

PROJECT NAME: **STATEWIDE**  
 PROJECT NO.: **NHG MARK (205)**  
 DESIGN FILE NAME: IIB010.dgn  
 PROJECT MANAGER: A GAMBLE  
 IPARM FILE NAME: IIB010rsdet.i

PLOT DATE: 31-MAY-2012 13:11  
 DRAWN BY: KAS  
 SHEET: 26 OF 26