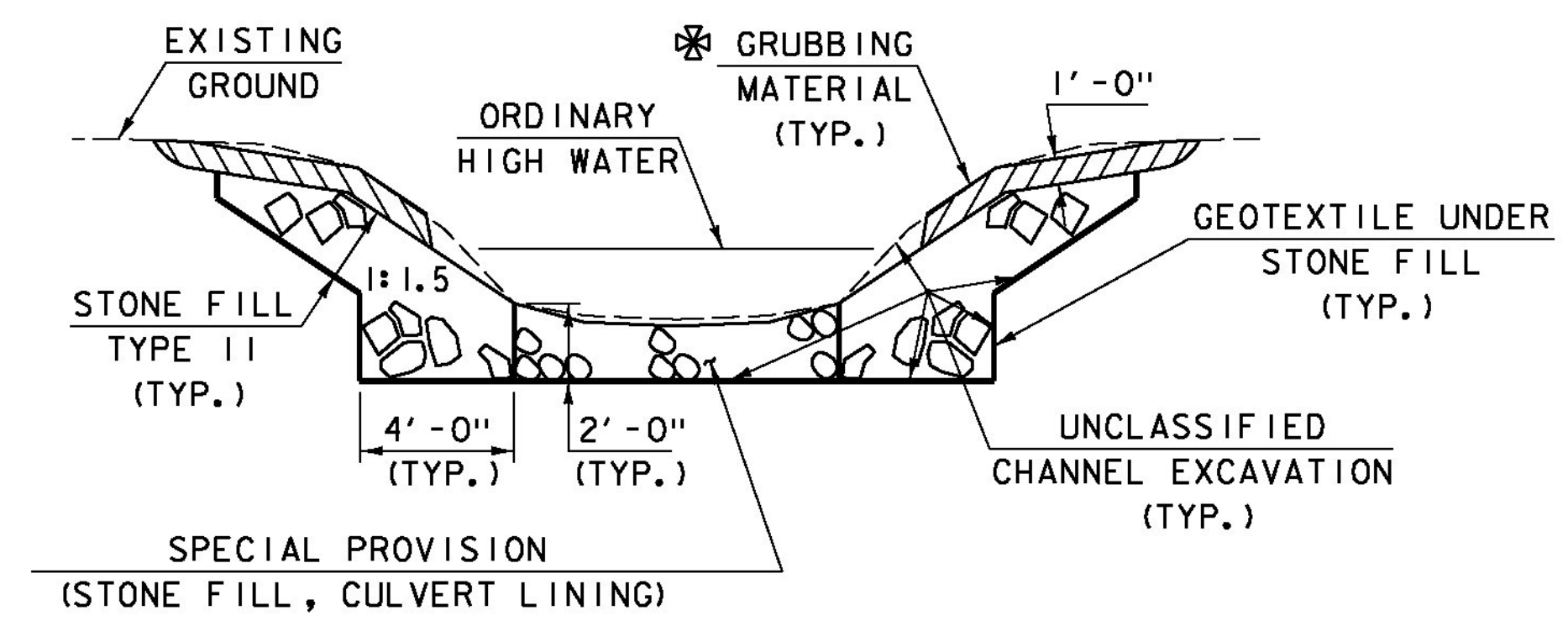
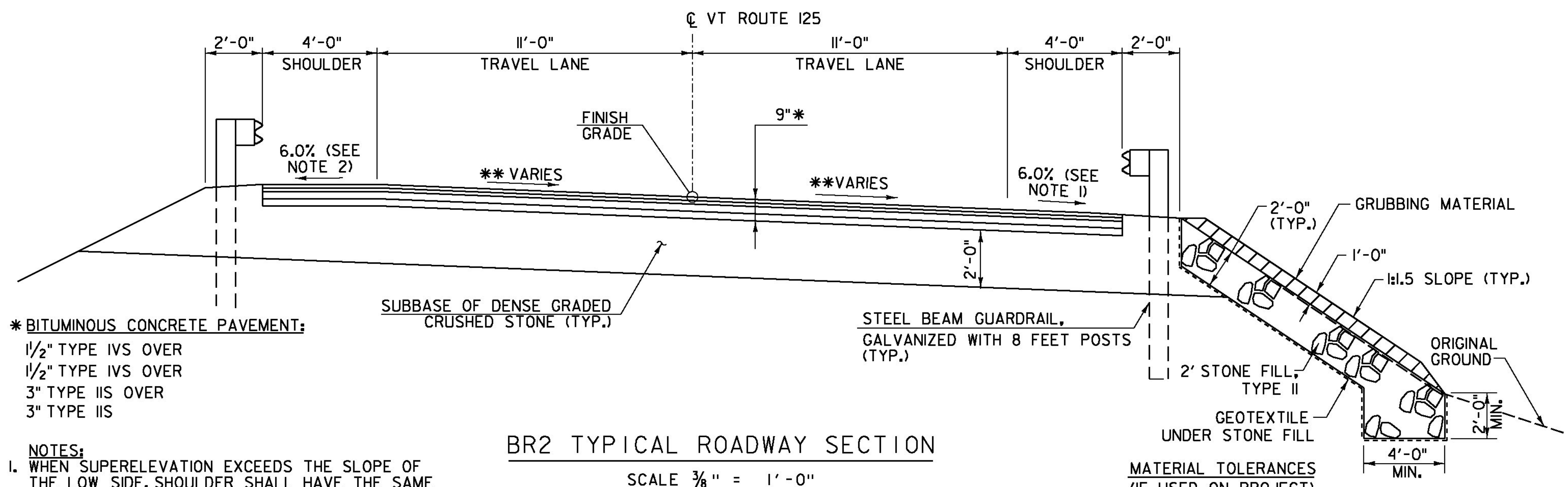


TYPICAL APPROACH SECTION - BR2  
NOT TO SCALE



CHANNEL TYPICAL SECTION  
NOT TO SCALE

✱ WHENEVER CHANNEL SLOPE INTERSECTS ROADWAY SUBBASE, GRUBBING MATERIAL SHALL BEGIN AT THE BOTTOM OF SUBBASE.



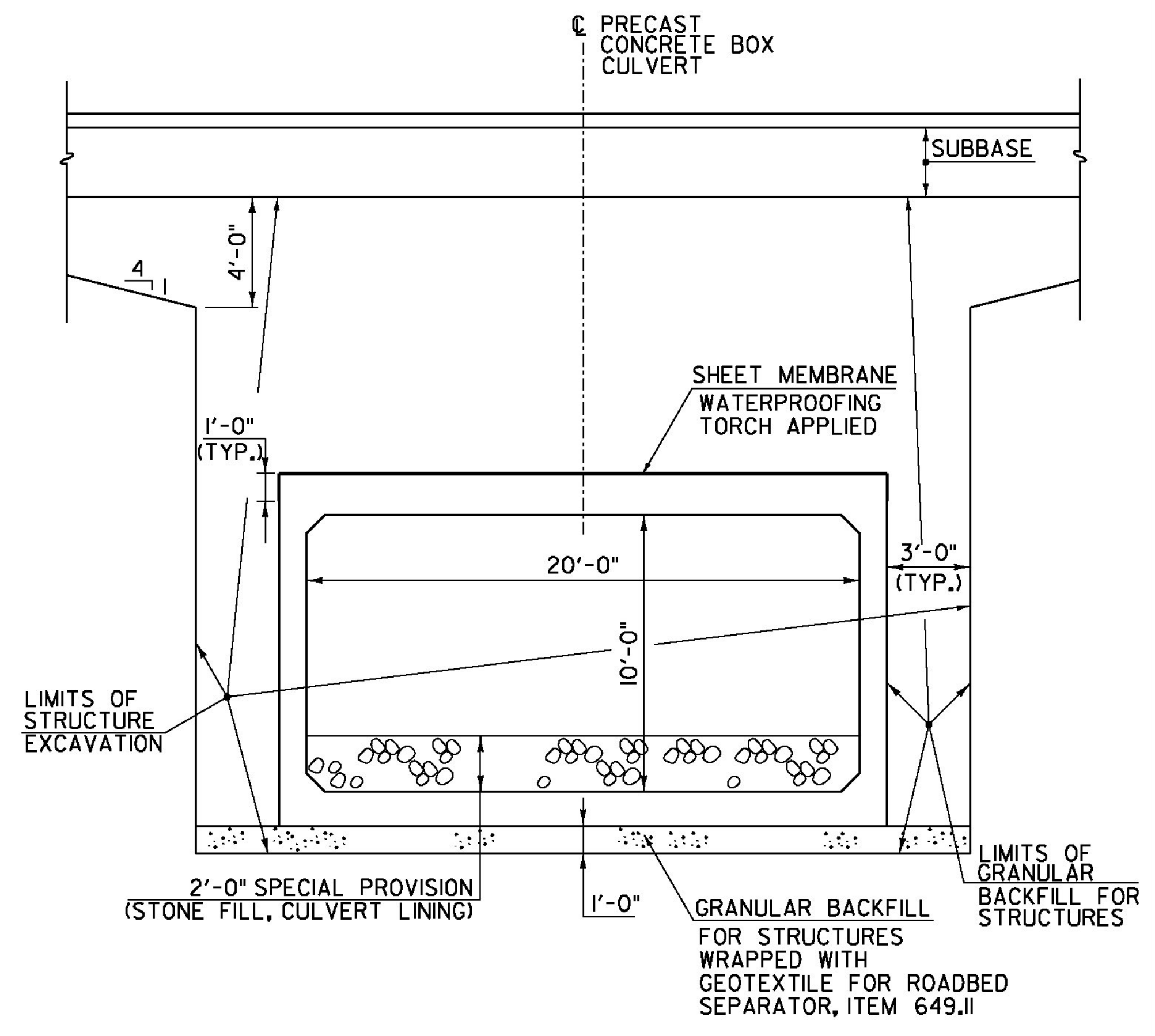
BR2 TYPICAL ROADWAY SECTION  
SCALE 3/8" = 1'-0"

\* BITUMINOUS CONCRETE PAVEMENT:  
1/2" TYPE IVS OVER  
1/2" TYPE IVS OVER  
3" TYPE IIS OVER  
3" TYPE IIS

NOTES:  
1. WHEN SUPERELEVATION EXCEEDS THE SLOPE OF THE LOW SIDE, SHOULDER SHALL HAVE THE SAME SLOPE AS THE TRAVEL WAY.  
2. MAXIMUM HIGHSIDE SHOULDER ROLLOVER SHALL NOT EXCEED 7%.

\*\* SEE BANKING DIAGRAM ON SHEET 11, MATCH EXISTING CROSS SLOPE ON APPROACHES.

MATERIAL TOLERANCES (IF USED ON PROJECT)	
SURFACE	
- PAVEMENT (TOTAL THICKNESS)	+/- 1/4"
- AGGREGATE SURFACE COARSE	+/- 1/2"
SUBBASE	+/- 1"
SAND BORROW	+/- 1"



PRECAST CONCRETE BOX TYPICAL SECTION - BR2  
SCALE: 1/4" = 1'-0"

NOTE:  
FOR WINGWALL EARTHWORK SECTION, SEE STRUCTURAL PLAN AND DETAILS - BR2.

PROJECT NAME:	BRIDPORT	FILE NAME:	z11c264typsec.dgn	PLOT DATE:	8/22/2014
PROJECT NUMBER:	STP CULV(29)	PROJECT LEADER:	M. CHENETTE	DRAWN BY:	L. BUXTON
		DESIGNED BY:	J. HUNGERFORD	CHECKED BY:	M. CHENETTE
		TYPICAL SECTIONS - BR2			SHEET 12 OF 57

