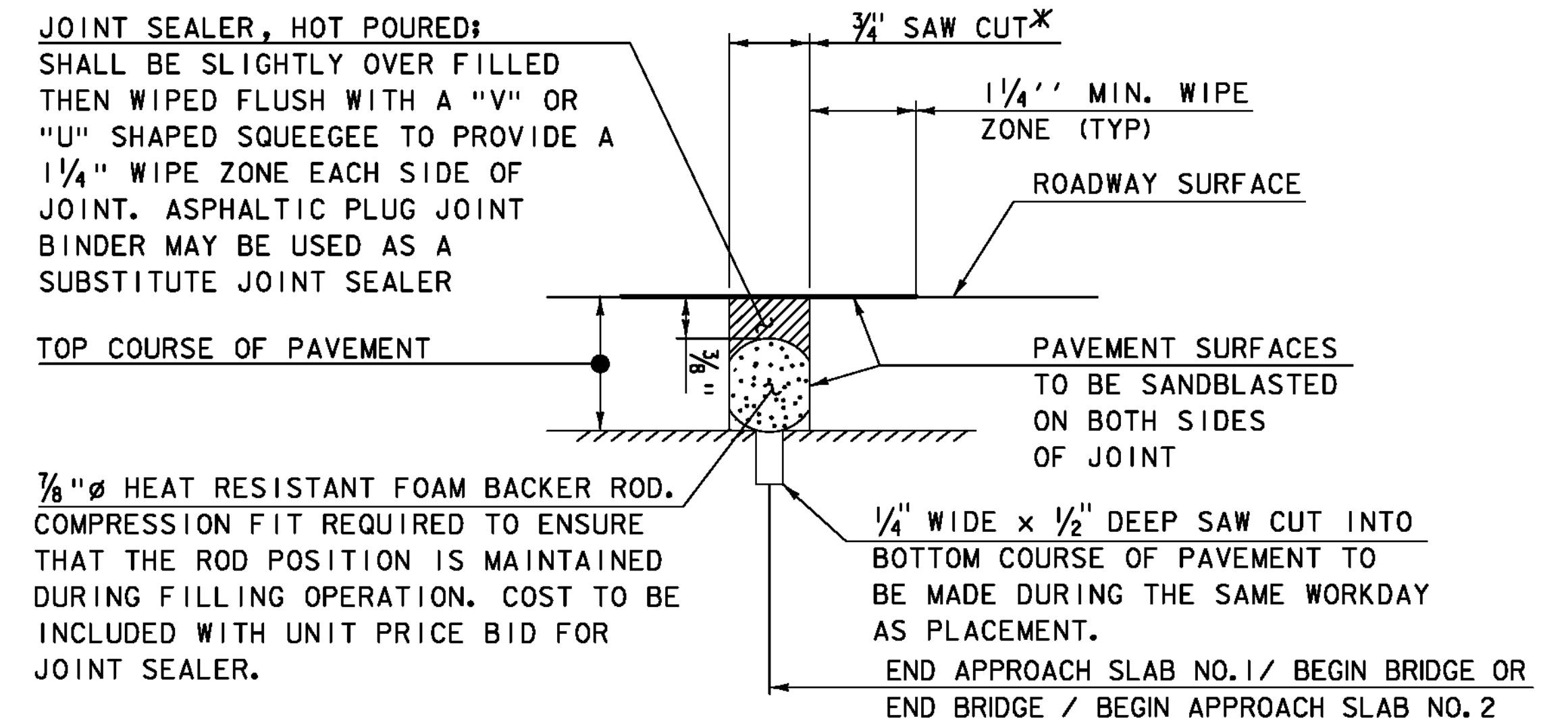
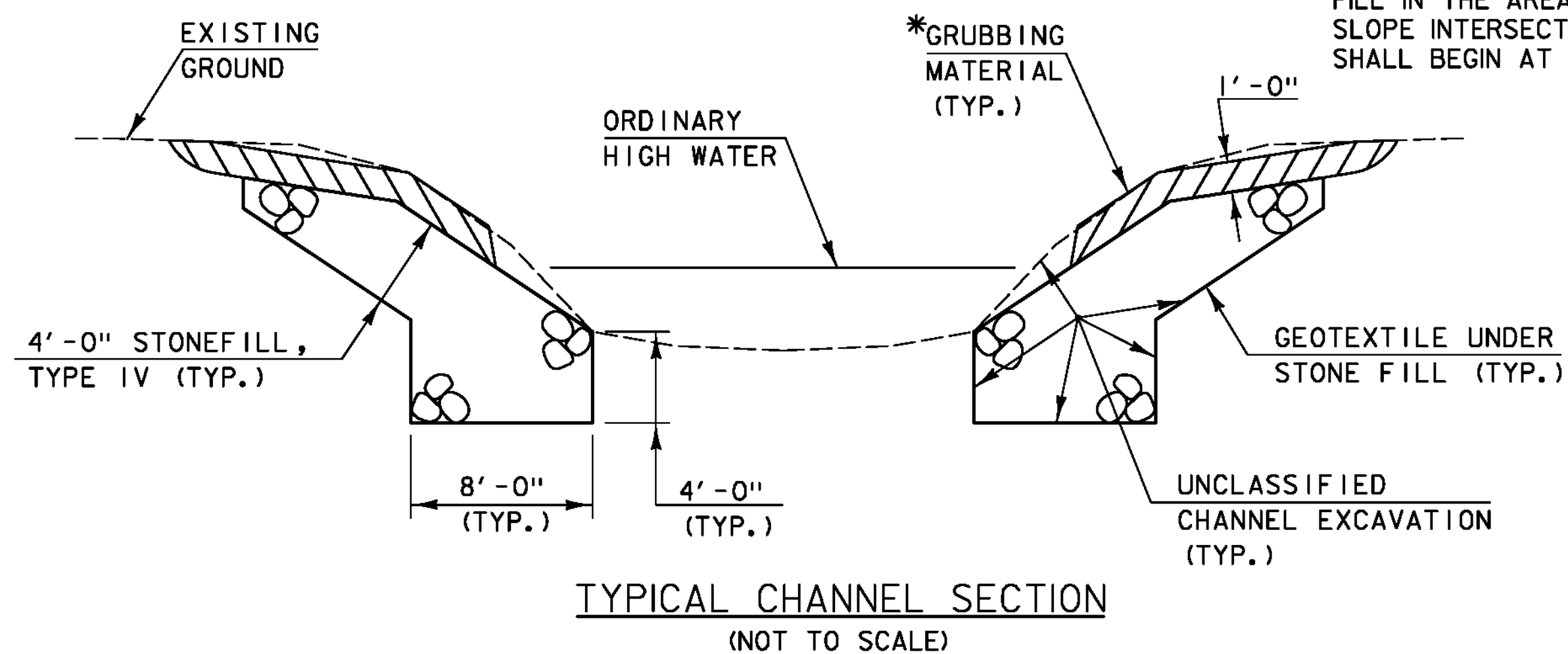
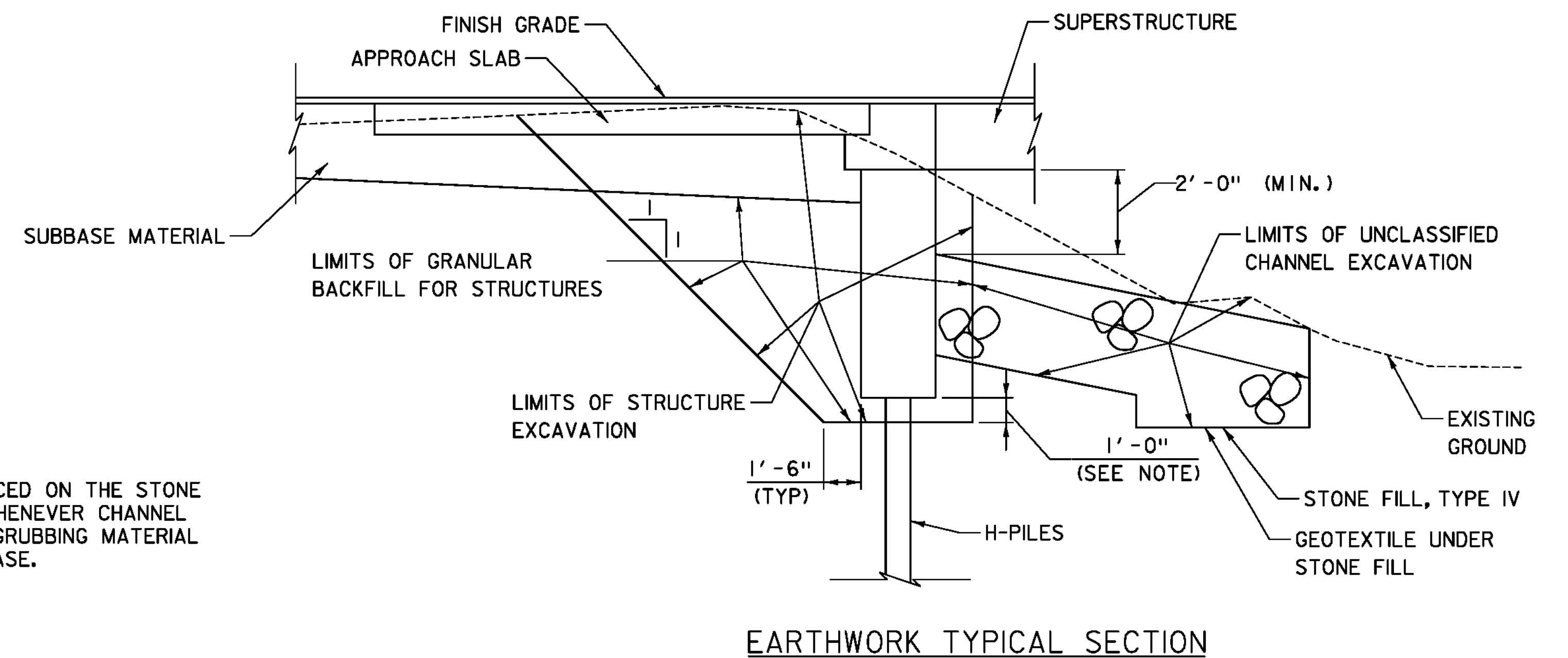


\*\* COMPACTED PAVEMENT SHALL BE  $\frac{1}{8}''$  HIGHER THAN ADJACENT CONCRETE CURB ON EXTERIOR BEAMS

\* GRUBBING MATERIAL SHALL NOT BE PLACED ON THE STONE FILL IN THE AREA UNDER THE BRIDGE. WHENEVER CHANNEL SLOPE INTERSECTS ROADWAY SUBBASE, GRUBBING MATERIAL SHALL BEGIN AT THE BOTTOM OF SUBBASE.



\* JOINT IS TO BE LOCATED ACCURATELY BY STRING LINING, OR OTHER MEANS, PRIOR TO PAVING, SO THAT THE SAW CUTS WILL BE MADE DIRECTLY OVER THE END OF CONCRETE DECK. JOINT SHALL BE CUT DRY IN A SINGLE PASS AND BE SEALED WITHIN 24 HOURS OR PRIOR TO EXPOSURE TO TRAFFIC. JOINT SHALL BE CLEANED PRIOR TO APPLYING THE JOINT SEALER.



**NOTES:**

- 1 FT UNDERCUT AS DETERMINED NECESSARY BY RESIDENT ENGINEER.
2. ACTUAL LIMITS OF STRUCTURE EXCAVATION TO BE DETERMINED BY THE CONTRACTOR. HOWEVER, ONLY THE EXCAVATION BETWEEN THE LIMITS SHOWN WILL BE PAID FOR UNDER ITEM 204.25, "STRUCTURE EXCAVATION." EXCAVATION BY THE CONTRACTOR OUTSIDE THESE LIMITS WILL BE AT THE EXPENSE OF THE CONTRACTOR.

PROJECT NAME: PLYMOUTH  
PROJECT NUMBER: ER BRS 0149(5)

TYLINTERNATIONAL

FILE NAME: zilc330bdr\_ttypical\_01.dgn  
PROJECT LEADER: J. OLUND  
DESIGNED BY: D. MYERS  
BRIDGE, EARTHWORK, & CHANNEL SECTIONS

PLOT DATE: 9/20/2012  
DRAWN BY: S. MORGAN  
CHECKED BY: S. KELLER  
SHEET 4 OF 46