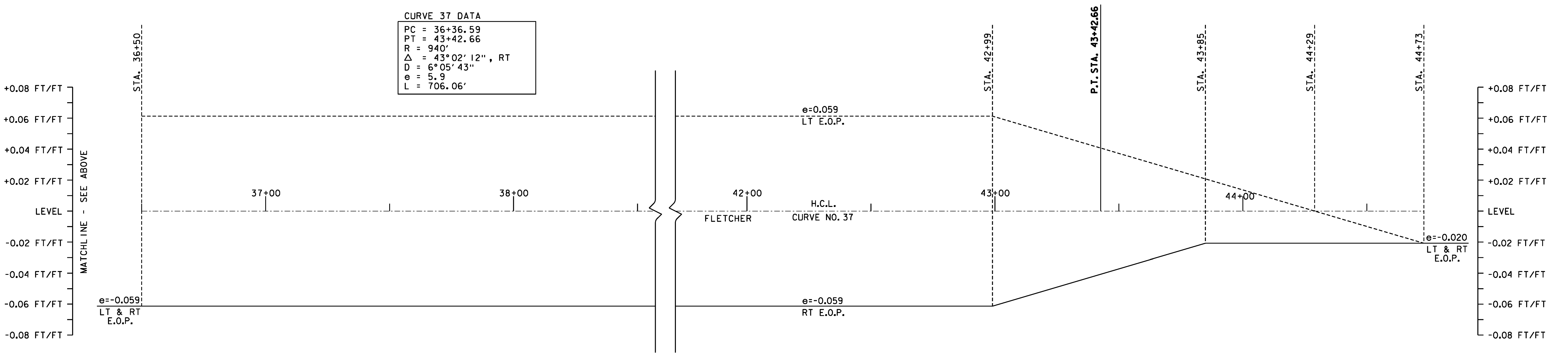


CURVE 36 BANKING TRANSITION DIAGRAM



CURVE 37 BANKING TRANSITION DIAGRAM

SUPERELEVATION BANKING NOTES:

1. THE CONTRACTOR IS RESPONSIBLE FOR ESTABLISHING AND MAINTAINING THE HORIZONTAL AND VERTICAL GEOMETRY OF THE ROADWAY.
2. SUPERELEVATION RATES AND RUNOFF LENGTHS WERE DETERMINED USING A DESIGN SPEED EQUAL TO THE POSTED SPEED. A MAXIMUM SUPERELEVATION RATE OF 0.08 IS USED IN AREAS WITH A POSTED SPEED ABOVE 30 MPH. IN AREAS WITH AN INTERSECTING SIDE ROAD A MAXIMUM SUPERELEVATION RATE OF 0.06 WAS USED. SEE VAOT STANDARD B-1 FOR MORE INFORMATION.

<p>NOT TO SCALE</p> <p>SUPERELEVATION BANKING TRANSITION DIAGRAM SHEET #15</p>	NOTE: CURVE #35 DOES NOT REQUIRE BANKING
	PROJECT NAME: CAMBRIDGE-BAKERSFIELD
	PROJECT NUMBER: STP 2926(I)
FILE NAME: p10b258.dgn	PLOT DATE: 11/15/2012
DESIGNED BY: STANTEC	DRAWN BY: STANTEC
IPARM FILE: p10b258sbd15.i	CHECKED BY: STANTEC
	SHEET 116 OF 387

