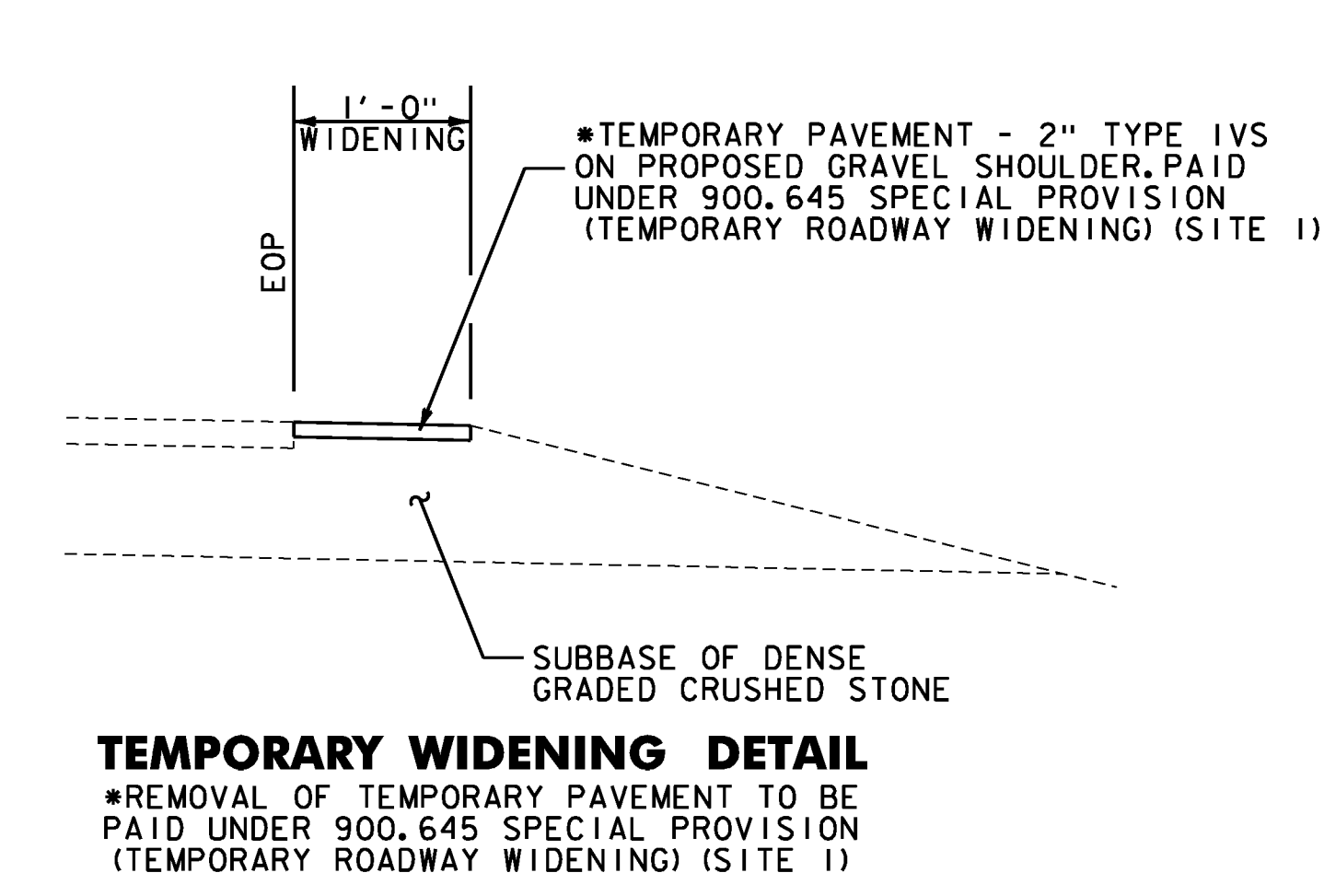
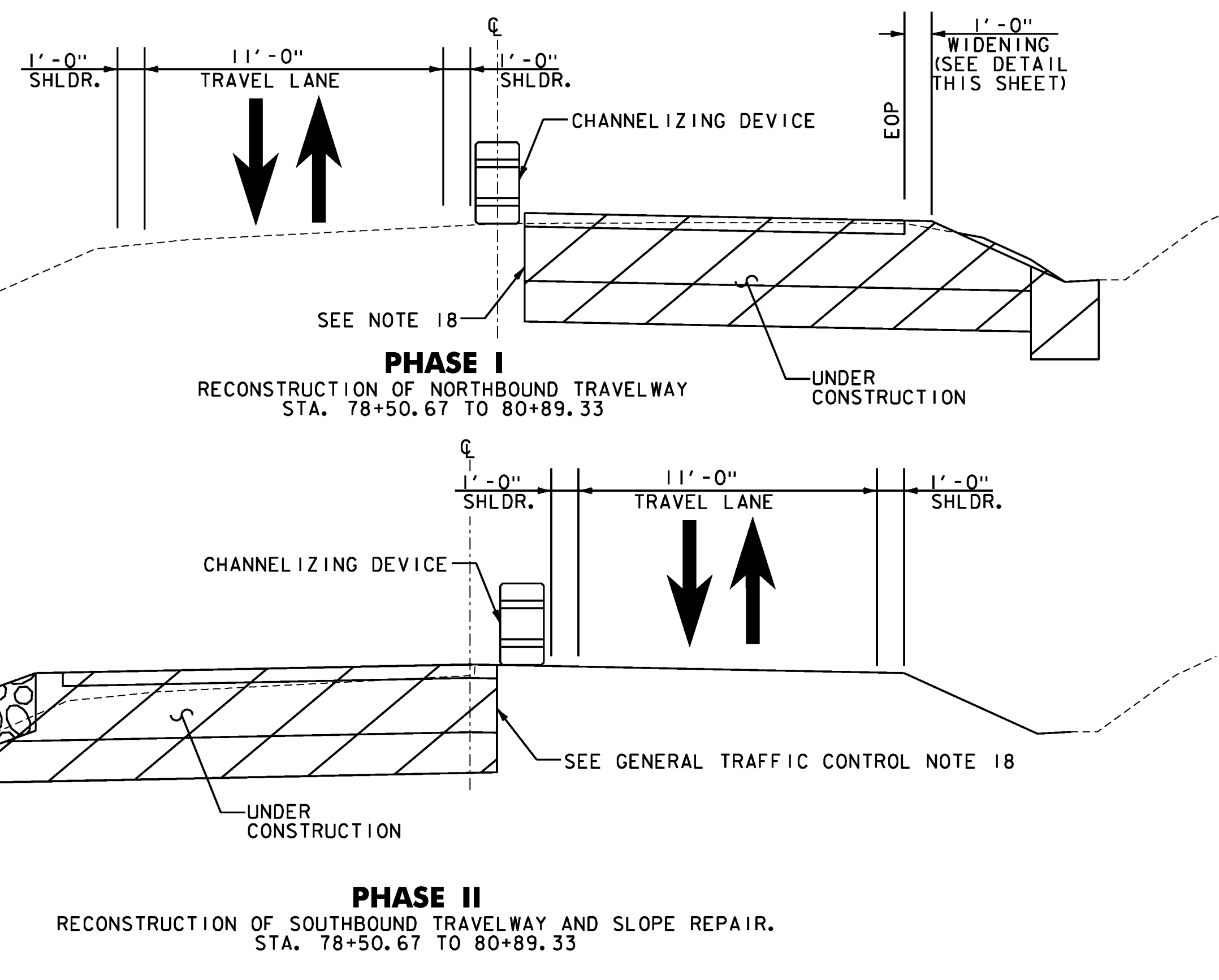


**GENERAL TRAFFIC CONTROL NOTES**

- IT IS ANTICIPATED THAT CONSTRUCTION OF THIS PROJECT WILL REQUIRE TEMPORARY INFRASTRUCTURE SUCH AS TEMPORARY WIDENING AND SIGNAL SYSTEMS AS DETAILED IN THE FOLLOWING TRAFFIC CONTROL PLANS. THE TRAFFIC CONTROL PLANS WERE DEVELOPED BASED ON THE PROPOSED TRAFFIC CONTROL SEQUENCE SHOWN ON TEMPORARY TRAFFIC CONTROL SHEETS 3 AND 4. THE CONTRACTOR SHALL MODIFY THE TRAFFIC CONTROL SEQUENCE AND PLANS AS NECESSARY FOR SPECIFIC SITUATIONS. THE CONTRACTOR'S ALTERNATE PROPOSAL SHALL BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. THE CONTRACTOR MUST ALLOW AT LEAST 4 WEEKS FOR REVIEW AND APPROVAL OF A COMPREHENSIVE PLAN AND 2 WEEKS FOR REVIEW AND APPROVAL OF MINOR CHANGES/DETAILS. IF THE PLAN IS NOT APPROVED THE CONTRACTOR SHALL HAVE 1 WEEK TO SUBMIT A REVISED PROPOSAL. ALL MODIFIED PROPOSALS, PLANS AND DETAILS MUST BE DESIGNED AND IMPLEMENTED IN ACCORDANCE WITH THE MUTCD AND VTRANS STANDARDS.
- SIGNS SHALL BE INSTALLED SO AS NOT TO OBSTRUCT EXISTING SIGNS OR CORNER SIGHT DISTANCE FROM HIGHWAYS, STREETS, ROADS OR DRIVES. PAYMENT TO BE INCIDENTAL TO 641.10, TRAFFIC CONTROL.
- ALL SIGNS SHALL BE IN ACCORDANCE WITH THE CURRENT EDITIONS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND THE "STANDARD HIGHWAY SIGNS AND MARKINGS" BOOK (SHSM) PUBLISHED BY THE FEDERAL HIGHWAY ADMINISTRATION (FHWA).
- EXISTING STEEL BEAM GUARDRAIL SHALL REMAIN IN PLACE UNTIL TEMPORARY TRAFFIC BARRIER IS IN PLACE.
- SOLID SUBSTRATE CONSTRUCTION SIGNS SHALL HAVE RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING "AMERICAN SOCIETY FOR TESTING AND MATERIALS" (ASTM) D 4956 TYPE VII, VIII OR IX REQUIREMENTS, UNLESS OTHERWISE NOTED. SOLID SUBSTRATE REGULATORY SIGNS (WHITE BACKGROUND) SHALL HAVE RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING ASTM D 4956 TYPE III.
- ROLL UP SIGNS SHALL HAVE RETROREFLECTIVE SHEETING EQUAL TO OR EXCEEDING ASTM D 4956 TYPE VI.
- SIGNS SHALL BE ERECTED BEFORE THE START OF ANY WORK AND SHALL BE COVERED UNTIL WORK COMMENCES. EACH SIGN SHALL BE ERECTED IN A NEAT AND WORKMANLIKE MANNER. SIGNS SHALL BE REMOVED UPON COMPLETION OF THE WORK AT THE DISCRETION OF THE ENGINEER.
- FIXED SIGNS SHALL BE SET SECURELY IN THE GROUND. THE BOTTOM OF A SIGN SHALL BE AT LEAST SEVEN FEET ABOVE THE EDGE OF PAVEMENT. THE NEAREST EDGE OF A SIGN SHALL BE AT LEAST SIX FEET OUTSIDE THE SHOULDER POINT OR FOUR FEET OUTSIDE GUARDRAIL. SEE VTRANS STANDARD E-120.
- WHERE SIGN INSTALLATIONS ARE NOT PROTECTED BY GUARDRAIL OR OTHER APPROVED TRAFFIC BARRIERS, ALL SIGN STANDS AND POST INSTALLATIONS SHALL BE "NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM" (NCHRP) REPORT 350 COMPLIANT. NO SIGN POSTS SHALL EXTEND OVER THE TOP OF THE SIGN INSTALLED ON SAID POST(S). WHEN ANCHORS ARE INSTALLED STUB SHALL NOT BE GREATER THAN FOUR INCHES ABOVE EXISTING GROUND.
- THE NUMBER OF CHANNELIZING DEVICES, TYPE III BARRICADES, AND OTHER TRAFFIC CONTROL DEVICES SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY, THE ACTUAL NUMBER REQUIRED ARE TO BE DETERMINED BASED ON INDIVIDUAL DETOUR CONDITIONS (TAPERS, SPEED LIMITS, LENGTH OF DETOUR, CURVE, ETC.). WARNING LIGHTS SHALL NOT BE USED ON CHANNELIZING DEVICES.
- COSTS ASSOCIATED WITH ALL CHANNELIZING DEVICES WITH THE EXCEPTION OF TEMPORARY TRAFFIC BARRIER AND TEMPORARY PAVEMENT MARKINGS SHALL BE PAID INCIDENTAL TO ITEM 641.10 TRAFFIC CONTROL.
- DURING NON-WORK PERIODS, ALL EQUIPMENT SHALL BE MOVED TO A LOCATION OFF PAVED SHOULDERS AND OUTSIDE THE CLEAR ZONE (20') OR PROTECTED BY TRAFFIC BARRIER OR GUARDRAIL.
- AT THE DISCRETION OF THE ENGINEER BUFFER SPACE LENGTHS MAY BE EXTENDED BEYOND MINIMUM VALUES.
- DROP OFF PROTECTION IN CONSTRUCTION AREAS SHALL CONFORM TO VAOT STANDARDS E-108 AND E-108A.
- ALL NON-OPERATING SIGNAL HEADS SHALL BE REMOVED OR COMPLETELY COVERED.
- TRAFFIC SHALL NOT BE CHANGED FROM ONE PHASE TO THE NEXT PHASE UNTIL ALL TEMPORARY MARKING, SIGNING AND SIGNAL WORK ARE COMPLETED. ANY CONFLICT IN MARKING SHALL BE REMOVED.
- UNLESS COVERED UNDER INDIVIDUAL PAY ITEMS OR NOTED OTHERWISE, ALL COST FOR WORK SHOWN ON THE FOLLOWING SHEETS FOR TEMPORARY TRAFFIC CONTROL DEVICES INCLUDING TRAFFIC SIGNALS, TRAFFIC BARRIERS, RETROREFLECTIVE DRUMS, SIGNS, AND SIGN POSTS WILL BE CONSIDERED TO BE INCLUDED IN THE CONTRACT LUMP SUM PRICE FOR ITEM 641.10 TRAFFIC CONTROL.
- NORTHBOUND AND SOUTHBOUND LANES ARE TO BE EXCAVATED INCREMENTALLY. THE VERTICAL DROP SHALL MAINTAIN COMPLIANCE WITH STANDARDS E-108 AND E-108A FOR CHANNELIZING DEVICES. THESE PHASING DETAILS SHOW THE LIMIT OF WORK AND ALL THE EXCAVATION FOR PHASE I DOES NOT NEED TO BE COMPLETED PRIOR TO COMMENCING EXCAVATION OF PHASE II.

**TEMPORARY TRAFFIC SIGNAL NOTES:**

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING SIGNAL PHASING. THE CONTRACTOR SHALL SUBMIT PHASING DIAGRAM TO THE ENGINEER FOR APPROVAL. THE CONTRACTOR SHALL MAKE SIGNALS OPERATIONAL ONLY AFTER RECEIVING APPROVAL OF THE PHASING DIAGRAM BY THE ENGINEER. DEVELOPMENT OF THE PHASING DIAGRAM SHALL BE PAID INCIDENTAL TO ITEM 678.40 TEMPORARY TRAFFIC SIGNAL SYSTEM.
- DESIGN OF THE SIGNAL SUPPORTS AND ANY REQUIRED GUYING IS THE RESPONSIBILITY OF THE CONTRACTOR.
- SIGNAL TIMING/TIMING ADJUSTMENTS REQUESTED BY THE ENGINEER SHALL BE ACCOMPLISHED WITHIN A 48 HOUR PERIOD AND PAYMENT SHALL BE INCIDENTAL TO ITEM 678.40, "TEMPORARY TRAFFIC SIGNAL SYSTEM". THE ENGINEER SHALL MAKE SEVERAL TRIAL RUNS TO DETERMINE THE PROPER ALL-RED CLEARANCE INTERVAL.
- SIGNAL FACES SHALL BE LED AND CONSIST OF 12" LENSES. (RED, YELLOW, AND GREEN)
- THE BOTTOM OF THE HOUSING OF A SIGNAL FACE SUSPENDED OVER A ROADWAY SHALL NOT BE LESS THAN 16.5 FEET NOR MORE THAN 19 FEET ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY. THE BOTTOM OF A SIGNAL FACE NOT MOUNTED OVER A ROADWAY SHALL NOT BE LESS THAN 8 FEET NOR MORE THAN 15 FEET ABOVE THE GROUND.
- SIGNAL FACES FOR ANY ONE APPROACH SHALL NOT BE LESS THAN 8 FEET APART MEASURED HORIZONTALLY BETWEEN CENTER FACES.
- CONSULT THE M.U.T.C.D. FOR INFORMATION CONCERNING SIGNAL PLACEMENT.
- THE SIGNAL SYSTEM SHALL CONSIST OF POLES, SIGNS AND POSTS, WARNING SIGNS, LUMINAIRES, FLASHING BEACONS, ASSOCIATED PAVEMENT MARKINGS, AND SIGNAL EQUIPMENT TO PROVIDE FOR AN ADEQUATE DESIGN. THE CONTRACTOR WILL BE RESPONSIBLE FOR PERMITS AND COSTS ASSOCIATED WITH PROVIDING ELECTRICAL POWER.
- INSTALL WIRING BETWEEN SIGNAL POLES TO PROVIDE FOR A SAFE INSTALLATION. ATTACHMENT TO UTILITY POLES TO BE COORDINATED BY THE CONTRACTOR WITH THE UTILITY COMPANY.
- TEMPORARY POLES TO BE PLACED OUTSIDE THE 20 FOOT CLEARZONE.
- POLES SUPPORTING SPAN WIRES AND/OR MAST ARMS SHALL BE ADEQUATELY BRACED OR GUYED AND SHALL NOT BE PLACED SO AS TO CREATE A HAZARD TO THE TRAVELING PUBLIC.
- ALL TEMPORARY SIGNAL EQUIPMENT, SIGNS, ETC., SHALL BELONG TO THE CONTRACTOR AT THE END OF THE PROJECT AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR REMOVAL INCLUDING ANY TEMPORARY PAVEMENT MARKINGS, UTILITY POLES, WIRES, ETC.
- STOP BARS SHALL BE LOCATED A MINIMUM OF 40' AND A MAXIMUM OF 120' FROM THE NEAREST SIGNAL HEAD.
- SEE STD. E-121 FOR SIGN PLACEMENT. SEE STANDARDS E-171A AND E-172 FOR ADDITIONAL INFORMATION ON SIGNALS.
- ALL ELECTRICAL WORK SHALL CONFORM WITH THE REQUIREMENTS OF THE NATIONAL ELECTRICAL CODE AND STATE INSPECTOR.
- ALL STOP SIGNS AND ANY TRAFFIC SIGNS MADE IRRELEVANT DUE TO THE TEMPORARY SIGNAL SHALL BE COVERED DURING OPERATION OF THE TEMPORARY SIGNAL OR AT THE DISCRETION OF THE ENGINEER. THE COSTS OF COVERING AND UNCOVERING THESE SIGNS SHALL BE PAID INCIDENTAL TO OTHER TRAFFIC CONTROL ITEMS.
- CONSTRUCTION APPROACH SIGNS SHALL BE PROVIDED ON EACH APPROACH PER THE CONSTRUCTION APPROACH SIGNING SHOWN ON SHEETS 374 & 375. ADDITIONAL CONSTRUCTION APPROACH SIGNS SHALL BE INSTALLED AS REQUIRED BY THE ENGINEER PER STANDARDS E-100A, E-101, E-102 AND E-102A. PAYMENT FOR THESE SIGNS, MARKERS, ETC., SHALL BE PAID INCIDENTAL TO CONTRACT ITEM 641.10.
- CONTRACTOR SHALL MEET WITH EACH PROPERTY OWNER AFFECTED BY THE TEMPORARY SIGNAL. CONTRACTOR SHALL SUGGEST TO PROPERTY OWNER THAT PROPERTY OWNER SHOULD WAIT UNTIL TRAFFIC IS FLOWING IN THE DIRECTION THE PROPERTY OWNER WISHES TO TRAVEL AND ENTER AT THE END OF THAT LINE.



**TEMPORARY TRAFFIC CONTROL SHEET 1**

PROJECT NAME:	FLETCHER
PROJECT NUMBER:	STP 027-1(22)
FILE NAME:	...\\plotfiles\z11b064bdr_t.cpl.dgn
PLOT DATE:	11/15/2012
PROJECT LEADER:	GAE
DRAWN BY:	STANTEC
DESIGNED BY:	ISM
CHECKED BY:	MCF
<b>TEMPORARY TRAFFIC CONTROL SHEET 1</b>	
SHEET	372 OF 387